

SITE PLAN & ARCHITECTURAL
COMMISSION
AGENDA
DECEMBER 4, 2015



Commission Members

Jeff Bender, Chair

Jim Quinn, Vice Chair

Daniel Ames

Jim Catt

Bill Chmelir

Tim D'Alessandro

Bob Neathamer

Rick Whitlock

City Council Liaison - Dick Gordon

City Council Liaison Alt. - Eli Matthews

Regular Commission meetings are held on
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380



Site Plan and Architectural Commission

Agenda

Public Hearing

December 4, 2015

12:00 P.M.

Council Chambers— City Hall, Room 300
411 West Eighth Street, Medford, Oregon

-
10. Roll Call.
 20. Consent Calendar/Written Communications. None.
 30. Minutes.
 - 30.1 Approval of minutes for the November 6, 2015, meeting.
 40. Oral and Written Requests and Communications.
 50. Public Hearings.
 - New Business.
 - 50.1 AC-15-128 Consideration of plans to construct a 100,000 square foot storage and distribution facility on a portion of an 89.11 acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive, within the I-G (General Industrial) zoning district. Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent).
 - 50.2 AC-15-131 Consideration of plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zoning district. On Track, Inc., Applicant (Daniel Horton, Agent).
 - 50.3 AC-15-133 Consideration of a Final Site Plan for the future development Northgate Market Place Phase 2 (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site within a C-R (Regional Commercial) zoning district. Generally located north of West McAndrews Road, east of N. Central Avenue west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238). Regency Centers, L.P., Applicant (Cardino, Inc., Agent).
 60. Written Communications. None.
 70. Unfinished Business. None.
 80. New Business.
 90. Report from the Planning Department.
 100. Messages and Papers from the Chair.
 110. Propositions and Remarks from the Commission.
 120. City Council Comments.
 130. Adjournment.
-



Site Plan and Architectural Commission

Minutes

From Public Hearing on November 6, 2015

The regular meeting of the Site Plan and Architectural Commission was called to order at noon in the Council Chambers on the above date with the following members and staff in attendance:

Commissioners Present

Jeff Bender, Chair
Jim Catt
Bill Chmelir
Tim D'Alessandro
Bob Neathamer
Rick Whitlock
Dick Gordon, City Council liaison

Staff Present

Jim Huber, Planning Director
Kelly Akin, Principal Planner
John Huttli, Deputy City Attorney
Doug Burroughs, Public Works Department
Alex Georgevitch, Public Works Department
Debbie Strigle, Recording Secretary

Commissioners Absent

Jim Quinn, Vice Chair, Excused
Dan Ames, Unexcused

10. Roll Call.

20. Consent Calendar/Written Communications. (voice vote)

20.1 AC-15-097 Final Order for the consideration of plans for the construction of Joseph Office Park Phase II, a 5,940 square foot office building, located on a 0.6 acre site on the north side of East Barnett Road, approximately 650 feet west of North Phoenix Road within a C-S/P (Service Commercial – Professional Office) zone district. Mark McKechnie, Applicant (Oregon Architecture, Inc., Agent).

20.2 AC-15-109 Final Order for the consideration of plans to construct a 5,115 square foot warehouse building on approximately 0.16 acres within a Heavy Commercial (CH) zone district. The subject site is generally located 140 feet south of the terminus of Eads Street, 180 feet north of Earhart Road and bounded by Interstate 5 on the east (408 Eads Street). R.V. Concepts LLC, Applicant (Neathamer Surveying, Inc., Agent).

Chair Bender pulled Consent Calendar Item 20.3 from the calendar for discussion.

Motion: Adopt consent calendar items 20.1 and 20.2.

Moved by: Commissioner Whitlock Seconded by: Commissioner Chmelir

Voice Vote: Motion passed 5-0-1, with Commissioner Neathamer abstaining.

20.3 AC-15-110 Final Order for the consideration of plans to replace an existing coffee kiosk with a 468 square foot building located on the southeast corner of Stewart Avenue and South Holly Street, within the C-C (Community Commercial) zone district (295 Stewart Avenue). Tom Casey, Trustee, Applicant (Thornton Engineering, Inc., Agent).

Kelly Akin, Principal Planner, gave a brief PowerPoint presentation of the November 6, 2015, Revised Commission Report. Ms. Akin stated that it is not possible to construct less than the entire Holly Street driveway as required by the Commission. She noted that combining the two different design types would result in an unsafe condition. The City Engineer and Mike Thornton, the project engineer, agree that the driveway can be constructed to code standards. Staff recommends that the Commission strike

the parenthetical reference in the Public Works Department staff report, Item A.3., "(unless otherwise approved by the City Engineer)", and modify Condition of Approval 8 to reflect the recommendation. Staff prepared a Revised Commission Report and amended the conditions of approval, now Exhibit A-2, for the Commission to consider.

Motion: Approve and adopt the Final Order for AC-15-110 per the Revised Commission Report dated November 6, 2015, including Exhibits A-2 through S, and all conditions contained therein.

Moved by: Commissioner Whitlock Seconded by: Commissioner Neathamer

Roll Call Vote: Motion passed, 6-0

30. Minutes.

30.1 The minutes for the October 16, 2015, meeting, were approved as submitted.

40. Oral and Written Requests and Communications. None.

50. Public Hearings. None.

60. Written Communications. None.

70. Unfinished Business. None.

80. New Business. None.

90. Report from the Planning Department.

90.1 Ms. Akin stated there would be business scheduled for the next three meetings.

90.2 Ms. Akin reported that City Council would be having a public hearing on the continued Urban Growth Boundary Amendment on November 12, 2015.

90.3 Ms. Akin stated City Council would hear the Starbucks appeal of this Commission's decision on November 19, 2015.

90.4 Ms. Akin said there are some minor housekeeping amendments coming through.

100. Messages and Papers from the Chair. None.

110. Propositions and Remarks from the Commission.

110.1 Deputy City Attorney Huttli apologized for possibly misleading the Commission at the October 16, 2015, meeting on The Human Bean application in regards to improving the north half and deferring improvement of the south half.

Commissioner Whitlock said there had been testimony at the public hearing that there might be issues with deferring that improvement. He said when he made the motion at the October 16th meeting he recognized there was a strong possibility that they might not be able to do it. He said he appreciated Deputy City Attorney Huttli's legal guidance concerning exceptions.

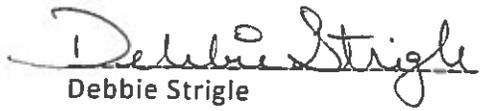
120. City Council Comments.

120.1 City Councilmember Gordon gave an update on the Skypark appeal to City Council. He said his personal opinion is that City Council is still struggling with that application. He mentioned there had been a study session two weeks ago that had been requested by the developer.

130. Adjournment.

130.1 The meeting was adjourned at approximately 12:20 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:



Debbie Strigle
Recording Secretary

Jeff Bender
Site Plan and Architectural Commission Chair

Approved: December 4, 2015



STAFF REPORT

for a Type-C quasi-judicial decision: **Architectural and Site Plan Review**

Project American Tire Storage and Distribution Facility
 Applicant: Joe McSweeney, Agent: ARCO Design/Build, Inc.

File no. AC-15-128

To Site Plan and Architectural Commission *for 12/4/2015 hearing*

From Aaron Harris, Planner II

Reviewer Kelly Akin, Principal Planner

Date November 25, 2015

BACKGROUND

Proposal

The proposal is for a new 100,000 square foot storage and distribution facility in the I-G (General Industrial) zoning district and the Airport Radar Overlay district. The project is located on an 89.11-acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive.

Subject Site Characteristics

Zoning	I-G	General Industrial
Overlay	AR	Airport Radar Overlay
GLUP	GI	General Industrial
Use	Vacant lot	

Surrounding Site Characteristics

North	I-G	General Industrial
	I-L	Light Industrial
	AD-MU	Airport Development-Mixed Use (County zoning)
	LI	Light Industrial (County zoning)
East	I-H	Heavy Industrial
South	I-L	Airport
West	I-L	Airport

Related Projects

Land Division LDP-15-136

Applicable Criteria

Medford Land Development Code (MLDC) §10.290

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

Corporate Names

Airport One, LLC is the property owner on record. The Oregon Secretary of State Business Registry lists William Bagley as the Registered Agent.

ISSUES AND ANALYSIS

Project Summary

The subject site is currently vacant. The applicant is proposing to construct a new one-story, 100,000 square foot storage and distribution facility.

Site Layout

The project area of this site is 5.77 acres and the parent parcel is 89.11 acres total. Land use application LDP-15-136 proposes to partition the project site from the parent parcel. The site is located at the southern terminus of Industry Drive, approximately 300 feet south of the intersection of Industry Drive and Enterprise Drive. The project proposal is within the I-G (General Industrial) zoning district and the Airport Radar Overlay district.

The building is set back from the street right-of-way by 90 feet. The majority of the site's street frontage shall be landscaped. The proposed facility will be dedicated to truck traffic shipping/receiving with no retail customer base. On-site pedestrian traffic is expected to be kept to a minimum for safety and security purposes.

Bufferyard

The project proposal is located in an I-G zoning district and abuts the I-L zoning district to the north. Abutting uses in the I-L zoning district include Roger's Machinery Co., a wholesaler of compressed air machinery; Eric's Electric Service, an electrical contractor; and Hoag Roofing, a roofing contractor. Coker Butte Lateral, an existing irrigation canal, runs east to west along the northern boundary of the project proposal site.

Bufferyard standards at MLDC §10.790(E) require a ten-foot landscaped bufferyard and a six-foot concrete or masonry wall. MLDC §10.790(E)(6)(c) provides the approval authority the discretion to make adjustments to the bufferyard requirements when the proposed project abuts existing development and the adjacent uses are sufficiently compatible that the full buffering, otherwise required, is not necessary and the uses are not expected to change significantly over time. MLDC §10.790(E)(6)(d) provides the approval authority the discretion to make adjustments to the bufferyard requirements where a project abuts an irrigation canal, natural waterway, or other such element.

The applicant has submitted justifications for an adjustment to the standards via email and has requested relief from the bufferwall requirement (Exhibit K). Staff has identified three justifications to address why the project proposal should be granted relief from the required six-foot concrete or masonry wall. First, the proposal includes a 40-foot landscaped bufferyard which exceeds the bufferyard width requirement by 30 feet. Second, the bufferyard includes a bioswale that is necessary to accommodate both the project proposal and future development on the parent parcel. The bioswale and bufferwall would be placed in the same general location if the bufferwall were required. Lastly, the adjacent uses are sufficiently compatible so that the full buffering standards are not necessary. Staff recommends that the Planning Commission approve the modification request.

Architecture

According to the applicant's project description, the building's exterior skin will be textured painted concrete wall panels. The building's colors will be mainly gray with blue accents. Architectural elements include glazing in aluminum frames at the office and pre-engineered metal canopies over main entry doors. Emphasis has been placed on the office corner with the use of the glazing and higher wall panels to create an accent at that location. Additional emphasis on the office was not intended because the proposed facility will not do retail business. The remainder of the facility retains the textured painted wall panels. The east elevation, being the main service location, incorporates the loading docks.

Parking and Loading

The project proposal includes 38 standard parking spaces and two ADA accessible parking spaces. According to the applicant, 12 employees will work on-site during the

largest shift. The proposal conforms to the off-street parking standards at MLDC §10.743-1. As a condition of approval, preferential parking for vanpools and carpools shall be provided in conformance with MLDC §10.809.

Bicycle Parking

The project proposal includes indoor bicycle parking accommodations adjacent to the office area. A railing will be installed to secure bicycles while inside the facility (Exhibit J). As a condition of approval, bicycle parking shall be provided for eight bikes per MLDC §10.748. As a condition of approval, bicycle parking signage shall be provided in conformance with MLDC §10.750(5).

Landscape Plan

The project proposal includes 450 feet of landscaped street frontage with 14 trees and 106 shrubs (Exhibit E). The proposal conforms to the street frontage landscape standards at MLDC §10.797.

The project proposal includes six trees and 78 shrubs for parking area landscaping, and far exceeds the square footage requirements for landscaped parking areas (Exhibit E). The proposal conforms to the parking area landscape standards at MLDC §10.746.

The project proposal includes tree species that will exceed 40 feet in height at full maturity. The site is located within the Airport Radar Overlay and MLDC §10.355 stipulates that no on-site objects may exceed 40 feet in height when development is located within the Overlay. As a condition of approval, the applicant shall submit a revised landscape plan with suitable tree species that will not exceed 40 feet in height prior to submission for building permits.

Concealments

The project proposal states that there will be no exterior trash enclosure and that trash will be handled on the inside of the building. The proposal conforms to the trash concealment standards at MLDC §10.781.

The project proposal includes roof-mounted HVAC equipment that will be screened from the roofline and therefore concealed from view. The proposal conforms to the HVAC concealment standards at MLDC §10.782.

Access

The project proposal includes an extension of Industry Drive, south of its current terminus, to provide site access. As a condition of approval, the applicant shall either provide a secondary access road per the Fire Department report (Exhibit N) or provide

an approved alternate means of fire protection per the Deputy City Manager's email received November 16, 2015 (Exhibit O).

Lighting

The project proposal includes 400 watt wall packs that will be mounted to the building's exterior. Lighting will be shielded downward to minimize off-site glare. As a condition of approval, all lighting shall meet the code stipulations at MLDC §10.764, including an exterior lighting plan submitted to the City prior to the installation of any new exterior lighting.

Paving

The project proposal includes a graveled maintenance access area adjacent to the northernmost portion of the building. As a condition of approval, this area shall be paved to meet the code stipulations at MLDC §10.746.

Site Design Concerns & Issues

Agency comments are included at Exhibits L–T. There are adequate facilities to serve the development according to agency comments.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit B) and recommends the Commission adopt the findings as presented and add the conditions of approval in Exhibit A to ensure compliance with the approval criteria.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare a Final Order for approval of AC-15-128 per the staff report dated November 25, 2015, including all exhibits and conditions.

EXHIBITS

- A Conditions of Approval, dated November 25, 2015
- B Applicant Narrative, received September 16, 2015
- C Site Plan, received October 14, 2015
- D Storm Water and Detention Plan, received September 16, 2015
- E Landscape Plan, received September 16, 2015
- F Floor Plan, received September 16, 2015
- G Roof Plan, received September 16, 2015
- H Exterior Elevations, received September 16, 2015
- I Colored Elevations, received September 16, 2015

- J Bike parking email from agent, received November 5, 2015
- K Bufferyard email from agent, received November 9, 2015
- L Jackson County Airport Authority email, received November 3, 2015
- M Rogue Valley Sewer Services memo, received November 3, 2015
- N Fire Department report, received November 5, 2015
- O Deputy City Manager email to agent, received November 16, 2015
- P Water Commission memo, received November 11, 2015
- Q Public Works staff report, received November 11, 2015
- R Building Department memo, received November 11, 2015
- S Parks Department memo, received November 10, 2015
- T ODOT memo, received November 6, 2015
Vicinity map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA:

DECEMBER 4, 2015

American Tire Storage and Distribution Facility
AC-15-128
Conditions of Approval
November 25, 2015

DISCRETIONARY CONDITIONS

1. The applicant is granted relief from the bufferyard wall requirement at MLDC §10.790(E).

CODE CONDITIONS

1. All proposed lighting shall meet the code stipulations at MLDC §10.764, including an exterior lighting plan submitted to the City prior to issuance of building permits.
2. To assure compliance with MLDC §10.355, the applicant shall submit a revised landscape plan with suitable tree species that will not exceed 40 feet in height prior to issuance of building permits.
3. Preferential parking for vanpools and carpools shall be provided in conformance with MLDC §10.809 prior to receiving a certificate of occupancy.
4. Bike parking shall be provided for eight bikes in order to accommodate 20% of the number of spaces provided for automobiles per MLDC §10.748 prior to receiving a certificate of occupancy.
5. Bike parking signage shall be provided in conformance with MLDC §10.750(5) prior to receiving a certificate of occupancy.
6. The graveled maintenance access area shall be paved per MLDC §10.746 prior to receiving a certificate of occupancy.
7. The applicant shall comply with the Jackson County Airport Authority memo received November 3, 2015 (Exhibit L).
8. The applicant shall comply with the Rogue Valley Sewer Services memo received November 3, 2015 (Exhibit M).
9. The applicant shall comply with the Fire Department report received November 5, 2015 (Exhibit N). The applicant may provide an approved alternative means of fire protection to comply with site access provisions per the Deputy City Manager's email, received November 16, 2015 (Exhibit S).
10. The applicant shall comply with the Parks Department memo received November 10, 2015 (Exhibit O).

**American Tire Storage and Distribution Facility
AC-15-128
Conditions of Approval
November 25, 2015**

11. The applicant shall comply with the Medford Water Commission memo received November 11, 2015 (Exhibit P).
12. The applicant shall comply with the Public Works Department staff report received November 11, 2015 (Exhibit Q).
13. The applicant shall comply with the Building Department memo received November 11, 2015 (Exhibit R).

Section I - Narrative
ARCO Design/Build, Inc.

RECEIVED

SEP 16 2015

PLANNING DEPT.

The proposed development is a 100,000 SF storage and distribution facility for American Tire Distributors (ATD). The facility will consist of 16 truck dock loading positions. Approximately 97,000 SF of the space is dedicated to racked storage. There is approximately 3,000 SF of administrative office space.

CITY OF MEDFORD

EXHIBIT B

FILE # AC-15-128

Section II - Compatibility - Criterion No. 1
Questionnaire Responses
ARCO Design/Build, Inc.

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SEP 16 2015

PLANNING DEPT.

- A To the north, property development consists of various businesses such as supply companies, mini-storage units, service companies (i.e. Hunter Communications), and two separate parcels of an auto salvage yard. All structures are pre-engineered metal building type with standard sheet metal exterior skins. Colors are largely earth tone consisting of greens, tans, and whites. Several buildings also incorporate blue in to their exterior color scheme.
- To the south, property is undeveloped.
- To the west, property is undeveloped. The Jackson County Airport property is located approximately 1/2 mile away.
- To the east, property is undeveloped except for the business on Crater Lake Avenue which is approximately 3/4 of a mile away.
- B The proposed development, like those adjacent, is intended for industrial use. The exterior skin will be textured painted concrete wall panels. Colors will be mainly gray with blue accents (see attached colored elevations). In addition, architectural elements include glazing in aluminum frames at the office and pre-engineered metal canopies over main entry doors. Given the industrial nature of the project, emphasis has been put on the office corner with the use of the glazing and higher wall panels to create an accent at that location. The remainder of the facility retains the textured painted wall panels. The east elevation, being the main service location, incorporates the loading docks. The proposed facility will do no retail business, so additional emphasis on the office was not intended since way-finding for customer traffic was not needed.
- C The building design will consist of structural concrete wall panels throughout the perimeter. Wall panels will be constructed using aesthetic reveal patterns and a textured paint coating as well as textured accent paint bands in order to provide relief to the facade. The front office corner (east elevation) was emphasized to provide additional interest on the east side, which is the only portion that is visible from the public roadway.
- D As an industrially zoned area located over a mile from the nearest residential development, and with no retail customer base, minimal pedestrian traffic is expected. Roadway and sidewalk improvements are being provided by the property seller under a separate contract.
- D.a The site does not fall within 600 feet of a transit stop.
- E Little to no pedestrian traffic expected on the proposed project site. The proposed facility will be dedicated to truck traffic shipping/receiving with no retail customer base. For safety and security reasons, pedestrian traffic at the site is expected to be kept to a minimum.
- F Employee and truck traffic will both enter from Industry Drive.
- G The proposed site does not contain any existing trees.
- H The stormwater detention facility will consist of a pond/bio-swale with the appropriate landscape requirements to be located on the northern portion of the property.
- I The landscape architect that has been retained for this project is very familiar with the city landscape requirements and per the provided landscape and irrigation plan, all requirements have been met.

CITY OF MEDFORD

EXHIBIT B

FILE # AC-15-128

- J Site lighting will be provided by 400W wall packs that will be mounted to the buildings exterior. Wall pack lighting is shielded downward to minimize the amount of light that escapes the property while still providing optimal on-site lighting. Due to the proximity of truck and employee parking areas to the building, exterior pole site lighting is not going to be required.
- K A small monument sign will be located at the buildings entrance drive along Industry Drive. Typically, these signs are less than 75 SF in size and stand approximately 6 to 8 feet tall. This sign will contain the ATD name/logo as well as the facility address.
- L Fencing is proposed to at the truck maneuvering court area for both safety and security reasons. Fencing will be 6' tall galvanized chain link, consistent with fencing already in place at some of the northern adjacent properties.
- M All trucks are to be loaded at building dock doors, complete with dock seals. These seals help to minimize any noise from the loading process. There is no outside loading or storage area.
- N Existing developments to the north are all industrial use developments. The proposed facility blends well with this use. In addition, the construction type proposed lends a more "permanent" aesthetic to the area with the intent to provide a visual upgrade to the surrounding properties.
- O None requested at this time.
- P None requested at this time.

RECEIVED

OCT 14 2015

Planning Dept



VICINITY MAP

CITY OF MEDFORD, JACKSON COUNTY,
OREGON MAP/TAX LOT 1.375, R 1W TL 2500
TAX LOT 2500

PARENT PARCEL = 89.11 ACRES
PROPOSED SITE = 5.77 ACRES
ZONING = I-C (GENERAL INDUSTRIAL)

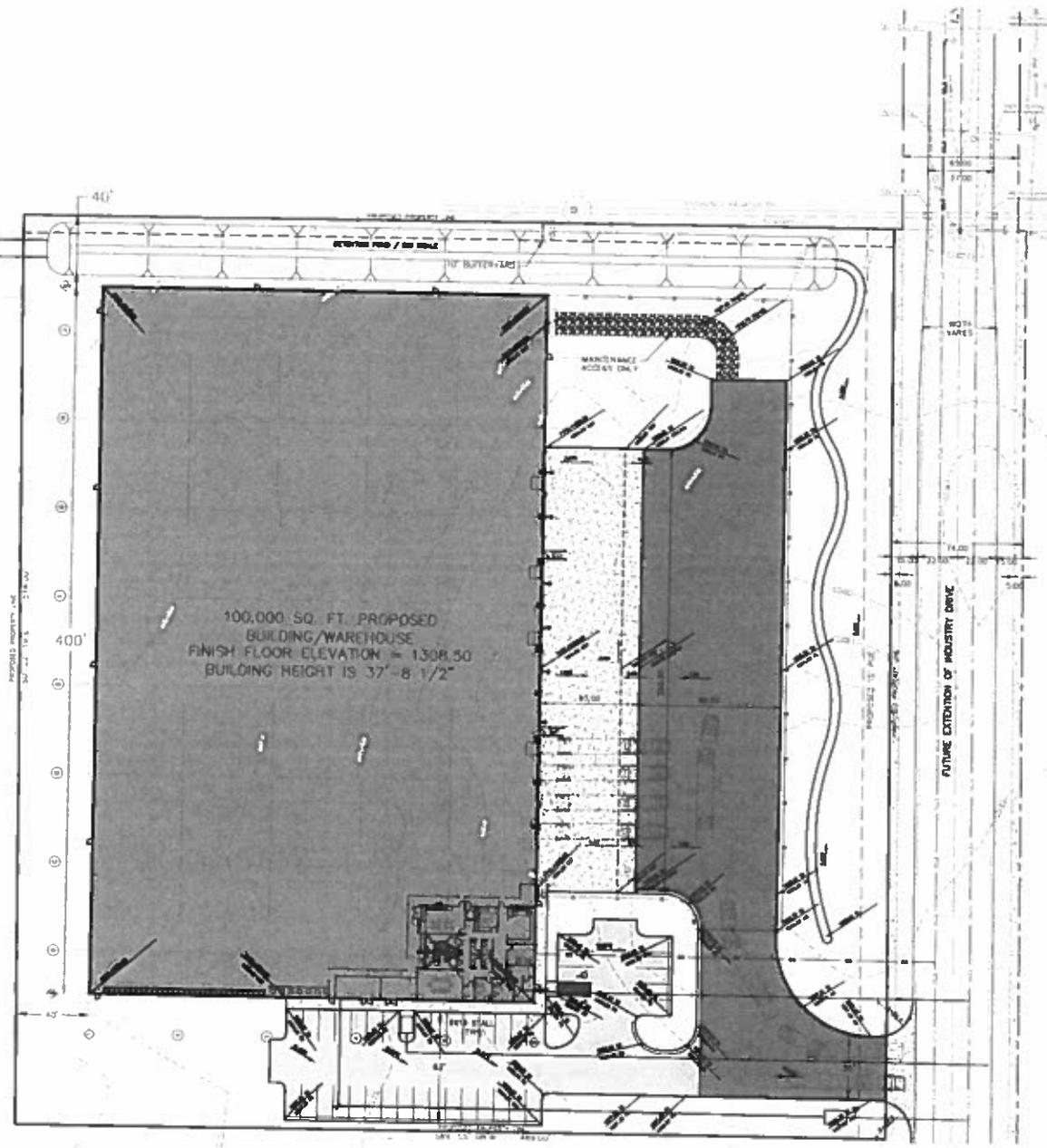
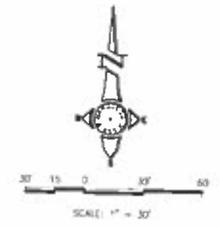
FIRE DISTRICT - MEDFORD FIRE DISTRICT
ZONING OVERLAY - AH
BUILDING AREA = 100,000 GSF WAREHOUSE

VEHICULAR PARKING (MLDC TABLE 10.743-1)
100,000 GSF/1000 = 100 x 2 = 20+12 EMPLOYEE
SPACES FOR A TOTAL MIN. SPACES OF 32 SPACES
PARKING SPACES PROVIDED = 40 SPACES (2 H.C.)

THERE WILL BE NO EXTERIOR TRASH ENCLOSURE,
TRASH IS HANDLED ON THE INSIDE OF THE BUILDING

ALL MECHANICAL EQUIPMENT IS ON THE ROOF AND IS
SCREENED FROM THE ROOFLINE AND NOT VISIBLE
FROM THE STREET

ALL EXTERIOR LIGHTING IS PROVIDED FROM WALL
PACKS MOUNTED TO PROPOSED BUILDING AND NO
POLE MOUNTED LIGHTS WILL BE PROVIDED.



ARCO
DESIGN/BUILD

American
Contractors
Medford, Oregon
Contractor: ARCO Design/Build

PRELIMINARY
THESE DRAWINGS SHALL
NOT BE USED FOR
CONSTRUCTION
ISSUING
RECORDATION
COMPLETION
ISSUANCE OF A PERMIT
DATE 03

PERMIT RECORD

REVISIONS

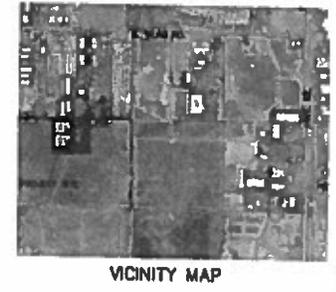
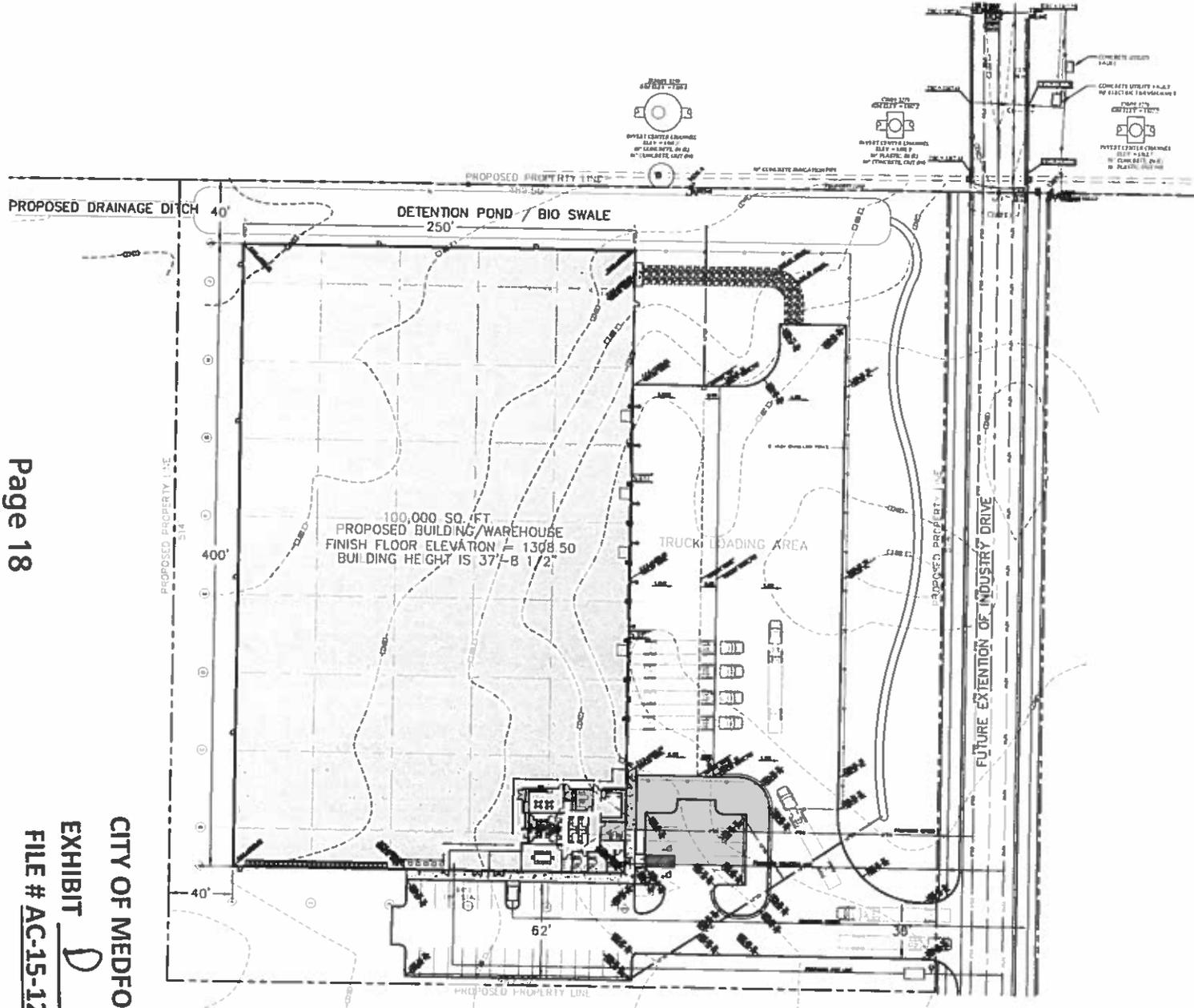
ISSUE DATE

JOB # 15-1123.1

SITE PLAN

C-001

26



CITY OF MEDFORD, JACKSON COUNTY, OREGON
 MAP/TAX LOT T.37S. R.1W.1L.2500
 TAX LOT 2500

89.11 ACRES
 PROPOSED SITE ACRES = 5.77
 ZONING = I-G (GENERAL INDUSTRIAL)

FIRE DISTRICT - MEDFORD FIRE DISTRICT
 ZONING OVERLAY = AR
 BUILDING AREA = 100,000 GSF WAREHOUSE

VEHICULAR PARKING (M.L.D.C. TABLE 10 743-1)
 100,000 GSF/1000 = 100 x 2 = 20x12 EMPLOYEE
 SPACES FOR A TOTAL MIN. SPACES OF 32 SPACES
 PARKING SPACES PROVIDED = 40 SPACES (2 H.C.)

THERE WILL BE NO EXTERIOR TRASH ENCLOSURE,
 TRASH IS HANDLED ON THE INSIDE OF THE BUILDING.

ALL MECHANICAL EQUIPMENT IS ON THE ROOF AND IS
 SCREENED FROM THE ROOFLINE AND NOT VISIBLE
 FROM THE STREET

ALL EXTERIOR LIGHTING IS PROVIDED FROM WALL
 PACKS MOUNTED TO PROPOSED BUILDING AND NO
 POLE MOUNTED LIGHTS WILL BE PROVIDED

PLANNING DEPT.
 SEP 16 2015
 SCALE: 1" = 30'

GMA
 GEOTECHNICAL CONSULTANTS

MARQUESS
 CIVIL ENGINEERING

BCP
 CONSULTING ENGINEERS

ARCO
 DESIGN/BUILD

AMERICAN
 CIVIL ENGINEERS

Medford, Oregon
 Contractor: ARCO Design/Build

PRELIMINARY
 THESE DIMENSIONS SHALL
 NOT BE USED FOR
 CONSTRUCTION
 BEING
 RECORDATION
 CONCERNING
 ISSUANCE OF A PERMIT
 APR 22

RECORDED

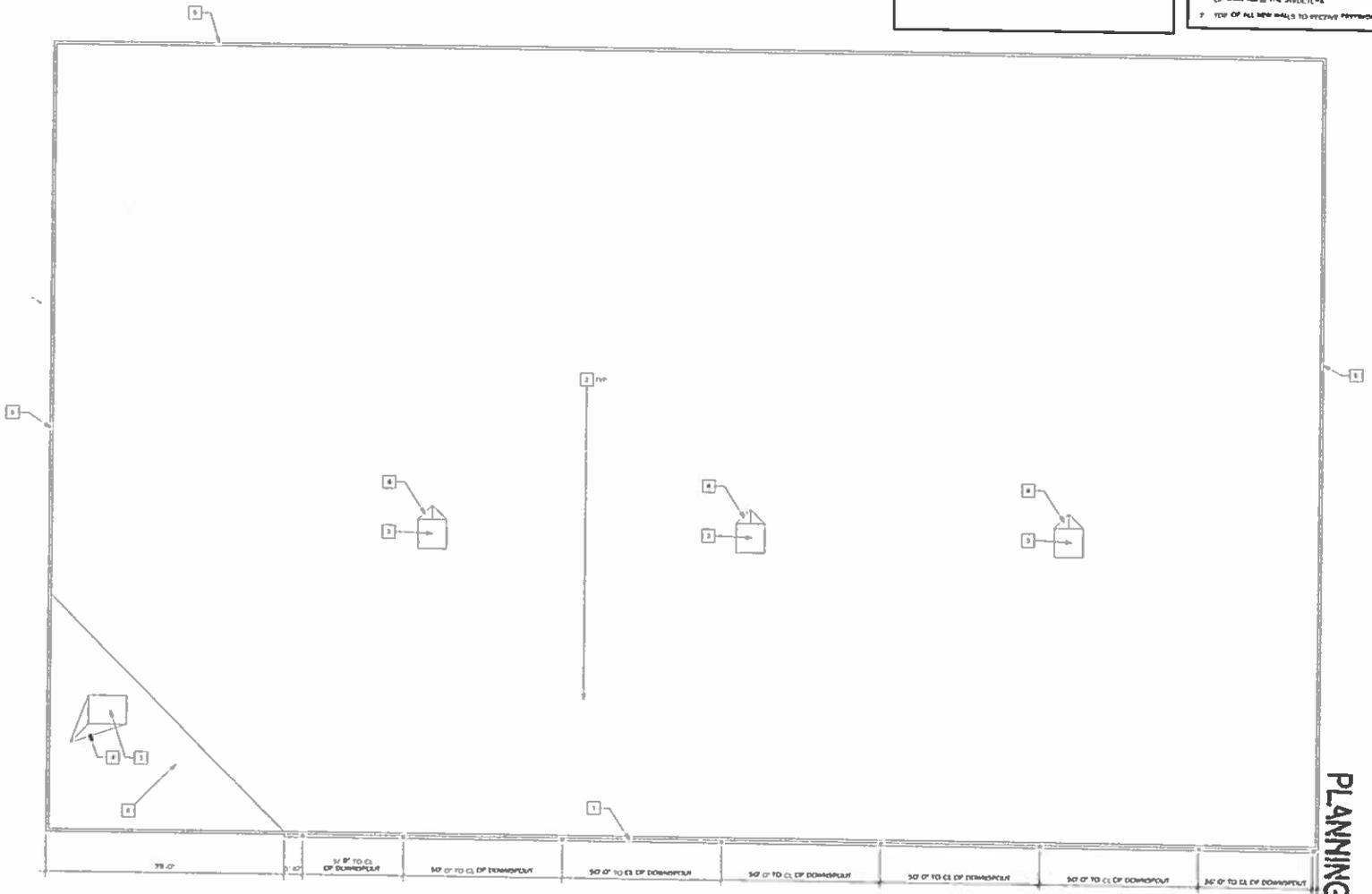
REVISIONS

DATE PREPARED: 08/28/15
 DRAWN BY: JRM
 CHECKED BY: JRM
 JOB NO: 15-128
 SHEET NO: 18

C-002

STORM WATER
 DETENTION PLAN

OVERALL ROOF PLAN
 SCALE: 3/8"=1'-0"



- ROOF PLAN KEY NOTES**
- 1 PPE FINISHED METAL GUTTERS AND DOWNSPOUTS
 - 2 SLOPED STRUCTURE IS 6" PER FOOT MINIMUM
 - 3 HVAC EQUIPMENT - SEE MECHANICAL DRAWINGS
 - 4 CRACK-IT ROOF SLOPE IS REGULATED AFTER NO RECEIVED EQUIPMENT (TYP.)
 - 5 PPE FINISHED METAL COPING CAP (TYP.)
 - 6 CRACK-IT ROOF SLOPE IS REQUIRED TO DRAIN WATER AROUND HEIGHT PARAMETER

- GENERAL ROOF PLAN NOTES**
- 1 FLASH MECHANICAL EQUIPMENT AND PENETRATIONS ON THE ROOF ACCORDING TO MANUFACTURER'S SPECIFICATIONS
 - 2 ALL FLASHINGS TO BE WRAPPED WITH METALFLAME UNDER COPING CAP AND TERMINATION FLASH TO BE WRAPPED
 - 3 GALVANIZED METAL ROOF DETAIL SHALL BE FINISHED WHITE
 - 4 FLASHING AND COLLECTOR FLASHING FOR ROOF TOP UNITS SHALL BE COMPLETED IN ACCORDANCE WITH SBMACAM
 - 5 10' OF MINIMUM DISTANCE ANY VENT TO AIR INTAKE
 - 6 ALL SLOPING PATTERNS SHOWN ON ROOF PLAN ARE REPRESENTATIVE OF SHAPING IN THE SUBJECT, PER
 - 7 TOP OF ALL NEW WALLS TO RECEIVE FINISHED METAL COPING

RECEIVED
 SEP 16 2015
 PLANNING DEPT.



AMERICAN ARCHITECTURE
 Project, Planning, and Construction Services
 Contracted By: ARCO Design/Build



PROJECT RECEIVED

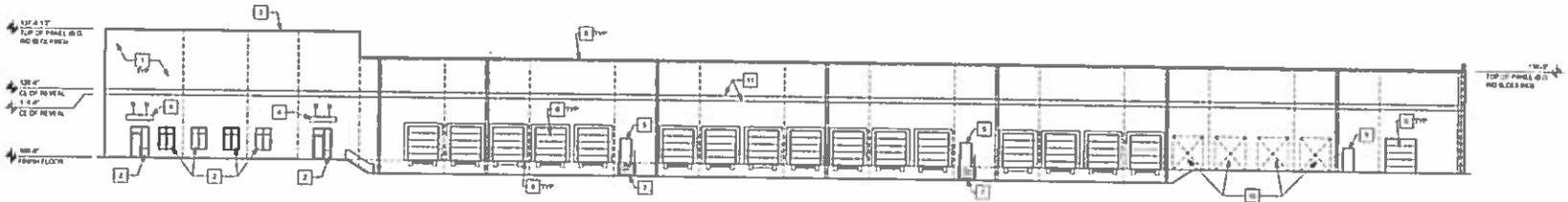
NO. OF SHEETS

DATE OF SHEET

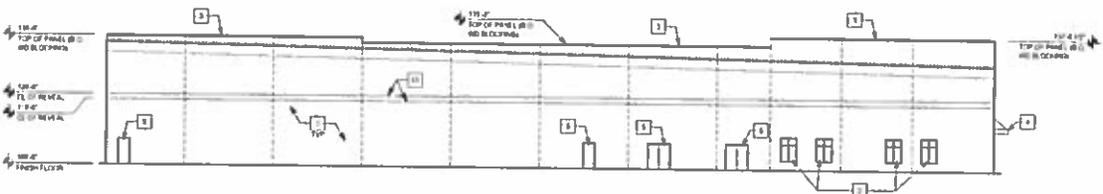
JOB #

DATE

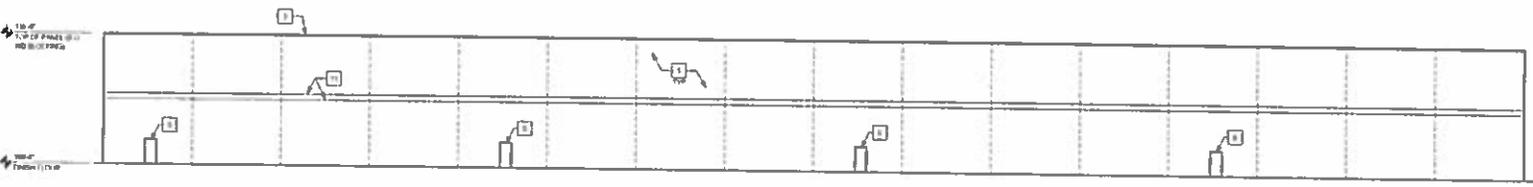
A2.4
 ROOF PLAN



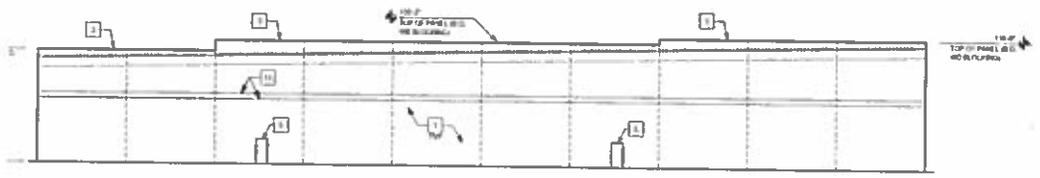
1 EAST ELEVATION
SCALE: 1/8"=1'-0"



2 SOUTH ELEVATION
SCALE: 1/8"=1'-0"



WEST ELEVATION
SCALE: 1/8"=1'-0"



NORTH ELEVATION
SCALE: 1/8"=1'-0"

ELEVATION KEY NOTES	
1	PAINTED 1 1/2" CONCRETE PANELS WITH REVEAL
2	INSULATED GLAZING IN ALUMINUM STOREFRONT SYSTEM
3	PRE-FINISHED METAL COPING CAP - COLOUR SELECTED FROM AMMAY ACTUESS'S STANDARD COLOUR LIST
4	PRE-MANUFACTURED METAL CANOPY CAP OR TO BE SELECTED FROM AMMAY ACTUESS'S STANDARD COLOUR LIST
5	PAINTED HOLLOW METAL MAIN DROP
6	PRE-FINISHED OVERHEAD DOOR
7	GALVANNEZED METAL EXTERIOR STAR WITH REVEAL AND SHIMMER SHIMMER
8	PRE-MANUFACTURED DOOR SHELTER OR TO BE SELECTED FROM AMMAY ACTUESS'S STANDARD COLOUR LIST
9	PRE-FINISHED METAL GUTTER AND DRAIN
10	2 1/2" x 42" SIBCOLOUR PANEL
11	3/4" DEEP REVEAL PAINTED

SEP 16 2015
PLANNING DEPT.

GMA
ARCHITECTS

BCP
BRADWOOD CONSULTING PARTNERS

ARCO
DESIGN/BUILD

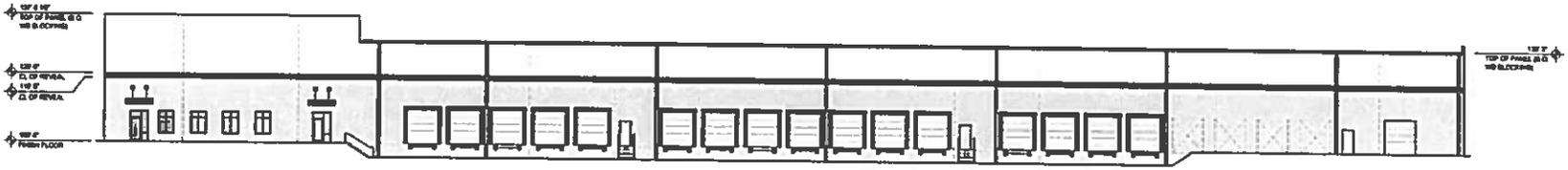
Method: Design
Contractor: ARCO Design/Build



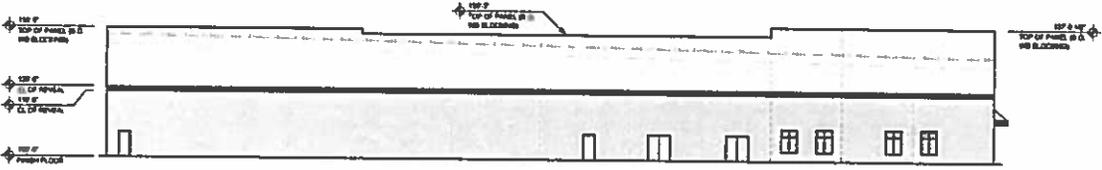
PROJECT RECEIVED

NO PROJECT

RECEIVED
A5.1
EXTERIOR ELEVATIONS



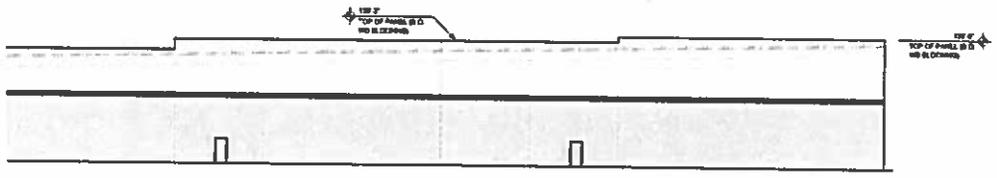
1 EAST ELEVATION
A5.2 SCALE 1/8"=1'-0"



2 SOUTH ELEVATION
A5.2 SCALE 1/8"=1'-0"



ELEVATION
1/8"=1'-0"



ELEVATION
1/8"=1'-0"

Page 23

CITY OF MEDFORD
EXHIBIT I
FILE # AC-15-128

PLANNING DEPT.

SEP 16 2015

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GMA
ARCHITECTS

BCP
brookwood
capital
partners

ARCO
DESIGN/BUILD

Medical Center
AMERICAN
Low Architecture
CONTRACTORS
Contractor: ARCO Design/Build

RIGHT RECORDS

REVISIONS

DATE

JOB # 011502

A5.2

COLORED ELEVATIONS

Aaron J. Harris

RECEIVED

From: Cronan, Josh <jcronan@arcodb.com>
Sent: Thursday, November 05, 2015 9:02 AM
To: Aaron J. Harris
Subject: RE: AC-15-128 - agency comments and code requirements

NOV 05 2015
Planning Dept.

Aaron,
Thank you for your email.

With regards to bicycle parking:

1. It will be the intent for bicycle parking to be located inside of the warehouse facility, which will meet the requirement of being in a lockable, secure enclosure. Bicycle parking will be in the warehouse space adjacent to the office area. A railing will be installed between the office and warehouse space that will service as a resting place for bicycles and will also allow for bicycles to be secured while inside the facility.

With regards to the buffer yard requirement:

1. This item has been discussed previously and it is our understanding that the planning department has agreed to recommend an exception to this requirement. Due to the proximity of the planned drainage spillway and pond along the north end of the site, placement of a wall in this area is not ideal.

If you have any questions regarding these items please do not hesitate to give me a call or shoot me an email. Thanks again. JC

Josh Cronan
Project Manager
ARCO Design/Build, Inc.
380 Interstate North Pkwy | Suite 210 | Atlanta, GA 30339
O: 770.541.1700 x4104 | F: 770.541.1699 | C: 404.520.3115
Download: [vCard](#)

ARCO INTEGRITY • SOLUTIONS • RELATIONSHIPS
DESIGN / BUILD

From: Aaron J. Harris [mailto:Aaron.Harris@cityofmedford.org]
Sent: Thursday, November 5, 2015 11:17 AM
To: Cronan, Josh
Subject: AC-15-128 - agency comments and code requirements

Greetings Josh,

This email is a recap of our phone conversation yesterday afternoon. Attached to this email you will find the Airport Authority's email. I have also attached Planning Department comments regarding the bufferyard and landscaping requirements per our phone conversation.

Please respond to this email stating 1) the proposed number of spaces for bicycle parking for the project, 2) if the proposed parking is indoors or outdoors, and 3) the proposed type of bicycle facilities, if any. Below I have cut and pasted the applicable code sections, and have highlighted the section that grants authority to the decision-making body to grant relief to the standards.

Aaron J. Harris

RECEIVED

From: Cronan, Josh <jcronan@arcodb.com>
Sent: Monday, November 09, 2015 9:38 AM
To: Aaron J. Harris
Subject: RE: AC-15-128 bufferyard narrative

NOV 09 2015

Planning Dept.

Aaron,

The sections you highlighted are what I referred to when I spoke with Kelly.

1. The adjacent properties contain businesses that operate similarly to the intended purpose of the proposed facility.
 - a. One of the adjacent businesses is a salvage yard.
 - b. Another adjacent business is an electrical contracting company that receives regular truck deliveries as well as uses its property for outside storage of materials.
 - c. The proposed facility will not utilize any outside storage. As a tire distributor, this business will not generate trash in the form of boxes, plastic wraps, pallets, etc.
 - d. The only noise that will be generated by the proposed facility is that of truck engine noise. Although, I would think that due to the close proximity of the airport that noise would not be a determining factor.
2. The other reason why a wall along the property line would not make sense is due to the drainage swale that has to be installed along the north end of the property. I do not point this out as a "convenience of site design" for our site, but because this drainage swale is part of the overall site design that will accommodate future development in this area as well.

hopefully this is enough detail to convey the reasoning behind our request to have exemption from constructing a wall between the two properties. Please let me know if there is anything else we can do to help or any other information you might need. Thanks again. JC

Josh Cronan
Project Manager
ARCO Design/Build, Inc.
380 Interstate North Pkwy | Suite 210 | Atlanta, GA 30339
O: 770.541.1700 x4104 | F: 770.541.1699 | C: 404.520.3115
Download: [vCard](#)

ARCO INTEGRITY • SOLUTIONS • RELATIONSHIPS
DESIGN / BUILD

From: Aaron J. Harris [mailto:Aaron.Harris@cityofmedford.org]
Sent: Monday, November 9, 2015 12:01 PM
To: Cronan, Josh
Subject: AC-15-128 bufferyard narrative

Greetings Josh,

Kelly informed me that you stopped by the office last week while I was on my lunch break. Sorry I missed you! This email is just a follow-up per your conversation with Kelly to request a brief narrative explaining why the tire distribution facility should not be required to adhere to the stipulations for a bufferyard as set forth at co

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NOV 03 2015

Aaron J. Harris

From: Marcy Black <BlackMA@jacksoncounty.org>
Sent: Tuesday, November 03, 2015 9:48 AM
To: Aaron J. Harris
Subject: File No. AC-15-128 - Project Name: Joe McSweeney

Planning Dept.

Aaron:

The Airport requests an Avigation, Noise & Hazard Easement be required as part of the process. In addition, an FAA Form 7460-1, Notice of Proposed Construction or Alteration must be submitted to the FAA for their review and approval. The form may be submitted electronically at: <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>.

Thank you for the opportunity to comment.

Marcy Black
Deputy Director-Administration



RECEIVED

NOV 03 2015

ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

Planning Dept.

November 3, 2015

City of Medford Planning Department
411 West 8th Street
Medford, Oregon 97501

Re: AC-15-128, McSweeney Warehouse (371W06B, Tax lot 2500)

ATTN: Aaron,

Sewer service to the proposed development will require a main line extension from the end of the existing sewer on Industry Drive.

Rogue Valley Sewer Services requests the proposed development be subjected to the following conditions:

- Applicant must submit engineered sewer plans in accordance with Rogue Valley Sewer Services standards for review and approval prior to development.
- No building occupancy will be allowed until all required mainline sewer extensions are accepted by RVSS.

Feel free to contact me with any questions related to this project.

Sincerely,

Carl Tappert, PE
Manager

Page 1 of 1



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

RECEIVED

NOV 05 2015

Planning Dept.

LAND DEVELOPMENT REPORT - PLANNING

To: City of Medford Public Works

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent)

File #: AC - 15 - 128

Site Name/Description:

Consideration of plans to construct a 100,000 square foot storage and distribution facility on a portion of an 89.11 acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive, within the I-G (General Industrial) zoning district; Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent). Aaron Harris, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
----------------------------	-----------

Requirement SECONDARY FIRE APPARATUS ACCESS - COMMERCIAL	OFC D104
---	-----------------

Secondary fire access road required.

SECTION D104-COMMERCIAL AND INDUSTRIAL DEVELOPMENTS

D104.1 Buildings exceeding three stories or 30 feet in height. Buildings or facilities exceeding 30 feet (9144 mm) or three stories in height shall have at least two means of fire apparatus access for each structure.

D104.2 Buildings exceeding 62,000 square feet in area. Buildings or facilities having a gross building area of more than 62,000 square feet (5760 m2) shall be provided with two separate and approved fire apparatus access roads.

Exception: Projects having a gross building area of up to 124,000 square feet (11 520 m2) that have a single approved fire apparatus access road when all buildings are equipped throughout with approved automatic sprinkler systems.

D104.3 Remoteness. Where two access roads are required, they shall be placed a distance apart equal to not less than one half of the length of the maximum overall diagonal dimension of the property or area to be served, measured in a straight line between accesses.

Requirement FIRE HYDRANTS	OFC 508.5
----------------------------------	------------------

Fire hydrants with reflectors will be required for this project.

Hydrant locations shall be as follows: A total of seven fire hydrants are required. One fire hydrant shall be provided on Industry Drive near the fire sprinkler fire department connection. Six internal fire hydrants shall be placed around the building (300' maximum spacing between fire hydrants).

Additional hydrants may be required to comply with the requirement of proximity to fire department connections (for

CITY OF MEDFORD

EXHIBIT N

FILE # AC-15-128



Medford Fire Department

200 S. Ivy Street, Room #180

Medford, OR 97501

Phone: 774-2300; Fax: 541-774-2514;

www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: City of Medford Public Works

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent)

File #: AC - 15 - 128

Site Name/Description:

fire sprinkler and standpipe systems, the fire department connection shall be located at an approved location away from the building and within 75' of a fire hydrant. The fire department connection shall be located on the same side as the fire department access route.).

The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Plans and specifications for fire hydrant system shall be submitted to Medford Fire Department for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).

Requirement ADDRESS IDENTIFICATION

OFC

505.1

Building numbers shall be placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background.

Temporary signs shall be installed at each street intersection when construction of new roadways allows passage of vehicles.

Requirement FD APPARATUS ACCESS ROAD DESIGN

OFC

503.2.1

Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 60,000 pounds.

(See also OFC 503.4; D102.1)

The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).

Requirement AERIAL APPARATUS ACCESS

OFC

D105

An aerial fire apparatus road meeting the conditions of OFC D105.3 shall be provided along one entire side of the building.

SECTION D105-AERIAL FIRE APPARATUS ACCESS ROADS



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501

Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: City of Medford Public Works

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent)

File #: AC - 15 - 128

Site Name/Description:

D105.1 Where required. Buildings or portions of buildings or facilities exceeding 30 feet (9144 mm) in height above the lowest level of fire department vehicle access shall be provided with approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Overhead utility and power lines shall not be located within the aerial fire apparatus access roadway.

D105.2 Width. Fire apparatus access roads shall have a minimum unobstructed width of 26 feet (7925 mm) in the immediate vicinity of any building or portion of building more than 30 feet (9144 mm) in height.

D105.3 Proximity to building. At least one of the required access routes meeting this condition shall be located within a minimum of 15 feet (4572 mm) and a maximum of 30 feet (9144 mm) from the building, and shall be positioned parallel to one entire side of the building.

Requirement FD ACCESS TO BUILDINGS AND FACILITIES

OFC 503.1.1

At a minimum, an access road shall be extended along the entirety of the south side and west sides. A fire department turn-around shall be provided at the southwest corner if a secondary access road is not extended to Airway Drive.

Fire apparatus access roads shall be provided and maintained in accordance with Sections 503.1.1 through 503.1.3 (See Appendix D).

Approved fire apparatus access roads shall be provided for every facility, building or portion of a building hereafter constructed or moved into or within the jurisdiction. The fire apparatus access road shall comply with the requirements of this section and shall extend to within 150 feet (45 720 mm) of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.

Exception: The fire code official is authorized to modify Sections 503.1 and 503.2 where any of the following applies:

1. The building is equipped throughout with an approved automatic sprinkler system installed in accordance with Section 903.3.1.1, 903.3.1.2 or 903.3.1.3.
2. Fire apparatus access roads cannot be installed because of location on property, topography, waterways, nonnegotiable grades or other similar conditions, and an approved alternative means of fire protection is provided.
3. There are not more than two Group R-3 or Group U occupancies (OFC 503.1.1).



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: City of Medford Public Works

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent)

File #: AC - 15 - 128

Site Name/Description:

Requirement FIRE DEPARTMENT TURN-AROUND

OFC

503.2.5

If a secondary access road is not extended to Airway Drive, a fire department turn-around will be required at the southwest corner of the property.

Dead-end Fire Apparatus access roads in excess of 150 feet in length shall be provided with approved provisions for the turning around of fire apparatus.

The Fire department turn-around area must be posted with "NO PARKING-FIRE LANE" signs. These signs shall be spaced at 50' intervals along the fire lane and at fire department designated turn-around's.

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

Aaron J. Harris

RECEIVED

From: Chris A. Reising
Sent: Friday, November 13, 2015 8:59 AM
To: 'jcronan@arcodb.com'
Cc: Bill W. Hoke; Greg G. Kleinberg; Aaron J. Harris
Subject: ATD Medford

NOV 16 2015

Planning Dept.

Josh:

As we discussed on the phone, under the authority granted by Oregon Fire Code section 503.1.1, Fire Marshal Greg Kleinberg is willing to consider a proposal from you for an "approved alternate means of fire protection" to obviate the code-mandated "two means of fire apparatus access" as found in Oregon Fire Code section D104. Your proposal should include the following key points:

1. All other requirements included in the staff report resulting from the Land Development Meeting of Nov. 11 will still apply.
2. A second driveway from Industry Drive will be provided for fire department access, spaced as far as practical from the primary driveway entrance to the site.
3. An automatic smoke-detection system shall be installed throughout the high-piled storage area. The system shall be monitored.
4. The effectiveness of the ESFR sprinkler system will be improved with either a pressure increase or increased density, or other method acceptable to the Fire Marshal.

In addition to the above, in lieu of the required on-site fire hydrants, you could substitute stand-pipes connected to the fire suppression system, and extending hose connections to the building exterior to satisfy this requirement. This substitution would be subject to plan review and approval.

Chris

Chris Reising, P.E., S.E.

Deputy City Manager

Development Services

City of Medford

Lausmann Annex

200 S. Ivy St

Medford, OR 97504-3100

541-774-2351

CITY OF MEDFORD

EXHIBIT

FILE # AC-15-128



Staff Memo

RECEIVED

NOV 11 2015

Planning Dept.

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-15-128

PARCEL ID: (Proposed) 371W TL 2500

PROJECT: Consideration of plans to construct a 100,000 square foot storage and distribution facility on a portion of an 89.11 acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive, within the I-G (General Industrial) zoning district; Joe McSweeney, Applicant (ARCO Design/Build, Inc., Agent). Aaron Harris, Planner.

DATE: November 9, 2015

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of 12-inch water lines is required. Proposed water lines in the Public Right-of-way are required to be installed per the latest Medford Water Commission Standards. Proposed "on-site" water lines are required to be installed in paved travel lanes. Water lines shall not extend through landscaping, parking spaces, or under proposed trees. Applicants' civil engineer shall coordinate with MWC engineering staff on layout of onsite water lines, hydrants, meters and back flow devices.
4. Applicant shall coordinate with Medford Fire Department for on-site fire hydrant locations for this development.
5. Dedication of a 10 foot wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement shall be submitted to MWC for review and recordation prior to construction.
6. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35> .

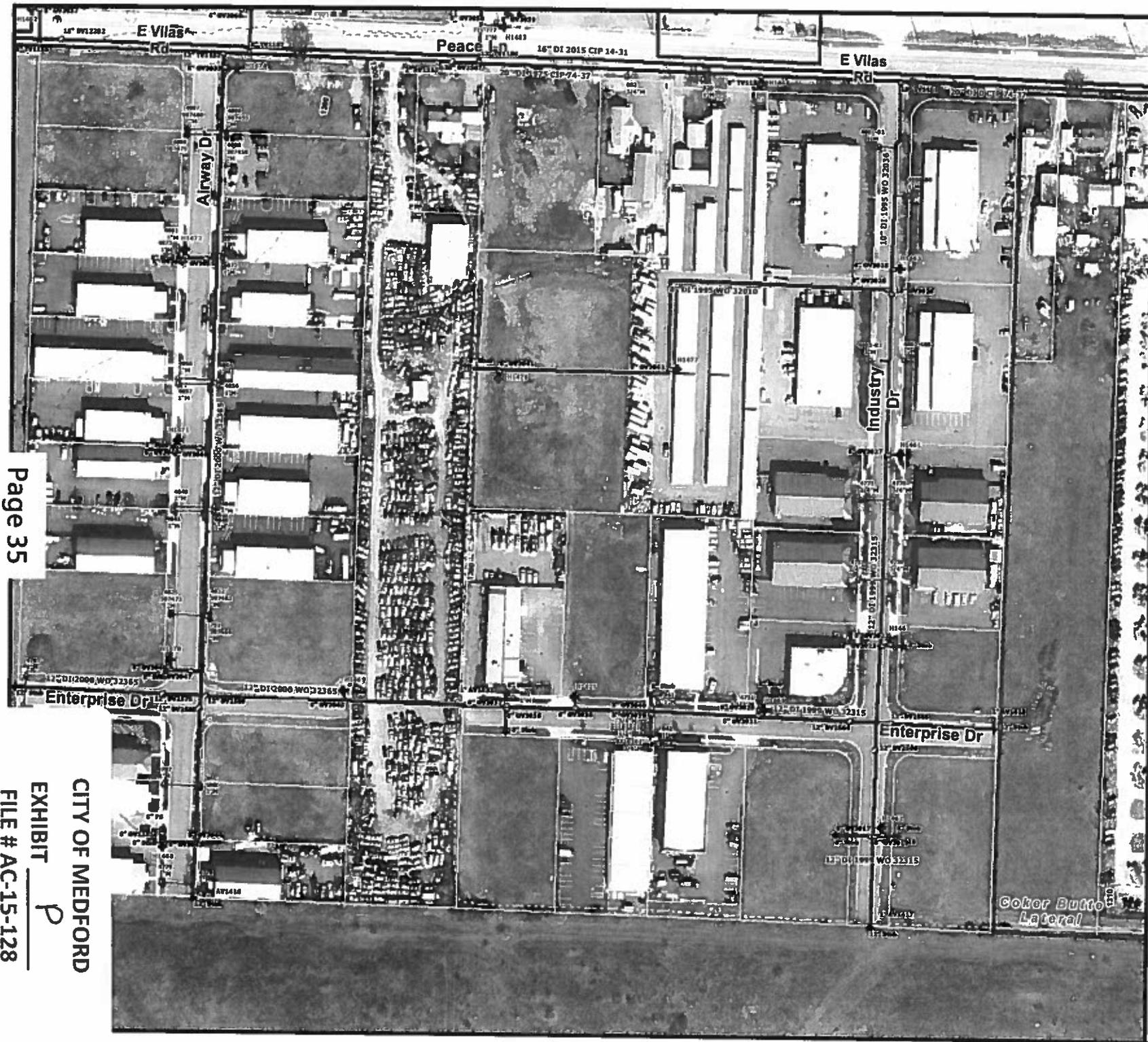
Continued to next page



Continued from previous page

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required. (See Condition 3 above)
3. Static water pressure is expected to be between 80 and 85 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
4. MWC-metered water service does not exist to this property.
5. Access to MWC water lines is available. There is a 12-inch water line stubbed for extension at the south end of Industry Drive.



0 100 200 Feet
Scale: 1"=100'

**Water Facility Map
for
AC-15-128**

Legend

- ⊙ Air Valve
- ⊙ Sample Station
- ⊙ Fire Service
- ⊕ Hydrant
- ▲ Reducer
- ⊠ Blow Off
- ⊕ Plugs-Caps

Water Meters:

- Active Meter
- On Well
- Unknown
- Vacant

Water Valves:

- ⊙ Butterfly Valve
- ⊙ Gate Valve
- ⊙ Tapping Valve

Water Mains:

- Active Main
- - - Abandoned Main
- Reservoir Drain Pipe
- Pressure Zone Line

Boundaries:

- ▭ Urban Growth Bound
- ▭ City Limits
- ▭ Tax Lots

MWC Facilities:

- C** Control Station
- P** Pump Station
- R** Reservoir



This map is a public good and information developed by Medford Water Commission. It is a work of public information. Medford Water Commission assumes no liability for errors, omissions, or misleading information. Please use as a general reference only.

Page 35

CITY OF MEDFORD
EXHIBIT P
FILE # AC-15-128



Continuous Improvement Customer Service

RECEIVED

NOV 11 2015

Planning Dept.

CITY OF MEDFORD

Revised Date: 11/11/2015

File Number: AC-15-128

PUBLIC WORKS DEPARTMENT STAFF REPORT
American Tire Distributors (ATD)

Project: Consideration of plans to construct a 100,000 square foot storage and distribution facility on a portion of an 89.11 acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive, within the I-G (General Industrial) zoning district.

Applicant: Joe McSweeney, Applicant (ARCO Design/Build, INC., Agent). Aaron Harris, Planner.

NOTE:

The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of building permits. Construction plans for the improvements would need to be approved by the Public Works Engineering Department prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

A. **STREETS**

1. **Dedications**

Industry Drive is classified as a Major Collector Street with a required right-of-way width of 74-feet. Prior to issuance of any permit for construction, the developer shall dedicate for public right-of-way, sufficient width of land along the frontage of this development to comply with the full width of right-of-way. Medford Land Development Code (MLDC 10.451).

The Developer shall receive S.S.D.C. (Street System Development Charge) credits for the public right-of-way dedications on Stewart Avenue, per the value established by the Medford Municipal Code, Section 3.815. **Should the developer elect to have the value of the S.S.D.C. credits determined by an appraisal, a letter to that effect must be submitted to the City Engineer within 60 working days of the date of the Final Order by the Site Plan and Architectural Commission. The City will then select an appraiser and a deposit will be required as stated in Section 3.815.**

Streets, as shown on the Site Plan, in which any portion terminates to a boundary line of the Development shall be dedicated to within one foot of the boundary line, and the remaining one foot shall be granted in fee, as a non-access reserve strip to the City of Medford. Upon approved dedication of the extension of said streets, the one-foot reserve strip shall automatically be dedicated to the public use as part of said street without any further action by the City of Medford. (MLDC 10.439)

In accordance with MLDC 10.471, **the property owner shall dedicate a 10 foot wide public utility easement (PUE) adjacent to the right-of-way line in Industry Drive along this Developments respective frontage.**

The right-of-way and PUE dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: right-of-way and PUE dedications, a copy of a current lot book report, preliminary title report, or title policy; a mathematical closure report (if applicable), and the Planning Department file number, all for review and signature acceptance by the City Engineer prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the areas dedicated.

2. Public Improvements

a. Public Streets

The Developer shall construct **Industry Drive** to Major Collector Street standards in accordance with the MLDC, Section 10.428 and 10.442. The roadway improvements shall include 44-feet of paving from curb face to curb face. Improvements shall also include a 10-foot planter strip, 5-foot sidewalk and street lights along this development's frontage. The City will pay S.S.D.C. credits per the Medford Municipal Code.

The Developer is eligible to receive SSDC credits for the improvements on Industry Drive, in accordance with the Medford Municipal Code Section 3.815.

b. Street Lights and Signing

1. Traffic Signs and Devices – City installed, Developer paid

A. 1 – Dead-end Barricade.

2. Street Lighting – Developer Provided and Installed

A. 3 – 250 watt HPS spaced 185-feet apart starting at the existing light located on Industry Drive.

B. 1 – New base mounted cabinet (BMC)

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this frontage.

3. Access and Circulation

Driveway access to the proposed development site shall comply with MLDC 10.550. The driveway approach shall have 20 foot curb radii, concrete valley gutters and ADA ramps at the throat of the driveway.

The Developer shall provide cross access easements for all of the properties adjacent to in accordance with MLDC 10.550. A draft of the cross access easement shall be submitted with the building permit application for review prior to recording and shall include an exhibit map showing the cross access locations and route from the public right-of-way to the property line and shall be described to include the proposed driveway for this Development.

4. MLDC Section 10.668 Analysis

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless: (1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose, and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or (2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and are supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, and pedestrians. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the required dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Also, the City is allowed to consider the benefits to the development from the dedication and improvements when determining "rough proportionality."

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Industry Drive:

The City assesses System Development Charges (SDC's) to help pay for acquisition of right-of-way and construction of additional Arterial & Collector street capacity required as a result of new development. Because a mechanism exists in the form of SDC credit for right-of-way dedication and street improvements in accordance with Medford Municipal Code (MMC) 3.815 and other applicable parts of the Code, to fairly compensate the applicant, the conditions of MLDC, Section 10.668 are satisfied.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. Contact RVSS for sanitary sewer connections.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

The Design Engineer shall provide an investigative report of the off-site drainage on the developments perimeter, a distance not less than 100 feet in all directions. All off-site drainage affecting the development shall be addressed on the drainage plan. A hydrology map depicting the amount of area the development will be draining shall be submitted with hydrology and hydraulic calculations. The opening of each curb inlet shall be sized in accordance with ODOT design standards. These calculations and maps shall be submitted with the public improvement plans for approval by the Engineering Division.

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto

an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stromwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

Detention and Water Quality for the public improvements shall be located in the right-of-way or within a public easement and separated from the private detention and water quality, unless an agreement is reached with the City of Medford for private maintenance.

Capacity of ditch where drainage is proposed is unknown: The Developer shall provide calculations showing the ditch has downstream capacity to convey the 10-year storm, or development will need to provide detention based on the 25-year storm.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Wetlands

The Developer shall contact the Division of State Lands for the approval or clearance of the subject property with regards to wetlands and/or waterways.

6. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the DEQ. Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering

Division of Medford Public Works Department for approval. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

1. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

2. System Development Charges

Buildings in this development are subject to street, sanitary sewer treatment and stormdrain system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued. The sanitary sewer collection SDC fees shall be paid to Rogue Valley Sewer Services prior to building permits being issued.

Prepared by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

American Tire Distributors (ATD)

AC-15-128

A. Streets

1. Street Dedications to the Public:

Dedicate 74-foot right-of-way for Industry Drive.

Dedicate a 10 foot wide PUE adjacent to the right-of-way line in Industry Drive along this Developments respective frontage.

2. Improvements:

Construct public improvements for full street width of Industry Drive.

Provide 1 – Dead-end Barricade.

Install street lights and New base mounted cabinet (BMC)

B. Sanitary Sewer:

The site is situated within the RVSS area.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

Contact the Division of State Lands for the approval or clearance of the subject property with regards to wetlands and/or waterways.

Provide copy of an approved Erosion Control Permit (1200C) from the DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



RECEIVED

NOV 11 2015

Planning Dept.

Memo

To: Aaron Harris, Planning Department
From: Mary Montague, Building Department
CC: American Tire; Joe McSweeney
Date: November 11, 2015
Re: AC-15-128

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit will be required if more than 50 cubic yards is disturbed.
4. A separate demolition permit will be required for demolition of any structures not shown on the plot plan.
5. Proposed buildings may require design by an Oregon licensed architect or engineer pursuant to code section 107.1 of the 2014 Oregon Structural Specialty Code.
6. This occupancy is S-1; Construction Type shall be Type 1-A per Table 503.
7. Provide fire sprinklers per Fire Code 903.2.9.
8. Verify construction does not interfere with Airport radar Overlay per Municipal Code 10.355.

RECEIVED

NOV 10 2015

Planning Dept.

MEDFORD PARKS & RECREATION

HEALTHY LIVES. HAPPY PEOPLE. STRONG COMMUNITY.



CITY OF MEDFORD
Interoffice Memo

TO: Planning Department
FROM: Tim Stevens- Park Maintenance Supervisor
SUBJECT: LANDSCAPE REVIEW OF FILE AC-15-128, American Tire
DATE: November 10, 2015

I have reviewed the applicant's landscape plan and recommend it be accepted as submitted.

This report addresses horticultural concerns only. Applicant shall comply with all aspects of Medford Code 10.780 Interpretation of the Medford Code will be per the Planning Department. Aesthetic considerations will be per the Site Plan and Architectural Review Commission or Planning Commission upon their review.



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CITY OF MEDFORD

EXHIBIT 5

FILE # AC-15-128

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NOV 09 2015

Aaron J. Harris

From: MOREHOUSE Donald <Donald.MOREHOUSE@odot.state.or.us>
Sent: Friday, November 06, 2015 2:47 PM
To: Aaron J. Harris
Cc: GRIFFIN Jeremiah M; HARSHMAN Cathaleen A * Cathy; RANDLEMAN Richard; WANG Wei * Michael
Subject: AC-15-128

Planning Dept.

Aaron,

Thank you for sending agency notice of a consideration of plans to construct a 100,000 square foot storage and distribution facility on a portion of an 89.11 acre lot located at the southern terminus of Industry Drive, 300 feet south of the intersection of Industry Drive and Enterprise Drive, within the I-G (General Industrial) zoning district. We reviewed this and determined that it would not significantly affect state transportation facilities under the State Transportation Planning Rule (OAR 660-012-0060) or State Access Management Rule (OAR 734-051-000). The following is a general comment:

- This proposed project is adjacent to the ODOT OR 62: Corridor Solutions Unit 2 Phase 1 Project. Please contact Richard Randleman (Project Leader) at 541-864-8828 for any project related information.

Don Morehouse
Senior Transportation Planner
ODOT Region 3, District 8 (Rogue Valley Tech Center)
Ph: (541) 774-6399
Fax: (541) 774-6349
Donald.Morehouse@odot.state.or.us



City of Medford
 Planning Department

Vicinity
 Map

File Number:
AC-15-128



Project Name:

American Tire

Map/Taxlot:

Proposed 371W TL2500



09/18/2015

 Subject Area

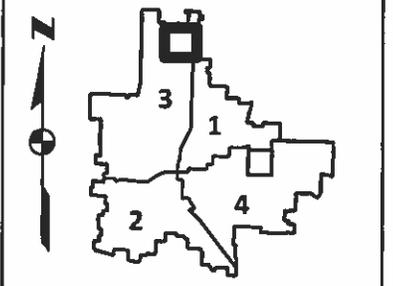
 Medford Zoning

 UGB

 Tax Lots

 Airport Radar

Medford UGB with Wards





STAFF REPORT

for a Type-C quasi-judicial decision: **Site Plan & Architectural Commission Review**

Project Mountain Vista Apartments
Applicant: On Track, Inc.; Agent: Daniel Horton

File no. AC-15-131

To Site Plan and Architectural Commission *for 12/4/2015 hearing*

From Sarah Sousa, Planner IV

Reviewer Kelly Akin, Principal Planner

Date November 25, 2015

BACKGROUND

Proposal

Plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zoning district.

Subject Site Characteristics

Zoning MFR-20
GLUP UH (Urban High Density Residential)
Use Single Family Home

Surrounding Site Characteristics

North

Zoning: County RR-2.5 (Rural Residential – 2.5 acre minimum lot size)
Use: Auto Repair Shop

South

Zoning: SFR-6 (Single Family Residential – 6 dwelling units per gross acre)
Use: Single Family Homes

East

Zoning: MFR-20
Use: Apartments

West

Zoning: MFR-15 (Multiple Family Residential – 30 dwelling units per gross acre)
Use: Single Family Home

Related Projects

CP-98-002 GLUP Map Amendment from UR to UH
ZC-00-010 Zone Change from RR-5 to MFR-20
PA-14-062 Pre-Application for Multi-Family Units
PLA-14-113 Property Line Adjustment

Applicable Criteria

Medford Land Development Code (MLDC) §10.290

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and
Above listed Criterion is not applicable to this application as regulated by ORS 197.307(4).
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

Corporate Names

On Track, Inc. is a non-profit organization that provides addiction recovery and treatment services including special needs housing. On Track, Inc. is registered as a non-profit organization with the Oregon Secretary of State Business Registry and lists Jim Maize as President.

ISSUES AND ANALYSIS

ORS 197.307(4) – Needed Housing

The Applicant's Narrative explains Oregon Revised Statutes (ORS) related to needed housing and its impact upon the subject application. ORS 197.303(1) defines needed housing as "housing types determined to meet the need for housing within an urban growth boundary at particular price ranges and rent levels." ORS 197.307(4) dictates that local governments may only impose clear and objective standards, conditions and procedures regulating the development of needed housing. As this application is for a

needed housing type listed within the Housing Element of the Medford Comprehensive Plan, the Commission may only impose clear and objective standards as listed in Medford Land Development Code (MLDC) Section 10.290(2) and may not impose MLDC Section 10.290(1) as it does not relate to objective standards.

Project Summary

The applicant is proposing to construct six two-story apartment buildings and a community building on the subject site for a total of 43,637 square feet. Buildings A, B, C, D, & E consist of eight units and Building F has six units. The units will be one, two and three bedroom apartments with a total of 46 units. The property abuts an apartment complex to the east also developed and managed by the applicant. The property just to the west is a large, mostly vacant property owned by the applicant as well.

Site Layout (Exhibit B)

Homeview Drive is currently stubbed to the property from the east. The applicant proposes to extend this minor residential street through the subject site and stub it to the western property line. As a result, the project area will be divided on each side of the street with Buildings A-E and the community building on the north side of Homeview Drive and Building F on the south side. It is important to note that although the County Assessor's Office typically assigns new tax lot numbers to properties divided by street right-of-way, this will continue to be one legal parcel unless a land partition is approved.

Although the property fronts upon Stewart Avenue, vehicular access will be from Homeview Drive. Two points of access at the western end of the site would provide the only vehicular ingress and egress for the project, one for the north side of the project and one for the south. These access points are directly aligned from each other across Homeview Drive leading to parking areas. In regards to the northern side of the project, vehicular access will be limited to a 41 space parking area as vehicular access is not shown around the individual buildings. For Building F on the southern side of Homeview Drive, an 11 space parking lot will serve as the only vehicular area as there is no paving around the building.

There is a proposed pattern of walkways that provide functional pedestrian circulation throughout the project. Two concrete walkways extend from the planned sidewalk improvement on Stewart Avenue that travel in front of each building and extend through parking areas to Homeview Drive. A concrete walk also extends from Homeview Drive to the southern end of the project.

Density

The subject site is zoned MFR-20, with an allowable density range of 15 to 20 dwelling units per gross acre. In this case, the range is 45 to 60 dwelling units. The applicant is proposing 46 dwelling units, which is within the allowable range.

Architecture

The applicant's questionnaire describes the style and materials of the proposed buildings (Exhibit I). The proposed materials include cement fiber lap, board and batten, and shingle siding with architectural grade composition roofing. Wall and roof lines are articulated to provide relief of massing. The architectural style is similar to the adjacent apartments to the east also managed by the applicant.

Amenities

The site has been designed with amenities for the future residents. A one-story community building is shown at the eastern side of the project that includes a multi-purpose room, conference room, office, computer room, kitchen, and four covered outdoor patios. Near the community building is a designated area with a play structure, splash pad water feature, grassy recreation space, and a metal trellis with benches. There is also a small covered picnic area at the western end of the site and courtyards between the proposed buildings.

Parking

Medford Land Development Code Section 10.743-1 requires a minimum of 1.5 parking spaces per dwelling unit for multiple family residential units. The minimum number of vehicular parking spaces required for this project is 69. There is no maximum number for this category. The applicant is proposing 73 total spaces, which meets the minimum required.

Three parking areas are proposed for the project. The first 21 spaces are accessed from the neighboring apartment complex from the east. This will provide a convenient location for the buildings at the eastern side of the project. The bulk of the parking (41 spaces) is within the parking lot to the western end of the site. The final 11 spaces are shown within a parking lot to the southern end of the project, sufficient for the 6 units in the adjacent Building F.

Bicycle Parking

The bicycle parking standards of Medford Land Development Code Section 10.747-748 require one space per unit for multiple-family residential projects with four or more units. This project requires a total of 46 bicycle parking spaces. A total of 46 spaces are accommodated on site under covered bike shelters in front of each building.

Landscape Plan

A variety of landscaping is proposed for the project along the frontage of each street, around the buildings, within the parking areas, as well as a Type A Bufferyard to the south end of the site (Exhibit E).

Medford Land Development Code Section 10.780(G)(5) limits the amount of high water use landscape elements in multiple family residential projects to 40 percent. The landscape plan shows areas of lawn throughout the project but lists the total high water percentage at 26 percent.

The Medford Parks & Recreation Department Memo lists one condition for a revised landscape plan (Exhibit N). The memo points to areas of the south lawn that reduces to less than six feet in width. Medford Land Development Code Section 10.780(G)(6)(a) requires the turf areas have a minimum average width of eight feet and shall be no less than six feet in width at any point. A condition has been included requiring the correction to the landscape plan prior to issuance of the first building permit for vertical construction. All of the landscape standards related to frontage landscaping, parking area planters, and the bufferyard requirements have been met.

Concealments

Multiple trash enclosures are proposed throughout the project site. A detail of the enclosure has been provided demonstrating compliance with Code requirements as well as the standards of the trash collection provider (Exhibit B). Each enclosure would be constructed of a split faced concrete block and wood composite gate. Mechanical units will be screened within landscaped areas.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings (Exhibit I) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Direct staff to prepare a Final Order of Approval per the staff report dated November 25, 2015, including Exhibits A-S.

EXHIBITS

- A Conditions of Approval dated November 25, 2015
- B Site Plan received October 22, 2015
- C Conceptual Grading & Drainage Plan received September 17, 2015
- D Conceptual Utility Plan received October 22, 2015
- E Landscape Plan received September 17, 2015

- F Building Elevations received September 17, 2015
- G Floor Plans received September 17, 2015
- H Lighting Specifications received October 22, 2015
- I Applicant's Questionnaire received September 17, 2015
- J Applicant's Code Compliance Checklist received September 17, 2015
- K Public Works Department Report received November 11, 2015
- L Medford Fire Department Report received November 11, 2015
- M Building Department Memo received November 11, 2015
- N Medford Parks Department Memo received November 10, 2015
- O Medford Water Commission Memo received November 11, 2015
- P Rogue Valley Sewer Services letter received November 3, 2015
- Q Jackson County Roads letter received November 9, 2015
- R Rogue Valley Transportation District email received November 18, 2015
- S Oregon Department of Transportation email received November 6, 2015
- V Vicinity map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA:

DECEMBER 4, 2015

EXHIBIT A

Mountain Vista Apartments
AC-15-131
Conditions of Approval
November 25, 2015

CODE REQUIREMENTS

Prior to issuance of the first building permit, the applicant shall:

1. Submit a joint access/parking agreement with the adjacent property to the east (372W35AD TL 1200) acknowledging access to the property for use of the parking spaces 1-21.
2. Submit a revised Landscape Plan which corrects the width of the lawn area on the south end of the project consistent with Medford Land Development Code Section 10.780(G)(6)(a).
3. Submit an Irrigation Plan, per Medford Land Development Code Section 10.780(F).
4. Comply with the Fire Department Report received November 11, 2015 (Exhibit L).
5. Comply with the memo from the Medford Water Commission Memo received November 11, 2015 (Exhibit O).
6. Comply with the Public Works Department Memo received November 11, 2015 (Exhibit K).
7. Comply with the Rogue Valley Sewer Services letter received November 3, 2015 (Exhibit P).

CITY OF MEDFORD
EXHIBIT # A
File # AC-15-131

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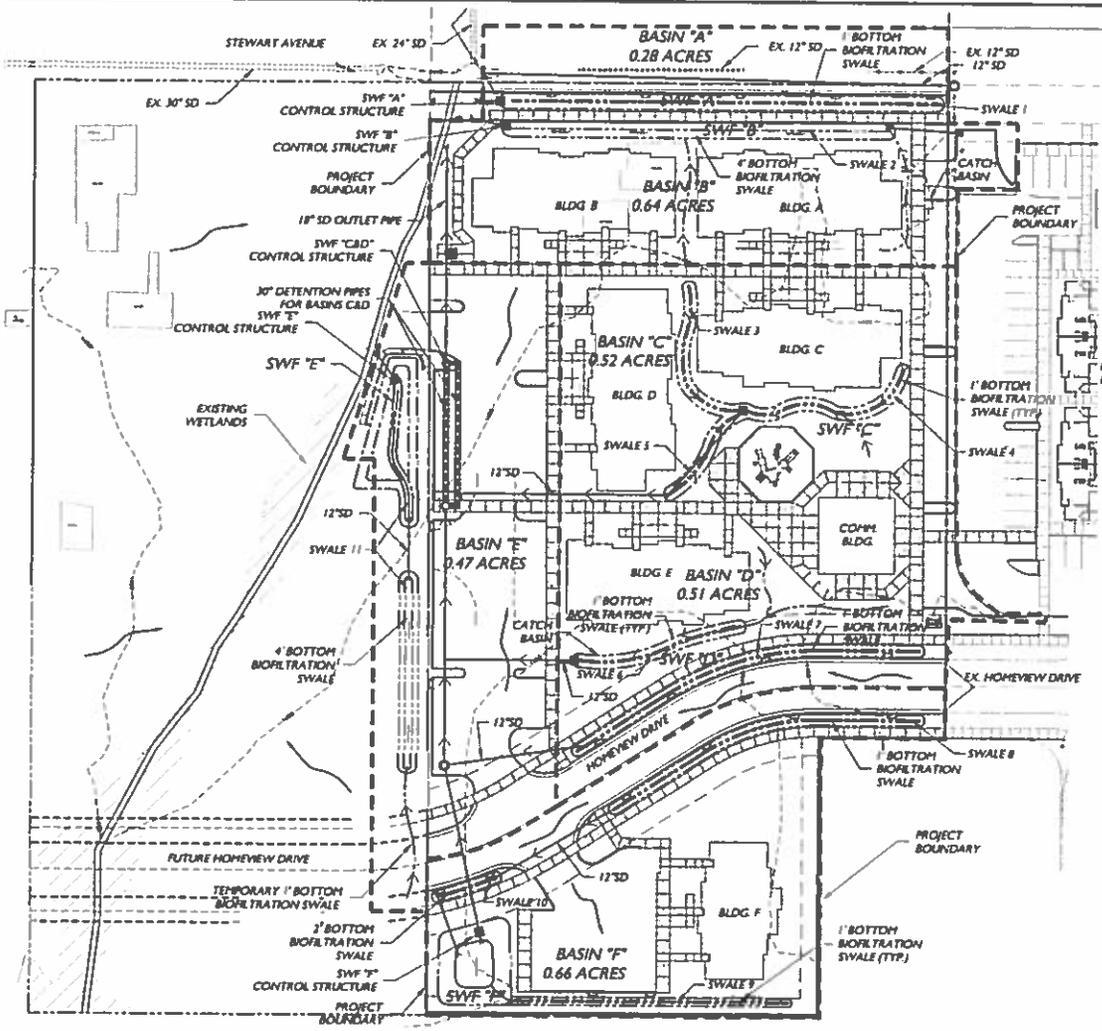
SEP 17 2015

PLANNING DEPT.

NORTH
SCALE: 1"=30'

SWALE DATA TABULATION

SWALE	LENGTH	SLOPE %	Basin	TREATMENT AREA	CURVE NUMBER	2 YEAR FLOW (CFS)	50 YEAR FLOW (CFS)
1	226.7'	0.35%	A	0.28 AC	84	0.108	0.034
2	196.7'	0.35%	B	0.64 AC	80	0.188	0.094
3	87.7'	0.35%	C	0.16 AC	88	0.040	0.020
4	386.7'	0.35%	C	0.28 AC	88	0.083	0.013
5	379.7'	0.35%	C	0.10 AC	89	0.029	0.013
6	88.7'	0.35%	D	0.35 AC	90	0.102	0.052
7	183.7'	0.35%	D	0.16 AC	90	0.047	0.024
8	173.7'	0.35%	F	0.13 AC	88	0.035	0.018
9	148.7'	0.35%	F	0.46 AC	88	0.133	0.087
10	33.7'	0.35%	F	0.07 AC	88	0.018	0.010
11	176.7'	1.0%	E	0.17 AC	86	0.203	0.102



Page 55

CITY OF MEDFORD
EXHIBIT # C
File # AC-15-131



P.O. BOX 1751 • MEDFORD, OREGON 97501
TEL: (503) 770-6868 • FAX: (503) 770-3120

DRAWN BY: BSH	DATE: 08/13
CHECKED BY: AMB	DATE: 08/13
APPROVED BY: RVS	DATE:
APPROVED:	DATE:
APPROVED:	DATE:
APPROVED:	DATE:

NO. 1 REVISION	DATE	BY

NOT FOR CONSTRUCTION

CONSTRUCTION ENGINEERING CONSULTANTS
MOUNTAIN VISTA PROJECT
CONCEPTUAL GRADING AND DRAINAGE PLAN
REV: 011 ENGINEER DATE
C-1

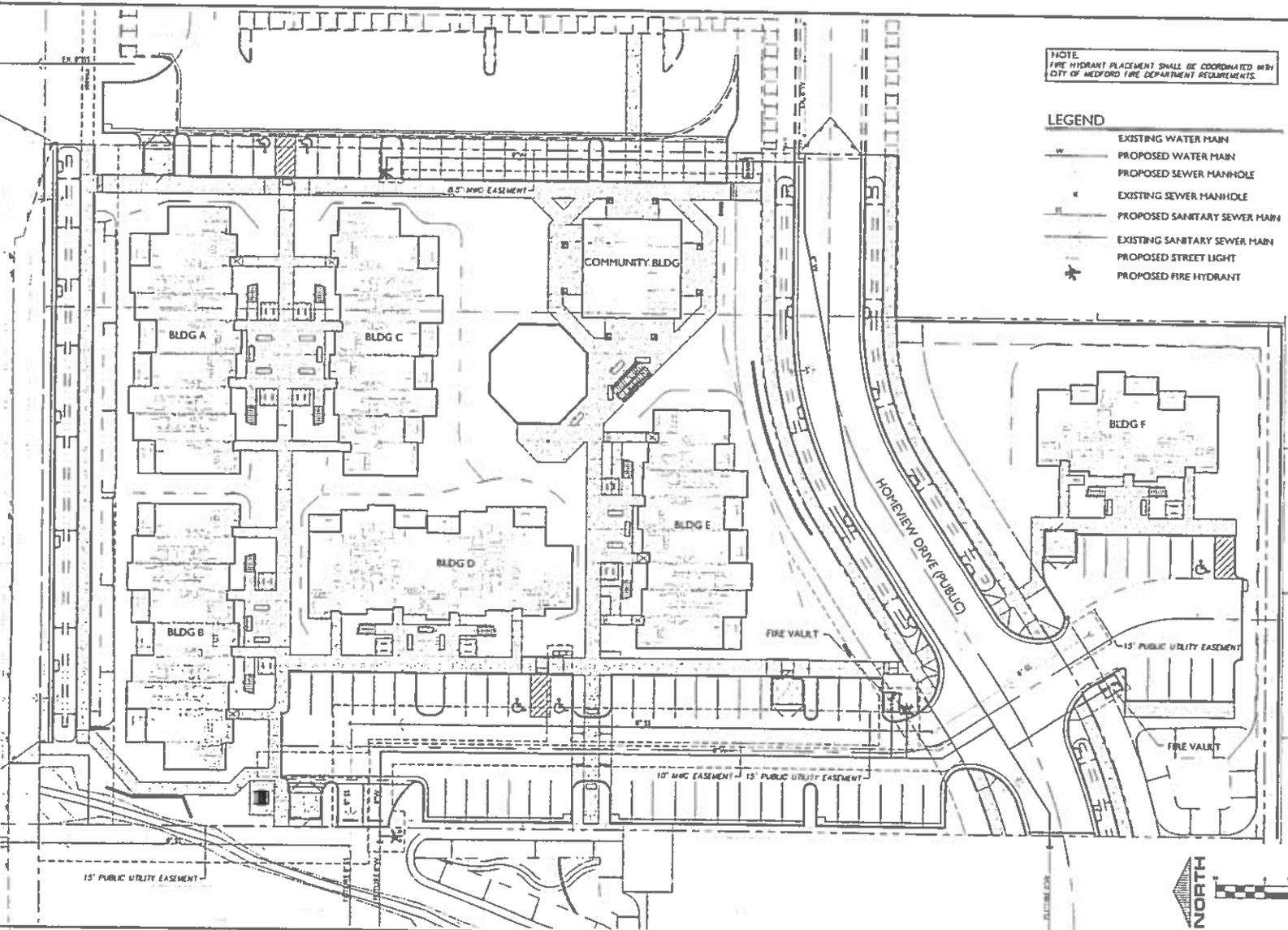
10

STEWART AVENUE (PUBLIC)

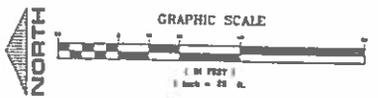
CITY OF MEDFORD
EXHIBIT # D
File # AC-15-131

NOTE:
FIRE HYDRANT PLACEMENT SHALL BE COORDINATED WITH
CITY OF MEDFORD FIRE DEPARTMENT REQUIREMENTS.

- LEGEND**
- EXISTING WATER MAIN
 - - - PROPOSED WATER MAIN
 - PROPOSED SEWER MANHOLE
 - EXISTING SEWER MANHOLE
 - - - PROPOSED SANITARY SEWER MAIN
 - EXISTING SANITARY SEWER MAIN
 - PROPOSED STREET LIGHT
 - * PROPOSED FIRE HYDRANT



RECEIVED
OCT 22 2015
PLANNING DEPT.



DRAWN BY: BSH	DATE: 10/15
CHECKED BY: AMB/WHK	DATE: 10/15
APPROVED PYS	DATE:
APPROVED:	DATE:
APPROVED:	DATE:
APPROVED:	DATE:

VERIFY SCALES!!
ONE OF ONE ONLY IN ORIGINAL SET
IF NOT ONE, COPY OF THIS SHEET
ADJUST SCALES ACCORDINGLY

0 = 1"

NO.	REVISION	DATE	BY

NOT FOR CONSTRUCTION

CITY OF MEDFORD - ENGINEERING DEPARTMENT

MOUNTAIN VISTA

CONCEPTUAL UTILITY PLAN

REV. CITY ENGINEER: DATE:

PROJECT NO. P-15020
DRAWING NO. 2 OF 2

NAME: CONCEPTUAL UTILITY ENGINEERING PLAN SHEET TITLE: 10-11 FILED DATE: 10/15/15

PLANT LIST

Table with columns: Quantity, Common Name, Botanical Name, Size. Lists various plants like Cedar, Cherry, Dogwood, etc.

PLANTING

- 1. Plant material to be provided as established with tolerance... 2. No planting to proceed until irrigation system is fully functioning... 3. All plant material to be installed within the tolerance of the root ball...

Treatment: Double Hydroponic Heavy Fertilizer, 1000 ppm, 1000000 ppm

Turf Areas: Install turf with Perennial Plugplant blend



LANDSCAPE AREA CALCULATIONS

Total square footage of landscaped areas is 12721. Total square footage of planting area is 1022. Percentage of area landscaped with high water use plants is 28%.

GENERAL CONSTRUCTION NOTES

- 1. SITE CONDITIONS NOTES: A. The Landscape Architect shall be notified by the Contractor... B. The Contractor shall be notified by the Landscape Architect... C. The Contractor shall be notified by the Landscape Architect... D. The Contractor shall be notified by the Landscape Architect...

GRADING

- 1. All grading shall be to a minimum depth of 12" or deeper as determined by... 2. All grading shall be to a minimum depth of 12" or deeper as determined by... 3. All grading shall be to a minimum depth of 12" or deeper as determined by...

IRRIGATION DETAILS

- 1. An automatic irrigation system to be provided for all plant materials... 2. The system shall be designed to provide a minimum of 1.5" of water per week... 3. The system shall be designed to provide a minimum of 1.5" of water per week...

PLANNING DEPT.

SEP 17 2015

Landscape Plan

Scale 1" = 30'



Madara Design Inc. Landscape Architect, Design & Construction. 541-584-7055. madaradesign@gmail.com

MOUNTAIN VISTA APARTMENTS HOMEVIEW DRIVE Medford, Or

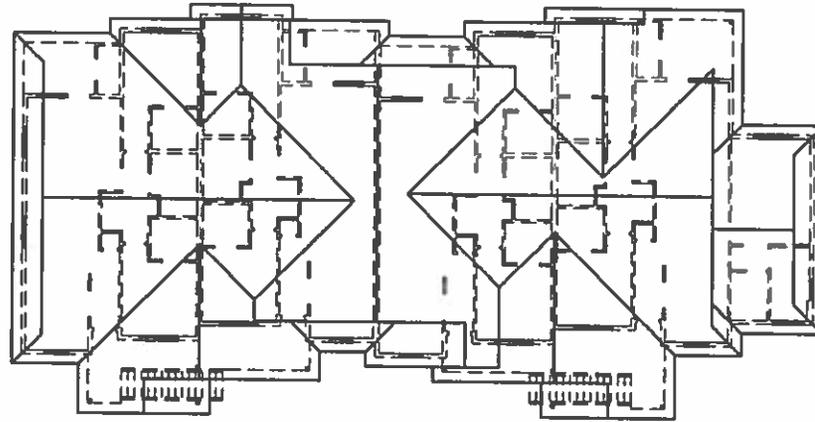
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LANDSCAPE PLAN

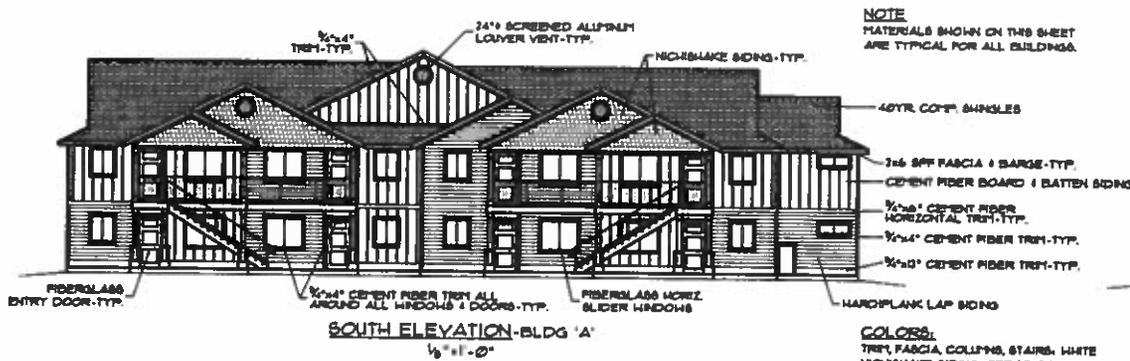
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NOTE: IF THIS SHEET IS LESS THAN 24" x 36" IT HAS BEEN REDUCED AND IS NOT TO SCALE.

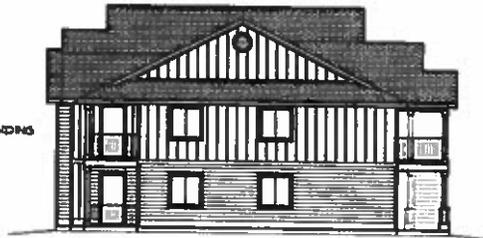
CITY OF MEDFORD EXHIBIT # E File # AC-15-131



ROOF PLAN-BLDG 'A'
1/8"=1'-0" - B PLEX

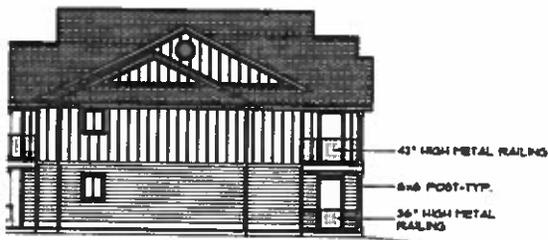


SOUTH ELEVATION-BLDG 'A'
1/8"=1'-0"

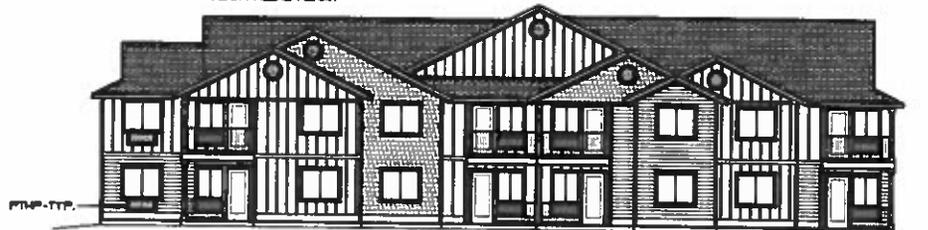


WEST ELEVATION-BLDG 'A'
1/8"=1'-0"

COLORES:
TRIM, FASCIA, COLUMNS, STAIRS, WHITE
NICKSHAKE SIDING, CEDAR COLOR
HARDPLANK LAP SIDING, EARTH TONE COLOR
BOARD & BATTEN SIDING, COMPLEMENTARY EARTH TONE COLOR
METAL RAILINGS, BLACK
EXTERIOR DOORS, FOREST GREEN
ROOF, MEDIUM GRAY



EAST ELEVATION-BLDG 'A'
1/8"=1'-0"



NORTH ELEVATION-BLDG 'A'
1/8"=1'-0"

BUILDING 'A'
MOUNTAIN VISTA
HOMEVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
CONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

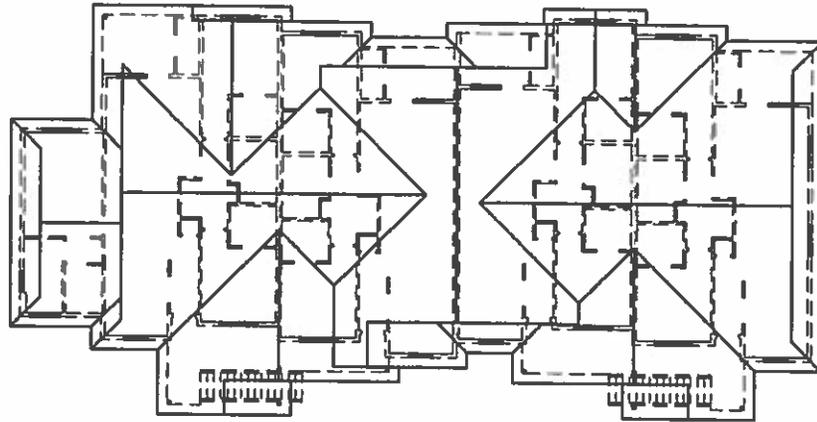
PROJECT NUMBER: 15-10

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EAGLE POINT, OR 97524
PHONE: 541-852-1234

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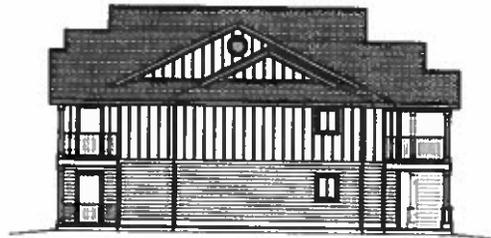
DATE
15 SEPTEMBER 2015
DRAWING NUMBER
3 OF 15



ROOF PLAN-BLDG 'B'
1/8"=1'-0" - 3 PLEX

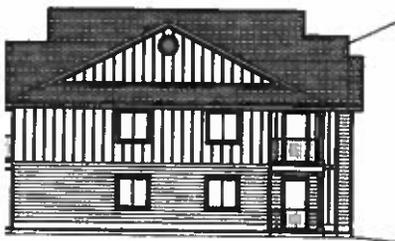


SOUTH ELEVATION-BLDG 'B'
1/8"=1'-0"



WEST ELEVATION-BLDG 'B'
1/8"=1'-0"

SEE SHEET 3 FOR MATERIALS AND COLORS



EAST ELEVATION-BLDG 'B'
1/8"=1'-0"



NORTH ELEVATION-BLDG 'B'
1/8"=1'-0"

BUILDING 'B'
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

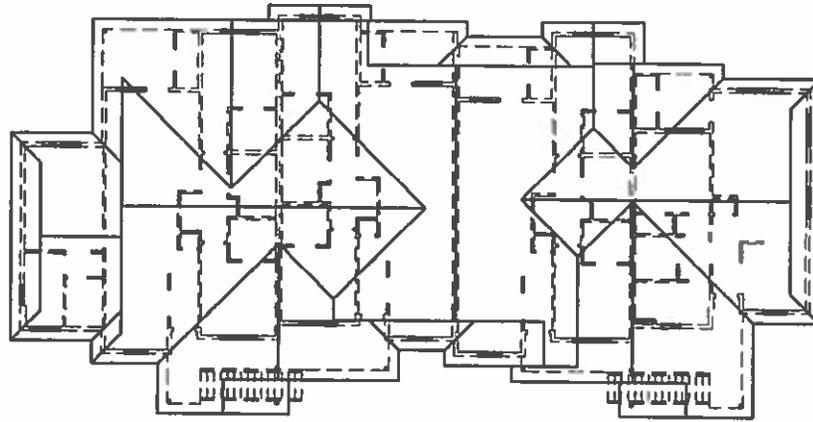
A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
321 W. MAIN ST.
MEDFORD, OR

PROJECT NUMBER
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5 SEPTEMBER 2008
5 OF 15

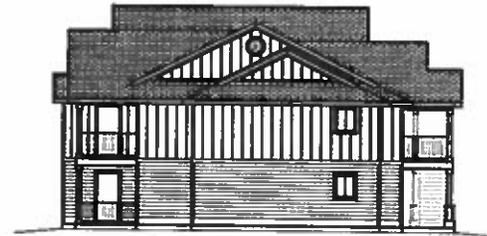
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EXHIBIT # F
File # AC-15-131
2 of 7



ROOF PLAN-BLDG 'C'
1/8"=1'-0" - 8 PLEX

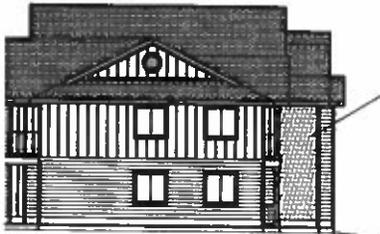


NORTH ELEVATION-BLDG 'C'
1/8"=1'-0"



EAST ELEVATION-BLDG 'C'
1/8"=1'-0"

SEE SHEET 3 FOR MATERIALS AND COLORS



WEST ELEVATION-BLDG 'C'
1/8"=1'-0"



SOUTH ELEVATION-BLDG 'C'
1/8"=1'-0"

BUILDING 'C'
MOUNTAIN VISTA
HOMEVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

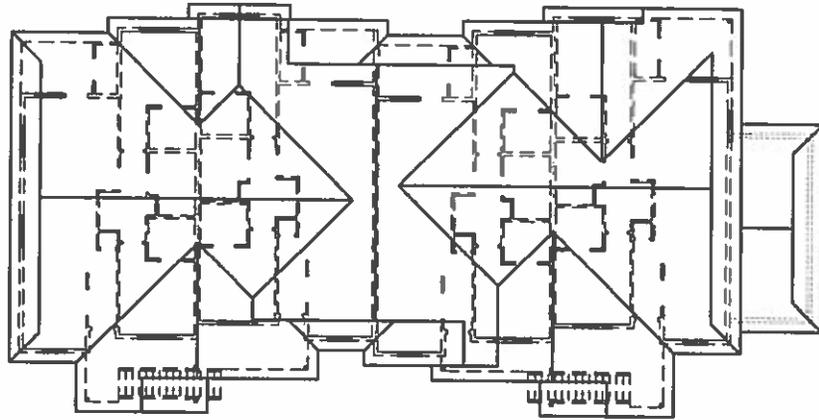
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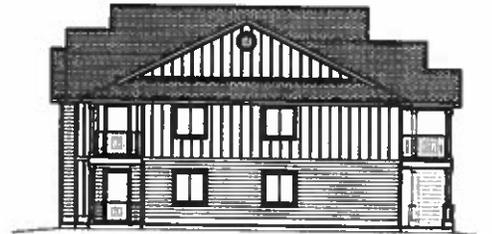
DWG
10 SEPTEMBER 2005
1 OF 15



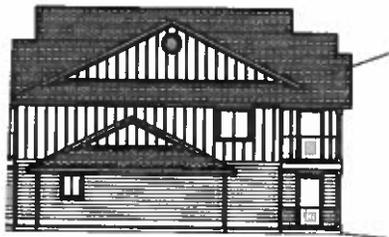
ROOF PLAN-BLDG 'D'
1/8"=1'-0" - 8 PLEX



WEST ELEVATION-BLDG 'D'
1/8"=1'-0"



NORTH ELEVATION-BLDG 'D'
1/8"=1'-0"



SOUTH ELEVATION-BLDG 'D'
1/8"=1'-0"



EAST ELEVATION-BLDG 'D'
1/8"=1'-0"

SEE SHEET 3 FOR MATERIALS AND COLORS

BUILDING 'D'
MOUNTAIN VISTA
HOTVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS

LIMITED PARTNERSHIP

221 W. MAIN ST.
MEDFORD, OR

PROJECT OWNER

SEP 17 2015

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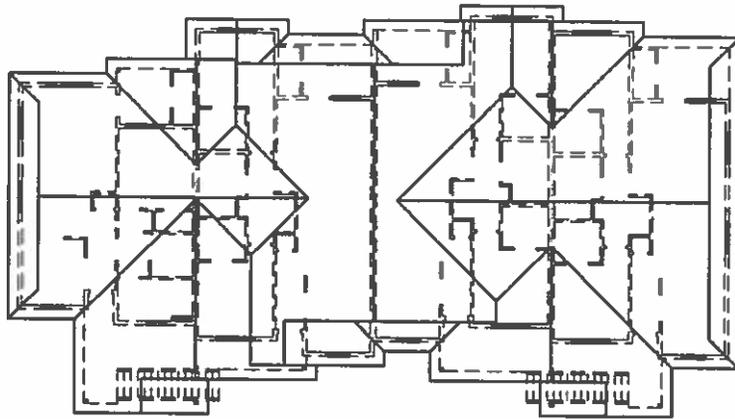
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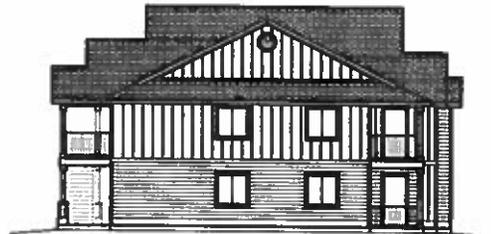
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EXHIBIT # F
File # AC-15-131
4 of 7



ROOF PLAN-BLDG 'E'
1/2"=1'-0" - 8 PLEX

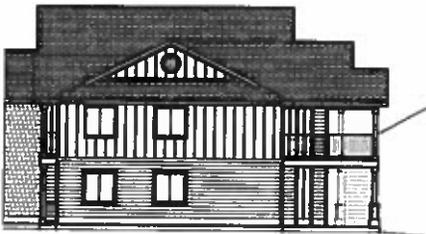


NORTH ELEVATION-BLDG 'E'
1/2"=1'-0"



WEST ELEVATION-BLDG 'E'
1/2"=1'-0"

SEE SHEET 3 FOR MATERIALS AND COLORS



EAST ELEVATION-BLDG 'E'
1/2"=1'-0"



SOUTH ELEVATION-BLDG 'E'
1/2"=1'-0"

BUILDINGS 'E'
MOUNTAIN VISTA
HOMEVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

PLANNING DEPT.

PROJECT NUMBER: 14-20

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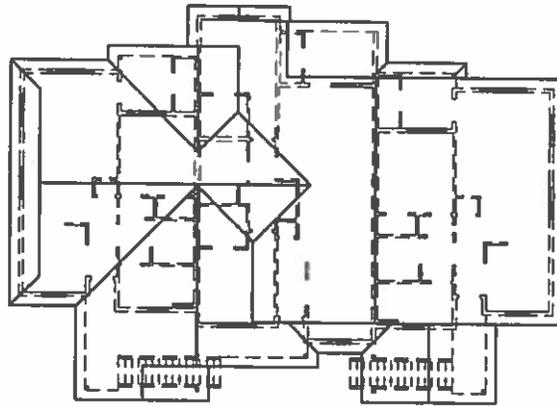
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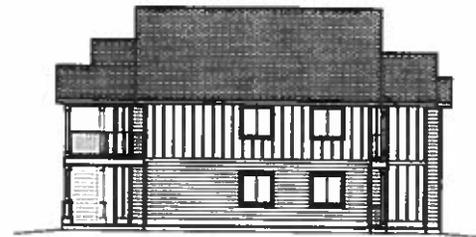
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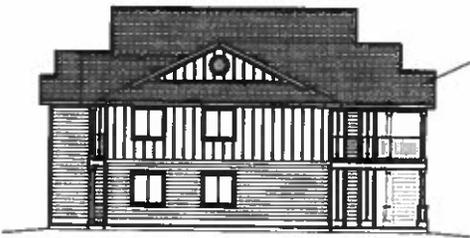
ROOF PLAN-BLDG F'
1/8"=1'-0" - 6 FLEX



WEST ELEVATION-BLDG F'
1/8"=1'-0"



SOUTH ELEVATION-BLDG F'
1/8"=1'-0"



NORTH ELEVATION-BLDG F'
1/8"=1'-0"



EAST ELEVATION-BLDG F'
1/8"=1'-0"

SEE SHEET 3 FOR MATERIALS AND COLORS

BUILDING F'
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
321 W. MAIN ST.
MEDFORD, OR

PROJECT NUMBER: 14-10

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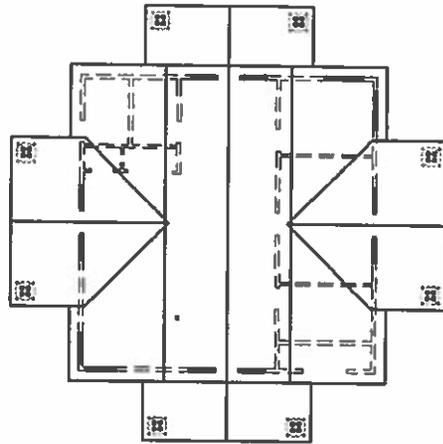
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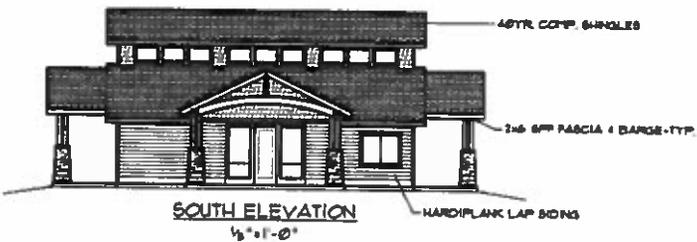
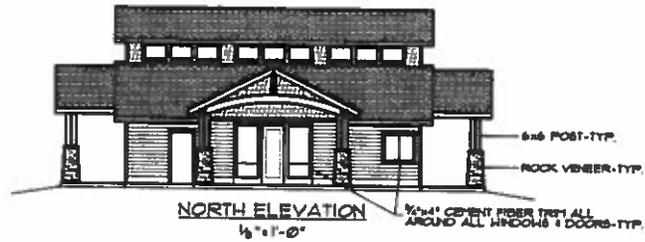
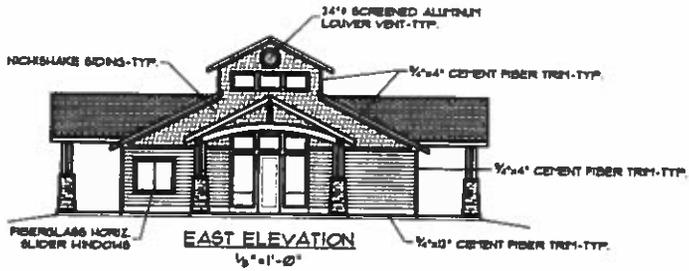
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File # AC-15-131
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ROOF PLAN
1/2" = 1'-0"



COMMUNITY BUILDING
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

PROJECT NUMBER 1100

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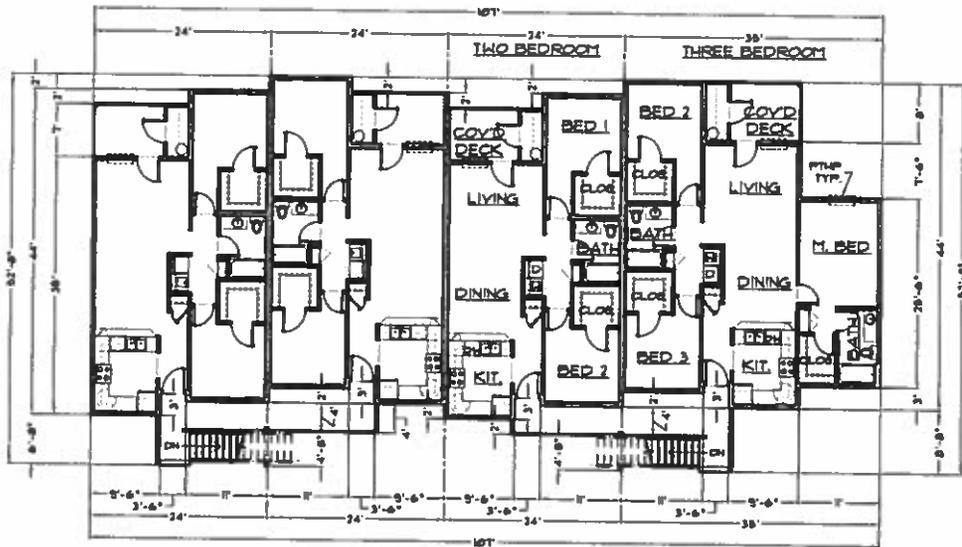
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2006 CITY OF MEDFORD
221 W. MAIN ST., P.O. BOX 100
MEDFORD, OR 97504
PHONE: 541-850-1047

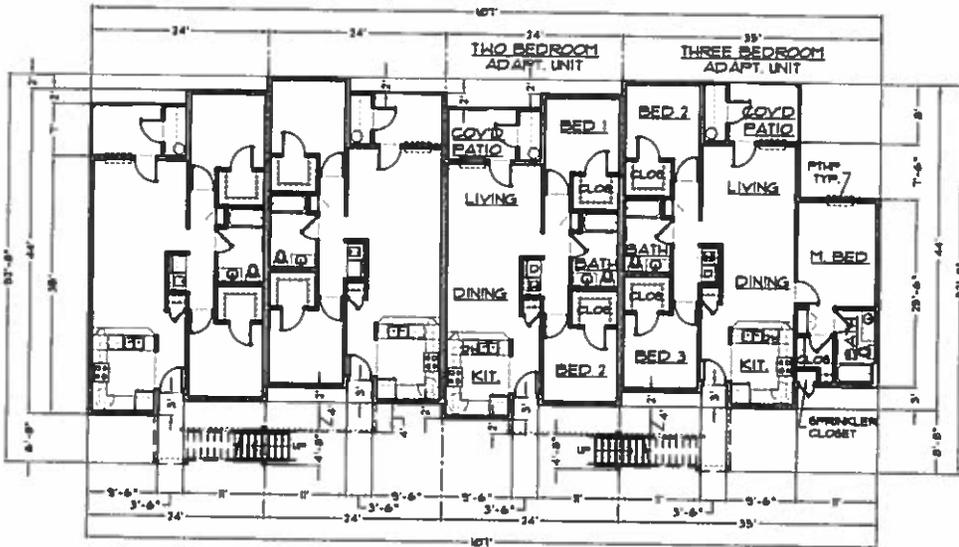
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DRAWING NUMBER: 15 OF 15

CITY OF MEDFORD
EXHIBIT # F
File # AC-15-131
7 of 7



UPPER FLOOR PLAN-BLDG 'A'
1/2"=1'-0" - 8 FLEX



LOWER FLOOR PLAN-BLDG 'A'
1/2"=1'-0" - 8 FLEX

BUILDING 'A'
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

PROJECT NUMBER: 14-18

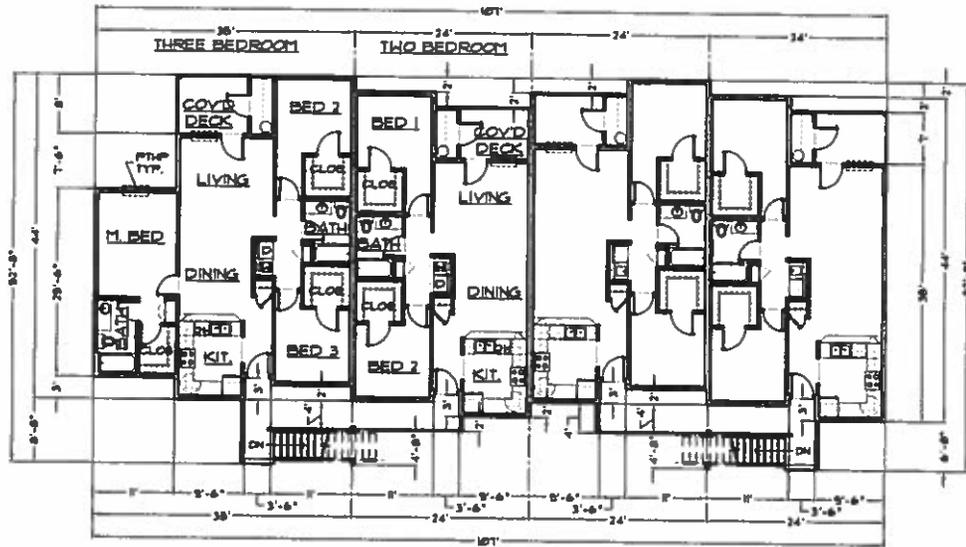
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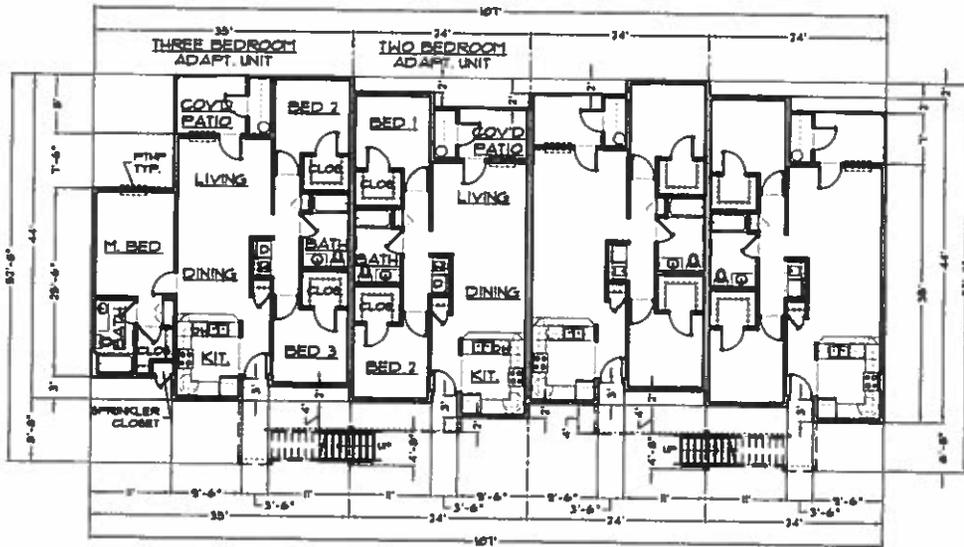
DATE: 09 SEPTEMBER 2015
DRAWING NUMBER: 2 OF 15

CITY OF MEDFORD
EXHIBIT # G
File # AC-15-131
1 of 7

12



UPPER FLOOR PLAN-BLDG 'B'
1/2"=1'-0" - 8 FLEX



LOWER FLOOR PLAN-BLDG 'B'
1/2"=1'-0" - 8 FLEX

BUILDING 'B'
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

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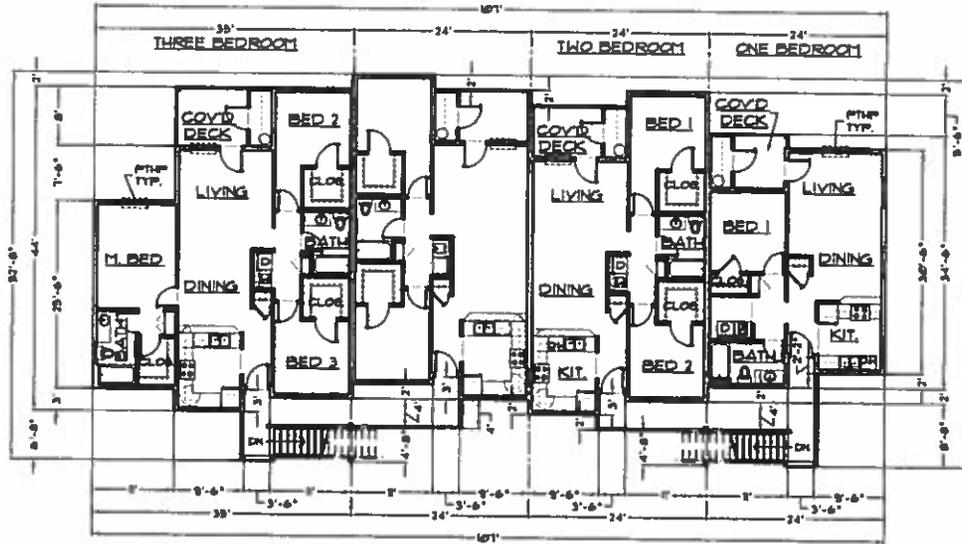
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SALE POINT, OR 97141
PHONE: 503-860-1211

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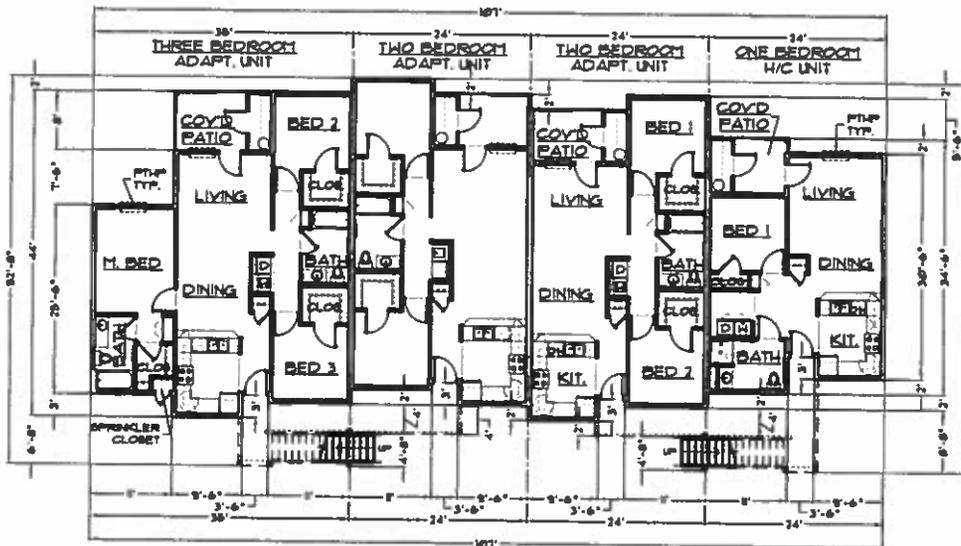
DATE: 08 SEP 15
DRAWING NUMBER: 4 OF 15

CITY OF MEDFORD
EXHIBIT # G
File # AC-15-131
2 of 7

14



UPPER FLOOR PLAN-BLDG 'C'
1/2"=1'-0" - 8 FLEX



LOWER FLOOR PLAN-BLDG 'C'
1/2"=1'-0" - 8 FLEX

BUILDING 'C'
MOUNTAIN VISTA
HOMEVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
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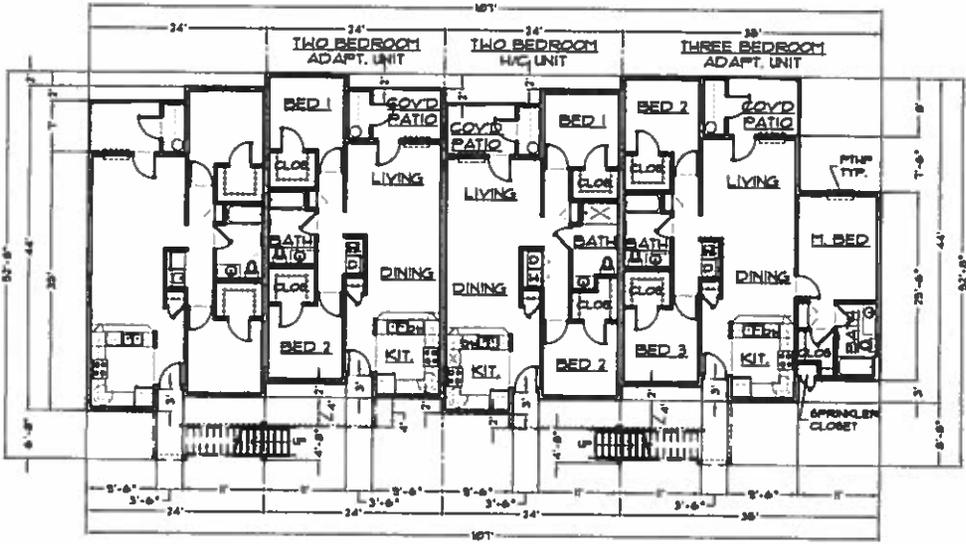
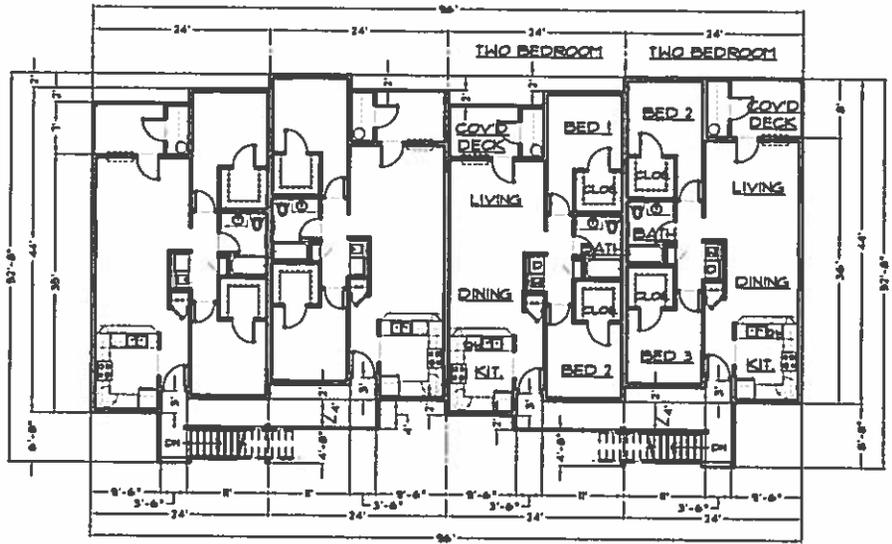
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ARCHITECTURE, INC. 508
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SALMON POINT, OR 97534
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6 SEPTEMBER 2015
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6 OF 15

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EXHIBIT # G
File # AC-15-131
3 of 7



BUILDING 'D'
MOUNTAIN VISTA
HOMEVIEW DR
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

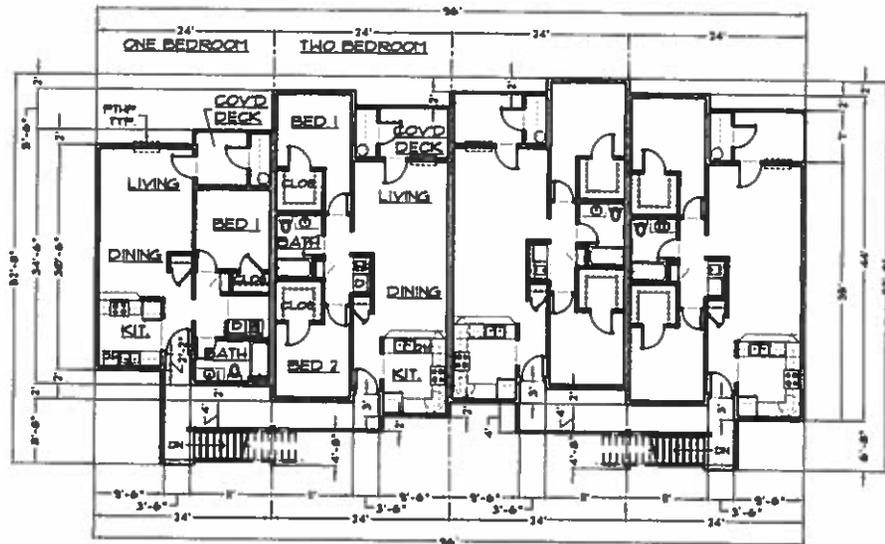
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SEP 17 2015
PLANNING DEPT

BLANCHARD ARCHITECTURE, INC.
609 W. MAIN ST., P.O. BOX 642
SASLE POINT, OR 97534
PHONE: 541-923-1044

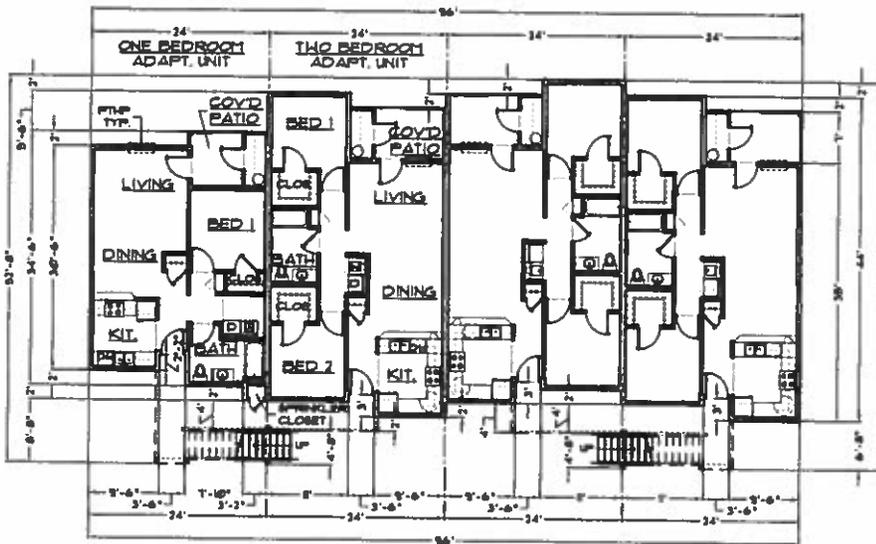
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15 SEPTEMBER 2015
8 OF 15

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EXHIBIT # G
File # AC-15-131
4 of 7



UPPER FLOOR PLAN-BLDG 'E'
1/2"=1'-0" - 8 PLEX



LOWER FLOOR PLAN-BLDG 'E'
1/2"=1'-0" - 8 PLEX

BUILDING 'E'
MOUNTAIN VISTA
HOMEVIEW DR.
MEDFORD, OR

A MULTIPLE FAMILY
HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
APARTMENTS
LIMITED PARTNERSHIP
221 W. MAIN ST.
MEDFORD, OR

PROJECT NUMBER: 14-18
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SEP 17 2014
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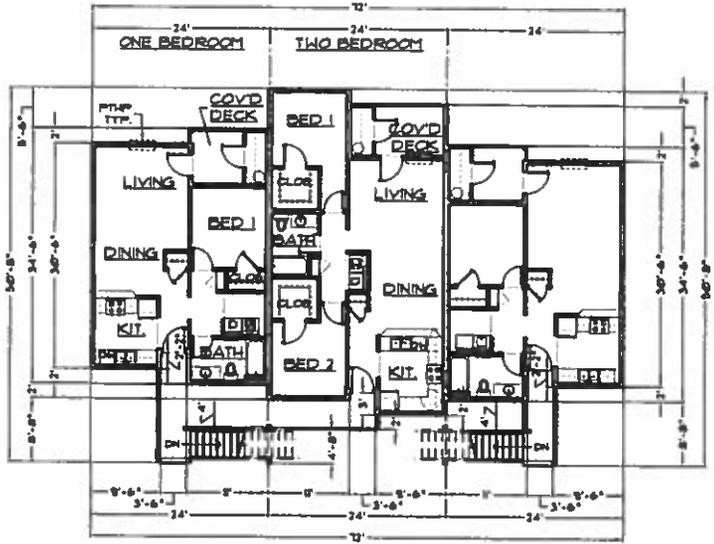
100 W. MAIN ST., MEDFORD, OR 97504
SCALE: 1/8"=1'-0"
DATE: 04-14-2014

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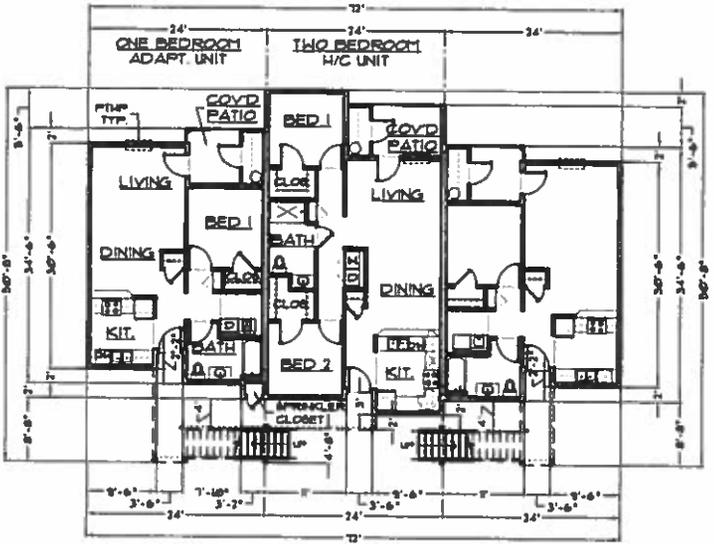
SEP 15 2014
10 OF 15

CITY OF MEDFORD
EXHIBIT # G
File # AC-15-131
5 of 7

70



UPPER FLOOR PLAN-BLDG F
1/8"=1'-0" - 6 FLEX



LOWER FLOOR PLAN-BLDG F
1/8"=1'-0" - 6 FLEX

BUILDING F
MOUNTAIN VISTA
 HOMEVIEW DR.
 MEDFORD, OR

A MULTIPLE FAMILY
 HOUSING DEVELOPMENT FOR
ONTRACK MOUNTAIN VISTA
 APARTMENTS
 LIMITED PARTNERSHIP
 221 W. MAIN ST.
 MEDFORD, OR

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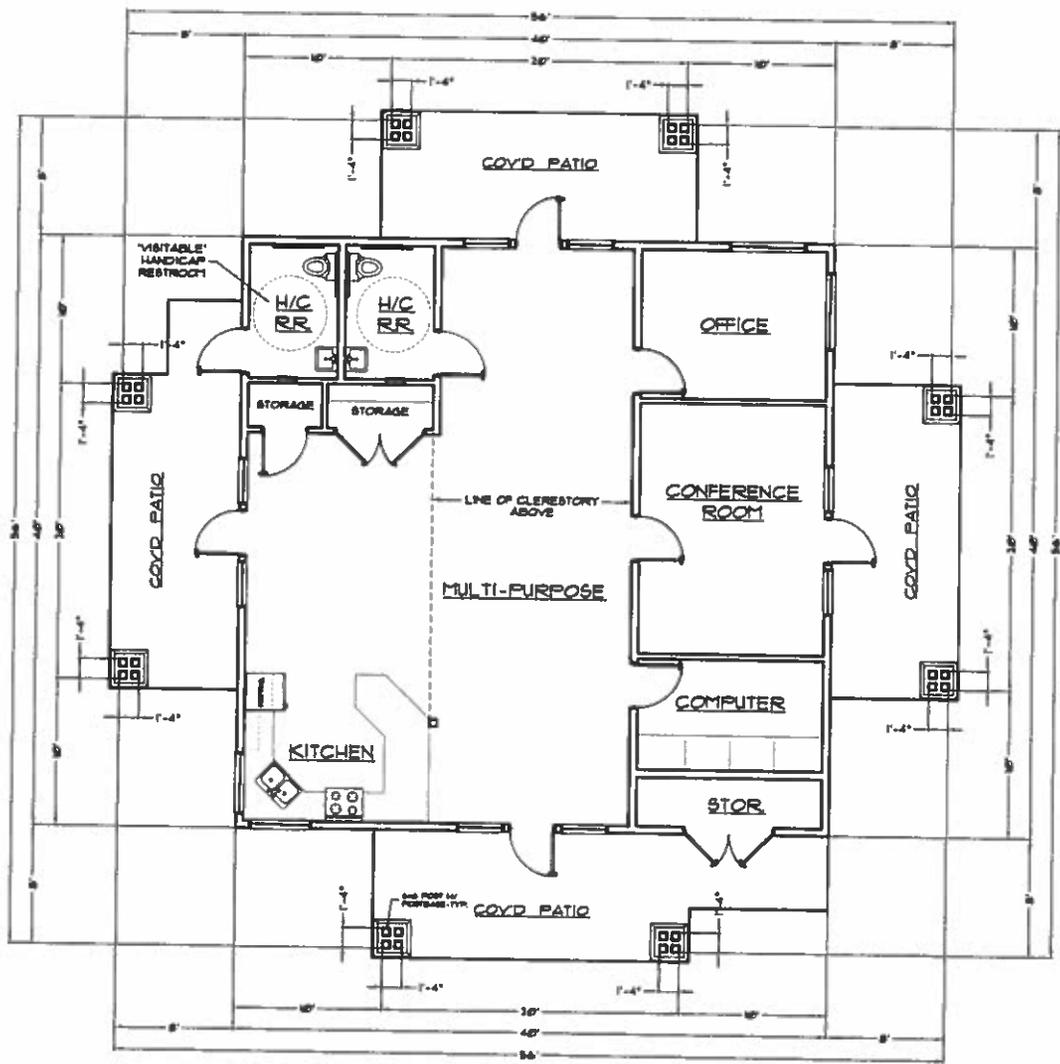
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 SALEM, OR 97314
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22



FLOOR PLAN-COMMUNITY BUILDING
 1/2"=1'-0" - 1600

CITY OF MEDFORD
 EXHIBIT # G
 File # AC-15-131
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COMMUNITY BUILDING
 MOUNTAIN VISTA
 HOMEVIEW DR.
 MEDFORD, OR

A MULTIPLE FAMILY
 HOUSING DEVELOPMENT FOR
 ONTRACK MOUNTAIN VISTA
 APARTMENTS
 LIMITED PARTNERSHIP
 221 W. MAIN ST.
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PROJECT NUMBER 15-11-10
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 EAGLE POINT, OR 97124
 PHONE: 541-832-1244

The Design, Specifications, and other Documents prepared by the Architect for this project are to be used only in connection with this project. The Architect and its Associates do not assume any liability and other reserved rights for the Documents herein.

DATE
 15 SEPTEMBER 2015
 DRAWING NUMBER
 14 OF 15

SOLID STATE AREA LIGHTING

DSAR SERIES-LED

SPECIFICATIONS

HOUSING

Top Driver Compartment is heavy cast, low copper aluminum. Internal driver bracket assembly uses a .125" thick aluminum mounting plate. Optical housing is spun from .080 thick aluminum, hemmed at the edge, and internally finished in high-gloss white paint for maximum reflectance.

POWER TOWER™ LED MODULE

Module consists of a radial array of 6 LED Capsules aligned at 60° increments. LED Capsules are mounted to a sealed barrier plate separating the LED module from the LED Driver compartment. Each LED Capsule contains a vented circuit board with 6 or 8 LED's mounted to it. LED's are Luxeon Rebel High Output, Neutral White nominal 4100K CCT for the entire module (Cool White nominal 6500K are available). Circuit boards are mechanically attached to the aluminum heat sink back of each capsule. A clear acrylic front lens and bottom end cap protect the LED's within each capsule. The module is covered by a clear polycarbonate prismatic refractor for IES Distribution Types II, III, IV, and V.

LED DRIVER(s)

Drivers are UL and cUL recognized mounted on a single plate and factory wired with quick-disconnect. Constant current drivers are electronic and have a power factor of >.90 and a minimum operating temperature of -40°F. Drivers accept an input of 120-277V, 50/60Hz. (Consult factory for dimming applications.)

FINISH

Electrostatically applied TGIC Polyester Powder Coat on substrate prepared with 20 PSI power wash at 140°F. Four step iron phosphate pretreatment for protection and paint adhesion. 400°F bake for maximum hardness and durability. Texture finish is standard.

PROJECT NAME: _____

FIXTURE TYPE: _____

RECEIVED

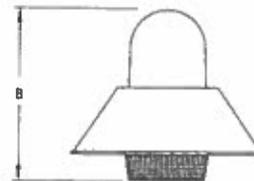
OCT 22 2015

PLANNING DEPT.



DSAR

PATENT PENDING



FIXTURE	A	B
DSAR1	20" 508mm	19" 483mm
DSAR25	25" 635mm	22" 559mm



Light Pollution Control Classification System
LPC Full cutoff 0% Uplight Luminaire



2011098

U.S. Architectural Lighting

600 West Avenue O, Palmdale, CA 93551
Phone: (661) 233-2000 Fax: (661) 233-2001
www.usalighting.com

CITY OF MEDFORD

EXHIBIT # H

File # AC-15-131

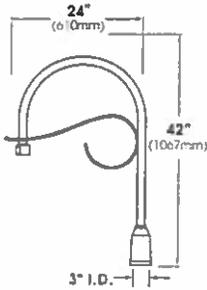
1 of 2

DSAR SERIES - LED

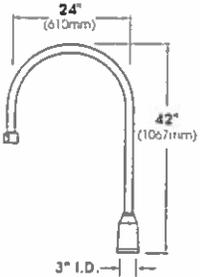
S P E C I F I C A T I O N S

MOUNTING STYLES

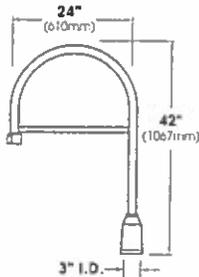
XPD



XPK

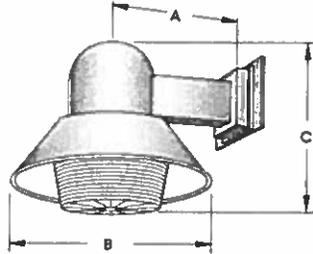


XPS



ARMS ARE HEAVY WALL EXTRUDED ALUMINUM CONSTRUCTION. FOR ADDITIONAL ARM OPTIONS SEE ARM SECTION.

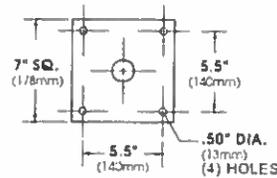
WALL MOUNT



FIXTURE	A	B	C
DSAR1	14" 356mm	20" 508mm	19" 483mm
DSAR25	18" 457mm	25" 635mm	22" 559mm

EXTRUDED ALUMINUM ARM AND CAST ALUMINUM WALL BRACKET ASSEMBLY PROVIDED WITH BUILT IN GASKETED WIRE ACCESS FOR FIXTURE/SUPPLY WIRE CONNECTION.

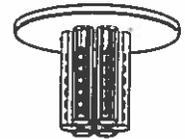
WALL PLATE



POWER TOWER™ MODULES



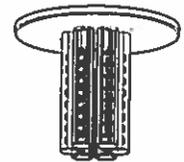
DSAR1 E.P.A. = 0.99
Available in:
36 LED Module
48 LED Module



36 LED Module*



DSAR25 E.P.A. = 1.38
Available in:
36 LED Module
48 LED Module



48 LED Module*

*Modules shown depict a typical Type III light distribution Reflector-Prism pattern.

ORDERING INFORMATION

LUMINAIRE-FITTER	OPTICS	LED COLOR VOLTAGE	MOUNTING	FINISH	OPTIONS
LUMINAIRE	OPTICS	LED	MOUNTING	FINISH	OPTIONS
<input type="checkbox"/> DSAR1 <input type="checkbox"/> DSAR25	<input type="checkbox"/> TYPE II <input type="checkbox"/> TYPE III <input type="checkbox"/> TYPE IV <input type="checkbox"/> TYPE V	<input type="checkbox"/> 36LED (36 Watts) <input type="checkbox"/> 48LED (48 Watts) COLOR <input type="checkbox"/> *NW - NEUTRAL WHITE (4100K) *STANDARD <input type="checkbox"/> CW - COOL WHITE (6600K) OTHER LED COLORS AVAILABLE CONSULT FACTORY VOLTAGE <input type="checkbox"/> 120-277V <input type="checkbox"/> 347-480V	ARM MOUNT <input type="checkbox"/> XPK <input type="checkbox"/> XPS <input type="checkbox"/> XPD <input type="checkbox"/> T WALLMOUNT <input type="checkbox"/> WM	STANDARD TEXTURED FINISH <input type="checkbox"/> BLACK RAL-9005-T <input type="checkbox"/> WHITE RAL-9003-T <input type="checkbox"/> GREY RAL-7004-T <input type="checkbox"/> DARK BRONZE RAL-8019-T <input type="checkbox"/> GREEN RAL-6005-T FOR SMOOTH FINISH REMOVE SUFFIX 'T' (EXAMPLE: RAL-9005) SEE USALIG.COM FOR ADDITIONAL COLORS	<input type="checkbox"/> INTERNAL HOUSE SIDE SHIELD HS <input type="checkbox"/> PHOTO CELL + VOLTAGE (EXAMPLE: PC120V) PC+V <input type="checkbox"/> SINGLE FUSE (120V . . . 277V) SF <input type="checkbox"/> DOUBLE FUSE (208V . . . 240V) DF CONSULT FACTORY FOR DIMMING OPTIONS

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SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

APPLICANT'S QUESTIONNAIRE

The Applicant's Questionnaire is a tool to help applicants write findings of fact that will demonstrate that a proposed development complies with the decision criteria that the Site Plan and Architectural Commission (SPAC) uses to approve applications. The decision criteria set forth in Section 10.290 of the *Medford Land Development Code* is as follows:

- (1) *The proposed development is compatible with uses and development that exist on adjacent land; and*
- (2) *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC Section 10.253.*

Instructions

A completed, signed written response to this questionnaire *shall* be included with all applications for Site Plan and Architectural Review. The questions herein are designed and organized to elicit factual information that can help support your project during the review process. Please prepare your responses on separate sheets of paper and sign and date the document.

Section I - Narrative

Write a brief narrative that describes your proposed development.

RESPONSE:

Mountain Vista is a 46 unit multiple family project with a community building located on Stewart Avenue, Medford, Oregon. The proposed project will consist of six - two story wood framed six-plex and eight-plex apartment buildings and a one story community building with offices, meeting rooms, and a multi-purpose area with kitchen. The apartments will be one, two, and three bedroom flats with individual exterior entries and covered patios and decks. Site amenities include a play area with a pre-manufactured play structure, a splash pad water feature, communal courtyards between buildings, covered exterior gathering areas around community building, and landscaped outdoor recreation areas. All buildings are designed for durability and low maintenance as well as meeting the standards for Earth Advantage 'Gold' certification. All applicable local, State, and Federal codes will be met including accessibility.

Mountain Vista will be accessed off of Homeview Drive and its extension as will be required for this project. Dedication and improvement of Stewart Avenue is also anticipated by the applicant. Adjacent properties are Skyvista Apartments to the East; single family dwellings in SFR6 zoning to the South; vacant MFR 20 property to the West; and Stewart Avenue to the North. The proposed project will consist of five- two story eight-plex apartment buildings and a one story community building North of the Homeview Drive extension and a two story six-plex South of Homeview and adjacent to SFR6 zoning. Twenty foot building setbacks and a ten foot Type 'A' landscape buffer per MLDC will be provided at a minimum in this area. All MLDC requirements will be met with no exceptions requested.

CITY OF MEDFORD
EXHIBIT # 1
File # AC-15-131

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

This application for SPAC approval is an application for development of needed housing. The following is an excerpt from a previous application written by attorneys as an explanation of the law as it applies to needed housing:

ORS 197.303(1) defines "needed housing" as "housing types determined to meet the need shown for housing within an urban growth boundary at particular price ranges and rent levels" and includes both "multiple family housing for both owner and renter occupancy" and "government assisted housing." ORS 197.303(1) Local governments must permit needed housing in one or more zoning districts and "with sufficient buildable land to satisfy that need." ORS 197.307(3). Local governments may only impose "clear and objective standards, conditions and procedures regulating the development of needed housing." ORS 197.307(4).

The local government's standards applicable to needed housing must be clear and objective on the face of the ordinance. ORS 227.173(2). Furthermore, the local government bears the burden of demonstrating that the standards, conditions, and procedures are capable of being imposed only in a clear and objective manner. ORS 197.831. Finally, these "clear and objective" standards, conditions, and procedures must not have the effect of "discouraging needed housing through unreasonable cost or delay." ORS 197.307(4).

The legislature's policy goal in enacting the "needed housing" statutes was to prevent local governments from using their land use regulations to exclude certain housing types, particularly manufactured housing, which the legislature believed was needed to satisfy low and moderate-income housing demand." *Rogue Valley Association of Realtors v. City of Ashland*, 35 Or LUBA 139, 156 (1998) af'd 158 Or App 1, 970 P2d 685, rev den 328 Or 594 (1999). In fact, ORS 197.307(1) states that the "availability of affordable, decent, safe and sanitary housing opportunities for persons of lower, middle and fixed income ***** is a matter of statewide concern."

LUBA has interpreted these statutes to preclude local governments from applying subjective, discretionary criteria to applications for needed housing:

"'Needed housing' is not to be subjected to standards, conditions or procedures that involve subjective, value-laden analyses that are designed to balance or mitigate impacts of the development on (1) the property to be developed or (2) the adjoining properties or community. Such standards, conditions or procedures are not clear and objective and could have the effect 'of discouraging needed housing through unreasonable costs or delay.'"

Rogue Valley Association of Realtors, 35 Or LUBA at 158 (Emphasis supplied.). Further, LUBA has specifically held that a standard requiring that a development be "compatible with surrounding development" was not clear and objective. *Rogue Valley Association of Realtors*, 35 Or LUBA at 157 (citing Land Conservation and Development Commission policy that served as basis for legislative enactment of needed housing statute). See also *Clark v. Coos County*, 53 Or LUBA 325 (2007) (local code approval standard that requires that proposed uses be compatible with surrounding uses is subjective). The Court of Appeals has also held that a local code standard is not "clear and objective" when it can "plausibly be interpreted in more than one way." *Tirumali v. City of Portland*, 169 Or App 241, 246, 7 P3d 761 (2000).

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

The City has determined that it has a need for 4,586 units of multiple-family housing over the planning period from 2009-2029. See Table 34 of GLUP Housing Element. The City also concluded that in 2008, there was a need for 4,450 housing units affordable for households earning less than \$25,000 a year. GLUP Housing Element at 44. Further, the City has estimated that 22% of the City's needed housing over the planning period will develop in the UH plan district. GLUP Housing Element at 60.

Therefore, this application which proposes 46 units of affordable multiple-family housing in the UH district-constitutes an application for development of "needed housing" for purposes of ORS 197.307. As a result, the City must only apply clear and objective standards, conditions and procedures to the approval of this application. Furthermore, even these clear and objective standards, conditions and procedures cannot have the effect of discouraging needed housing through unreasonable cost or delay.

Section II- Compatibility: Criterion No. 1

The following questions are designed to demonstrate how your proposal is compatible with uses and development that exist on adjacent land. Using factual information, respond to the following questions on a separate sheet of paper: use supplemental information such as site photographs, aerial maps, etc. to augment your case.

RESPONSE: As the application form notes, the questions in this section all concern the compatibility of the proposed development. As such, these questions involve matters that are not clear and objective. Nevertheless, in an effort to fully describe the proposal and how it relates to surrounding uses, the applicant offers the following responses to these questions without waiving its legal right to have the application reviewed pursuant to ORS 197.307. The applicant's voluntary response to these questions does not constitute a waiver of the applicant's position that the answers to these questions cannot provide a basis to approve or deny the Application.

- A List existing uses and development adjacent to your project site. Along with this list, describe the architecture (materials, colors, etc.), age, and condition of the adjacent buildings (you may use photographs to supplement this information).

RESPONSE: Existing adjacent uses and development include Stewart Avenue to the North with commercial uses across the street; an existing 48 unit multiple family development to the East; single family residences in SFR6 zoning to the South; and vacant MFR 15 zoned land to the West. The adjacent multiple family development is two story, ten years old and in sound, well kept condition; the adjacent single family development is fifteen to twenty years old, mostly single story, and in sound, well kept condition.

- B Describe the building architecture and exterior treatments in your proposal, and how they fit with and complement adjacent buildings and development.

RESPONSE: The proposed development is comprised of two story apartments and a single story community building all with cement fiber lap, board & batten, and shingle siding; architectural grade composition roofing. Wall and roof lines are articulated to provide relief of massing. The remainder of this question concerns matters that are not clear and objective and do not apply under ORS 197.307.

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

- C Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass. The *Site Plan and Architectural Commission Design Guidelines* are a helpful reference, and can be found on the City's website, and at the Planning Department.

RESPONSE: Wall lines are offset along all facades and porches, balconies, and stairs breakup facades. Roof lines are articulated to add interest and breakup massing.

- D Describe how the placement and orientation of the proposed building(s) relate(s) to the street facilities, and how this orientation promotes a more pedestrian-friendly site design.

RESPONSE: Orientation of the buildings is to private internal driveway/parking areas with pedestrian sidewalk connections to Stewart Avenue and Homeview Drive. An internal network of sidewalks connects all units to the community building and play/recreation areas.

- a. If the site lies within 600-feet of an existing or planned transit stop, as designated by the Transportation System Plan (TSP), describe compliance with the standards of Section 10.808. New Commercial and Institutional Development.

RESPONSE: This provision is not applicable.

- E Describe the pedestrian facilities and amenities on your site (useable outdoor space, benches, etc), and how they will function for pedestrians.

RESPONSE: Pedestrian facilities include sidewalk connections between all buildings and community building, courtyards with benches in front of all buildings, a play area and significant patio areas with benches surrounding the community building.

- F Describe vehicle and pedestrian access to the site, and how it relates internally on the site, and to adjacent sites.

RESPONSE: Direct vehicular access to and from the site will be from the Property will be via two driveways from Homeview Drive. The East most driveway exists and will be shared with the existing Skyvista site in the form of an access easement. Parking areas are internal to the site for the convenience if the residents. Pedestrian access will be from both Homeview Drive and Stewart Avenue.

- G Describe if and how the proposed plan is sensitive to retaining any existing trees or significant native vegetation on the site. Should existing trees be preserved, a Tree Protection Plan shall also be included in this application.

RESPONSE: The site does not have existing trees or other significant native vegetation to retain.

- H Describe stormwater detention facilities on the site (underground storage, surface pond, etc), If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.

RESPONSE: Stormwater will be handled through a series of bioswales and surface detention ponds to treat and control runoff to pre-development conditions. Above-ground facilities will be landscaped as required for biofiltration and will include trees, shrubs, and groundcover.

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

- I Describe how your proposed landscaping design will enhance the building and other functions on the site.

RESPONSE: The use of trees, shrubs, turf, xeriscaping and other ground covers in the proposed landscape design will enhance the aesthetics of the project as well as providing shading and surfaces for recreational activities. The parking areas are broken up and shaded by trees and shrubs in multiple planter bays.

- J Describe how your exterior lighting illuminates the site, and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of Section 10.764.

RESPONSE: The exterior lighting design features hooded downlight fixtures, meeting night sky objectives, on ten foot poles in the parking areas and along pedestrian paths. Other exterior lighting includes photocell controlled soffit lighting at individual unit entries and patios, at stairways, and at the community building covered patios.

- K Describe any proposed signage, and how it will identify the location of the occupant and serve as an attractive complement to the site.

RESPONSE: A ground lighted, rock faced monument sign containing the project name is proposed at the vehicular entrance to the site. The sign will be designed to conform to City's signage code.

- L Explain any proposed fencing, including its purpose, and how you have incorporated it as a functional, attractive component of your development (See Sections 10.731- 10.733)

RESPONSE: Proposed fencing is a six foot split face concrete block wall along Stewart Avenue to serve as a visual and sound barrier; the same along boundary with single family development to the South to be incorporated as an element of the Type 'A' buffer; a six foot chainlink fence along the West property line for site security; and a 36" wrought iron picket type fence along both sides of Homeview Drive as a safety and aesthetic element. No other fencing is proposed.

- M Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of Sections 10.752-10.761.

RESPONSE: These standards do not apply, potential noise generated is not of a commercial or industrial source.

- N Explain anything else about your project that adds to the compatibility of the project with adjacent development and uses.

RESPONSE: This question concerns matters that are not clear and objective and do not apply under ORS 197.307

- O List and explain any exceptions or modifications requested and provide reasons for such.

RESPONSE: No exceptions or modifications are requested

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

- P Section 10.780(C)(2) - List any petition for relief of landscaping standards (ie., request an increase in turf area at a facility for active recreation; eliminate requirement for root barriers when trees are planted in structural soils). Provide rationale for requested deviation from standard.

RESPONSE: No petition for relief is requested

Signed:



Daniel Horton

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SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

Section II – Code Compliance: Criterion No. 2

PROJECT SITE

	PROPOSED	REQUIRED
• Zoning District	MFR-20	
• Overlay District(s)		
• Proposed Use	MULTIFAMILY	
• Project Site Acreage	2.86 AC	
• Site Acreage (+ right-of-way)	3.04 AC	
• Proposed Density (10.708)	154/AC	
• # Dwelling Units	46	
• # Employees	0	

STRUCTURES

	EXISTING	PROPOSED
• # Structures	1	7
• Structure Square Footage (10.710-10.721)	900 TO BE DEMO'D	43,637 SF

SITE DESIGN STANDARDS

	PROPOSED	REQUIRED
• Front Yard Setback (10.710-721)	15'	15'
• Side Yard Setback (10.710-721)	17'-20'	8'-20'
• Side Yard Setback (10.710-721)		
• Rear Yard Setback (10.710-721)	15'-20'	8'-20'
• Lot Coverage (10.710-721)	27%	40%

PARKING

	PROPOSED	REQUIRED
• Regular Vehicular Spaces (10.743)	68	66
• Disable Person Vehicular Spaces (10.746[8])	6	3
• Carpool/Vanpool Spaces (10.809)		
• Total Spaces (10.743)	74	69
• Bicycle Spaces (10.748)	46	46
• Loading Berths (10.742)		

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

LANDSCAPING		
	PROPOSED	REQUIRED
• Total Landscape Area (square feet)	37331	n/a
• Total Landscape Area in High Water Use Landscaping (square feet)	9829	n/a
• Total Landscape Area in High Water Use Landscaping (percentage)	26%	n/a
• Total % Landscape Coverage		na/
• Required Organic Content (cu.yd.)	112	112
• Frontage Landscaping (10.797)		
• Street:	Stewart Ave	
• Feet:	272	272
• # Trees:	11	11
• # Shrubs:	Native Grass Drainage Swale	n/a
• Street:	Homeview Drive	
• Feet:	233	233
• # Trees:	10	10
• # Shrubs:	Native Grass Drainage Swale	n/a
• Bufferyard Landscaping (10.790)		
• Type:	A	A
• Distance (ft):	358	358
• # Canopy Trees:	22	n/a
• # Shrubs:	46	n/a
• Fence/Wall:	CMU Block	CMU Block
• Parking Area Planter Bays (10.746)		
• Type:	Projection	PROTECTION
• # Bays:	12	12
• Area:	1683	1641
• # Trees:	9	9
• # Shrubs:	94	18

STRUCTURE

	PROPOSED
• Materials	ARCH GRADE COMP SHINGLE ROOF CEMENT FIBER LAP PANEL, SHINGLE SIDING
• Colors	MULTIPLE COMPLIMENTARY EARTH TONES

Please remember that the information you provide in response to the questionnaire must be included with your SPAR application submittal. Remember to sign and date your written response.



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CITY OF MEDFORD

LD Date: 11/11/2015
File Number: AC-15-131

**PUBLIC WORKS DEPARTMENT STAFF REPORT
MOUNTAIN VISTA APARTMENTS**

Project: Plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zoning district.

Applicant: ON-TRACK, INC., Applicant (Daniel Horton, Agent). Sarah Sousa, Planner.

NOTE:

The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of building permits. Construction plans for the improvements would need to be approved by the Public Works Engineering Department prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas.
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

A. STREETS

1. Dedications

Stewart Avenue is classified as a Major Arterial Street with a required right-of-way width of

100-feet. Prior to issuance of any permit for construction, the developer shall dedicate for public right-of-way, sufficient width of land along the frontage of this development to comply with the half width of right-of-way, which is 50 feet. The existing right-of-way south of the centerline appears to be 30 feet. **The amount of additional right-of-way needed appears to be 20 feet.** Medford Land Development Code (MLDC 10.451).

The Developer shall receive S.S.D.C. (Street System Development Charge) credits for the public right-of-way dedications on Stewart Avenue, per the value established by the Medford Municipal Code (MMC) , Section 3.815. **Should the developer elect to have the value of the S.S.D.C. credits determined by an appraisal, a letter to that effect must be submitted to the City Engineer within 60 working days of the date of the Final Order by the Site Plan and Architectural Commission. The City will then select an appraiser and a deposit will be required as stated in Section 3.815.**

Homeview Drive is classified as a Minor Residential Street with a required right-of-way width of 55-feet, and shall be dedicated through the length of this Development in an alignment which will line up with the connection on Lozier Lane.

Streets, as shown on the Site Plan, in which any portion terminates to a boundary line of the Development shall be dedicated to within one foot of the boundary line, and the remaining one foot shall be granted in fee, as a non-access reserve strip to the City of Medford. Upon approved dedication of the extension of said streets, the one-foot reserve strip shall automatically be dedicated to the public use as part of said street without any further action by the City of Medford. (MLDC 10.439)

In accordance with MLDC 10.471, **the property owner shall dedicate a 10 foot wide public utility easement (PUE)** adjacent to the right-of-way line in Stewart Avenue and Homeview Drive along this Developments respective frontage.

The right-of-way and PUE dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: right-of-way and PUE dedications, a copy of a current lot book report, preliminary title report, or title policy; a mathematical closure report (if applicable), and the Planning Department file number, all for review and signature acceptance by the City Engineer prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the areas dedicated.

2. Public Improvements

a. Public Streets

The Developer shall construct **Stewart Avenue** to major arterial street standards in accordance with the MLDC, Section 10.428 and 10.442. The roadway improvements shall include 35-feet of paving from the centerline to the south curb face and a minimum of 12-foot wide north of centerline or to the existing edge of paving, whichever is greater. Improvements shall also include a 10-foot planter strip, 5-foot sidewalk and street lights along this development's frontage on Stewart Avenue. The City will pay S.S.D.C. credits per MMC, Section 3.815.

The Developer shall build **Homeview Drive** according to City of Medford standards for a minor residential street. These improvements include a 28-foot wide paved section curb to curb, curb and gutter, 8-foot planter strip, 5-foot sidewalks and street lights.

b. Street Lights and Signing

1. Traffic Signs and Devices – City installed, Developer paid
 - A. 1 – Dead-end Barricade, west end of Homeview Drive.
2. Street Lighting – Developer Provided and Installed
 - A. 2 – 310 watt collector street lights on Stewart Ave; power to be from existing light source on Stewart Ave.
 - B. 2 – 100 watt residential street lights on Homeview Drive; power to be from new base mounted cabinet (BMC), or Engineer to confirm existing power source is adequate to handle two additional lights.

c. Pavement Moratoriums

There is a pavement cutting moratorium currently in effect along this frontage on Stewart Avenue, between Lozier Lane and S Columbus Avenue, which expires 8/13/2020.

3. Access and Circulation

Driveway access to the proposed development site shall comply with MLDC 10.550. Access to the proposed facility shall be restricted to Homeview Drive. No access shall be allowed onto Stewart Avenue. The driveway approaches shall have 20 foot curb radii, concrete valley gutters and ADA ramps at the throat of each driveway.

4. MLDC Section 10.668 Analysis

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless: (1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose, and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or (2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose.

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and are supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, and pedestrians. It can be found that the listed right-of-way dedications and

improvements have a nexus to these purposes and policies.

2. Rough proportionality between the required dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Also, the City is allowed to consider the benefits to the development from the dedication and improvements when determining “rough proportionality.”

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Stewart Avenue and Homeview Drive:

The additional right-of-way and improvements on Stewart Avenue will provide the needed width for a bike lane, planter strip and sidewalk. Stewart Avenue is a 35 mile per hour facility, which currently carries approximately 10,800 vehicles per day. The 10’ planter strip moves pedestrians a safe distance from the edge of the roadway. Stewart Avenue will be a primary route for pedestrians traveling to and from this development.

Local street right-of-way dedication and construction requirements identified by the Public Works Department and required by the City are the minimum required to protect the public interest and are necessary for additional or densification of development in the City without detracting from the common good enjoyed by existing properties. Developments are required to provide all internal local streets and half-street improvements to abutting streets, including associated right-of-way dedications, to ensure that new development and density intensification provides the current level of urban services and adequate street circulation is maintained.

Dedication of the Public Utility Easements (PUE) will benefit development by providing public utility services, which are out of the roadway and more readily available to each lot or building being served. The additional traffic of all modes of travel generated by this proposed development supports the dedication and improvements for all modes of travel and utilities. As indicated above, the area required to be dedicated for this development is necessary and roughly proportional to that required in similar developments to provide a transportation system that meets the needs for urban level services.

The City assesses System Development Charges (SDC’s) to help pay for acquisition of right-of-way and construction of additional Arterial & Collector street capacity required as a result of new development. Because a mechanism exists in the form of SDC credit for right-of-way dedication and street improvements in accordance with Medford Municipal Code (MMC) 3.815 and other applicable parts of the Code, to fairly compensate the applicant, the conditions of MLDC, Section 10.668 are satisfied.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. Contact Rogue Valley Sewer Service for sanitary sewer connections.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

The Design Engineer shall provide an investigative report of the off-site drainage on the developments perimeter, a distance not less than 100 feet in all directions. All off-site drainage affecting the development shall be addressed on the drainage plan. A hydrology map depicting the amount of area the development will be draining shall be submitted with hydrology and hydraulic calculations. The opening of each curb inlet shall be sized in accordance with ODOT design standards. These calculations and maps shall be submitted with the public improvement plans for approval by the Engineering Division.

The Developer shall provide a storm drain easement over Little Elk Creek measured 15-feet from the centerline of the creek to both sides.

The Developer shall provide a flood study to show the base flood elevations for Little Elk Creek.

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Wetlands

The Developer shall contact the Division of State Lands for the approval or clearance of the subject property with regards to wetlands and/or waterways.

6. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

1. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

2. System Development Charges

Buildings in this development are subject to street, sanitary sewer treatment and stormdrain system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued. The sanitary sewer collection SDC fees shall be paid to Rogue Valley Sewer Services prior to building permits being issued.

Prepared by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

Mountain Vista Apartments

AC-15-131

A. Streets

1. Street Dedications to the Public:

Dedicate approximately 20-feet of additional public right-of-way on Stewart Avenue.

Dedicate Homeview Drive right-of-way 55-feet wide.

Dedicate 10 foot Public Utility Easement (PUE) along all frontages.

2. Improvements:

Public improvements are required along Stewart Avenue in accordance with the MLDC, Section 10.428 and 10.442.

There is a pavement cutting moratorium currently in effect along Stewart Avenue.

The Developer shall construct new radius driveways on Homeview Drive.

B. Sanitary Sewer:

The site is situated within the RVS service area.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

Provide DSL signoff in regards to any wetlands on this site.

Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

RECEIVED

NOV 11 2015

PLANNING DEPT.

LAND DEVELOPMENT REPORT - PLANNING

To: Sarah Sousa

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Applicant (Daniel Horton, Agent)

File #: AC - 15 - 131

Site Name/Description:

Plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential - 20 dwelling units per gross acre) zoning district; ONTRACK, INC., Applicant (Daniel Horton, Agent). Sarah Sousa, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
----------------------------	-----------

Requirement ADDRESS IDENTIFICATION

OFC

505.1

Building numbers shall be placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background.

Temporary signs shall be installed at each street intersection when construction of new roadways allows passage of vehicles.

Requirement FIRE HYDRANTS

OFC

508.5

Fire hydrants with reflectors will be required for this project.

Hydrant locations shall be as follows: Approved as submitted.

Additional hydrants may be required to comply with the requirement of proximity to fire department connections (for fire sprinkler and standpipe systems, the fire department connection shall be located at an approved location away from the building and within 75' of a fire hydrant. The fire department connection shall be located on the same side as the fire department access route.).

The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Plans and specifications for fire hydrant system shall be submitted to Medford Fire Department for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).

Requirement FD APPARATUS ACCESS ROAD DESIGN

OFC

503.2.1

Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt concrete or other approved driving surface capable of supporting the imposed load.

CITY OF MEDFORD
EXHIBIT # L
File # AC-15-131



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: Sarah Sousa

LD Meeting Date: 11/11/2015

From: Greg Kleinberg

Report Prepared: 11/02/2015

Applicant: Applicant (Daniel Horton, Agent)

File #: AC - 15 - 131

Site Name/Description:

60,000 pounds.
(See also OFC 503.4; D102.1)

The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).

Requirement "NO PARKING-FIRE LANE" SIGNS REQUIRED

OFC

503.3

Parking shall be posted as prohibited on one side of Homeview Drive. Additionally, parking shall be posted as prohibited on both sides of Homeview Drive west of the driveways for fire department turn-around purposes.

Fire apparatus access roads 20-26' wide shall be posted on both sides as a fire lane. Fire apparatus access roads more than 26' to 32' wide shall be posted on one side as a fire lane (OFC D103.6.1).

Where parking is prohibited for fire department vehicle access purposes, NO PARKING-FIRE LANE signs shall be spaced at 50' intervals along the fire lane and at fire department designated turn-around's. The signs shall have red letters on a white background stating "NO PARKING FIRE LANE TOW AWAY ZONE ORS 98.810 to 98.812" (See handout).

Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12).

This restriction shall be recorded on the property deed as a requirement for future construction.

Requirement FIRE DEPARTMENT TURN-AROUND

OFC

503.2.5

The end of Homeview Drive and the driveways shall be used as a temporary fire department turn-around until future connection is established.

Dead-end Fire Apparatus access roads in excess of 150 feet in length shall be provided with approved provisions for the turning around of fire apparatus.

The Fire department turn-around area must be posted with "NO PARKING-FIRE LANE" signs. These signs shall be spaced at 50' intervals along the fire lane and at fire department designated turn-around's.

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.



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NOV 11 2015
PLANNING DEPT.

Memo

To: Sarah Sousa, Planning Department
From: Mary Montague, Building Department
CC: On Track, Inc.; Daniel Horton, Agent
Date: November 11, 2015
Re: AC-15-131

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit will be required if more than 50 cubic yards is disturbed.
4. A separate demolition permit will be required for demolition of any structures not shown on the plot plan.
5. Proposed buildings may require design by an Oregon licensed architect or engineer pursuant to code section 107.1 of the 2014 Oregon Structural Specialty Code.
6. Fire Sprinklers are required per Fire Code and 2014 OSSC [F] 903.2.8. Fire Sprinklers are a separate submittal.

CITY OF MEDFORD
EXHIBIT # M
File # AC-15-131

MEDFORD PARKS & RECREATION

HEALTHY LIVES. HAPPY PEOPLE. STRONG COMMUNITY.



CITY OF MEDFORD
Interoffice Memo

RECEIVED

NOV 10 2015

PLANNING DEPT.

TO: Planning Department
FROM: Tim Stevens- Park Maintenance Supervisor
SUBJECT: LANDSCAPE REVIEW OF FILE AC-15-131, Mountain Vista
DATE: November 10, 2015

Per City of Medford code 10.780 (G.)6.a Turf grass area shall have a minimum average width of (8) feet and shall be no less than six (6) feet in width at any point. South lawn area needs to be adjusted to fulfill this requirement.

This report addresses horticultural concerns only. Applicant shall comply with all aspects of Medford Code 10.780 Interpretation of the Medford Code will be per the Planning Department. Aesthetic considerations will be per the Site Plan and Architectural Review Commission or Planning Commission upon their review.



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CITY OF MEDFORD

EXHIBIT # N

File # AC-15-131



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford
FROM: Rodney Grehn P.E., Water Commission Staff Engineer
SUBJECT: AC-15-131
PARCEL ID: 372W35AD TL 1400
PROJECT: Plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zoning district; ONTRACK, INC., Applicant (Daniel Horton, Agent). Sarah Sousa, Planner.
DATE: November 9, 2015

RECEIVED

NOV 11 2015

PLANNING DEPT.

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of 8-inch water lines is required. Proposed "on-site" water lines are required to be installed in paved travel lanes. Water lines shall not extend through landscaping, parking spaces, or under proposed trees. Applicants' civil engineer shall coordinate with MWC engineering staff on layout of onsite water lines, hydrants, meters and back flow devices.
4. Dedication of a 10 foot wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement shall be submitted to MWC for review and recordation prior to construction.
5. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

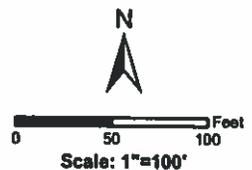
COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required. (See Condition 3 above)



Staff Memo

3. MWC-metered water service does not exist to this property.
4. Static water pressure is expected to be between 60 and 68 psi at ground level.
5. Access to MWC water lines is available. An 8-inch water line is stubbed for extension at the existing westerly end of Homeview Drive, and there is a 16-inch water line on the north side of Stewart Avenue.



**Water Facility Map
for
AC-15-131**

Legend

- Air Valve
 - Sample Station
 - Fire Service
 - Hydrant
 - Reducer
 - Slow Off
 - Plug-Caps
- Water Meters:**
- Active Meter
 - On Well
 - Unknown
 - Vacant
- Water Valves:**
- Butterfly Valve
 - Gate Valve
 - Tapping Valve

- Water Mains:**
- Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line

- Boundaries:**
- Urban Growth Boundary
 - City Limits
 - Tax Lots

- MWC Facilities:**
- Control Station
 - Pump Station
 - Reservoir



This map is based on data supplied by the Medford Water Commission. It is not a warranty of accuracy. The Commission is not responsible for any errors or omissions. For more information, please contact the Medford Water Commission.



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 7502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

RECEIVED

NOV 03 2015

PLANNING DEPT.

November 3, 2015

City of Medford Planning Department
411 West 8th Street
Medford, Oregon 97501

Re: AC-15-131, Mountain Vista Apartments, REF PA-14-062 (372W35AD, Tax lot 1400)

ATTN: Sarah,

The subject property is currently served by a connection to the 12 inch sewer main in Stewart Avenue. This existing service must be abandoned when the existing home is removed.

The conceptual utility plan indicates that a public sewer main is proposed along the westerly side of the property. Presumably service laterals would be extended from this main to serve the new buildings. The sewer main line must be designed and constructed in accordance with RVSS standards. Service laterals must be sized in accordance with the Oregon Plumbing Specialty Code.

Rogue Valley Sewer Services requests the proposed development be subjected to the following conditions:

- Applicant must submit Engineered sewer plans in accordance with Rogue Valley Sewer Services Specifications for review and approval prior to development.
- No building occupancy will be allowed until all required mainline sewer extensions are accepted by RVSS.
- Developer must properly abandon the existing service lateral. RVSS will issue a no-cost permit to verify that the pipe is properly abandoned.

Feel free to contact me with any questions related to this project.

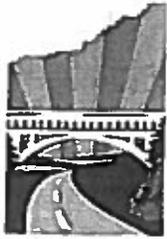
Sincerely,

Carl Tappert

Carl Tappert, PE
Manager

Page 1 of 1

CITY OF MEDFORD
EXHIBIT # P
File # AC-15-131



JACKSON COUNTY

Roads

RECEIVED

Roads
Engineering NOV 09 2015

Kevin Christiansen
Construction Manager PLANNING DEPT.

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
christike@jacksoncounty.org

www.jacksoncounty.org

November 2, 2015

Attention: Sarah Sousa
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Development off Stewart Avenue – a city maintained road.
Planning File: AC-15-131.

Dear Sarah:

Thank you for the opportunity to comment on the request for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zone district. Jackson County Roads has no comment.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,

Kevin Christiansen
Construction Manager

Sarah K. Sousa

From: Townsend, Paige <ptownsend@rvtd.org>
Sent: Wednesday, November 18, 2015 3:48 PM
To: Sarah K. Sousa
Cc: drharchitects@embarqmail.com
Subject: Re: On Track bus stop off Stewart

RECEIVED
NOV 18 2015
PLANNING DEPT.

Hi Sarah,

Dan and I spoke about this On Track application. Because we will be pursuing a bus stop on the adjacent property to the east we will not be asking for a condition for approval. The process will be easier with the property to the east already being finalized and occupied according to Dan.

Let me know if you have any questions!

Paige Townsend
RVTD Senior Planner
541-608-2429
www.rvtd.org

On Tue, Nov 3, 2015 at 1:59 PM, Townsend, Paige <ptownsend@rvtd.org> wrote:
Hi Sarah,

I am reviewing the application for the On Track development off Stewart Ave., AC-15-131. RVTD will likely have service that runs in front of this development and so we are requesting a bus stop easement as part of the conditions for approval. I have left a message with Horton Architecture to look at what is possible, given the wall right behind the sidewalk. I will let you know what we come up with and try to attend next Wed. LDC meeting.

Thanks!

Paige Townsend
RVTD Senior Planner
[541-608-2429](tel:541-608-2429)
www.rvtd.org

Sarah K. Sousa

From: MOREHOUSE Donald <Donald.MOREHOUSE@odot.state.or.us>
Sent: Friday, November 06, 2015 2:51 PM
To: Sarah K. Sousa
Subject: AC-15-131

RECEIVED

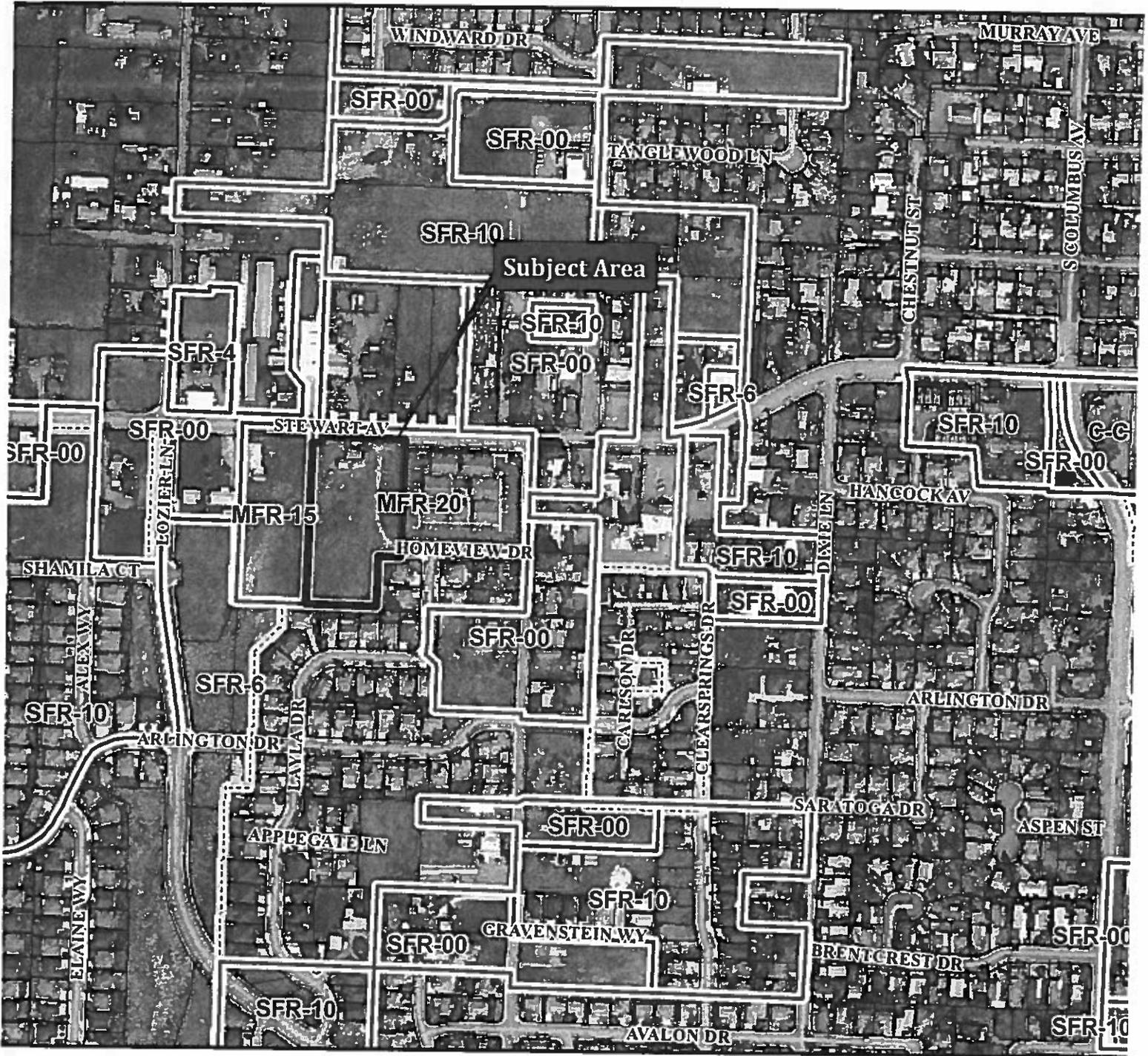
NOV 06 2015

PLANNING DEPT.

Sarah,

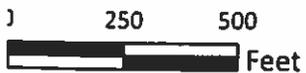
Thank you for sending agency notice of plans for the construction of a 46-unit multi-family residential project on a 2.84 acre parcel, located on the south side of Stewart Avenue, between Lozier Lane and Orchard Home Drive, within the MFR-20 (Multiple-Family Residential – 20 dwelling units per gross acre) zoning district. We reviewed this and determined that it would not significantly affect state transportation facilities under the State Transportation Planning Rule (OAR 660-012-0060) or State Access Management Rule (OAR 734-051-000). We have no further comments at this time.

Don Morehouse
Senior Transportation Planner
ODOT Region 3, District 8 (Rogue Valley Tech Center)
Ph: (541) 774-6399
Fax: (541) 774-6349
Donald.Morehouse@odot.state.or.us

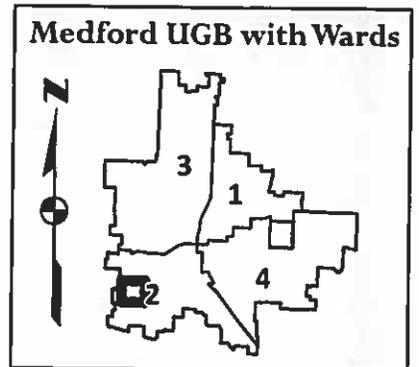


Project Name:
**Mountain Vista Site Plan -
Multi-Family**

Map/Taxlot:
372W35AD TL 1400



-  Subject Area
-  Medford Zoning
-  City Limits
-  Tax Lots
-  MUD





Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT

for a Type-C quasi-judicial decision: **Architectural and Site Plan Review**

Project Northgate Marketplace Phase II
 Regency Centers, L.P., Applicant; Cardino, Inc., Agent

File no. AC-15-133

To Site Plan and Architectural Commission *for 12/04/2015 hearing*

From Desmond McGeough, Planner II *[Signature]*

Reviewer Kelly Akin, Principal Planner

Date November 25, 2015

BACKGROUND

Proposal

Consideration of a Final Site Plan for the future development Northgate Marketplace Phase II (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site within a C-R (Regional Commercial) zoning district. The subject site is generally located north of West McAndrews Road, east of N. Central Avenue west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238).

Subject Site Characteristics

Zoning	C-R	Regional Commercial
GLUP	CM	Commercial
Use	Vacant land within the Northgate Marketplace development	

Surrounding Site Characteristics

North	I-L, C-S/P	Vacant Land – Northgate Office Park
South	C-R I-H	Vacant Land – Northgate Marketplace, Metal Fabrication
East	C-R, C-H	Northgate Marketplace Phase I, Miscellaneous Commercial
West	I-G/GI	Timber Products, Vacant Land

Related Projects

- CP-06-065 Comprehensive Plan Amendment to GI to CM, SC
- ZC-06-066 Zone Change to C-R, C-S/P and I-L
- CP-11-041/ Amend the conditions of approval associated with the 2006
- ZC-11-042 Comprehensive Plan and Zone Change approvals the Northgate Centre Development.
- AC-11-076 Northgate Marketplace Master Site Plan
- AC-11-093 Northgate Marketplace Phase I Site Plan
- AC-15-032 Costco Warehouse at Northgate Marketplace (Withdrawn)

Applicable Criteria

Medford Land Development Code (MLDC) §10.290

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

Corporate Names

Corporation Service Company is the Registered Agent of Regency Centers, L.P. according to the Oregon Secretary of State Business Registry. Patrick G. Huyke is the Registered Agent of Alba Village LLC.

ISSUES AND ANALYSIS

Northgate Centre - Application History

On March 1, 2006, a Comprehensive Plan Map Amendment request (CP-06-065), for amending the General Land Use Plan (GLUP) map from H-I (Heavy Industrial), G-I (General Industrial) and C-M (Commercial) designations to the H-I (Heavy Industrial), G-I (General Industrial), C-M (Commercial) and SC (Service Commercial) designation was submitted concurrently with a Zone Change application. The Planning Commission heard testimony on both the Comprehensive Plan Amendment and the Zone Change applications. On October 26, 2006, the Planning Commission forwarded a favorable recommendation on the Comprehensive Plan Amendment to the City Council. The Planning Commission also found that the zone change application met the required

criteria of the Land Development Code, subject to conditions of approval. The Final Order for Zone Change ZC-06-066 was approved November 9, 2006, subject to Council approval of the concurrent application for GLUP Map Amendment.

The Medford City Council conducted a public hearing regarding the Comprehensive Plan Amendment application on November 16, 2006, and deliberated the item at the December 7, 2006 Council meeting. The Medford City Council at its December 21, 2006, meeting passed Ordinance No. 2006-264, subject to conditions, approving CP-06-065, a minor amendment of the Comprehensive Plan. The City Council also adopted Ordinance No. 2006-296, an ordinance affirming the decision of the Planning Commission regarding the Northgate Zone Change application (ZC-06-066). The applicants entered into a Disposition and Development Agreement (DDA) with the City of Medford on November 6, 2008, which addressed off-site infrastructure requirements and the approval process for Master Site Planning and Final Site Planning approvals.

The applicants submitted development applications (CP-11-041, ZC-11-042) on March 15, 2011, to amend the conditions of approval associated with the 2006 Comprehensive Plan and Zone Change approvals of the Northgate Centre Development (CP-06-065 & ZC-06-066 respectively). In summary, the Comprehensive Plan Amendment application and the associated Zone Change amendment requested the following: 1) allow for the off-site traffic mitigation improvements to be phased in conjunction with the built-out square footage of the development; 2) remove requirements for specific architectural features and design theme as a condition of Council approval of the Concept Plan; and 3) allow for flexibility for future project design revisions to be brought forward as design review applications to the Site Plan and Architectural Commission (SPAC) rather than through an amendment to the original Comprehensive Plan Amendment (CP-06-065), which is heard by the City Council.

The Planning Commission held a public hearing on June 9, 2011, on the consolidated application and recommended approval to the City Council regarding the Comprehensive Plan Amendment application. The Commission also directed staff to prepare a Final Order for approval of the zoning amendment application, subject to Council approval of the Comprehensive Plan Amendment application (CP-11-041). On July 9, 2011, the City Council approved application CP-11-041 (Ord # 2011-122), changing the conditions of approval for the Northgate Centre development. The Planning Commission approved the Final Order changing the conditions of approval regarding the zoning amendment application on July 14, 2011.

On August 18, 2011, the applicant received City Council approval on an Amended Disposition and Development Agreement, which incorporated the new conditions of approval, consistent with the approval of CP-11-041 and ZC-11-042 (Ord. 2011-163). There were no other substantive changes proposed for the DDA.

Per the 2006 approval of the Comprehensive Plan Amendment and Zone Change applications as amended, the applicant must receive SPAC approval of a Master Site Plan for each component of the Northgate Centre prior to the submittal of a final site plan. A master site plan for the Northgate Marketplace Component was considered, and the Final Order was approved by the Site Plan and Architectural Commission on September 2, 2011. The Site Plan and Architectural Commission subsequently held a public hearing on the Northgate Marketplace Phase I (AC-11-093) on September 16, 2011 and adopted the Final Order for approval of the Phase I plan on October 7, 2011.

Northgate Centre Overview

The subject application is the second phase submitted for the Northgate Marketplace master planned commercial development. The Northgate Marketplace is one of three components encompassed by the Northgate Centre development. Overall, Northgate Centre consists of 84.07 net acres, located upon three of four quadrants of where State Highways 238, 62 and 99 intersect. The project site was once home to the Medco Lumber Mill. To date, Northgate Marketplace Phase I and associated internal collector drive aisles providing access to W. McAndrews Road, Central Avenue, Center Steet and Highway 238 have been constructed. The remaining property within the Northgate Marketplace, Office Park and Business Park is currently vacant.

Northgate Center Development Concept Plan (ZC-06-065 / CP 06-066)

Component	Location	Zone District	Use	Acres	Building Square Footage
Northgate Marketplace	SE Quadrant	C-R	Mixed Retail	40.05	417,500
Northgate Centre Office Park	NE Quadrant	SC, I-L	Office, Restaurants	18.85	219,300
Northgate Centre Business Park	NW Quadrant	I-G	Industry, Manufacturing	22.85	180,000

Northgate Marketplace site is entirely located within the C-R (Regional Commercial) zoning district. Northgate Centre Business Park is located in both I-G (General Industrial) and I-L (Light Industrial) zone districts, and the Northgate Centre Office Park is located within I-L and C-S/P (Service Commercial – Professional Office) zone districts.

Northgate Marketplace, previously known as Alba Village, is located within the southeast portion of the Northgate Centre development, at the southeast corner of Highway 62 and Central Avenue. Alba Village was initially planned as a 417,500 square foot lifestyle center containing a mix of small and medium sized boutique retail shops.

In 2011, the applicant received City Council and Planning Commission approvals to remove the specific concept site plan for the retail center and specific architectural design requirements conditioned with the 2006 Zone Change and Comprehensive Plan amendment approvals.

Subsequently, the Applicant filed a master site plan for the Northgate Marketplace Development (AC-11-076) to provide direction regarding the general layout, ingress points, vehicular and pedestrian circulation, juxtaposition of future buildings, and design continuity throughout the development. Shortly after the approval of the master plan (AC-11-076) the Site Plan and Architectural Commission approved the site plan application for Northgate Marketplace Phase I (AC-11-093).

The Northgate Centre Office Park component is located at the northeast corner of Highway 62 and Central Avenue (north of the subject site). The 2006 master conceptual plan includes a 219,300 square foot office park located upon the 18.83 acres. The office park will provide an up-scale urban campus containing professional offices mixed with restaurant uses.

The proposed Northgate Centre Business Park is located at the northwest corner of the intersection and is located within the I-G zone district. This component is intended to serve as an incubator for traded sector manufacturing. The industrial park provides 180,000 square feet of industrial space located on a 22.85 acre site.

The Northgate Centre Development is limited to a maximum amount of building square footage as a condition of approval for the Comprehensive Plan Amendment and Zone Change applications. The building square footage limitation has the effect of placing a traffic generation "trip cap" to address transportation system facility adequacy issues identified in the traffic impact analysis (TIA), which is part of the approved Comprehensive Plan Amendment and Zone Change applications.

Northgate Centre Disposition and Development Agreement

Per the approved Disposition and Development Agreement, the approved Northgate Marketplace Master Site Plan will be effective for the entire term of the DDA. The term of the DDA established is period of 10 years, commencing on the Effective Date on which the DDA is executed. Therefore the Northgate Marketplace Master Plan is effective through August 18, 2021. The Developers shall have the option of extending the Term upon written notice to the City.

Per the DDA, each component of the development shall be subject to the Medford Land Development Code (MLDC) standards which are in place at the time of the approval of a Master Site Plan for each subject component. Therefore, since the Master Plan for the

Northgate Marketplace was approved on September 2, 2011, MLDC standards in effect at that time are applicable to the Phase II site.

Development Agreement Phase I Off-Site Transportation Improvements

Per the DDA and conditions of approval for the Northgate Centre Comprehensive Plan Amendment and Zone Change, the applicant was required to construct at their sole expense, four off-site transportation improvements prior to development of Phase I. The applicant constructed the following offsite improvements with Phase I:

Phase I off site transportation improvements

Location	Improvement
Biddle Road at McAndrews Road	Construct turn lane
Highway 238 at Ross Lane	Construct traffic signal
Highway 238 at NB Sage Road	Construct northbound to eastbound right turn lane and half street improvement for future five lane section of Sage Road
McAndrews Road (south side, Central Avenue to Court Street)	Construct sidewalk along south side of McAndrews Road

The remaining required off-site transportation improvements will be phased in conjunction with the built-out square footage of the development. The applicant will construct the off-site improvements in accordance with the DDA timing provisions (Exhibit RR - DDA Exhibit 3), which triggers specific off-site improvements as building square footage exceeds a target threshold. Northgate Marketplace Phase II does not exceed a target threshold that would trigger any further offsite transportation improvements.

Northgate Marketplace Master Plan (AC-11-076)

Northgate Marketplace On-Site Development Phasing

Per the approved Master Site Plan for Alba Village, applicant proposes to construct the retail development in four phases. Final site plans for phasing may run concurrently as a single application submittal. As permitted by the development agreement, a portion of what was originally identified as Phase III will be constructed as part of Phase II. The balance of the Northgate Marketplace may potentially be finished as a third phase to the project. Identified below is the Master Plan Phasing as identified in 2011.

Master Plan Phasing

Phase	Building	Building Area In Square Feet
1 (NE Quadrant)	A	54,400
	B	19,200
	C	6,000
Phase I Total		79,600
2 (NW Quadrant)	G	6,900
	H	3,000
	J	120,600
	K	18,600
	N	6,400
Phase 2 Total		155,500
3 (SE Quadrant)	D	79,800
	E	17,200
	F	22,300
Phase 3 Total		119,300
	Q	52,000
	R	3,000
Phase 4 Total		61,900
Grand Total Northgate Marketplace		417,500

With the commencement of Phase I, the applicant constructed all four access points into the Northgate Marketplace development. Phase I currently has vehicular access to Central Avenue on the west and to McAndrews Road on the south via a temporary pavement surface that will be removed and replaced with adjacent phases as they are developed.

Phase II Compliance with Northgate Marketplace Master Plan (formerly Alba Village)

Phase I of Northgate Marketplace has been developed in accordance with the approved master plan and subsequent final site plan for Phase I. The subject application for Phase II is very much the same as previously envisioned by the 2011 Master Site Plan (AC-11-076). However, the applicant is proposing some minor amendments to the Master Site Plan for the development via the submitted Master Site Plan, which also

serves as the Final Site Plan for a specific portion of Northgate Marketplace that will be known as Phase II. The proposal demonstrates that the revision does not increase the total amount of building square footage approved in the Northgate Marketplace retail component of Northgate Centre. An exhibit showing the 2011 approved master site plan (AC-11-076) is attached to this report as "Exhibit C". A side by side exhibit of the 2011 Master Plan and the proposed Master Plan is also attached as "Exhibit D". The revised phasing plans for Phase II and Phase III-A, III-B (formerly Phase III and Phase IV) are provided in the table below.

Revised Phase II, III-A, III-B

Phase	Building	Building Area In Square Feet
2 (NW Quadrant)	G	8,500
	H	4,500
	I	7,000
	J	6,500
	K	143,540
	L	5,400
	M	4,000
Phase 2 Total		178,990
3B (SE Quadrant)	D	60,200
	E	12,900
	F	15,500
Phase 3 Total		88,600
3A (SW Quadrant)	N	61,750
	R	6,100
Phase 3A Total		67,850
Grand Total Northgate Marketplace		415,500

Northgate Marketplace Phase II Final Site Plan Design

The Phase II final site plan (Exhibits J & G) demonstrates a total of 178,990 square feet of commercial retail space encompassed by several buildings across the 17.81 acre site. Phase II includes two larger in-line retail shop buildings, the larger building being 90,000 square feet (Building K1) and the smaller being 47,500 square feet (Building K3 & K4), which are bridged together by a smaller building of 6,040 square feet (Building K2). The

final site plan also features six pad sites located in the central portion of the Northgate Marketplace development which surround the development roundabout (Buildings G, H, I, J, L, M). The applicant intends to construct building K1, K2 and K3 in the initial stages of the development of Phase II. Building K4 will be constructed after the other three buildings.

With the submittal of this Final Site Plan application for Northgate Marketplace Phase II, pad site building locations, building footprints and finished building elevations have been provided for review and consideration of SPAC. The subject pad site elevations are consistent with the architecture and materials employed upon of the pad sites within Phase I of the development.

It should be noted that that Northgate Disposition and Development Agreement, allows for smaller pad site buildings, where the general size and location have been submitted for consideration by SPAC as part of a final site plan, to be submitted without detailed architectural elevations. Finished building plans and elevations can be submitted and approved in via a "Stage Three" administrative approval process, as provided within the DDA.

While the Applicant does not suggest future deviations from the submitted building pad site plans or elevations contained within this application, it should be noted that it is possible through the "Stage Three" administrative design review to modify elevations as to address needs of a particular tenant, as long as the architecture is generally consistent with the character of the center. Further information regarding the Stage Three approval process is provided further below.

The main parking field for Phase II is located between the in-line shops on the west and the smaller pad site buildings on the east. There are three points of ingress into the larger Phase II parking field that tie into the internal driveways of the shopping center. A smaller parking field is located between buildings G and H on the east side of the internal access drive. There are two points of ingress into the smaller parking field from the internal vehicular access driveways.

In the 2011 Master Plan, it can be observed that one leg of the roundabout fed directly into the parking field south of the traffic circle. In the 2015 Master Plan revision, one leg of the roundabout has been removed, reducing intersecting driveways from four to three. Removal of one driveway will allow traffic to move through the feature more fluidly and provide safer and more efficient access into the parking fields.

There is one building proposed in Phase II (Building K1) that is larger than 50,000 square feet. This building is subject to the all design features, plaza requirements and building articulation provided by the "Large Retail Development" provisions contained in Section 10.725 of the *Medford Land Development Code*, which is discussed further below.

Buildings K3 and K4 combined are slightly less than 50,000 square feet. Though not subject to the required design features specified by the MLDC, the elevations proposed for these buildings are consistent with the design standards.

The Final Site Plan demonstrates large areas of landscaping along the arterial streets, significantly exceeding the 10 foot required setback. The proposed tree schedule utilizes a blend of deciduous trees, flowering trees and coniferous trees for spring and fall color and to provide year round foliage. The rear façade of buildings containing service area loading docks are generously screened by plant materials. It should be noted that the applicant has placed many plazas in the area surrounding the roundabout, enhancing the node as activity center and focal point for the development. Phase II project landscaping is discussed in detail further below.

MLDC 10.725(C) Large Retail Development Site Design Standards

Building K-1 is proposed to be larger than 50,000 square feet and is subject to all design features contained in *MLDC 10.725 Special Site Design Standards for Large Scale Retail Development*. The design standards include the following:

1) *Buffering*. Where a lot(s) containing a large retail structure or a large retail structure complex abuts land zoned for residential uses and no public street separates the residential zoned land from the lot(s), a Type B bufferyard, as shall be installed.

Not Applicable.

2) *Pedestrian walkways*. Continuous pedestrian walkways shall be provided from the public sidewalk or right-of-way to the principal public entrance(s) of all large retail structures. Walkways shall feature abutting landscaped areas that include trees, shrubs, benches, planting beds, live ground covers, or other such plantings or features for no less than fifty percent (50%) of the length of the walkway.

The final site plan submittal for Phase II complies with this requirement as continuous walkways are provided throughout Phase II linking store fronts to the public right-of-way and future walkways located in subsequent phases. Landscaped areas adjoining the walkways include trees, shrubs, and ground covers for nearly 100 percent of the entire length of development walkways.

3) *Continuous pedestrian walkways on building facade featuring a public entrance*. Such walkways shall be located at least six (6) feet from the building façade, with planting beds for foundation landscaping, except where features such as covered entrances are part of the building facade.

The sidewalk outside of building K1 does have a continuous frontage that is approximately 12 feet in width. However, it does not appear a planter has been included at the base of the building for foundation plantings. As such, staff has included a Condition of Approval (Exhibit A) requiring the applicant to add a six foot wide area for plantings in the central portion of the K1 building. Staff would also recommend that Building K2 also be shifted slightly back to the west approximately three feet as to provide room for additional plantings and to provide a small centrally located plaza along the frontage of the in line shops. This would have the extra benefit of accentuating the corners of the two larger buildings (Building K1 and K3) and provide further building animation along the along the in-line building frontage.

- 4) *Weather protection.* Pedestrian walkways shall include weather protection features such as awnings within thirty feet of all customer entrances.

Though only applicable to Building K1, Building K1, K3 and K4 comply with this standard.

- 5) *Walkway crossing driving surface.* Crossings shall be distinguished from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks or stamped concrete.

The site plan does not specifically identify which type of surface will be applied at the pedestrian crossings. A Condition of Approval (Exhibit A) has been included requiring the submittal of a revised final site plan that identifies which type of decorative paving treatment will be applied at each of the pedestrian crossings on the drive aisles and at crossings through the parking field.

- 6) *Public plaza.* Each large retail structure shall provide a public plaza to attract tenants and to provide the community with a pleasant, comfortable place to rest and interact. Plazas provided by large retail structures shall comply with the following:

- Seating Areas shall be provided in the public plaza and shall be shaded with trees, cloth canopies, or structures over fifty percent (50%) of their area.

Exhibit "K" detailing Plaza G, J, D1& D2 identify plaza areas that are almost fully shaded. The plazas have been further enhanced with a water features to soften noise generated from automobiles, trellis structures, lighting for safety and a fire pit has been included on the south side of building "I". The well designed plazas meet the design requirements and number of plazas required for Phase II.

It should be noted that the location of the plazas are not in immediate vicinity of the building (Building K1) that triggers the public plaza requirement. As such, staff would encourage the inclusion of a small plaza area with seating at the southeast corner of building K1 by slightly shifting Building K2 to the west, providing a small alcove between frontages of the larger buildings. An alcove would have the additional benefit providing additional building animation and visual interest in the long line of building frontage. A Discretionary Condition of Approval (Exhibit A) has been included requiring the applicant to provide a small alcove by moving the Building K2 to the west by approximately 3 feet as to provide a public plaza fronting Building K2. The subject plaza should include textured pavement, seating areas and landscaping in accordance with MLDC Section 10.775(C).

- Live Plantings. A minimum of twenty percent (20%) of the public plaza shall be landscaped with live plantings.

The total area of Plaza G is 9,400 square feet with a landscape area requirement of 1,880 square feet. The total area of Plaza J is 2,800 square feet with a landscape area requirement of 560 square feet. Plaza areas D1 and D2 combined encompass 8,800 square and have landscape requirement of 1,760. The site plan plaza tabulation does not identify a specific square footage of landscaping area within each plaza. However, Exhibit "K" provides a detailed plan view of all four plaza areas that surround the roundabout.

The exhibit demonstrates each plaza has landscaped planter beds that are interspersed throughout and plantings that surround each plaza. The landscape areas of the plazas appear to well exceed the 20% landscaping requirement. The Phase II Final Site Plan complies with the plaza landscaping requirement.

- Each plaza shall have focal points and/or other amenities. The focal points and other amenities shall be constructed of materials that are equal or superior to the principal materials of the building and landscaping. Focal points and/or amenities may include the following:
 - i. An outdoor playground with safe play structures for children;
 - ii. A water feature, clock tower, or similar focal feature;
 - iii. A covered community bulletin board (kiosk);
 - iv. Art works;
 - v. Space for small or temporary food vendors.

As noted above, in addition to being very shaded, plaza areas have been augmented with a water features, trellis structures, lighting features for and

a fire pit on the south side of building "I". The Phase II landscape plan complies with this requirement.

Architecture, Color and Materials

The overall architectural character of the development is expressed by the applicant as "prairie contemporary" with an emphasis on clean, horizontal lines. The buildings consist of a complementary mixture of split face block and smooth face CMU block, brick, timber trellises and accents, standing seam metal roofing and roof parapets, which artistically combine to provide a healthy composition of materials without becoming excessively ornate. The building exterior colors are strongly grounded with earth-toned brick wall panels and column elements. The body color of site buildings include tan, grey, cream and rust colored panels and banding.

Exterior elevations (Exhibit X through DD) of building K1, K2, K3 and K4 have been scaled down by extensive use of varied materials, building massing, building offsets, trellises, varied roof lines, canopies and deep cornices. Roof architecture materials primarily consist of standing seam metal and parapet walls with various detailing elements. The roof architecture is highly diversified, encompassing shed roof elements, large overhangs, and undulating roof heights.

MLDC 10.725(A) Large Retail Development Building Façades

Building K1 is required to comply with the Special Architectural Design Standards for Large Retail Structures (MLDC 10.725), which include the following:

- 1) *One feature from three of the following groups:*

Group	Feature Option	Feature Applied
1	Canopies, awnings, or porticos	✓
	Overhangs	✓
2	Recesses, projections	✓
	Architectural details, such as tile and moldings that are integrated into the building structure and design	
	Transparent windows, display windows	✓
3	Covered walkways	✓
	Arches	
4	Raised corniced parapets over public entrances	✓
	Peaked roofs	✓

Building K1 complies with this requirement.

- 2) *Architectural projections or recesses.* Three percent of the length of the façade and for 20% of the façade length (features incorporated into design at least every 100’).

Building K1 complies with this requirement.

- 3) *Animating Features.* Building facades shall have covered walkways, transparent windows, entrances, awnings, or other such features along at least sixty percent of their length.

Building K1 complies with this requirement.

- 4) *Windows.* When large retail structures contain one or more businesses of less than 50,000 square feet of gross ground floor area with separate exterior entrances, each such business shall have transparent windows along at least fifty percent of the horizontal length of its storefront. The bottom of the windows shall be no more than four feet above the adjacent exterior grade.

Building K1 complies with this requirement.

- 5) *Repeating Elements.* All building facades shall include a repeating element that includes at least three of the following elements, one of which shall repeat horizontally:

Feature	Building A
Color Change	✓
Texture Change	✓
Material Change	✓
Architectural Bays	

Building K1 complies with this requirement.

- 6) *Materials and Colors.* At least seventy-five percent of exterior building materials shall include brick, cedar shakes or shingles, beveled or shiplap siding, or other narrow-course horizontal boards or siding, vertical board and batten siding, durable stucco, rock, stone, or tinted and textured concrete masonry units. Other materials may be permitted if authorized by the approving authority.

Building K1 complies with this requirement.

- 7) *Entrances.* Each large retail structure shall have at least one (1) clearly visible and architecturally prominent public entrance.

Building K1 complies with this requirement.

8) *Roof Features.* Roofs shall have no less than two of the following three features:

Feature	Building A
Parapets	√
Overhanging eaves or cornices	
Sloping roofs (1:3 to 1:1 pitch)	√

The elevations submitted for Building K1 meet all architectural requirements established by *MLDC 10.725* for retail structures over 50,000 square feet.

Pad Site Buildings – Stage Three Review

As noted, specific elevations for all pad sites have been submitted for review and approval. As provided within the approved Disposition and Development Agreement, once a Final Site Plan is approved by SPAC for a project component, the Planning Director may approve specific architectural plans (i.e. elevations, floor plans, material board and roof plan) for individual buildings whose size and location were considered by SPAC, but the architecture was not finalized as part of the Final Site Plan submittal.

Prior to approving architectural plans pursuant to this subsection, the Planning Director shall make a determination that proposed plans are in substantial conformity with the architectural style and materials for the buildings approved by the SPAC in the Final Site Plan approval. The Director shall make the determination within twenty-five (25) working days after the filing of the plans with the Planning Department. The Director's determination of architectural consistency and substantial conformance with the Final Site Plan is Class E, ministerial action.

Should the applicant seek modification to any of pad site building elevation via a "Stage Three Review", notice of the a Planning Director's determination regarding a building elevation submittal in a Stage Three review process will be provided to the applicant and provided to the Commission at the following SPAC public meeting following the determination.

Phase II Landscaping Plan

The Phase II Landscape Plan (Exhibit N) provides detailed representations of the landscaping treatments that will be employed throughout Phase II of the development. The arterial road frontages are landscaped with a variety of ground covers, providing

nearly full surface coverage. Site exterior landscaping areas, in most circumstances, far exceed requirements of the *MLDC*.

An existing circular raised planter bed constructed of brick is located in the northeast portion of Phase II in the center of the roundabout (Phase I construction). As discussed in the section pertaining to public plazas, the roundabout will be augmented by four plaza features containing water features, trellises, benches, tables, lighting and a fire pit along the outside of the feature.

The proposed tree schedule for the development utilizes a blend of deciduous trees, flowering trees for spring and fall color, and coniferous trees to provide year round foliage. The Parks and Recreation Department staff has reviewed the Phase II Landscape Plan and plant materials and recommends that SPAC approve landscape plan as submitted with exception of providing an alternative tree species to the *Fraxinus Americana* for parking lot trees for long term viability and to provide structural soils in parking lot planters to meet soil volume requirements.

Per the development agreement, the requirement for structural soils for smaller parking lot landscaping islands is not applicable to the subject development, as the standard was adopted after 2011. Staff would highly encourage the application of structural soils in some of the smaller landscape islands. A condition of approval has been included requiring the applicant comply with the Parks Department memo recommendation for utilizing an alternative tree species in the parking lot and for the applicant to coordinate with staff to determine a suitable replacement.

The Oregon Department of Transportation also had a comment regarding the street trees fronting Highway 238. While ODOT has no general objection with including landscaping in the Highway 238 right-of-way, it does have objection to the inclusion of "code required" landscape improvements being placed within the right of way. The basis of this concern is that ODOT may need to utilize the subject right-of-way at a point in the future and the landscaping could be eliminated. Another reason is that "required landscaping" placed in the ODOT right-of-way to meet a standard could be considered a gifting by ODOT. As such, staff has included a condition of approval requiring the applicant to comply with the ODOT memo. There are currently 33 trees within the ODOT right of way along highway 238. Many of the trees within the right-of-way will need to be moved onto the developer's property.

Parking

Revised Master Plan

Per the 2011 Master Plan approval (AC-11-076) the applicant received approval for a total of 417,500 square feet of commercial space and provided a total of 1,714 parking

spaces to address their off-site parking needs. This provided an overall shopping center parking ratio of 4.11 spaces per 1,000 square feet.

Per the *MLDC*, the minimum parking ratio for commercial development of 100,000 square feet or greater is 3.3 spaces per 1,000 square feet and the maximum ratio is 5 spaces per 1,000 square feet.

Through this request for revision to the Northgate Marketplace Master Plan and request for Phase II site plan approval, the applicant is proposing an overall reduction in the amount of square footage and parking spaces for the development. The submitted revised master plan demonstrates a total of 415,240 square feet of commercial space and a reduction in parking by 133 spaces, providing a total of 1,581 parking spaces. Therefore, the proposed parking ratio provided for Northgate Marketplace is 3.81 spaces per 1,000 square feet. The minimum required parking for the center overall based on the commercial square footage is 1,370 and the maximum permitted would be 2,076. With the requirement of 3.3 spaces per 1,000 square feet the shopping center would have 211 spaces more than the minimum requirement.

The site plan tabulation suggests the Northgate Marketplace shopping center to be slightly underparked by a total of 32 spaces. The applicant has requested a 7.7% reduction in the number of parking spaces initially approved with the 2011 Master Site Plan, per the provisions of the DDA. However, the total parking required for the center by *MLDC* standards has been conservatively calculated by the applicant by providing a minimum of 3.3 spaces per 1,000 for building K, 3.7 spaces per 1,000 for building A, D and N, 4.5 spaces per 1,000 for smaller buildings under 25,000 square feet and 9 spaces per 1,000 for building "I" as a restaurant use. If calculated in such manner the center would be slightly under parked.

Staff would note, for purposes of comparison that the minimum required parking for an indoor mall is 2.9 spaces per 1,000 square feet and the maximum permitted is 4.0 spaces per 1,000 square feet. At 3.81 spaces per 1,000 square feet, the center parking would be on the higher end. The Code does not provide any definitive manner in what is considered "Commercial Development" and if such a category combines various types of uses such as retail, office and restaurant, or if "Commercial Development" is simply retail uses. The conservative calculation the applicant provides for minimum parking suggests the center overall would be slightly short of the code minimum by 32 spaces. A more liberal interpretation would suggest that the parking for the center exceeds minimum standards by 211 spaces.

To examine the range expressed by the code of 3.3 spaces to 5 spaces per 1,000 for commercial development to determine if center was appropriately parked at 3.81

spaces per 1,000 square feet, staff considered the development utilizing the following assumptions:

- 35% of the in-smaller line shops and pad sites will be occupied as restaurant uses requiring 9 spaces per 1000 square feet.
- Buildings "I" and "J" were calculated at restaurant uses at requiring 9 spaces per 1,000 square feet.
- The balance of the shopping center (379,869 square feet) calculated as retail commercial development, requiring a minimum of 3.3 spaces per 1,000 square feet.

Considering the minimum parking requirement for the center in this manner would yield a required parking of 1,571 spaces. Staff believes the Commission may find that "Commercial Development exceeding 100,000 square feet" to be considered a mix of retail, office, restaurant and other commercial uses and that the required parking for the center to be range from a minimum of 3.3 to a maximum of 5 spaces, as specified by the MLDC. At 1,581 spaces provided the 3.81 spaces per 1,000 square feet falls well within the parameters required expressed by Code.

Phase II, Parking

Phase II demonstrates a total of 715 parking spaces, or approximately 3.99 parking spaces per 1,000 square feet. Per the *MLDC*, the minimum amount of parking required for "Commercial Development" is a minimum of 3.3 spaces per 1,000 square feet of development and a maximum of 5 spaces per 1,000 square feet. Total minimum amount of parking required for Phase II under this calculation is a minimum of parking of 590 parking stalls, thus phase 2 would be over parked by 125 units. Using the same assumptions listed above for Buildings "I" and "J" to be developed with restaurants, Phase II would require 667 total spaces, and would have 48 more spaces required than the minimum. The applicant has provided a total of 20 handicap parking stalls located in spaces closest to the store entries and a total of 72 bicycle parking spaces.

Transit Facilities

Per *MLDC* 10.807, transit improvements shall be provided at the time of development of major developments when the commercial building exceeds 60,000 square feet. In the review of the Alba Village Master Site Plan application in 2011, RVTD requested a bus pull-out on Highway 238 to mitigate traffic safety concerns. A bus pull out lane and shelter was constructed with Phase I on Highway 238, approximately 100 feet east of the north access driveway of the Northgate Marketplace development.

Lighting

The proposed Lighting Plan (Exhibit O) meets the requirements of *MLDC* 10.764(5) for an exterior lighting plan. The plan features 1, 2, 3 and 4 head shoe box lighting features, ranging in height between 12 and 25 feet. Twenty-five foot poles will be evenly distributed in the parking lot areas, while shorter poles will be utilized in the pedestrian pathway areas. Pole lighting will be outfitted with a 45 degree cut-off angle to reduce glare and protect night skies. Additional lighting will be placed throughout the Phase I in the form of wall sconces, recessed down lighting and bollard lighting. Staff has included a discretionary Condition of Approval requiring the applicant coordinate with the Planning Department an remove some of the light poles out of smaller landscape islands so that light blockage does not become an issue as trees mature. Light poles can be mounted on pedestals outside of the landscape islands.

Signage

Four major mulit-tenant identification pylon signs are planned for the Northgate Marketplace development. Phase II contains one of the proposed signs, approximately 200 feet west of the intersection of Highway 238 and the north entry into the center. An exhibit of the pylon signs, approved as part of the initial master plan (AC-11-076) submittal has been attached as Exhibit "II" for informational purposes. The center identification pylon signs demonstrate high architectural quality by utilizing design elements found throughout the project including stone veneer, metal structural elements and a standing seam roof. The proposed signage consistent with the shopping center ground signage requirements permitted by section 10.1700 for height and square footage. The *MLDC* allows one twenty-foot high ground sign per arterial road frontage. Approval of this final site plan for Phase II does not constitute approval of the proposed of any signage. The applicant shall obtain a sign permit for the pylon signs of identified within this application.

Pedestrian and Vehicular Traffic

This project is particularly well-designed for pedestrian access. Each building is linked by a network of pedestrian pathways. Raised curbed concrete walkways are provided for safe pedestrian access through parking fields. Pedestrian crossings at the roundabout have textured paving, and bulb-outs are utilized at crossings. Approximately 24,025 square feet of outdoor plaza space is included in Phase II, which will include benches and bicycle parking, fire pits, trellises, water features, and lighting. Two pedestrian walkways traverse through the center of the Phase II main parking field between Building K and the pad sites adjacent to the internal vehicular collector driveway on the east side.

As noted above, the overall Northgate Marketplace site has four vehicle access points to the surrounding arterial street network. The entrance off of Highway 238 has a right-in, right-out and left-hand turn into the center. The Central Avenue access point has full traffic turning movement. The McAndrews Road access allows for a right-in and right-out, and the existing Ohio Street access point provides a signalized right-in, right-out and forward-through movement to and from Riverside Avenue. The internal circulation pattern is thoughtfully planned out and features a roundabout that takes in traffic from Highway 238 and Court Street. Staff supports the revised roundabout feature, having three legs rather than the initially approved four legs and the revised collector driveway circulation configuration, which provides a more distributed spacing to ingress point into each parking field. It is expected that the proposed changes better internal traffic circulation pattern though the center as a whole.

Concealments

There are no fences proposed with the subject Phase II final site plan. A vegetated landscape screen has also been employed on the back side of shopping center providing a vegetative buffer from the loading areas on the rear side of Building K along Central Avenue and additional landscape buffer to the industrial zoned property immediately to the west. There are several trash receptacles located throughout the Phase II site plan, some within the parking area and some on the back side of Building K. A detailed exhibit of the enclosures has not been provided on the plan, as such, staff has added a Condition of Approval (Exhibit A) requiring the trash receptacles to comply with Section 10.781 of the MLDC.

Storm Drainage

The applicant has submitted a Conceptual Storm Drainage Plan (Exhibit S). The applicant is proposing a large open storm water retention facility along the north and west side of the development. The Public Works report notes that all storm drainage lines shall be outside of the public right-of-way and public utility easements. No other issues were identified by staff.

Committee Comments

No comments were received from a committee such as BPAC.

FINDINGS OF FACT

MLDC 10.290

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the Applicant's Questionnaire and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. The subject revised master plan for Northgate Marketplace and Final Site Plan are consistent with all previous approvals and the MLDC. No Exception is needed for this application.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit B) and recommends the Commission adopt the findings as presented, subject to one finding pertaining to the MLDC code compliance regarding the parking area.

Staff does not believe the site as a whole to be deficient to the required parking specified by the MLDC, and the requesting parking reduction via application of DDA provisions is not needed as to comply with minimum standards. Staff recommends that the Commission find that the 1,581 (3.81 spaces per 1,000) parking spaces sufficient to meet the parking needs for the center and that the parking provided overall fully meets the requirements specified by the MLDC. Staff offers the following information in support of that conclusion:

- The Commission can find that the appropriate interpretation for "Commercial Development", and particularly Commercial Development of more than 100,000 square feet, would consider a include a variety of commercial/retail uses such as offices, medical offices, large and small retail, restaurants, and furniture stores. This would be similar to how the indoor mall parking provides one standard and range of parking for the wide variety of potential uses that are permissible in the C-R zone district.

- Commission may find that small pad sites intended for retail development need not be considered under a higher standard just because they are not attached to a larger structure. However, that uses that are not retail, such as a restaurant, requiring a much higher parking ratio, could be considered under a higher standard. Due to the significant amount of "Commercial Development" (over 100,000 square feet staff recommends that all retail/commercial development in the center, should be based on and aggregate of square footage at 3.3 space per 1,000 minimum requirement, as identified by the MLDC, rather than the various commercial sizes as provided by the site plan tabulation.
- Section on 10.744 of the MLDC allows for the consideration of shared parking calculations. It may be appropriate to consider a lesser parking requirement for restaurant uses in a large center, as uses such as office and some retail operations may have limited hours and be closed during peak operational hours of the restaurant.

Staff recommends that the Commission may find that the 3.81 spaces per 1,000 square feet to be consistent with the Medford Land Development Code and the Northgate Marketplace Master Plan.

RECOMMENDED ACTION

Accept the Applicant's findings as modified by the staff report and adopt a Final Order for approval of AC-15-133 per the report dated November 25, 2015, including Exhibits A through RR.

EXHIBITS

- A Conditions of Approval; dated November 25, 2015
- B Applicant's Narrative & Code Compliance Worksheet, October 1, 2015
- C Approved Master Site Plan, 2011 (AC-11-076)
- D 2011 Master Site Plan & 2015 Proposed Master Site Plan Comparison Exhibit
- E Cover Sheet Northgate Marketplace-Master Site Plan, Phase II; received October 1, 2015
- F Applicant's vicinity map; received October 1, 2015
- G 2015 Proposed Revisions to Master Site Plan and Phase II Final Site Plan including project data, received October 1, 2015
- H Existing Conditions; received October 1, 2015
- I Demo Plan; received October 1, 2015
- J Final Site Plan – Phase II; received October 1, 2015
- K Plaza detail exhibit; received October 1, 2015
- L Drainage Plan; received October 1, 2015
- M Utility Plan; received October 1, 2015
- N Landscape Plan; received October 1, 2015
- O Site Lighting Plan, received October 1, 2015
- P Building G Floor Plan and Roof Plan, received October 1, 2015

- Q Building G Elevations received October 1, 2015
- R Building H Floor Plan and Roof Plan, received October 1, 2015
- S Building H Elevations received October 1, 2015
- T Building I Floor Plan and Roof Plan, received October 1, 2015
- U Building I Elevations received October 1, 2015
- V Building J Floor Plan and Roof Plan, received October 1, 2015
- W Building J Elevations received October 1, 2015
- X Building K1 Floor Plan and Roof Plan, received October 1, 2015
- Y Building K1 Elevations received October 1, 2015
- Z Building K2 Floor Plan and Roof Plan, received October 1, 2015
- AA Building K2 Elevations received October 1, 2015
- BB Building K3 & K4 Floor Plan and Roof Plan, received October 1, 2015
- CC Building K3 & K4 Elevations received October 1, 2015
- DD Building K1 through K4 Composite Front Elevation received October 1, 2015
- EE Building L Floor Plan and Roof Plan, received October 1, 2015
- FF Building L Elevations received October 1, 2015
- GG Building M Floor Plan and Roof Plan, received October 1, 2015
- HH Building M Elevations received October 1, 2015
- II Approved pylon signage (AC-11-076)
- JJ Master Site Plan (AC-11-076) site resources exhibit
- KK Trip Generator Accounting from Mackenzie, received October 2015
- LL Report from Public Works Department, dated November 4, 2015
- MM Memo from Medford Water Commission dated October 28, 2015
- NN Report from Fire Department, prepared October 23, 2015
- OO Interoffice Memo from Parks Planner, dated October 29, 2015
- PP Correspondence from Rogue Valley Sewer Services, dated October 22, 2015
- QQ Correspondence from ODOT, received November 5, 2015
- RR Off-site transportation improvement schedule (Exhibit 3 – DDA)
Vicinity Map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA:

DECEMBER 4, 2015

EXHIBIT A

Northgate Marketplace Phase II Final Site Plan & Master Site Plan Revision –
AC-15-133
Conditions of Approval
November 25, 2015

CODE REQUIREMENTS

1. The applicant shall comply with all applicable conditions of approval contained in application CP-06-065, ZC-06-066, CP-11-041, ZC-11-042, AC-11-076 and conditions provided by the Amended Disposition and Development Agreement approved August 18, 2011 (Ord. # 2011-163)
2. The applicant shall:
 - a. Comply with the Public Works Department Staff Report, dated November 4 2015; (Exhibit LL);
 - b. Comply with the Medford Water Commission Memorandum dated October 28 2015; (Exhibit MM);
 - c. Comply with the Fire Department Report, prepared October 23, 2015. (Exhibit NN);
 - e. Comply with the E-mail Correspondence received from ODOT, dated November 5, 2015; (Exhibit QQ);
 - f. Provide a revised Landscape Plan with the building permit plan submittal that demonstrates all required street frontage landscaping required by the MLDC along Highway 238 (Rossanley Dr.) is located south of the right-of-way, on the applicant's property.
 - g. Provide a revised site plan with the building permit plan submittal that provides a detail showing the receptacle enclosures are consistent with the design standards specified in MLDC 10.721
 - h. Provide a revised site plan and landscaping plan with the building permit plan submittal that demonstrates a 6-foot wide landscaped planter strip, adjacent to the K1 building frontage, in central portion of the structure to comply with MLDC 10.725(C)
 - i. Provide a revised site plan and landscaping plan with the building permit plan submittal that demonstrates that each pedestrian crossing utilizes a change of material to differentiate the pedestrian crossing from the asphalt driving lanes in conformance with section 10.775.

EXHIBIT A

Northgate Marketplace Phase II Final Site Plan & Master Site Plan Revision –
AC-15-133
Conditions of Approval
November 25, 2015

DISCRETIONARY CONDITIONS

3. The bicycle racks shall consist of the “inverted U” style or “post and ring” style as recommended by the Bicycle and Pedestrian Committee. The applicant shall provide a revised site plan with the building permit application that provides a bicycle rack detail consistent with this recommendation.
4. Coordinate with the Parks Department to determine a suitable replacement for Fraxinus Americana and submit a revised Landscaping Plan with the building permit submittal with alternative plant material.
5. Provide a revised Phase II Lighting Plan and Site Plan with the building permit plan submittal that removes parking lot lighting from the smaller planter islands and places such equipment into the asphalt area on pedestals.
6. The applicant shall provide a revised site plan and landscape plan with the submittal for building permit that demonstrates a small alcove fronting Building K2 by moving the Building K2 to the west by approximately 3, and provideing public plaza fronting Building K2 that include textured pavement, seating areas and landscaping in accordance with MLDC Section 10.775(C).

“A”

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Northgate Marketplace

Phase II

RECEIVED
OCTOBER 1, 2015

Commercial Retail
Medford, Oregon

PLANNING DEPARTMENT

An Application For:
Site Plan and Architectural Review

Submitted October 1, 2015

Applicant:
Regency Centers, L.P.
5335 SW Meadows
Suite 295
Lake Oswego, OR 97035
Contact: Craig Ramey
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Prepared by:
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CITY OF MEDFORD
EXHIBIT # 5311
File # AC-15-133

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EXHIBITS

- A. SPAC Application Form
- B. Applicant Questionnaire/Narrative
- C. Plan Set (civil, architectural, landscape)
- D. Trip Generation Accounting Letter
- E. Legal Description of Project Site (title)
- F. Mailing Labels

B

GENERAL INFORMATION

Applicant: **Regency Centers**
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Portland, Oregon 97221
Phone: (503) 419-2500
Fax: (503) 419-2600
Contact: Kevin Brady
Kevin.Brady@cardno.com

Tax Lot Information: Map Tax Lot
372W24 800, 700

Location: Rossanley Drive (Highway 238), N. Central Avenue
and McAndrews Road

Current Zoning District: Regional Commercial (C-R)

Project Site Area: 17.81 acres

I. INTRODUCTION

SUMMARY OF PROPOSAL

The applicant requests Site Plan and Architectural Committee review and approval of Site Plan for Phase 2 of Alba Village, now known as Northgate Marketplace. The project is located at the north corner of the intersection of McAndrews Road and North Central Avenue on a 17.81 acre site, within the C-R (Regional Commercial) zone district, within the Alba Village Commercial Center at Northgate Centre. The proposal includes 7 new commercial retail buildings with associated parking and landscaping. Building K.4 will be constructed in the future, with substantial conformance with the proposed elevation in this application. The proposal also includes a network of pedestrian facilities, including plazas and open space areas.

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PROJECT LIMITS AND SURROUNDING USES

The following information, tables and figure describe and identify the project site's zoning designation and the zoning and land uses on surrounding properties.

Table A: SUBJECT SITE CHARACTERISTICS

Zoning	C-R	Regional Commercial
GLUP	CM	Commercial
Use	Vacant	N/A

Table B: SURROUNDING LAND USE

Location	Zoning Designation	Land Use
North	C-R	Vacant
East	C-R, C-C	Alba Village, General Retail
West	I-G/GI	Vacant
South	I-H	Alba Village (vacant)

PROJECT HISTORY

The subject application is the second phase submitted for the original Alba Village Master Planned commercial development, which is one component of the Northgate Centre development. The Northgate Centre development consists of 84.07 net acres located upon three of the four quadrants of the intersection of State Highways 238, 62 and 99. The project site was once home to the Medco Lumber Mill. Alba Village is located within the C-R (Regional Commercial) zoning district. Phase I has already been approved and is currently occupied with smaller retail shops and anchor stores. This application is considered Phase 2 of Alba Village, now referred to as Northgate Marketplace. The other components of Northgate Center are: Northgate Business Park, located in both 1-G (General Industrial) and 1-I (Light Industrial) zone districts; and the Northgate Office Park, located within 1-I and C-S/P (Service Commercial/Professional Office) zone districts.

The Alba Village component of Northgate Centre is located within the southeast portion of the Northgate Centre on the southeast corner of Highway 62 and Central Avenue. Alba Village was originally designed as a 471,500 square foot lifestyle center containing a mix of small and medium sized boutique retail shops. In 2011, City Council and Planning Commission approved removal of the specific concept site plan for the retail center and specific architectural design requirements that were conditioned as part of the 2006 Zone Change and Comprehensive Plan amendment approvals. The Site Plan and Architectural Commission held a public hearing on the Alba Village Master Site Plan (AC-11-076) on August 19, 2011, and issued a Final Order for approval of the Master Site Plan on September 2, 2011.

The Northgate Centre development is limited to a maximum amount of building square footage as a condition of approval for the Comprehensive Plan Amendment and Zone Change applications. The building square footage limitation has the effect of placing a traffic generation "trip cap" to address transportation system facility adequacy issues identified in the traffic impact analysis (TIA), which is part of the approved Comprehensive Plan Amendment and Zone Change applications.

Component	Location	Zone District	Use	Acres	Building Square Footage
Alba Village	SE Portion	C-R	Mixed Retail	40.05	471,500

On March 1, 2006, a Comprehensive Plan Map Amendment request {CP-06-065}, for amending the General Land Use Plan (GLUP) map from H-1 (Heavy Industrial), G-1 (General Industrial) and C-M (Commercial) designations to the H-1 (Heavy Industrial), G-1 (General Industrial), C-M (Commercial) and SC (Service Commercial) designation was submitted concurrently with a Zone Change application. The Planning Commission forwarded a favorable recommendation on the Comprehensive Plan Amendment to the City Council. The Planning Commission also found that the zone change application met the required criteria of the Land Development Code, subject to conditions of approval. The Final Order for Zone Change ZC-06-066 was approved November 9, 2006, subject to Council approval of the GLUP Map Amendment.

The Medford City Council at its December 21, 2006, meeting passed Ordinance No. 2006-264, subject to conditions, approving CP-06-065, a minor amendment of the Comprehensive Plan. The City Council also adopted Ordinance No. 2006-296, an ordinance affirming the decision of the Planning Commission regarding the Northgate Zone Change application (ZC-06-066). The applicants entered into a Disposition and Development Agreement (DOA) with the City of Medford on November 6, 2008, which addressed off-site infrastructure requirements and the approval process for Master Site Planning and Final Site Planning approvals.

Regency Centers submitted development applications (CP-11-041, ZC-11-042) on March 15, 2011, to amend the conditions of approval associated with the 2006 Comprehensive Plan and Zone Change approvals of the Northgate Centre Development (CP-06-065 & ZC-06-066 respectively). In summary, the Comprehensive Plan Amendment application and the associated Zone Change amendment requested the following: 1) allow for the off-site traffic mitigation improvements to be phased in conjunction with the built-out square footage of the development; 2) remove requirements for specific architectural features and design themes as a condition of Council approval of the Concept Plan; and 3) allow for flexibility for future project design revisions to be brought forward as design review applications to the Site Plan and Architectural Commission (SPAC) rather than through an amendment to the original Comprehensive Plan Amendment (CP-06-065), which is heard by the City Council.

The Planning Commission held a public hearing on June 9, 2011, on the consolidated application and recommended approval to the City Council regarding the Comprehensive Plan Amendment application and approved of the zoning amendment application, subject to Council approval of the Comprehensive Plan Amendment application (CP-11-041). On July 9, 2011, the City Council approved application CP-11-041 (Ordinance No. 2011-122), changing the conditions of approval for the Northgate Centre development. The Planning Commission approved the Final Order changing the conditions of approval regarding the zoning amendment application on July 14,

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2011. On August 18, 2011, the applicant received Council approval on an Amended Disposition and Development Agreement, which incorporated the new conditions of approval, consistent with the approval of CP-11-041 and ZC-11-042 (Ordinance No. 2011-163). There were no other substantive changes proposed for the DOA. Per the 2006 Comprehensive Plan Amendment and Zone Change application approvals, as amended, a SPAC approval of a Master Site Plan for each component of the Northgate Centre was required prior to a submittal of a final site plan for the respective portion. A master site plan for the overall Alba Village retail component was considered, and approved by the Site Plan and Architectural Commission on September 2, 2011. On October 11, 2011, the Site Plan and Architectural Commission approved a final site plan for Phase I of Alba Village (AC-11-093).

COMPLIANCE WITH MASTER PLAN AND PREVIOUS APPROVALS

Phase I of Alba Village is currently developed and occupied with smaller retail shops and anchor stores, such as Trader Joes and REI. The subject application is for Phase 2 of Alba Village, and the site plan area and building layout are essentially the same as previously envisioned by the 2011 Alba Village Master Plan for the southwest quadrant. The applicant proposes to modify the 2011 Alba Village Master Plan with the enclosed conceptual Master Site Plan, as well as establish a Final Site Plan for a specific portion of Alba Village, now identified as Phase 2. An overall Master Site Plan (Exhibit C, Sheet C1.1) for the entire Alba Village site has been submitted as part of this SPAC application, indicating the general building configurations and traffic patterns for the balance of the Alba Village site. This proposed Master Site Plan also demonstrates that the proposal does not increase the total amount of building square footage approved in the original Alba Village retail component of the Northgate Centre (CP-06-065 and ZC-06-066). The total building square footage approved in the original Alba Village Master Plan is 417,500 square feet, while the building square footage proposed in this Master Site Plan is 415,240 square feet. In addition, the trip cap vested in the approval for Northgate Centre was 1,882 average daily trips, while the proposed trip generation under the proposed Master Site Plan is 723 total trips. Based on building configuration and square footage, circulation efficiency, and trip generation, the proposed Master Site Plan complies with the 2011 Alba Village Master Plan and the requirements of the original Zone Change and Comprehensive Plan Amendment for Northgate Centre (CP-06-065 and ZC-06-066).

II. CITY OF MEDFORD MUNICIPAL CODE

The applicable City of Medford Municipal Code are set forth below along with findings demonstrating the project's consistency with these provisions.

CHAPTER 10 LAND DEVELOPMENT CODE

10.290 Site Plan and Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

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- (1) **The proposed development is compatible with uses and development that exist on adjacent land, and**

Response: The narrative below addresses all of the items listed in the Applicant's Questionnaire on page 8 of the SPAC application form (Section II – Compatibility: Criterion #1). Responses to these items demonstrate how the project is compatible with uses and development that exist on adjacent land:

- A. List existing uses and development adjacent to your project site. Along with this list, describe the architecture (materials, colors, etc.), age, and condition of the adjacent buildings (you may use photographs to supplement this information).**

Response: The proposed site and development plan is considered Phase II of the approved Master Plan for Alba Village Northgate Marketplace, which planned for large and medium format retail tenants. The intent is to use architectural materials, colors and approaches for Phase 2 that are compatible with the existing development in Phase I. Most other areas adjacent to the site are vacant.

- B. Describe the building architecture and exterior treatments in your proposal, and how they fit with and complement adjacent buildings and development.**

Response: The proposed style of architecture is a continuation of the Phase I theme. The character is described as "prairie contemporary" with an emphasis on clean, horizontal lines. The buildings consist of a complementary mixture of split face block, brick, corrugated metal siding, timber trellises and accents, and standing seam metal roofing, which artistically combine to provide a healthy composition of materials without becoming excessively ornate. The building exterior colors are strongly grounded with earth-toned brick wall panels and column elements. The body color of site buildings include tan, gray, cream and rust colored panels and banding.

- C. Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass. The Site Plan and Architectural Commission Design Guidelines are a helpful reference, and can be found on the City's website, and at the Planning Department.**

Response: As demonstrated in our architectural drawing package a number of architectural elements and exterior treatments have been use to break up large facades and give relief to building mass. We have incorporated pilasters, columns, changes of materials and colors, trellises and arcades and raised planting beds and landscaping.

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All of these techniques have been employed to provide the greatest amount of relief to the proposed building. Exterior elevations have been scaled down by extensive use of varied materials, building massing, building offsets, trellises, varied roof lines, canopies and deep cornices. Roof architecture materials primarily consist of standing seam metal parapet walls and metal accents. The roof architecture is highly diversified having shed roof elements, large overhangs, and undulating parapet walls.

- D. Describe how the placement and orientation of the proposed building(s) relate(s) to the street facilities, and how this orientation promotes a more pedestrian-friendly site design.**
- a. If the site lies within 600-feet of an existing or planned transit stop, as designated by the Transportation System Plan (TSP), describe compliance with the standards of Section 10.808, New Commercial and Institutional Development.**

Response: The placement and orientation of the buildings create a pedestrian-friendly environment which connects to the existing phase I and plans for future connections to phases III and IV. Larger scale buildings have been grouped on the west side of the site with their primary entrances oriented towards the pedestrian pathways. Loading areas for these buildings are screened from view and are heavily landscaped from the public streets. Numerous public plazas have been provided and located throughout the site. The main plaza is off of the primary roundabout and is the focal point of the site. Smaller single tenant and multiple tenant shop buildings are placed along the inner road network. Each building has adjacent outdoor areas, either for dining or sitting, and each building is linked to the pedestrian network of walkways and paths. In all, there is easy access from anywhere on the site to either an outdoor plaza or pedestrian walk. The existing bus shelter on Rossanley Drive adjacent to the site is easily accessible to all proposed buildings on the site through both public and private sidewalks.

- E. Describe the pedestrian facilities and amenities on your site (useable outdoor space, benches, etc.), and how they will function for pedestrians.**

Response: The Phase 2 Site Plan for the Northgate Marketplace proposal includes a strong and extensive network of pedestrian sidewalks, with various plaza areas interspersed within the network. This network provides complete connectivity within the site, as well as those areas connected with the public street system (see Exhibit C, Sheet C3.0). In addition, a main plaza area (Plaza G) is proposed near the central traffic circle and in between Buildings I and J, thereby providing a centrally focused gathering area large enough to accommodate a relatively large number of customers and shoppers. In addition, smaller pedestrian plazas are also located near the central traffic circle.

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This focused approach to plaza space provides greater connection among pedestrian walkways, buildings and the adjacent public street system. These plaza areas include such amenities as trees for shading, benches for sitting and resting, water features to soften the noise associated with auto traffic, fire pits for ambience and warmth and lighting for safety and ambience. Plaza areas are more specifically and graphically described in Exhibit 3, Sheet C3.1. Finally, nearly the entire pedestrian network is lined with landscape buffer strips and low-level lighting to provide greater safety for pedestrians, and also to soften the look and feel of pedestrian walkways.

F. Describe vehicle and pedestrian access to the site, and how it relates internally on the site, and to adjacent sites.

Response: All modes of transportation arriving to Northgate Marketplace will access the property from either Central Avenue, Rossanley Drive (Highway 238), Ohio Street or McAndrews Road before entering one of the main shopping center private roadways. See Exhibit 3, Sheet C3.0. Full pedestrian access from McAndrews Road will be fully available once Phase 3 is developed. No direct access to adjacent sites is proposed, though Phase 3 will include continuation of the internal private roadway and driveway system. Vehicle and pedestrian access through Roassanley Drive and Ohio Street intersect at a traffic circle that provides safe and efficient flow of vehicles entering and leaving Northgate Marketplace. Direct access to the site is also available from Central Avenue and McAndrews Road, where intersections have been constructed under current City standards. After entering the site through any one of the main access points from the public street system, one can access all of the buildings and amenities at Northgate Marketplace through the internal private roadway system. In addition, the extensive pedestrian network also provides internal access within the site, connecting all parking areas, buildings and plaza areas. This internal private roadway system and pedestrian network provides for a safe and efficient flow of vehicle and pedestrian movement. See Exhibit C, Sheet 3.1.

G. Describe if and how the proposed plan is sensitive to retaining any existing trees or significant native vegetation on the site. Should existing trees be preserved, a Tree Protection Plan shall also be included in this application.

Response: Site constraints have not allowed for the retention of any existing trees.

H. Describe stormwater detention facilities on the site (underground storage, surface pond, etc.). If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.

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Response: Stormwater runoff will be treated for the removal of pollutants and detained for peak runoff rate flow control. Stormwater quality treatment and detention will be provided through the use of aboveground and below ground treatment and detention facilities. More than half of the site will be treated by aboveground, vegetated facilities, with the remainder being treated underground. Six aboveground areas have been set aside for treatment and detention of the stormwater runoff. There will be two large water quality pond providing the primary surface treatment and detention for the site. The western water quality facility will have an open space of 14,000 square feet and treat 10,000 cubic feet. The Northern facility has an open space of 20,000 sf and treat 18,000 cubic feet. Three interior landscape islands will contain water quality facilities totaling more than 3,000 square feet of treatment and 6,000 square feet of open space, respectively. An additional 4,000 square foot water quality facility is located in the east corner of the property. The total area of this location exceeds more than 40,000 square feet of open space and will include water quality treatment swales and an extended detention basin. The site has a parcel size of 17.81 acres, or 601,564 square feet.

The open space set aside to be developed as stormwater treatment facilities is greater than 6-percent of the project gross area. The treatment facilities will be designed to meet the Rogue Valley Stormwater Quality Design Manual performance standards. The treatment and detention areas will be planted as vegetated swales per the Rogue Valley Stormwater Quality Design Manual. Plants in these areas will be able to handle occasional inundation. The larger pond area will be seeded with the appropriate detention pond seed mix. Additional required treatment and detention will be achieved through underground facilities. A flow control manhole will be installed to provide all low flow storms to proceed directly into the surface water quality facilities. Any larger storm events with too much capacity for the surface water quality facilities will bypass into a water quality manhole followed by a detention facility.

I. Describe how your proposed landscaping design will enhance the building and other functions on the site.

Response: All the pedestrian walkways leading to the building entrances have adjacent landscaping consisting of trees, shrubs, perennials and grasses to buffer the adjacent drive aisles and parking. Plantings along the building facade work with the building design elements (i.e. benches and planters) to enhance the pedestrian experience. See Exhibit C, Sheet L1.0.

J. Describe how your exterior lighting illuminates the site, and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of Section 10.764.

Response: A full Illumination Plan has been submitted as part of this application (Exhibit C, Sheet IL1.0). As indicated in the Illumination Plan, the applicant proposes the use of full cut off fixtures for all the exterior lighting, thereby reducing glare on adjacent properties and illumination into the night sky. The plan is based on the requirements of section 10.764, with the applicant's intention of submitting final Illumination Plan documents with building permit applications to further demonstrate compliance.

K. Describe any proposed signage, and how it will identify the location of the occupant and serve as an attractive complement to the site.

Response: The applicant is proposing to install a new tenant identification sign at Roassanley Drive (Highway 238) and the main access roadway, near Building I. See Exhibit C, Sheet 3.0. None of the signs will be internally illuminated, though there will be LED fixtures installed to illuminate the signage during business and night-time hours.

L. Explain any proposed fencing, including its purpose, and how you have incorporated it as a functional, attractive component of your development. (See Sections 10.731-10.733).

Response: No fencing is proposed as a part of this application.

M. Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of Sections 10.752-10.761.

Response: The proposed retail uses do not make any more noise than any typical retail user. The greatest potential impact areas related to operation are those activities related to the loading areas. The large loading areas are located behind Buildings K1 through K4. These locations are at a lower elevation from the adjacent roadway (Central Avenue) and the zoning across Central Avenue is Industrial. In addition, an extensive landscape buffer is proposed along the entire west portion of the site, per the Development Agreement. These loading area locations also allow the Buildings K1-K4 to act as a sound barrier to other portions of the site.

O. List and explain any exceptions or modifications requested and provide reasons for such.

Response: No exceptions or modifications are being requested as part of this proposal.

- P. Section 10.7BO(C)(2). List any petition for relief of landscaping standards (i.e., request an increase in turf area at a facility for active recreation: eliminate requirement for root barriers when trees are planted in structural soils). Provide rationale for requested deviation from standard.

Response: No petition for relief of the landscape standards is being requested.

- (2) The proposed development complies with the applicable provisions of all city ordinances, or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC Section 10.253. [Amd. Sec. 44, Ord. No. 7659, June 2, 1994; Amd. Sec. 3, Ord. No. 2005-276, Dec. 1, 2005.]

Response: All applicable ordinances of the Medford Land Development Code are addressed below in Article 5. The table below is a summary of the applicable development standards which are also addressed separately in the Site Plan and Architectural Review application form on pages 10 and 11. Applicable provisions are further addressed more specifically below in the specific Code sections.

Project Site		
	Proposed	
Zoning District	No change	
Overlay District(s)	N/A	
Proposed Use	Retail	
Project Site Acreage	17.81	
Site Acreage (+ Right of Way)	17.81	
# Employees	120	
Structures		
	Existing	Proposed
# of Structures	0	7
Structure Square Footage (10.710-10.721)	0	178,990
Site Design Standards		
	Proposed	Required
Front Yard Setbacks (10.710-10.721)	>10 FT	10 FT
Side Yard Setbacks (10.710-10.721)	>20 FT	>20 FT
Rear Yard Setbacks (10.710-10.721)	>20 FT	>20 FT
Lot Coverage (10.710-10.721)	23%	40%

Parking		
	Proposed	Required
Regular Vehicular Spaces (10.743)	715	664
Disable Person Vehicular Spaces (10.746)	20	20
Carpool/Vanpool Spaces (10.809)	12	10% employee parking
Total Spaces (10.743)	715	664
Bicycle Spaces (10.748)	72	72
Loading Berths (10.742)	4 Type A	4 Type B
Landscaping		
	Proposed	Required
Total Landscape Area (SF)	174,220 SF	109,000 SF
Total % Landscape Coverage	22.4%	14.05%
Structure		
	Proposed	
Materials	Architectural Panels (multiple profiles and colors) and difference colors and finishes of Architectural CMU	
Colors	Earth tone colors (browns and beiges)	

ARTICLE 5 SITE DEVELOPMENT STANDARDS

10.705 Building Height

- A. Calculation.** Building height shall be determined by measuring the vertical distance from the average contact ground level at the front wall of the building to the highest point of the roof surface for flat roofs; to the deck line of mansard roofs; and to the average height between eaves and ridge for gable, hip and gambrel roofs.

Response: The maximum proposed building height is 44 feet and the maximum allowed building height is 85 feet. The site is not within 150 of residential zoning districts, nor a Special Are Plan Designation. Therefore, the proposal complies with this standard.

10.706 Maximum Coverage Calculation

The maximum lot coverage shall be determined by multiplying the lot area by the zoning district coverage factor (the percent of the lot area that may be covered by structures).

Response: The proposed lot coverage for Phase 2 is 23.1% and the maximum allowed area is 40%, therefore, the proposal complies with this standard.

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10.721 Commercial and Industrial Site Development Standards

The following standards apply to commercial and industrial development. See Article III, Sections 10.326 through 10.332 for detailed descriptions of each zoning district, and Section 10.337 for conditional, special, and permitted uses.

COMMERCIAL AND INDUSTRIAL DEVELOPMENT								
DEVELOPMENT STANDARDS	C-N	C-S/P	C-C	C-R	C-H	I-L	I-G	I-H
MINIMUM & MAXIMUM AREA FOR ZONING DISTRICT (ACRES)	0.5 - 3.0	None						
MINIMUM LOT AREA (SQUARE FEET)	7,000	15,000		7,000	20,000	10,000		
MAXIMUM COVERAGE FACTOR (See 10.706)	30%	40%		60%	50%	90%		
MINIMUM LOT WIDTH	70 feet							
MINIMUM LOT DEPTH	100 feet							
MINIMUM LOT FRONTAGE	70 feet	30 feet	70 feet		30 feet	70 feet		
MINIMUM FRONT & STREET SIDE YARD BUILDING SETBACK	10 feet EXCEPT 20 feet for vehicular entrances to garages or carports							
MINIMUM SIDE AND REAR YARD BUILDING SETBACK	None EXCEPT 1/2 foot for each foot in building height over 20 feet							

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MAXIMUM BUILDING HEIGHT (See 10.705)	35 feet	85 feet EXCEPT 35 feet if structure is within 150 feet of a residential zoning district boundary or Special Area Plan designation.	35 feet	85 feet EXCEPT 35 feet if structure is within 150 feet of a residential zoning district boundary or Special Area Plan designation
MAXIMUM GROSS FLOOR AREA PER BUSINESS (SQUARE FEET) EXCEPT as noted in 10.337	2,500	None	50,000	None
PERMITTED OUTDOOR USES	See Note 1	See Note 2		See Note 3

Response: All of the standards indicated in the table above are met. The lot dimensions for the proposed Phase 2 boundaries meet the minimum lot area, depth and width standards. There are no setbacks proposed for less than 20 feet and no building heights over 85 feet. See Exhibit C, Plan Set and Exhibit A, Application Form.

10.722 Large Retail Structures, Purpose

- (1) The purpose of Sections 10.722 through 10.725 is to establish development standards for large retail structures that result in
 - (a) greater visual design interest; and
 - (b) pedestrian-oriented site design.
- (2) The approving authority has the discretion to allow modifications to the standards set forth in Section 10.725 if the approving authority adopts findings to support that any proposed modifications satisfy subsections (1)(a) and (1)(b) above.

Response: The development proposal for Northgate Marketplace (Phase 2 of Alba Village) incorporates a focus on both visual quality of elevations of proposed buildings, as well as a strong pedestrian network with associated facilities, such as plazas and open areas. These areas of focus are discussed in detail below in 10.725, Special Development Standards for Large Retail Buildings.

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10.724 Large Retail Structures, Applicability

- A. The requirements of Sections 10.722 through 10.725 shall apply to:**
- (1) New construction of a retail structure as defined herein, the footprint of which is at least 50,000 square feet; and,**
 - (2) A large retail structure created by construction which represents a fifty percent (50%) or greater increase in building footprint and results in a footprint of at least 50,000 square feet.**
 - (3) A large retail structure comprised of two (2) or more abutting buildings which have a common wall, roof or foundation, and whose combined footprint is at least 50,000 square feet.**
 - (4) A large retail structure comprised of two (2) or more abutting and structurally independent buildings which appear to be a single building and whose combined footprint is at least 50,000 square feet.**

Response: Building K.1 and Buildings K.1 through K.4 constitute a building over 50,000 square feet. Therefore, the standards for Large Retail Structures applies to this proposal, and those standards are addressed below.

- B. When overlay zoning district provisions differ from the standards of Section 10.722 through 10.725, the overlay zoning district provisions shall take precedence.**

Response: There are no overlay zones associated with this site, therefore, this section does not apply.

10.725 Large Retail Structures, Special Development Standards

A. Building Facades.

- (1) Architectural Projections or Recesses. With the exception of building facades less than one hundred seventy-five (175) feet long with off-street truck loading facilities, abutting building facades separated by a service drive, and facades abutting service drives and not visible from any public or private street, building facades greater than one-hundred (100) feet in length measured horizontally shall include architectural projections or recesses having a depth of at least three percent (3%) of the length of the façade and extending for at least twenty percent (20%) of the length of the façade. Such architectural features shall be incorporated into building façade design at least every one-hundred (100) horizontal feet. (See Figure 10.725-1)**

Response: The K.1 building is the only structure that falls into this category. There are two facades, north and east, which are greater than 175 feet in length. The north façade is 214 feet long.

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An architectural projection with a length of 43 feet (20%) extends away from the building by 7 feet (3%) and consists of brick pilasters and a projecting metal canopy with integrated tie rods and tie backs. Additional architectural features on this façade include protruding brick pilasters and horizontal brick detailing. The east façade is 407 feet long. It consists of two major entrance features which both project from the building face by 11 feet and comply with façade projection requirements. The first entry element consists of a painted metal "goalpost" structure with industrial detail features surrounded by brick pilasters. The second entry tower is a gable element with heavy timber truss framing members.

- (2) **Animating Features.** With the exception of building facades with off-street truck loading facilities and abutting building facades separated by a service drive, building facades visible from planned or existing streets, plazas or parking shall have covered walkways, transparent windows, entrances, awnings, or other such features along at least sixty percent (60%) of their horizontal length. (See Figure 10.725-2) The design of building facades visible from planned or existing streets, plazas or parking, and those with public entrances, shall contain at least one (1) feature from a minimum of three (3) of the following groups:

- Group 1
 - (a) Canopies, awnings, or porticos
 - (b) Overhangs
- Group 2
 - (a) Recesses, projections
 - (b) Architectural details, such as tile and moldings that are integrated into the building structure and design
 - (c) Transparent windows, display windows
- Group 3
 - (a) Covered walkways
 - (b) Arches
- Group 4
 - (a) Raised corniced parapets over public entrances
 - (b) Peaked roofs

Response: All proposed buildings comply with the animating features standards through a variety of elements including transparent windows, awnings, entries, trellises, and raised parapets. See Exhibit 3, Sheets A1.0 through A7.0.

- (3) **Windows.** When large retail structures contain (1) or more businesses of less than 50,000 square feet of gross ground floor area with separate exterior entrances, each such business shall have transparent windows along at least fifty percent (50%) of the horizontal length of its storefront. The bottom of the windows shall be no more than four (4) feet above the adjacent exterior grade.

Response: All proposed buildings under 10,000 square feet comply with the minimum window glazing along the length of the storefront. Tenant K.4 is 25,000 square feet and also complies with the code. Tenant K.3 which is 22,500 square feet has a frontage based on a proposed tenant interior prototype.

The glazing length is 50 feet which is 42% of the building frontage of 120 feet. All of these windows comply with the minimum sill height. See Exhibit C, Sheets A.1.0 through A.7.0.

- (4) **Repeating Elements.** All building facades shall include a repeating element that includes at least three (3) of the following elements, one of which shall repeat horizontally:
- (a) Color change
 - (b) Texture change
 - (c) Material change
 - (d) Architectural or structural bays provided through a change in plane of at least twelve (12) inches in width, such as an offset, reveal or projecting rib. (See Figure 10.725-3.)

Response: All proposed buildings comply with the repeating elements standards, including variations on building plane, material, texture and color. See Exhibit C, Sheets A.1.0 through A.7.0.

- (5) **Materials and Colors.**
- (a) At least seventy-five percent (75%) of exterior building materials shall include brick, cedar shakes or shingles, beveled or shiplap siding, or other narrow-course horizontal boards or siding, vertical board and batten siding, durable stucco, rock, stone, or tinted and textured concrete masonry units. Other materials may be permitted if authorized by the approving authority.

Response: Split face masonry units and the use of several façade colors strengthen the visual interest of the K building structure. Additional materials include various colors of stucco and brick and stone veneers. The smaller tenant buildings incorporate the same building elements. The subject application complies with this requirement. See Exhibit C, Sheets A.1.0 through A.7.0.

- (b) Exterior building materials shall not include: smooth-faced concrete masonry units (except for limited accent purposes); smooth-faced tilt-up concrete panels; or unarticulated board siding (e.g., T1-11 siding, plain plywood, sheet pressboard).

Response: Overall architectural character of the development could be described as modernist with strong contemporary influences. The building features clean lines with an emphasis on function, which emphasize the underlying purpose of the building. However, green building is also component of contemporary style with emphasis on energy efficiency and sustainable, natural, and recycled materials to create environmentally responsible buildings. The intention is to construct buildings which are energy efficient and environmentally responsible with recycled materials used through the building. Split face masonry unit panels, the use of several façade colors have been included in the design which provide visual interest.

Exterior elevations have been scaled down by use of varied materials, subtle building offsets, trellises, and large canopies and tower elements over the primary entries. The roof architecture provides varying roof heights, parapet walls and overhangs. See Exhibit C, Sheets A.1.0 through A.7.0.

(6) **Entrances.**

Each large retail structure shall have at least one (1) clearly visible and architecturally prominent public entrance.

Response: Each large retail structure, including Buildings K.1 and K.4, include clearly visible and prominent public entrances, therefore the proposal complies with this standard. See Exhibit C, Sheets A.1.0 through A.7.0.

B. **Roof Features.**

Roofs shall have no less than two (2) of the following features:

- (1) Parapets;
- (2) Overhanging eaves or cornices, extending at least three (3) feet past the supporting walls;
- (3) Sloping roofs with a roof plane of between 1:3 and 1:1 slope.

Response: Parapets and overhanging eaves or cornices are included in all buildings, therefore the proposal complies with this standard. See Exhibit C, Sheets A.1.0 through A.7.0.

C. **Site Design.**

Response: Proposed Building K.1 is proposed to be larger than 50,000 square feet and is subject to all design features contained in MLDC 10.725 Special Site Design Standards for Large Scale Retail Development.

- (1) **Buffering.** Where a lot(s) containing a large retail structure or a large retail structure complex abuts land zoned for residential uses and no public street separates the residential zoned land from the lot(s), a Type B bufferyard, as described in Section 10.790.E, shall be installed between the lot(s) and residential zoned land.

Response: There is no land abutting the site that is zoned residential, therefore, this standard does not apply.

- (2) **Pedestrian Walkways.** In addition to the standards of Sections 10.773, Pedestrian Walkway Connections and Routing and 10.775, Pedestrian Walkway Design Standards, the following shall apply:

- (a) Continuous pedestrian walkways shall be provided from the public sidewalk or right-of-way to the principal public entrance(s) of all large retail structures. With the exception of walkways located within structures, these pedestrian walkways shall feature abutting landscaped areas that include trees, shrubs, benches, planting beds, live ground covers, or

other such plantings or features for no less than fifty percent (50%) of the length of the walkway.

- (b) Continuous pedestrian walkways shall be provided along the full length of any building facade featuring a public entrance, and along any building facade abutting public parking areas. Such walkways shall be located at least six (6) feet from the building façade, with planting beds for foundation landscaping included between the building façade and the walkway, except where features such as covered entrances are part of the building facade.
- (c) Pedestrian walkways provided in conformance with (b) above shall include weather protection features such as awnings within thirty (30) feet of all customer entrances.
- (d) Pedestrian walkways crossing driving surfaces shall be distinguished from driving surfaces through the use of durable, low maintenance surface materials such as pavers, bricks or stamped concrete.

Response: The Phase 2 Site Plan proposal includes a strong and extensive network of pedestrian sidewalks, with various plaza areas interspersed within the network. This network provides complete connectivity within the site, as well as those areas connected with the public street system (see Exhibit C, Sheet C3.0). In addition, a main plaza area (Plaza G) is proposed near the central traffic circle and in between Buildings I and J, thereby providing a centrally focused gathering area large enough to accommodate a relatively large number of customers and shoppers. In addition, smaller pedestrian plazas are also located near the central traffic circle. This focused approach to plaza space provides greater connection among pedestrian walkways, buildings and the adjacent public street system. These plaza areas include such amenities as trees for shading, benches for sitting and resting, water features to soften the noise associated with auto traffic, fire pits for ambience and warmth and lighting for safety and ambience. Plaza areas are more specifically and graphically described in Exhibit 3, Sheet C3.1. Finally, nearly the entire pedestrian network is lined with landscape buffer strips and low-level lighting to provide greater safety for pedestrians, and also to soften the look and feel of pedestrian walkways.

- (3) **Parking.** Parking spaces shall be provided as in Section 10.743, Off-Street Parking Standards.

Response: For Phase 2, the total number of parking spaces required is 664 and the total number proposed is 715, therefore, the minimum parking requirements for Phase 2 are met. These calculations are based on both proposed building square footages and uses proposed. Specific calculations are identified in Project Data Table in Exhibit C, Sheet C1.1. Based on total building areas and types of uses proposed, the total minimum number of parking spaces required for the proposed Alba Village Master Plan is 1,613 spaces. The total proposed number of parking spaces proposed for the Alba Village Master Plan is 1,581 parking spaces.

See Project Data Table in Exhibit C, Sheet C1.1. Based on these calculations, the proposed parking is 2% less than the required-Code minimums. Under the Development Agreement, Section 4.4., Minor Modifications.D., a 10% maximum reduction is allowed under the authority of the Planning Director as a Class E Ministerial Action. The applicant requests that this Minor Modification be approved for a 2% reduction in parking for the Alba Village Master Plan.

(4) Public Plaza.

- (a) Each large retail structure shall provide a public plaza, as defined in Section 10.012 Definitions, Specific. The public plaza is intended to attract tenants and to provide the community with a pleasant, comfortable place to rest and interact.**
- (b) In addition to the requirements for public plazas in Section 10.012, Definitions, Specific, public plazas provided by large retail structures shall comply with the following provisions:**
 - i. Seating areas shall be provided in the public plaza and shall be shaded with trees, cloth canopies, or structures over fifty percent (50%) of their area.**
 - ii. A minimum of twenty percent (20%) of the public plaza shall be landscaped with live plantings subject to the landscape and irrigation requirements in Section 10.780, General Landscape and Irrigation Requirements.**
 - iii. Abutting large retail structures may connect their public plaza areas.**
- (c) Each public plaza shall have focal points and/or other amenities. The focal points and other amenities shall be constructed of materials that are equal or superior to the principal materials of the building and landscaping. Focal points and/or amenities may include the following:**
 - i. An outdoor playground with safe play structures for children;**
 - ii. A water feature, clock tower, or similar focal feature;**
 - iii. A covered community bulletin board (kiosk);**
 - iv. Art works;**
 - v. Space for small or temporary food vendors.**

Response: A main plaza area (Plaza G) is proposed near the central traffic circle and in between Buildings I and J, thereby providing a centrally focused gathering area large enough to accommodate a relatively large number of customers and shoppers. In addition, smaller pedestrian plazas are also located near the central traffic circle. This focused approach to plaza space provides greater connection among pedestrian walkways, buildings and the adjacent public street system. These plaza areas include such amenities as trees for shading, benches for sitting and resting, water features to soften the noise associated with auto traffic, fire pits for ambience and warmth and lighting for safety and ambience. Plaza areas are more specifically and graphically described in Exhibit 3, Sheet C3.1.

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10.727 Excavation and Grading Permit

When an excavation and grading permit is required by this Chapter or by Chapter 9, the application for the permit shall be reviewed and approved by the City of Medford Building Safety Director or designee. When a permit is required, no grading or excavation shall occur prior to issuance. The permit shall not be issued until a grading plan pursuant to Section 10.728 has been reviewed and approved. Additional supporting data consisting of a soils engineering report and engineering geology report may also be required. The plans and specifications shall be signed and prepared by a registered engineer.

Response: The applicant has applied for an Excavation and Grading Permit with the City of Medford. The permit is based on the Grading Plan (Exhibit C, Sheet C4.0), which includes all of the plan requirements listed in Section 10.728, Information to be Conveyed on the Grading Plan and in the Specifications. This permit is currently being reviewed under Permit # 15 2801. In conjunction with the City of Medford Grading Permit, the applicant has applied for, and anticipating approval for, a 1200 C permit with the Department of Environmental Quality.

10.729 Stormwater Quality and Detention Facilities, Private Property

- A. **Purpose.** It is the City's policy to maintain the natural hydrology and preserve water quality by mitigating the direct impacts of new development and preserving the environmental benefits of natural water bodies.
- B. **Applicability.** Stormwater quality and detention facilities shall be required for development and building permits, with the exception of single-family residences and duplexes, which meet any one (1) of the following conditions:
- (1) Building permits for development that creates 5,000 square feet or more impervious surface; or
 - (2) Building permits for development that adds or reconstructs 1,000 square feet or more of impervious surface, if that construction activity is part of a larger common plan of development that contains, or will contain, 5,000 square feet or more of impervious surface. A "common plan of development" means the overall plan for development of land, including any pre-existing development and approved plans for future development; or
 - (3) Building permits for development that existed prior to adoption of City regulations requiring stormwater detention facilities that add or reconstruct 1,000 square feet or more of impervious surface. These shall provide stormwater detention for only the added or reconstructed portion; or
 - (4) Subdivisions, partitions, or PUDs which will contain new private streets, Minimum Access Easements, or other easements creating 5,000 square feet or more of impervious surface.

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- C. **Location.** Stormwater detention facilities for development that does not include public rights-of-way, as per Section 10.486, shall be constructed on private property.
- D. **Construction and Maintenance Standards.**
 - (1) Stormwater detention facilities shall be constructed in conformance with Section 10.481, Improvement Standards Adopted.
 - (2) Stormwater detention facilities for development that does not include public rights-of-way shall be privately maintained and shall have an Operation and Maintenance Plan approved by the City prior to construction of the facility. An approved form of the Operation and Maintenance Plan is located in Appendix of the current adopted version of the Rogue Valley Stormwater Quality Design Manual.
 - (3) When the property owner will not be responsible for maintenance of the private stormwater detention facility or when there will be multiple responsible parties, an Operation and Maintenance Agreement, in a form acceptable to the Public Works Department and the City Attorney, shall be required, in addition to the Operation and Maintenance Plan.

Response: All construction and maintenance for all proposed stormwater facilities will meet the requirements of the City, including compliance of the Operation and Maintenance Plan and Agreement with the Rogue Valley Stormwater Quality Design Manual.
 The Operation and Maintenance Plan and Agreement will be provided with construction documents at time of submittal for associated permits.

10.731 General Provisions

Every fence shall be maintained in a condition of reasonable repair and shall not be allowed to become and remain in a condition of disrepair. Any fence which is, or has become, dangerous to the public safety, health, or welfare shall be considered a violation of this chapter. Barbed wire fencing and other fences constructed of sharp materials shall only be permitted in commercial or industrial zoning districts. Wood picket fences are permitted in all districts.

Response: No fencing is proposed as part of this application.

10.741 Parking and Loading, General Provisions

- A. **Off-Street Parking and Loading.**
 - (1) A development permit shall not be issued until plans and evidence are presented to show how the parking and loading requirements are to be fulfilled and that property is and will be available for exclusive use as parking and loading space. Off-street parking and loading

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spaces shall be provided and maintained as set forth in Sections 10.741 - 10.745 for all uses in all zoning districts.

- (2) Such off-street parking and loading spaces shall be provided at the time a certificate of occupancy is issued for a new building or the expansion of an existing building. Any use requiring one-half (1/2) or more of a parking or loading space shall be deemed to require the full space. Parking and loading spaces provided to meet the requirements of this code shall not be reduced in size or number to an amount less than required by this code for the use occupying the building or site. The provision and maintenance of off-street parking and loading spaces is a continuing obligation of the property owner. In the case of mixed uses, the total requirements for off-street parking facilities shall be the sum of the requirements for the various uses computed separately. Required parking spaces shall be available for the parking of operable automobiles of residents, customers and employees and shall not be used for the storage of vehicles or materials or for the parking of trucks used in conducting the business or use. A required loading space shall not be used for any purpose other than the immediate loading or unloading of goods.

Response: The applicant is proposing standard required parking and loading spaces, designed per the requirements of these sections 10.741. For Phase 2, the total number of parking spaces required is 664 and the total number proposed is 715, therefore, the minimum parking requirements for Phase 2 are met. These calculations are based on both proposed building square footages and uses proposed. Specific calculations are identified in Project Data Table in Exhibit C, Sheet C1.1.

Based on total building areas and types of uses proposed, the total minimum number of parking spaces required for the proposed Alba Village Master Plan is 1,613 spaces. The total number of parking spaces proposed for the Alba Village Master Plan is 1,581 parking spaces. See Project Data Table in Exhibit C, Sheet C1.1. Based on these calculations, the proposed parking is 2% less than the required-Code minimums. Under the Development Agreement, Section 4.4., Minor Modifications.D., a 10% maximum reduction is allowed under the authority of the Planning Director as a Class E Ministerial Action. The applicant requests that this Minor Modification be approved for a 2% reduction in parking for the Alba Village Master Plan.

B. Change of Use Parking and Loading Requirements.

The number of parking and loading spaces provided shall be increased when a change of use of either a structure or of land requires additional parking and loading spaces in compliance with this Code, except as provided below. Parking and loading spaces may be decreased when a change of use requires fewer spaces than originally provided. A principal use which is not deficient in the number of parking and loading spaces provided may expand without having to provide additional parking and loading spaces under certain circumstances. Such circumstances are: (1) When after the

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expansion or change in use, the number of parking and loading spaces provided still meets or exceeds the required minimum; or (2) If the expansion or change in use results in the need to provide no greater than twenty-five percent (25%) additional parking and/or loading spaces in order to meet the minimum number of required spaces, these additional spaces are waived.

Response: No change of use is proposed as part of this project. All proposed uses are considered new uses, and the associated parking requirements of these new uses are addressed in the section of the Code.

C. Parking and Loading Plan.

A parking plan, drawn to scale, shall accompany development permit applications for all developments excluding those for single and two-family dwellings. The parking plan shall show all those elements necessary to indicate that the requirements of this Code will be fulfilled and shall include, but are not limited to:

- (1) Delineation and typical dimensions of individual parking and loading spaces;
- (2) Dimensions of the circulation and maneuvering areas necessary to serve parking and loading spaces;
- (3) Access to streets, alleys and proper ties to be served;
- (4) Curb cuts;
- (5) Type of fencing or other screening materials;
- (6) Abutting land uses;
- (7) Grading, drainage, surfacing and subgrading details;
- (8) Location, height, and candlepower of lighting fixtures;
- (9) Delineations of all structures and other obstacles to parking and circulation on the site;
- (10) Specifications as to signs and bumper guards, and
- (11) Parking lot landscaping plan.

Response: All proposed parking is indicated in the Site Plan (Exhibit 3, Sheet C3.0). All dimensions for spaces and aisle widths, locations of access points, landscape areas, buildings and pedestrian facilities are all indicated in the plan.

10.743 Off-Street Parking Standards

- (1) **Vehicle Parking – Minimum and Maximum Standards by Use.** The number of required off-street vehicle parking spaces shall be determined in accordance with the standards in Table 10.743-1.
- (2) **Number of Required Parking Spaces.** Off-street vehicle parking spaces shall be provided as follows:
 - (a) **Parking Space Calculation.** Parking space ratios are based on spaces per 1,000 square feet of gross floor area, unless otherwise noted.

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(b) **Parking Categories.**

(i) Table 10.743-1 contains parking ratios for minimum required number of parking spaces and maximum permitted number of parking spaces for each land use.

A. **Minimum Number of Required Parking Spaces.** For each listed land use, the City shall not require more than the minimum number of parking spaces calculated for each use.

B. **Maximum Number of Permitted Parking Spaces.** The number of parking spaces provided shall not exceed the maximum number of parking spaces allowed for each listed land use.

Response: For Phase 2, the total number of parking spaces required is 664 and the total number proposed is 715, therefore, the minimum parking requirements for Phase 2 are met. These calculations are based on both proposed building square footages and uses proposed. Specific calculations are identified in Project Data Table in Exhibit C, Sheet C1.1.

Based on total building areas and types of uses proposed, the total minimum number of parking spaces required for the proposed Alba Village Master Plan is 1,613 spaces. The total proposed number of parking spaces proposed for the Alba Village Master Plan is 1,581 parking spaces. See Project Data Table in Exhibit C, Sheet C1.1. Based on these calculations, the proposed parking is 2% less than the required-Code minimums. Under the Development Agreement, Section 4.4., Minor Modifications.D., a 10% maximum reduction is allowed under the authority of the Planning Director as a Class E Ministerial Action. The applicant requests that this Minor Modification be approved for a 2% reduction in parking for the Alba Village Master Plan.

10.746 General Design Requirements for Parking

With the exception of storage of trailered items and recreational vehicles at single-family residences, all parking, loading, driveway, and vehicle maneuvering areas, including but not limited to, wheeled-vehicle sales lots, truck trailer parking areas, and on-site single-family residential driveways etc., shall be paved and improved pursuant to the following minimum design requirements of this section.

The design provisions of this section are applicable to all new parking facilities, parking lot expansions, and to areas where existing parking facilities are excavated and re-installed. A pre-existing parking lot that is legally nonconforming to a standard in subsections 10.746(3), (4), (9), or (10) and is improved with a surface pavement overlay only, without excavation of existing pavement, may remain nonconforming to that particular standard(s). All other provisions of this section are applicable to resurfaced parking lots.

Response: All design requirements for proposed parking areas are met, as indicated in the Site Plan (Exhibit C, Sheet C3.0). The site plan includes all dimensions for spaces, planter bays, drive aisles, compact and carpool spaces and associated pedestrian network of sidewalk and plaza areas.

11B"
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10.747 General Provisions, Bicycle Parking

The bicycle parking and storage provisions are intended to provide bicycle parking facilities to accommodate bicycle travel and encourage additional bicycle trips. Bicycle parking facilities shall be either lockable enclosures in which the bicycle is stored or stationary racks which accommodate bicyclist's locks securing the frame and both wheels. Bicycle racks or lockers shall be securely anchored to the surface or to a structure. If 10 or more bicycle parking spaces are required, then at least 50% of the bicycle parking spaces shall be covered. For the purposes of this section, covered parking may include placement underneath an awning, eave or other overhang or other facility as determined by the approving authority that protects the bicycle from direct exposure to the elements. Bicycle parking shall be separated from motor vehicle parking and maneuvering areas by a barrier or sufficient distance to prevent damage to parked bicycles. Any building expansion or any new construction, excluding two-family and three-family dwellings, shall bring the property into conformance with the Bicycle Parking and Storage Regulations. When required by this code, the site development plan shall include a bicycle parking plan, drawn to scale and submitted with the development permit application. The plan shall show all those elements necessary to indicate that the requirements of this code are being fulfilled.

Response: A total of 72 parking spaces are proposed and required for Phase 2, based on proposed building square footage for this phase. A total of 36 spaces are proposed to be covered, which is 50% of the 72 required.

10.772 Pedestrian Walkway General Provisions

These walkway provisions are intended to provide safe, direct and convenient pedestrian access in new office and commercial developments. Internal pedestrian circulation shall be provided in new office and commercial developments through clustering of buildings, construction of pedestrian ways, skywalks, where appropriate, and similar techniques.

Response: The Phase 2 Site Plan proposal includes an extensive network of pedestrian sidewalks, with various plaza areas interspersed within the network. This network provides complete connectivity within the site, as well as those areas connected with the public street system (see Exhibit C, Sheet C3.0). In addition, a main plaza area (Plaza G) is proposed near the central traffic circle and in between Buildings I and J, thereby providing a centrally focused gathering area large enough to accommodate a relatively large number of customers and shoppers. In addition, smaller pedestrian plazas are also located near the central traffic circle. This focused approach to plaza space provides greater connection among pedestrian walkways, buildings and the adjacent public street system. These plaza areas include such amenities as trees for shading, benches for sitting and resting, water features to soften the noise associated with auto traffic, fire pits for ambience and warmth and lighting for safety and ambience. Plaza areas are more specifically and graphically described in Exhibit 3, Sheet C3.1.

Finally, nearly the entire pedestrian network is lined with landscape buffer strips and low-level lighting to provide greater safety for pedestrians, and also to soften the look and feel of pedestrian walkways.

10.780 Landscape and Irrigation Requirements

Landscaping Design Standards.

- (1) **Other Applicable Code Sections.** In addition to meeting the landscaping design requirements of this Section, the landscape plan shall also meet landscaping design requirements of the following Sections where applicable ...

Response: The applicant has provided a Landscape Plan as part of this SPAC application. The Landscape Plan is provided in Exhibit C, Sheet L1.0 and L2.0. These detailed plans demonstrate compliance with all applicable regulations and standards required in Section 10.780.

10.781 Concealment of Trash Receptacles

Trash receptacles shall be kept in enclosures made of solid wood, metal, brick, or masonry block which conceal them from view. Access doors shall be made of solid wood or metal. When possible, enclosures should be incorporated into the building (i.e., wing walls, alcoves). If free standing, the enclosures shall be constructed of materials which are compatible with other structures on the site. Chain link fencing, with or without slats, for this purpose is prohibited.

Response: All proposed trash areas are enclosed. These enclosures provide screening that conceals equipment and trash items from view. Most trash areas are located next to buildings at the rear, thereby further concealing trash areas.

10.797 Street Frontage Landscaping Requirements

- A. This section establishes the minimum landscaping requirements along all street frontages outside of the public right-of-way. Plans submitted to comply with this section shall be approved by the approving authority.
 - (1) For land divisions with houses that do not face an arterial street, an arterial street frontage landscape plan shall be submitted showing a vertical separation feature that is a minimum of eight (8) feet in height. The separation feature shall create a solid visual screen. A fence or wall shall be engineered to stand straight. The separation feature shall be reduced in height where otherwise required in a front or side yard or clear vision triangle. The Planning Commission may allow adjustments to the above requirement in response to topography.
 - (2) For all other street frontages the number of plants required for distances above or below one hundred (100) feet shall be prorated with the resulting numbers of plants rounded so that one-half or more shall be deemed to require a full plant. All required planting shall be located in the required yard area adjacent to the street unless otherwise approved by the approving authority.

- (3) Existing plant materials which meet the requirements of this code may be counted as contributing to the total landscaping required by this section. Interstate 5 and other highway frontages shall be treated the same as city street frontage.
- (4) The following table specifies the type and number of plants required by this section.

Response: The frontage along Rossanley Drive (Highway 238) is approximately 766 feet. Based on this distance, and the requirements in this section for trees and shrubs, 31 trees and 192 shrubs are proposed in the Landscape Plan. The frontage along N. Central Avenue is approximately 1078 feet. Based on this distance, and the requirements in this section for trees and shrubs, 44 trees and 270 shrubs are proposed in the Landscape Plan. See Exhibit C, Sheet L1.0.

10.807 Transit Facilities for Major Industrial, Institutional, Commercial and Office Developments

Transit improvements, including provision of bus stops, pullouts, shelters, on-street parking restrictions, optimum road geometrics and similar facilities shall be provided at the time of development of major industrial, institutional, commercial and office developments when the building or group of buildings exceeds the following:

<u>Development Type</u>	<u>Gross Square Footage</u>
Commercial	60,000 sq. ft.
Industrial	120,000 sq. ft.

The transit provider shall identify the type of transit facility required. This determination shall be made either through an adopted plan or on a case by case basis in response to a development proposal review. Applicants for major developments shall consult with the transit provider on necessary transit facility improvements.

Response: There is an existing bus shelter serving the site located on Rossanley Drive near the main entrance to the site. This bus shelter serves Rogue Valley Transit District passengers, and is directly connected to plazas and buildings on the site through an extensive network of pedestrian walkways, both existing and proposed.

10.833 Restaurants – Outdoor Eating Areas

Outdoor eating areas shall be allowed for restaurants in all commercial and industrial zoning districts subject to the following:

- (1) Compliance with all other provisions of this chapter.

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- (2) **Historic Review or Site Plan and Architectural Review as applicable and approval when the outdoor eating area includes seating for more than 15 patrons. (Effective Dec. 1, 2013.)**
- (3) **Where adjacent or abutting a residential zone, outdoor activity shall only be allowed between the hours of 8:00 a.m. and 9:00 p.m.**

Response: An outdoor eating area is proposed for Building I and will accommodate more than 15 patrons. It is proposed that this area also be part of the review and approval of this application.

CONCLUSION

As evidenced throughout this project narrative and associated documents, the applicant's Site Plan and Architectural Review request is consistent with the applicable local decision criteria and standards. Based on the evidence provided within this narrative and attached materials, the applicant respectfully requests City of Medford's approval of these applications.

"B"

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SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

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 OCTOBER 1, 2015
 PLANNING DEPARTMENT

Section II – Code Compliance: Criterion No. 2

PROJECT SITE		
	PROPOSED	REQUIRED
• Zoning District	Regional Commercial	Regional Commercial
• Overlay District(s)	N/A	N/A
• Proposed Use	Commercial Retail	Commercial Retail
• Project Site Acreage	17.81 acres	15,000 square feet
• Site Acreage (+ right-of-way)	17.81 acres	15,000 square feet
• Proposed Density (10.708)	N/A	N/A
• # Dwelling Units	N/A	N/A
• # Employees	120	N/A

STRUCTURES		
	EXISTING	PROPOSED
• # Structures	0	7
• Structure Square Footage (10.710-10.721)	0	178,990

SITE DESIGN STANDARDS		
	PROPOSED	REQUIRED
• Front Yard Setback (10.710-721)	> 10 feet	> 10 feet
• Side Yard Setback (10.710-721)	> 20 feet	> 20 feet
• Side Yard Setback (10.710-721)	> 20 feet	> 20 feet
• Rear Yard Setback (10.710-721)	> 20 feet	> 20 feet
• Lot Coverage (10.710-721)	23.1 %	40 %

PARKING		
	PROPOSED	REQUIRED
• Regular Vehicular Spaces (10.743)	715	664
• Disable Person Vehicular Spaces (10.746[8])	20	20
• Carpool/Vanpool Spaces (10.809)	0	0
• Total Spaces (10.743)	715	664
• Bicycle Spaces (10.748)	72	72
• Loading Berths (10.742)	4 (TYP A)	7 (TYP B)

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SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

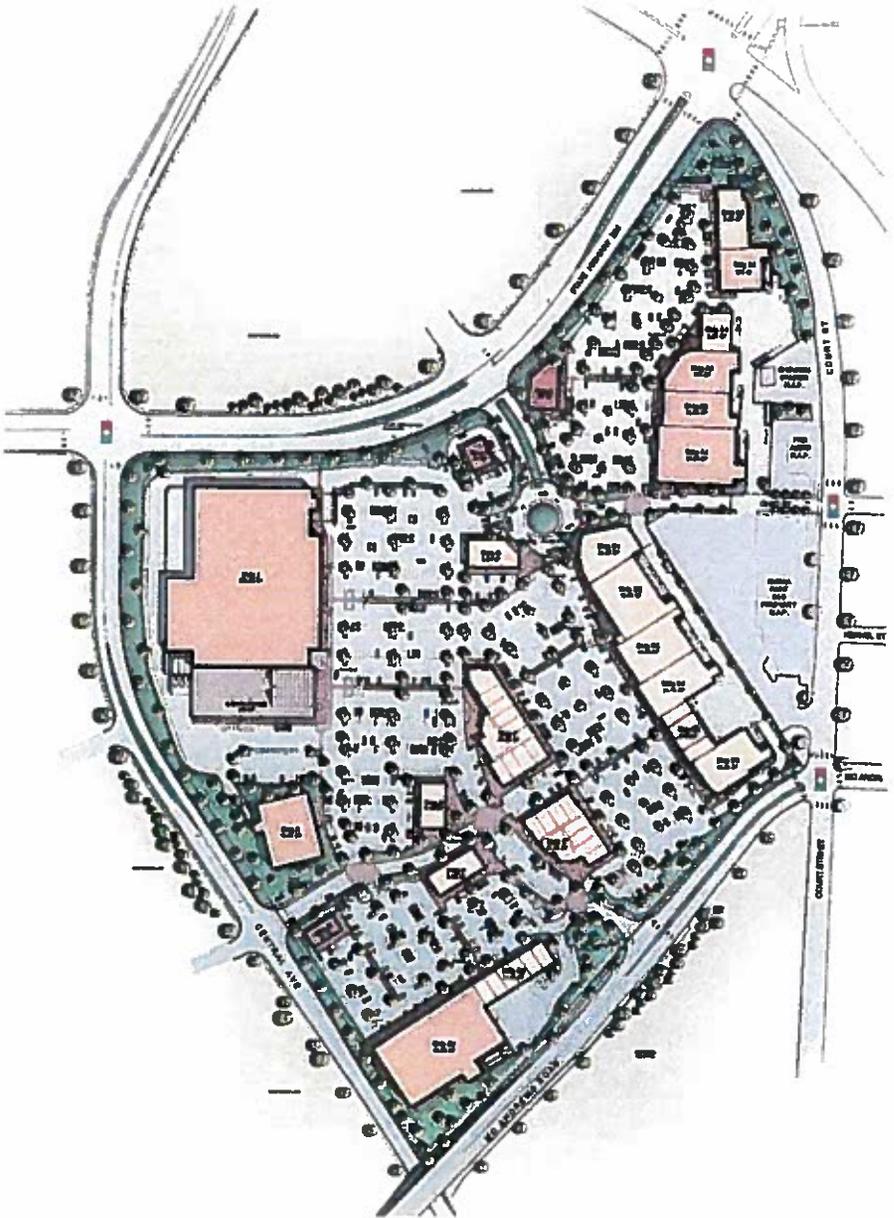
LANDSCAPING		
	PROPOSED	REQUIRED
• Total Landscape Area (square feet)	174,220	109,000
• Total Landscape Area in High Water Use Landscaping (square feet)	52,470	52,470
• Total Landscape Area in High Water Use Landscaping (percentage)	<30%	<30%
• Total % Landscape Coverage	22.4%	14.05%
• Required Organic Content (cu.yd.)	525	525
• Frontage Landscaping (10.797)		
• Street:	Rossanley Drive (Hwy 238)	
• Feet:	766	
• # Trees:	31	31
• # Shrubs:	192	192
• Street:	N. Central Avenue	
• Feet:	1,078	
• # Trees:	44	44
• # Shrubs:	270	270
• Bufferyard Landscaping (10.790)		
• Type:	N/A	N/A
• Distance (ft):	N/A	N/A
• # Canopy Trees:	N/A	N/A
• # Shrubs:	N/A	N/A
• Fence/Wall:	N/A	N/A
• Parking Area Planter Bays (10.746)		
• Type:	Islands and Projections	
• # Bays:	74	
• Area:	28,521	14,896
• # Trees:	90	90
• # Shrubs:	180	180

STRUCTURE	
	PROPOSED
• Materials	See materials board
• Colors	See materials board

Please remember that the information you provide in response to the questionnaire must be included with your SPAR application submittal. Remember to sign and date your written response.

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Approved Northgate Marketplace Master Site Plan (AC-11-076)



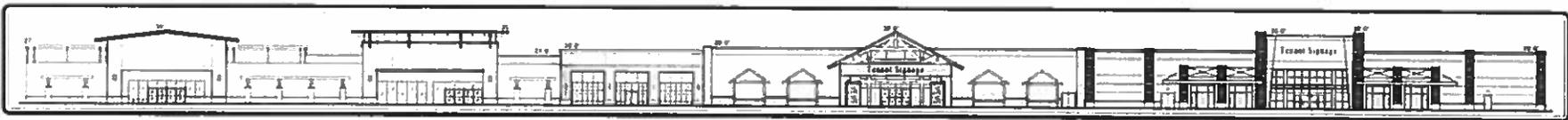
Site Plan - Colored



NORTHGATE MARKETPLACE - PHASE II

CITY OF MEDFORD, OREGON
HWY 238 & CENTRAL AVE, MEDFORD, OREGON 97501

SPAC APPLICATION



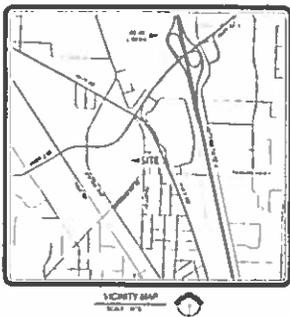
Regency Centers.

MARKETPLACE PHASE II

OCTOBER 1, 2015
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SITE INFORMATION

LOCATED IN SECTION 10, TOWNSHIP 37 NORTH, RANGE 8 WEST, COUNTY OF DEWEE, OREGON

DATE:

DATE OF PREPARATION AND SUBMITTAL OF APPLICATION AND DATE OF CITY COUNCIL ACTION

BASIS OF BEARINGS

DATE OF SURVEY AND SUBMITTAL OF APPLICATION AND DATE OF CITY COUNCIL ACTION

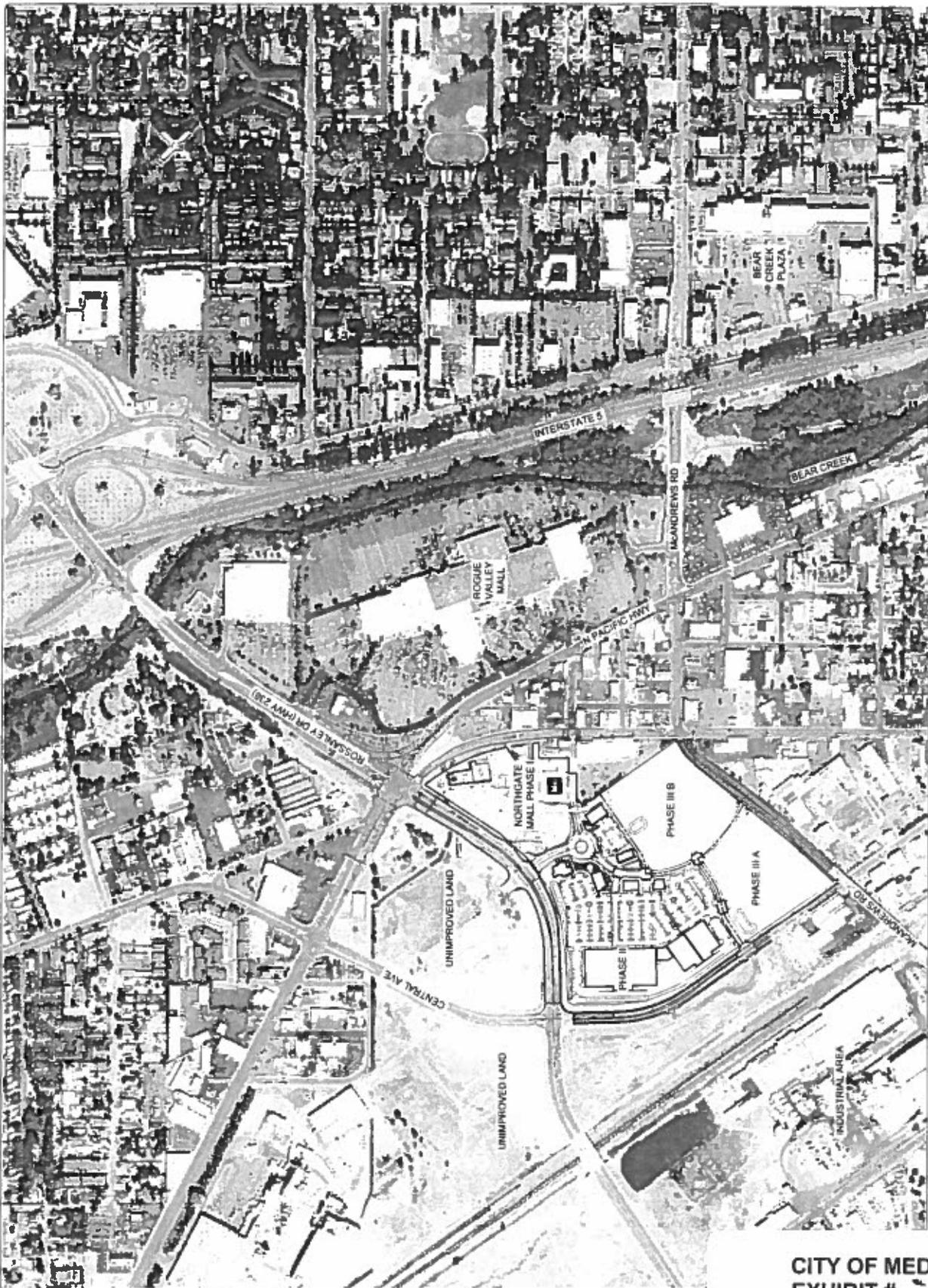
UNDERGROUND ACCURACY STATEMENT

THE INFORMATION CONTAINED HEREIN IS BASED ON THE BEST AVAILABLE INFORMATION AND IS NOT GUARANTEED. THE INFORMATION IS FOR GENERAL INFORMATION ONLY AND IS NOT TO BE USED FOR ANY OTHER PURPOSE. THE INFORMATION IS NOT TO BE USED FOR ANY OTHER PURPOSE. THE INFORMATION IS NOT TO BE USED FOR ANY OTHER PURPOSE.

FEMA NOTE

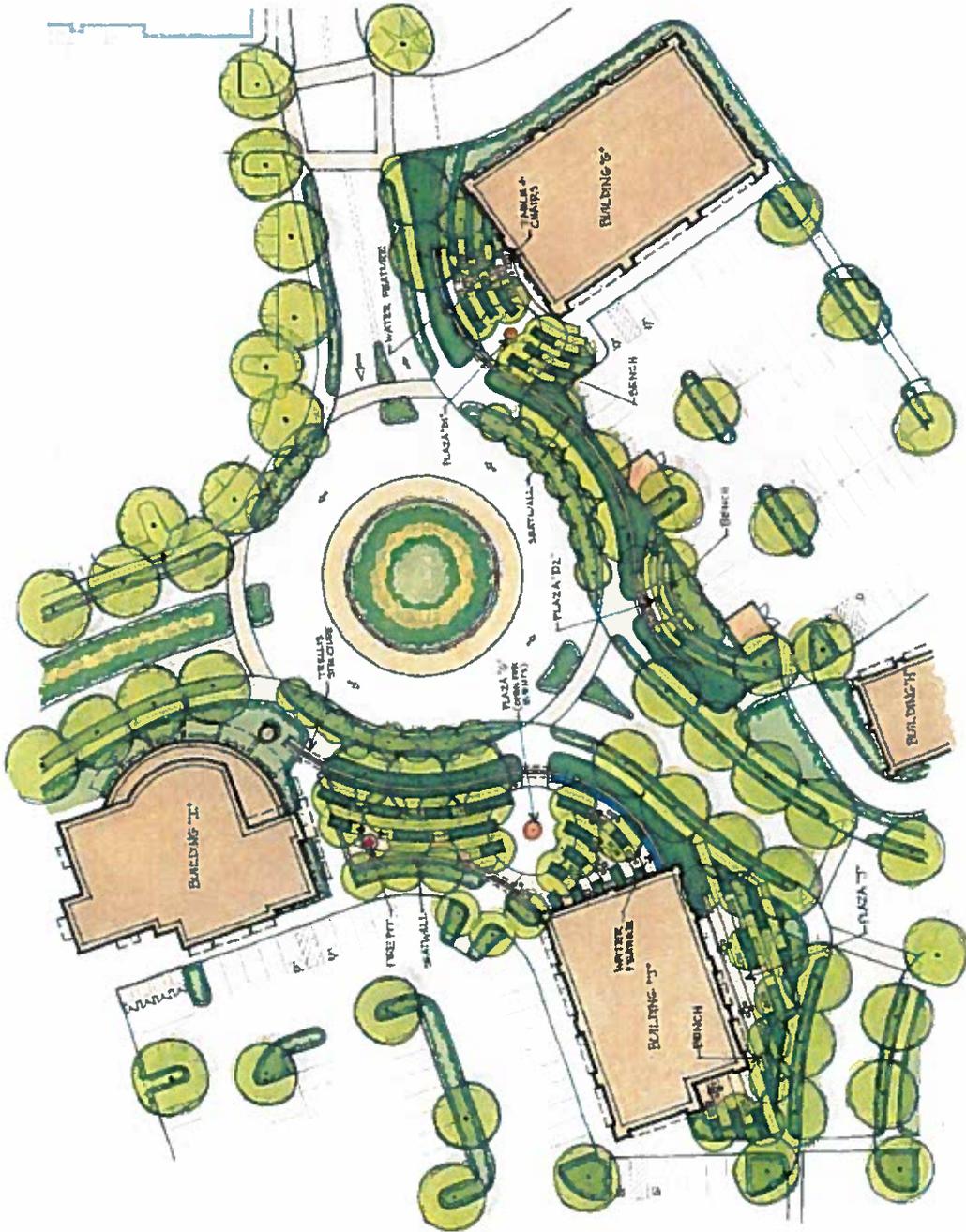
THIS PROJECT IS NOT LOCATED WITHIN ANY SPECIAL FLOOD HAZARD AREA AS SHOWN ON FEMA FLOOD MAPS. THE PROJECT IS NOT LOCATED WITHIN ANY SPECIAL FLOOD HAZARD AREA AS SHOWN ON FEMA FLOOD MAPS.

CITY OF MEDFORD
EXHIBIT #
File # AC-15-133



CITY OF MEDFORD
 EXHIBIT # _____
 File # AC-15-133

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CITY OF MEDFORD
 EXHIBIT # "K"
 File # AC-15-133

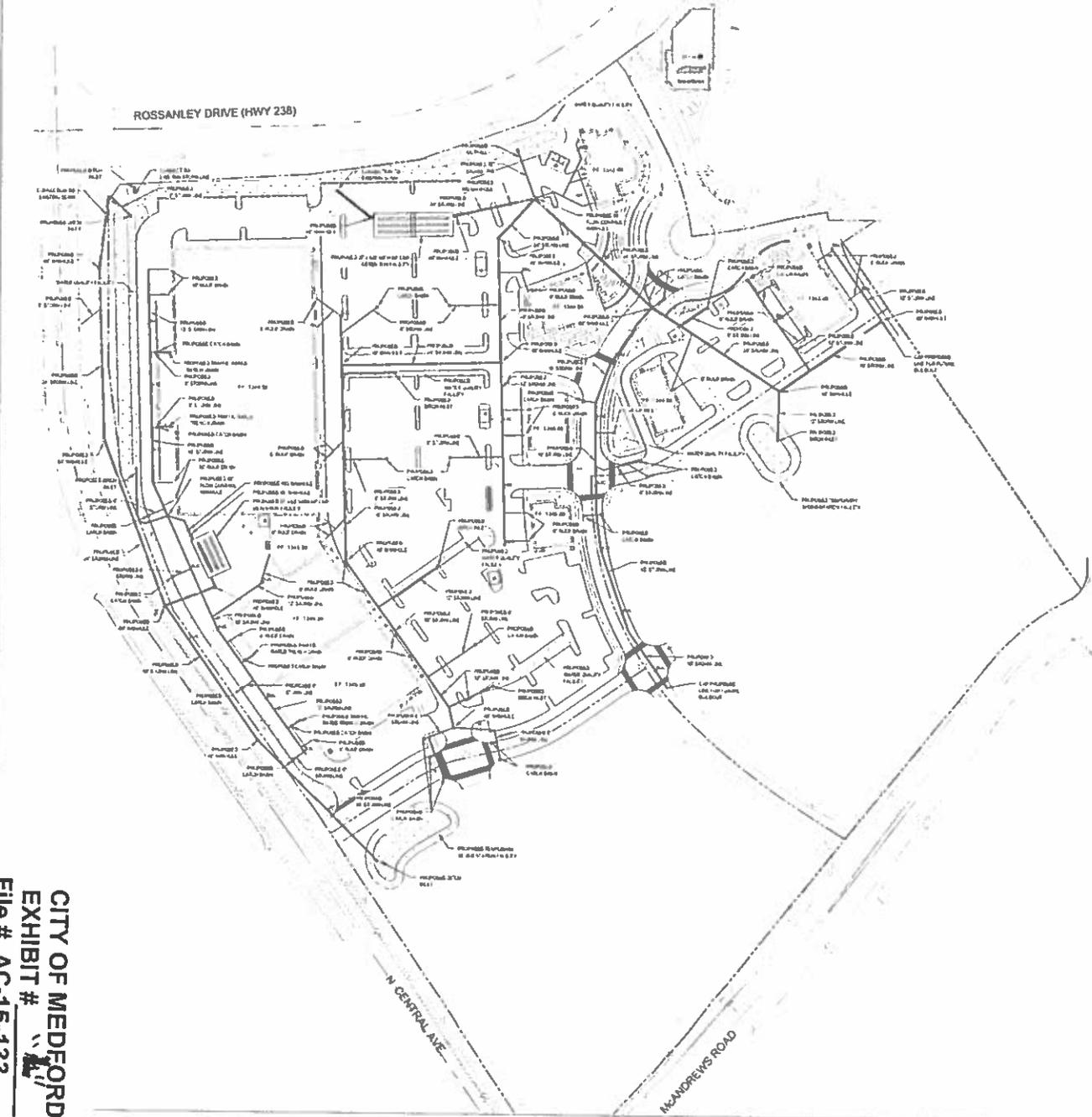
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OCTOBER 1, 2015

PLANNING DEPARTMENT

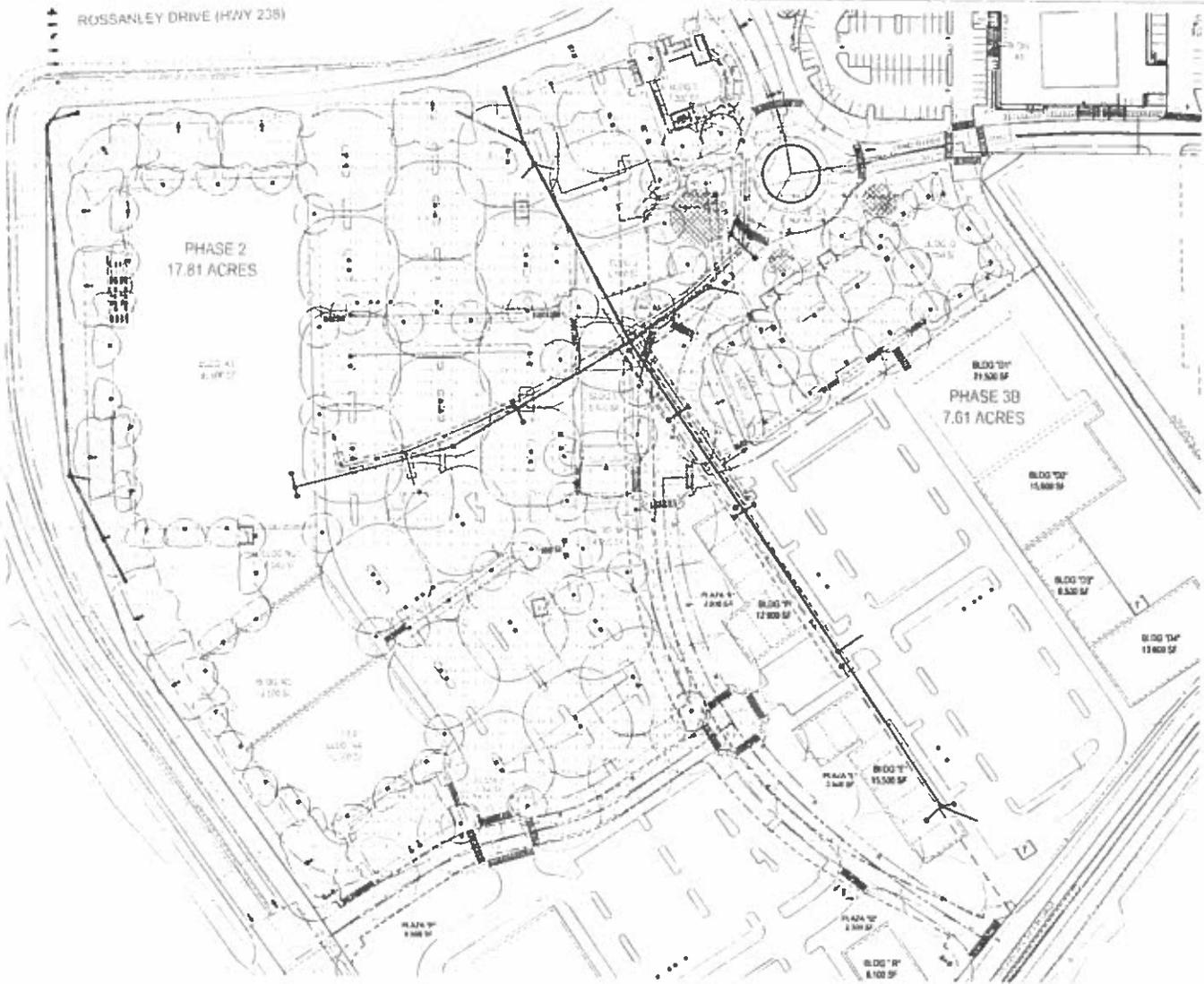
LEGEND

- PROPOSED 10' SIDEWALK
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- PROPOSED 1000' SIDEWALK



REVISIONS

REV #	DATE	BY:
1	9/11/15	TO
2	9/17/15	TO
3	9/21/15	TO



OPTION 1

Item	Description	Quantity	Unit	Notes
1	8,000' 7,150 SF	12	luminaire	Phase 2
2	8,000' 15,500 SF	18	luminaire	Phase 3B
3	8,000' 15,500 SF	12	luminaire	Phase 3C
4	8,000' 15,500 SF	12	luminaire	Phase 3D
5	8,000' 15,500 SF	12	luminaire	Phase 3E
6	8,000' 15,500 SF	12	luminaire	Phase 3F
7	8,000' 15,500 SF	12	luminaire	Phase 3G
8	8,000' 15,500 SF	12	luminaire	Phase 3H
9	8,000' 15,500 SF	12	luminaire	Phase 3I
10	8,000' 15,500 SF	12	luminaire	Phase 3J
11	8,000' 15,500 SF	12	luminaire	Phase 3K
12	8,000' 15,500 SF	12	luminaire	Phase 3L
13	8,000' 15,500 SF	12	luminaire	Phase 3M
14	8,000' 15,500 SF	12	luminaire	Phase 3N
15	8,000' 15,500 SF	12	luminaire	Phase 3O
16	8,000' 15,500 SF	12	luminaire	Phase 3P
17	8,000' 15,500 SF	12	luminaire	Phase 3Q
18	8,000' 15,500 SF	12	luminaire	Phase 3R
19	8,000' 15,500 SF	12	luminaire	Phase 3S
20	8,000' 15,500 SF	12	luminaire	Phase 3T
21	8,000' 15,500 SF	12	luminaire	Phase 3U
22	8,000' 15,500 SF	12	luminaire	Phase 3V
23	8,000' 15,500 SF	12	luminaire	Phase 3W
24	8,000' 15,500 SF	12	luminaire	Phase 3X
25	8,000' 15,500 SF	12	luminaire	Phase 3Y
26	8,000' 15,500 SF	12	luminaire	Phase 3Z

Item	Description	Quantity	Unit	Notes
1	8,000' 7,150 SF	12	luminaire	Phase 2
2	8,000' 15,500 SF	18	luminaire	Phase 3B
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8	8,000' 15,500 SF	12	luminaire	Phase 3H
9	8,000' 15,500 SF	12	luminaire	Phase 3I
10	8,000' 15,500 SF	12	luminaire	Phase 3J
11	8,000' 15,500 SF	12	luminaire	Phase 3K
12	8,000' 15,500 SF	12	luminaire	Phase 3L
13	8,000' 15,500 SF	12	luminaire	Phase 3M
14	8,000' 15,500 SF	12	luminaire	Phase 3N
15	8,000' 15,500 SF	12	luminaire	Phase 3O
16	8,000' 15,500 SF	12	luminaire	Phase 3P
17	8,000' 15,500 SF	12	luminaire	Phase 3Q
18	8,000' 15,500 SF	12	luminaire	Phase 3R
19	8,000' 15,500 SF	12	luminaire	Phase 3S
20	8,000' 15,500 SF	12	luminaire	Phase 3T
21	8,000' 15,500 SF	12	luminaire	Phase 3U
22	8,000' 15,500 SF	12	luminaire	Phase 3V
23	8,000' 15,500 SF	12	luminaire	Phase 3W
24	8,000' 15,500 SF	12	luminaire	Phase 3X
25	8,000' 15,500 SF	12	luminaire	Phase 3Y
26	8,000' 15,500 SF	12	luminaire	Phase 3Z



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PROVIDED, ALL DIMENSIONS AND LUMINAIRE LOCATIONS AND POSITIONS. THE ENGINEER AND/OR ARCHITECT IS RESPONSIBLE FOR THE LAYOUT TO EXISTING OR FUTURE FIELD CONDITIONS. ILLUMINATION LEVELS CALCULATED FROM DATA CONTAINED HEREIN ARE BASED ON CURRENT METHODS. ACTUAL PERFORMANCE OF ANY SYSTEM MAY VARY DUE TO VARIATIONS IN ELECTRICAL VOLTAGE UNDER VARIOUS FIELD CONDITIONS.

CITY OF MEDFORD
EXHIBIT # "G"
File # AC-15-133

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PROJECT NO. 1506

STLM CONSULTING SERVICES

Consider the Impact!

9/15 SCALE: 1"=60'

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PM: ROBBY

BY: TO

SHEET 1 OF 1

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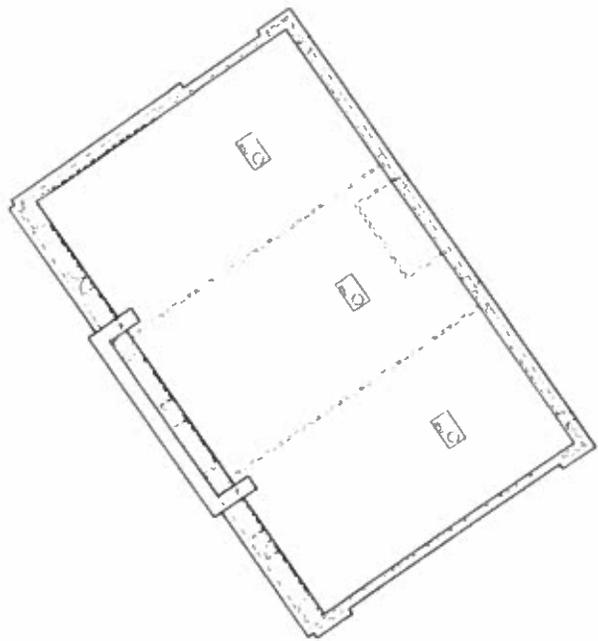
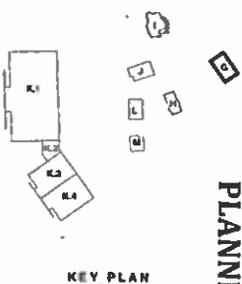
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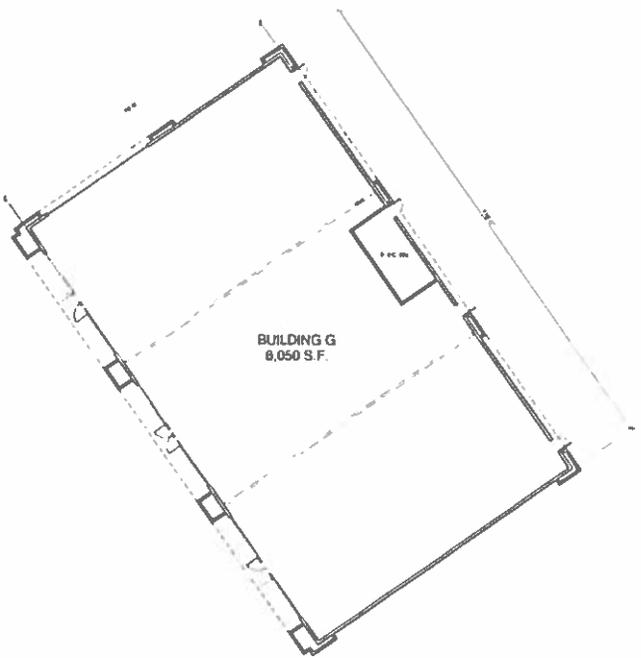


A1

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ROOF PLAN



FLOOR PLAN

FLOOR AND ROOF PLAN

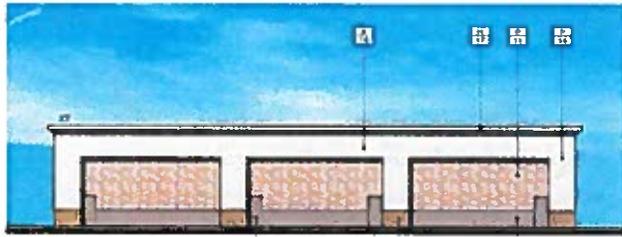


BUILDING G

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

CITY OF MEDFORD
EXHIBIT #
File # AC-15-133

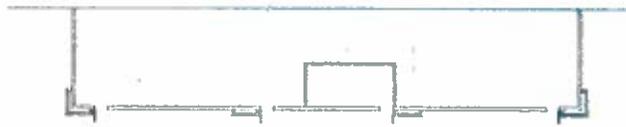




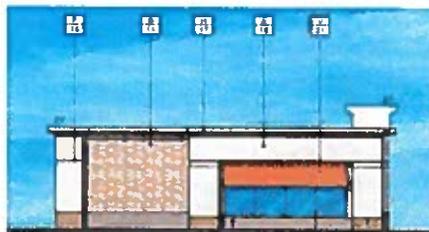
EAST ELEVATION



WEST ELEVATION



SOUTH ELEVATION



NORTH ELEVATION



MATERIAL AND FINISH SCHEDULE

MATERIAL / FORMS	COLOR / FINISH
1. EIFS SYSTEM WITH FINE SAND TEXTURE @ 8/8/14	1. PAINT COLOR TO MATCH BENJAMIN MOORE # 817 "ROCK CANDY"
2. BRICK VENEER	2. PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP KUMHO"
3. CASI BLOCK - SMOOTH FACE @ 8/8/14	3. PAINT COLOR TO MATCH BENJAMIN MOORE # 14725 "TO BREVY TANK"
4. CASI BLOCK - SPLIT FACE @ 8/8/14	4. PAINT COLOR TO MATCH BENJAMIN MOORE # 1033 "SEATTLE BEST"
5. ALUMINUM STOREFRONT SYSTEM WITH 1" CLEAN INSUL. CLADDING	5. PAINT COLOR TO MATCH BENJAMIN MOORE # 1487 "THERMAL ESCAPE"
6. PRE-CAST CONCRETE BASE	6. PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "ICE SP GREEN"
7. METAL CANOPY/TRELLIS OR BOON DECORATIVE ROOF STRUCTURE	7. "MARI ORIO GREEN P 60"
8. METAL CEILING	8. BRICK VENEER P03 S05 BY DELDOR OR INDIOT EQUIVALENT
9. EIFS PROF. E.D. TRIMMOLING CONCRETE CLASH	9. PAINT COLOR TO MATCH BENJAMIN MOORE # 3134-50 "GULL WING GRAY"
10. HOLLOW METAL DOOR AND FRAME	10. METAL MATERIAL SPLIT FACE "CASTLE WHITE"
11. METAL MULL UP/DOWN DOOR	11. METAL MATERIAL GRAYS & WHITES COLLECTION "ALPINE 1601"
12. PRE-CAST CONCRETE BASE CAP	12. CO. INC. "GRAY"
13. LIGHT FIXTURE	13. SANDY LimestonE FINISH
14. EIFS REVEAL/CONTROL JOINT	14. CLAM ANKA/PHU ALUMINUM
15. PRE-CAST CONCRETE WALL LAP	15. COLOR TO MATCH ADJ. WALL
16. FUTURE SIGNAGE BY TENANT	16. COLOR TO MATCH BRICK VENEER
17. UNITY STRUCTURE AND PINS	17. PAINT COLOR TO MATCH BENJAMIN MOORE # 808 "ANTIQUE PARICHMENT"
18. GALVANIZED CHAIN LINK FENCE	18. PAINT COLOR TO MATCH BENJAMIN MOORE # 1523 "EMBASSY GREEN"
19. BRICK ACCENTS TYP.	19. PAINT COLOR TO MATCH BENJAMIN MOORE # 816 "TO BREVY TANK"
20. FABRIC COVERED METAL AWNING	20. FABRIC DIANE/ELLA 9608 "BLACK" STRETCHED TIGHT FRAME 2X2 ALUM. TUBE POWDER COATED TO MATCH
21. STONE VENEER	21. DARK BRONZE
	22. FABRIC 12 INCH STRETCHED TIGHT FRAME 2X2 ALUM. COATED TO MATCH
	23. INTERIORD MARBLE GRANITE
	24. METAL MATERIAL "GRANITE"



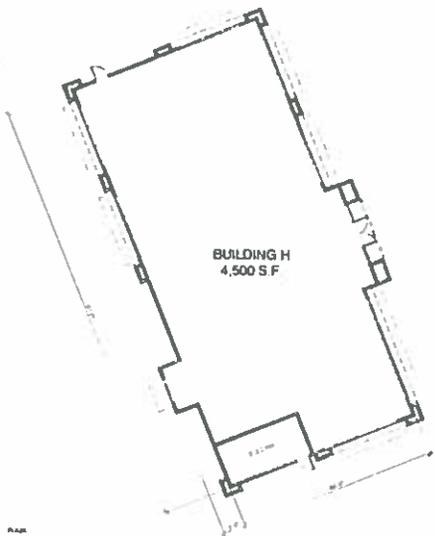
BUILDING G

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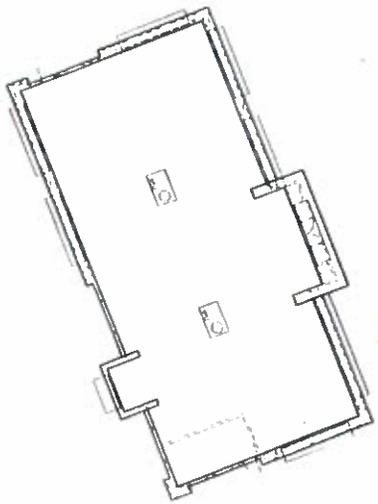
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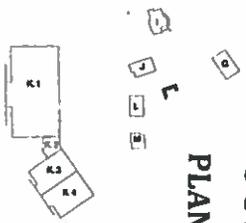
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FLOOR PLAN



ROOF PLAN



KEY PLAN

FLOOR AND ROOF PLAN

BUILDING H

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

02/07/16
 5/14/01
 02/07/16
 MTTA
 02/07/16



A3

250 Sutter Street, Suite 500
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 www.ch2mhill.com

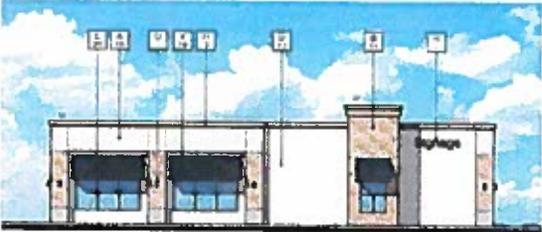
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 EXHIBIT # AC-15-133
 File # AC-15-133

25



EAST ELEVATION



WEST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION



MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND:

- 1. 6" S SYSTEM BRICK (NO. 3481) TEXTURE FINISH
- 2. BRICK VENEER
- 3. CLAD DOOR - FINISH FACE @ 2'x7'4"
- 4. CLAD DOOR - SPLIT FACE @ 2'x7'4"
- 5. ALUMINUM STORE FRONT SYSTEM WITH 1" GLASS PANEL - SLABING
- 6. PRE CAST CONCRETE BASE
- 7. METAL CANOPY: (1) HOLLOW METAL DECORATIVE ROOF STRUCTURE
- 8. METAL CANOPY
- 9. 6" S PROFILED THROUGH GLAZING
- 10. CONCRETE CURB
- 11. HOLLOW METAL DOOR AND FRAME
- 12. METAL ROLL UP DOOR AND TRACK
- 13. PRE CAST CONCRETE WALK CAP
- 14. LIGHT FIXTURE
- 15. 6" S H-V ALUMINUM JUNIT
- 16. PRE CAST CONCRETE WALL CAP
- 17. MULTIPLE DEGREE BY 10 MANI
- 18. ENTRY STRUCTURE AND PERG
- 19. GALVANIZED CHAIN LINK FENCE
- 20. BRICK ACCENTA TYP
- 21. FABRIC COATED METAL AWNING
- 22. STONE VENEER

COLOR LEGEND:

- 1. PAINT COLOR TO MATCH BENJAMIN MOORE # 931 "ROCK CANDY"
- 2. PAINT COLOR TO MATCH BENJAMIN MOORE # 724 "CRISP PINK"
- 3. PAINT COLOR TO MATCH BENJAMIN MOORE # 1103 "KUNCIY TAN"
- 4. PAINT COLOR TO MATCH BENJAMIN MOORE # 1435 "SEATTLE BIST"
- 5. PAINT COLOR TO MATCH BENJAMIN MOORE # 1487 "HERBAL ESCAPE"
- 6. PAINT COLOR TO MATCH BENJAMIN MOORE # 1437 "LOVE LANE"
- 7. "HARTFORD GREEN #1-90"
- 8. BRICK VENEER # 303 505 BY BENJAMIN MOORE # 1437 "LOVE LANE"
- 9. PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 "SOFT BLUE GRAY"
- 10. MULTIMATERIAL, SPLIT FACE "ARTISAN WHITE"
- 11. MULTIMATERIAL, GRAYS & WHITES CENTER "ARTISAN WHITE"
- 12. COLOR "GRAY"
- 13. MULTIMATERIAL, SPLIT FACE # 085W
- 14. CLEAR ANODIZED ALUMINUM
- 15. COLOR TO MATCH ADJ WALL
- 16. COLOR TO MATCH BRICK VENEER
- 17. PAINT COLOR TO MATCH BENJAMIN MOORE # 068 "ANTIQUE PARCHMENT"
- 18. PAINT COLOR TO MATCH BENJAMIN MOORE # 1522 "STARDUST LIGHT"
- 19. PAINT COLOR TO MATCH BENJAMIN MOORE # 951 "DEERBER WHITE"
- 20. FABRIC: SUNSHIELD WITH "BLACK" STRETCHED TIGHT FRAME 2 1/2 ALUM TUBE POWDER COATED TO MATCH FABRIC
- 21. DARK BRICK
- 22. FABRIC: SUNSHIELD WITH "LUDCAN" STRETCHED TIGHT FRAME 2 1/2 ALUM TUBE POWDER COATED TO MATCH FABRIC
- 23. BLENDED GRANITE & GRANITE GRANITE
- 24. MULTIMATERIAL, GRAYS "ARTISAN"

CITY OF MEDFORD
EXHIBIT # "S"
File # AC-15-133

BUILDING ELEVATIONS



Scale: 1/8" = 1'-0"

BUILDING H

NORTHGATE MARKETPLACE
MEDFORD, OR (GON R7501)

A4

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mcdonald@medford.gov

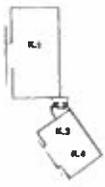
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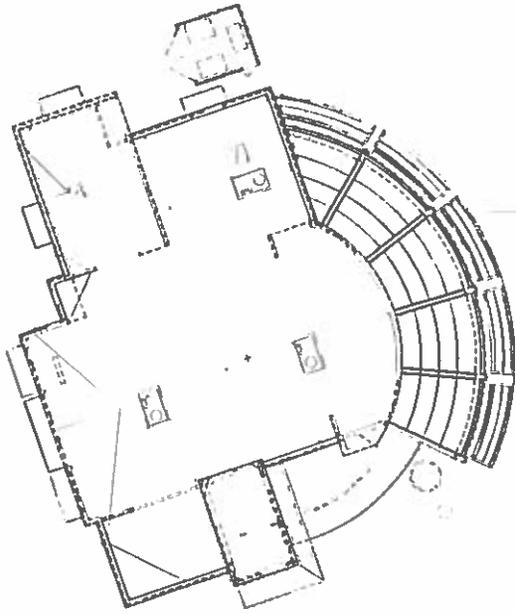
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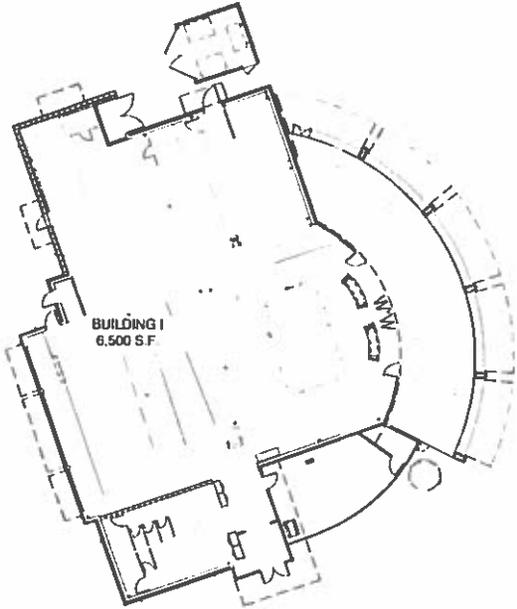


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 mgp@architecture.com



ROOF PLAN



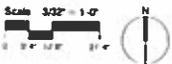
BUILDING I
6,500 S.F.

FLOOR PLAN

BUILDING I

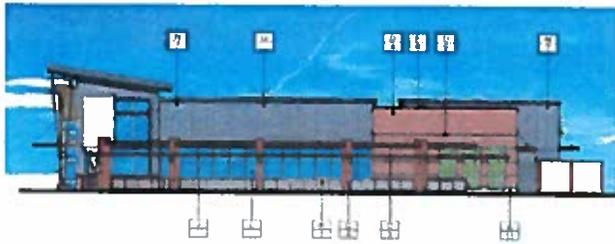
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 MEDFORD, OREGON 97501

FLOOR AND ROOF PLAN



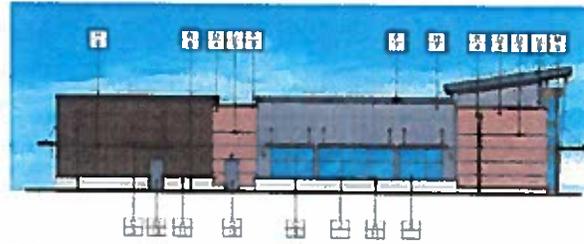
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 File # AC-15-133

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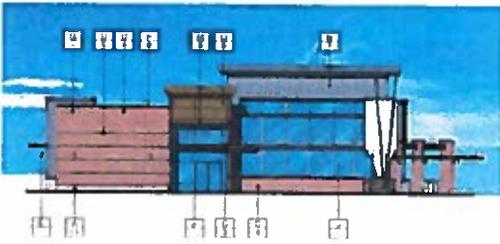
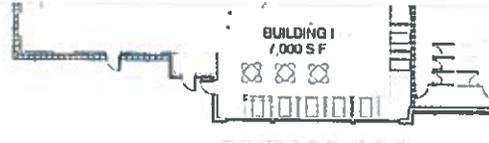
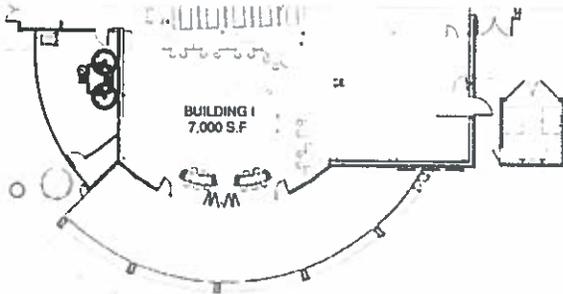
EAST ELEVATION

- TO PARAPET +24'-0" AFF
- TO PARAPET FRAMING +21'-4" AFF
- TO PARAPET FRAMING +10'-0" AFF
- TO CANOPY +10'-0" AFF
- FIN FLOOR +0'-0" AFF



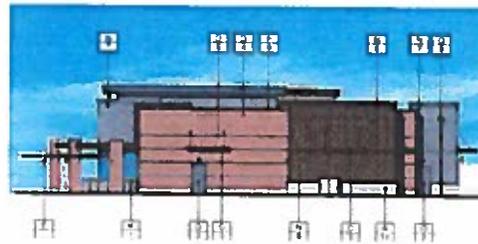
WEST ELEVATION

- TO PARAPET +24'-0" AFF
- TO PARAPET FRAMING +21'-4" AFF
- TO PARAPET FRAMING +10'-0" AFF
- TO CANOPY +10'-0" AFF
- FIN FLOOR +0'-0" AFF



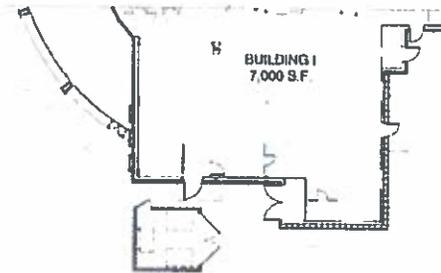
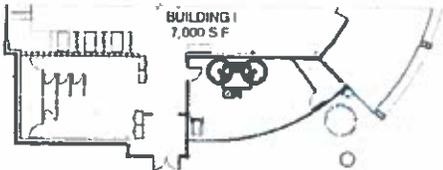
SOUTH ELEVATION

- TO PARAPET +24'-0" AFF
- TO PARAPET FRAMING +21'-4" AFF
- TO PARAPET FRAMING +10'-0" AFF
- TO CANOPY +10'-0" AFF
- FIN FLOOR +0'-0" AFF



NORTH ELEVATION

- TO PARAPET +24'-0" AFF
- TO PARAPET FRAMING +21'-4" AFF
- TO PARAPET FRAMING +10'-0" AFF
- TO CANOPY +10'-0" AFF
- FIN FLOOR +0'-0" AFF



MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND	COLOR LEGEND
CONCRETE BASE	PART. COLOR TO MATCH
CMLI FIN. FT. FACE TRASH ENCLOSURE WALL	BRUNNEN MEXICO # 3112 80 "3" ONE SHOWN
METAL CANOPY	PART. COLOR TO MATCH
BRICK VENEER	BRUNNEN MEXICO # 3121 80 "GRAY"
EXPOSING	PART. COLOR TO MATCH
METAL CYRUS	BRUNNEN MEXICO # 3121 80 "WHITALL BROWN"
BRASS	INTERSTATE BRICK W/ALUMI. MATTE T&T LUG
METAL WALL PANEL	INTERSTATE BRICK BLACK MATTE T&T LUG
BRONZE ALUMINUM ETCH-FRONT	CMLI INTERNAL COLOR COCOA SANDBLASTED
METAL RAILING	ACP SPAN COOL FACTURE #
ALUMINUM FOLDING STOREFRONT UNLAD. SHURLOCK	ACP SPAN COOL WEATHERED COPPER
ALUMINUM PANEL FASCIA	ACP SPAN RW WAVE, CMLI WEATHERED COPPER
METAL BEARING	GREEN
HM EXPOS	NATURAL
METAL LOUVER MECHANICAL SCREENS, DARK BRONZE	WATER
METAL PANELS	COLOR
DIVING POLES	
BLACK SPANDREL GLASS AT LOWER STONE FINISH	

ELEVATIONS



BUILDING I

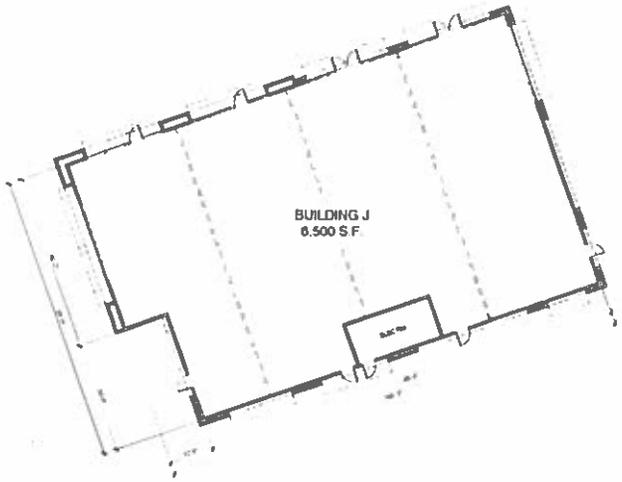
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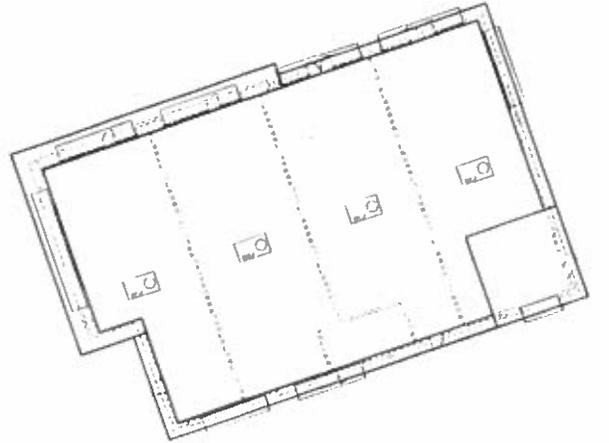
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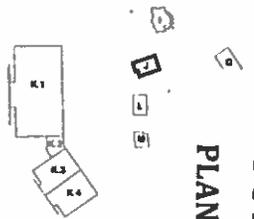




FLOOR PLAN



ROOF PLAN

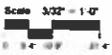


KEY PLAN

FLOOR AND ROOF PLAN

BUILDING J

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501



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 San Francisco, California 94109-4461
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 mcgovern@mcgovern.com

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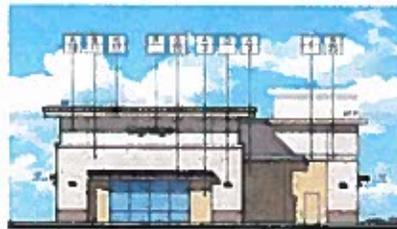
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 File # AC-15-133



NORTH ELEVATION



SOUTH ELEVATION



WEST ELEVATION



EAST ELEVATION



MATERIAL AND FINISH SCHEDULE

MATERIAL / FORM	COLOR / FINISH
EIFS SYSTEM WITH FINE SAND TEXTURE FINISH	PAINT COLOR TO MATCH BENJAMIN MOORE # 9117 "MOULD LAMPT"
FRN'S VENEER	PAINT COLOR TO MATCH
CAU BLOCK - SMOOTH FACE 8"X8"X16"	PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP NAVY"
CAU BLOCK - SPLIT FACE 8"X8"X16"	PAINT COLOR TO MATCH BENJAMIN MOORE # 141 "24"XK MARY TAN"
ALUMINUM STOREFRONT SYSTEM WITH 1" CLEAR INSUL. GLAZES	PAINT COLOR TO MATCH BENJAMIN MOORE # 1535 "SEATTLE" MIST"
PRE-CAST CONCRETE BASE	PAINT COLOR TO MATCH BENJAMIN MOORE # 1487 "HEMBAL ESCAPE"
METAL CANOPY TRELLIS/ARBOR DECORATIVE ROOF STRUCTURE	PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "GLEN O'GREEN"
METAL COPING	"TAN" CHOCOLATE # 1507
EIFS PROFILED TRIM MOLDING	BRICK VENEER #603-665 BY DELDEN OR INDICAT. EQUIVALENT
CONCRETE CLIMB	PAINT COLOR TO MATCH BENJAMIN MOORE # 2134-80 "GULL WING GRAY"
HOLLOW METAL DOOR AND FRAME	
METAL ROLL UP LOADING DOOR	
PRE-CAST CONCRETE BASE CAP	
LIGHT FIXTURES	
EIFS REVEAL/CONTROL Joints	
PRE-CAST CONCRETE WALL CAP	
FUTURE SIGNAGE BY TENANT	
EMPTY SIGNATURE AND PENS	
GALVANIZED CHAIN LINK FENCE	
WOOD ACCENTS - TYP	
FABRIC / FLOOR METAL AWNINGS	
STONE VENEER	
	PAINT COLOR TO MATCH BENJAMIN MOORE # 1531 "EMERALD GREEN FLY"
	PAINT COLOR TO MATCH BENJAMIN MOORE # 905 "BURNING WHITE"
	FABRIC: SUNBRELLA 8008 "BLACK" STRETCHED TIGHT FRAME: 242 ALUM. TUBE POWDER COATED TO MATCH FABRIC
	DARK BRONZE
	FABRIC: SUNBRELLA 8017 "FLUSCAN" STRETCHED TIGHT FRAME: 242 ALUM. TUBE POWDER COATED TO MATCH FABRIC
	INTERIOP MARBLE & GRANITE: GIALLO VENEZIANO GRANITE
	MATERIAL MATERIAL GRAYS & WHITE COLLECTION "CRAWL"

BUILDING ELEVATIONS



Scale: 1/8" = 1'-0"

BUILDING J

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

A8

250 Sutter Street, Suite 500
San Francisco, California 94109-4461
T: 415.774.0002 F: 415.774.1556
mcgratharchitect.com

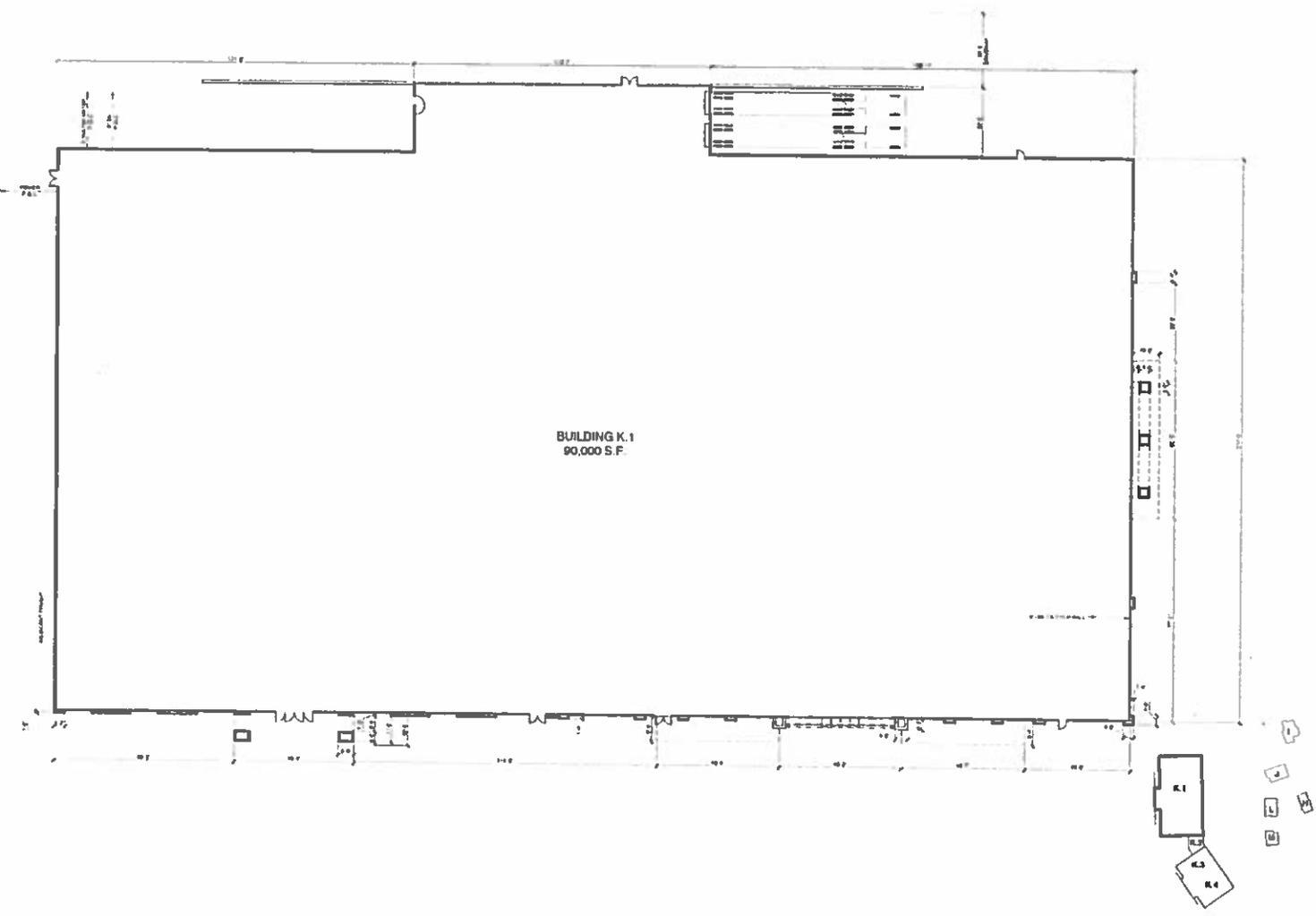
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BUILDING K.1
90,000 S.F.

A9

BUILDING K.1

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

250 Baker Street, Suite 800
San Francisco, California 94108-4461
P: 415.874.3082 F: 415.874.1558
mcparchitecture.com

FLOOR PLAN



100 LIGHTS
15 1/4" DIA
VACUUM
DRAINAGE

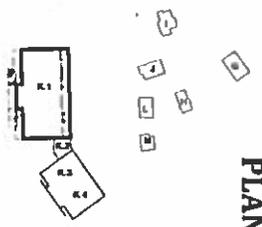
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lot 2

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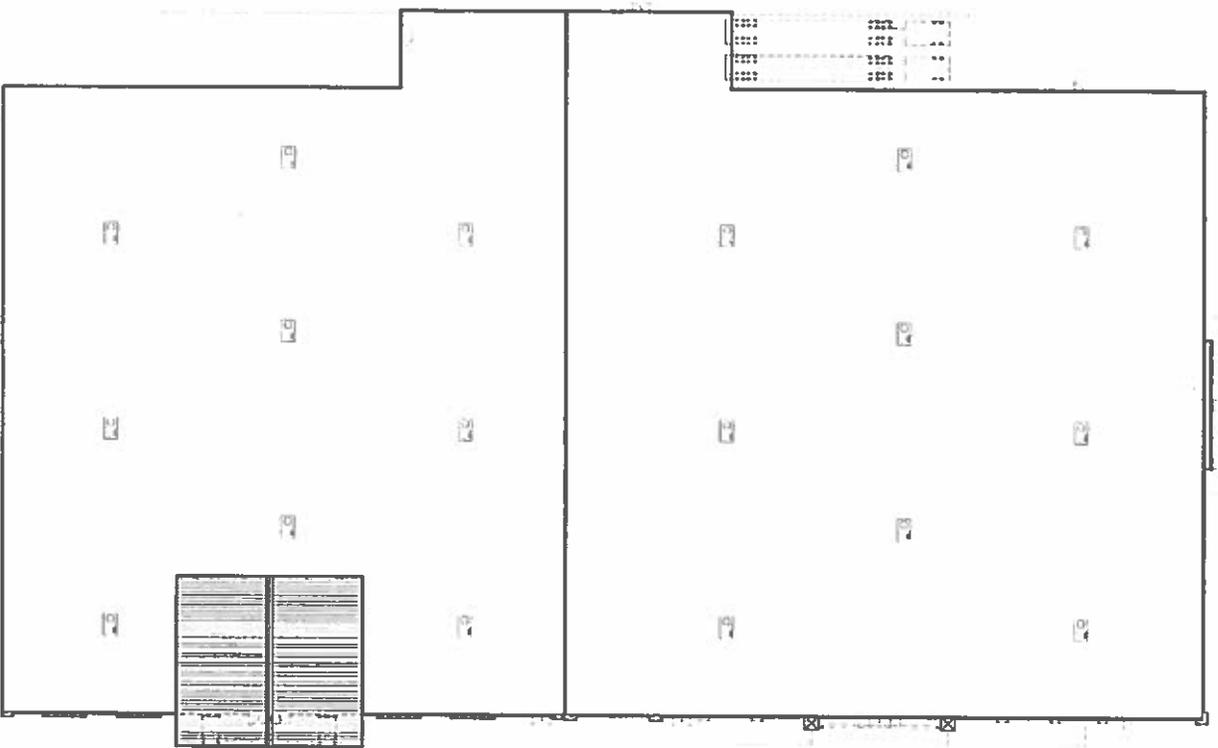


A9.1

252 Market Street, Suite 200
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BUILDING K.1

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ROOF PLAN



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2 of 2

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A11

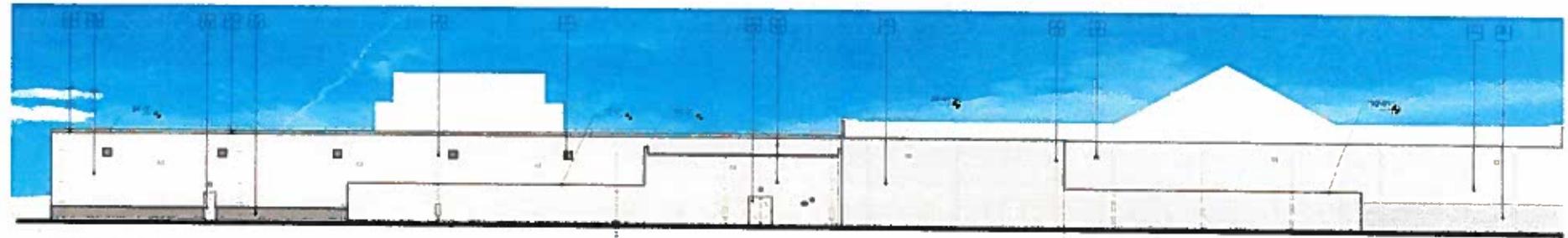
MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND

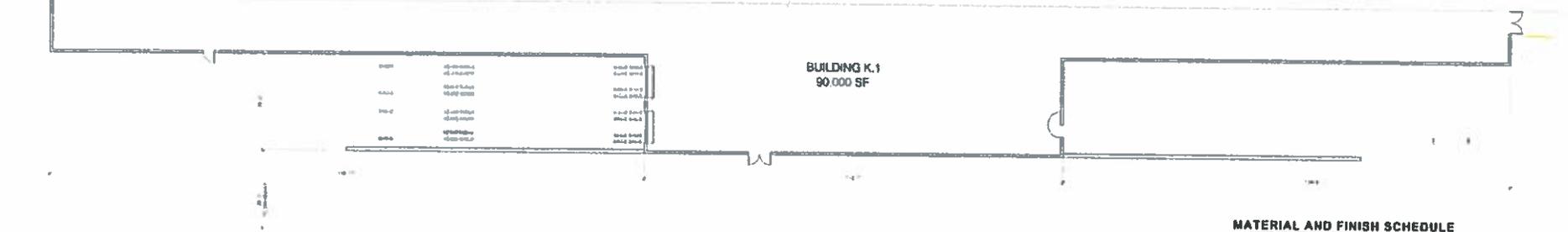
- EIFS SYSTEM WITH FINE SAND TEXTURE FINISH
- BRICK VENEER
- CMU BLOCK - 8"X8"X16" FACE 8"X8"X16"
- CAU BLOCK - SPLIT FACE 8"X8"X16"
- ALUMINUM STOREFRONT SYSTEM WITH 1" CLEAR INSUL. GLAZING
- PRE-CAST CONCRETE BASE
- METAL CANOPY/TRELLIS/ARBOR DECORATIVE HOOP STRUCTURE
- METAL COPING
- EIFS PROFILED TRIM/MOLDED
- CONCRETE CURB
- HOLLOW METAL DOOR AND FRAME
- METAL ROLL-UP LOADING DOOR
- PRE-CAST CONCRETE BASE CAP
- LIGHT FIXTURE
- EIFS REVEAL/CONTROL JOINT
- PRE-CAST CONCRETE WALL CAP
- FUTURE SIGNAGE BY TENANT
- ENTRY STRUCTURE AND PIERS
- GALVANIZED CHAIN LINK FENCE
- BRICK ACCENTS TYP
- METAL REVEAL
- WOOD SIDING
- STONE VENEER
- STANDING SEAM METAL ROOF
- DOWNSPOUT
- HALF ROUND LOG SIDING

COLOR LEGEND

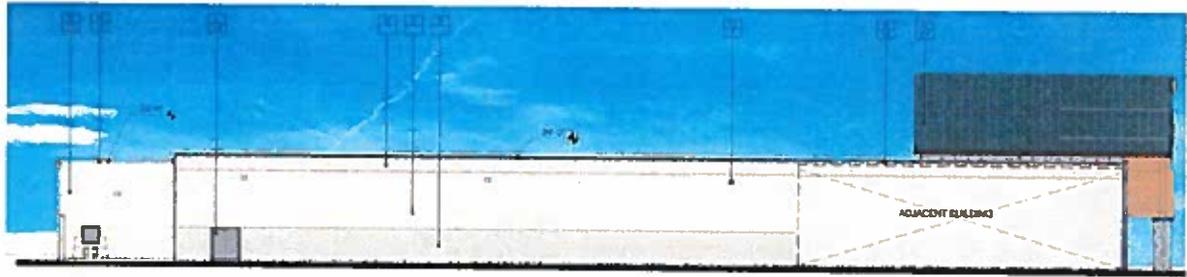
- PAINT - COLOR TO MATCH BENJAMIN MOORE # 937 "ROCK CANDY"
- PAINT - COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP RHINO"
- PAINT - COLOR TO MATCH BENJAMIN MOORE # HC 26 "QUINCY TAN"
- PAINT - COLOR TO MATCH BENJAMIN MOORE # 1535 "BEAT THE BEAT"
- PAINT - COLOR TO MATCH BENJAMIN MOORE # 1467 "HERBAL ESCAPE"
- PAINT - COLOR TO MATCH BENJAMIN MOORE # 2134 "SO "GULL WING GRAY"
- NATURAL MATERIAL - SPLIT FACE "CASTLE WHITE" COLLECTION "ALPINE MIST"
- C2B INC "GRAY" SMOOTH LINE STONE FINISH
- CLEAR ANODIZED ALUMINUM
- COLOR TO MATCH ADJ WA
- COLOR TO MATCH BRICK &
- INNOVATIVE POWER COAT D80 POWDER COAT
- AMERICAN WOODS - RECU SIDING - NATURAL FINISH
- AMERICAN WOODS - RECUP SIDING - STAINED FINISH
- CENTURION STONE 648 AS
- KAMMER - NIGHT HAWK C
- DIMENSIONAL METALS - PK GREEN



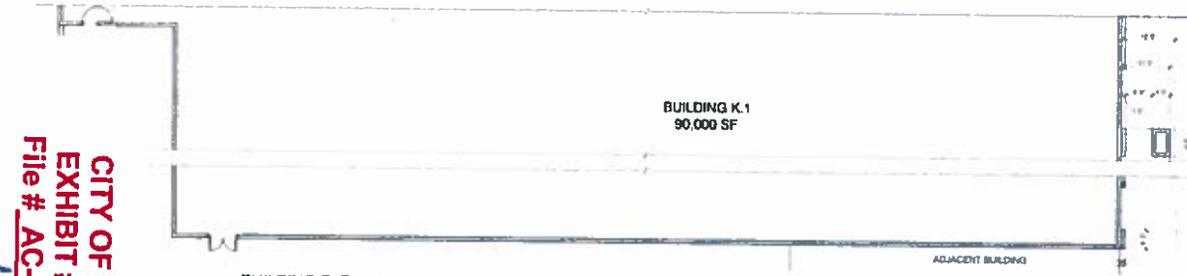
WEST ELEVATION



BUILDING K.1
90,000 SF



SOUTH ELEVATION



BUILDING K.1
90,000 SF

BUILDING K.1

BUILDING ELEVATIONS

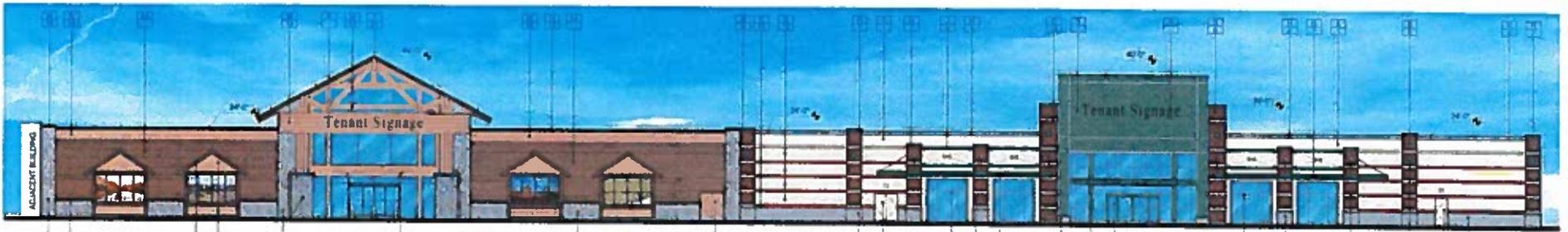


NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

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CITY OF MEDFORD
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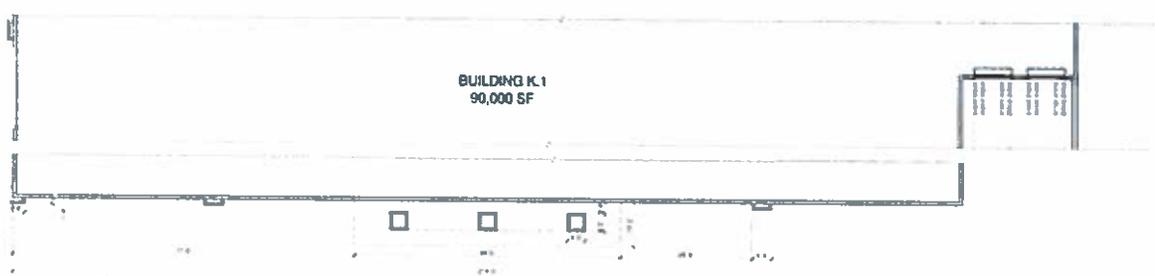
10/1/2015
15:14:21
REVIS AS
SUBMITTAL
10/1/2015
15:14:21
10/1/2015
15:14:21



EAST ELEVATION



NORTH ELEVATION



BUILDING ELEVATIONS

BUILDING K.1

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND

- 1. EPS SYSTEM WITH FINE SAND TEXTURE FINISH
- 2. BRICK VENEER
- 3. CLAU BLOCK - SMOOTH FACE 8'x8'x16"
- 4. CLAU BLOCK - SPLIT FACE 8'x8'x16"
- 5. ALUMINUM STORE FRONT SYSTEM WITH 1" CLEAR INSUL. GLAZING
- 6. PRE-CAST CONCRETE BASE
- 7. METAL CANOPY/TRELIS/GARDEN DECORATIVE WOOD STRUCTURE
- 8. METAL COPING
- 9. EPS PROFILED TRIM/MOLDING
- 10. CONCRETE CURBS
- 11. HOLLOW METAL DOOR AND FRAME
- 12. METAL ROLL-UP LOADING DOOR
- 13. PRE-CAST CONCRETE BASE CAP
- 14. LIGHT FIXTURE
- 15. EPS REVEAL/CONTROL JOINT
- 16. PRE-CAST CONCRETE WALL CAP
- 17. FUTURE SIGNAGE BY TENANT
- 18. ENTRY STRUCTURE AND PERS
- 19. GALVANIZED CHAIN LINK FENCE
- 20. BRICK ACCENTS TYP
- 21. METAL REVEAL
- 22. WOOD SIDING
- 23. STONE VENEER
- 24. STANDING SEAM METAL ROOF
- 25. DOWNSPOUT
- 26. HALF ROUND LOG SIDING

COLOR SCHEDULE

- 1. PAINT COLOR TO MATCH BENJAMIN MOORE # 937 "WOOD CAMO"
- 2. PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CONSP HAWK"
- 3. PAINT COLOR TO MATCH BENJAMIN MOORE # HC 25 "DUNACY TAN"
- 4. PAINT COLOR TO MATCH BENJAMIN MOORE # 1535 "SEATTLE MIST"
- 5. PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "OR EP CREEK"
- 6. "HARTFORD GREEN P-50"
- 7. BRICK VENEER #531-905 BY BELDEN CH WOOD TUBERVILLE MT
- 8. PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 50 "DULL WING GRAY"
- 9. MUTUAL MATERIAL SPLIT FACE "CASTLE WHITE"
- 10. MUTUAL MATERIAL GRAYS & WHITES COLLECTION "ALPINE MIST"
- 11. COI INC "GRAY" SMOOTH LIMESTONE FIN"
- 12. CLEAR ANODIZED ALUM
- 13. COLOR TO MATCH ADJ
- 14. COLOR TO MATCH BRIC
- 15. INNOVATIVE POWER COV COI POWDER COAT
- 16. ARRIGONI WOODS - RFG SCW3 - NATURAL FINISH
- 17. ARRIGONI WOODS - RFG SCW3 - STAINED FINISH
- 18. CENTURION STONE 484 A
- 19. KANFER - NIGHT HAWK
- 20. DIMENSIONAL METALS IN GREEN

A10

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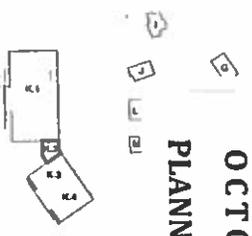
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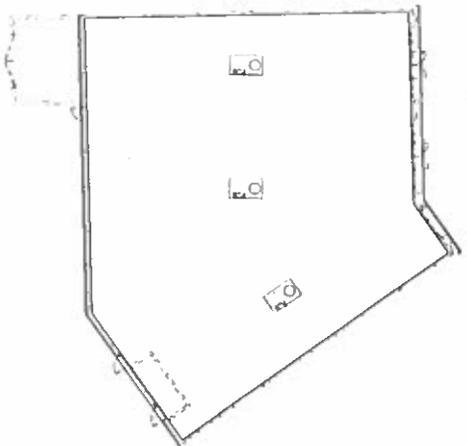
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KEY PLAN

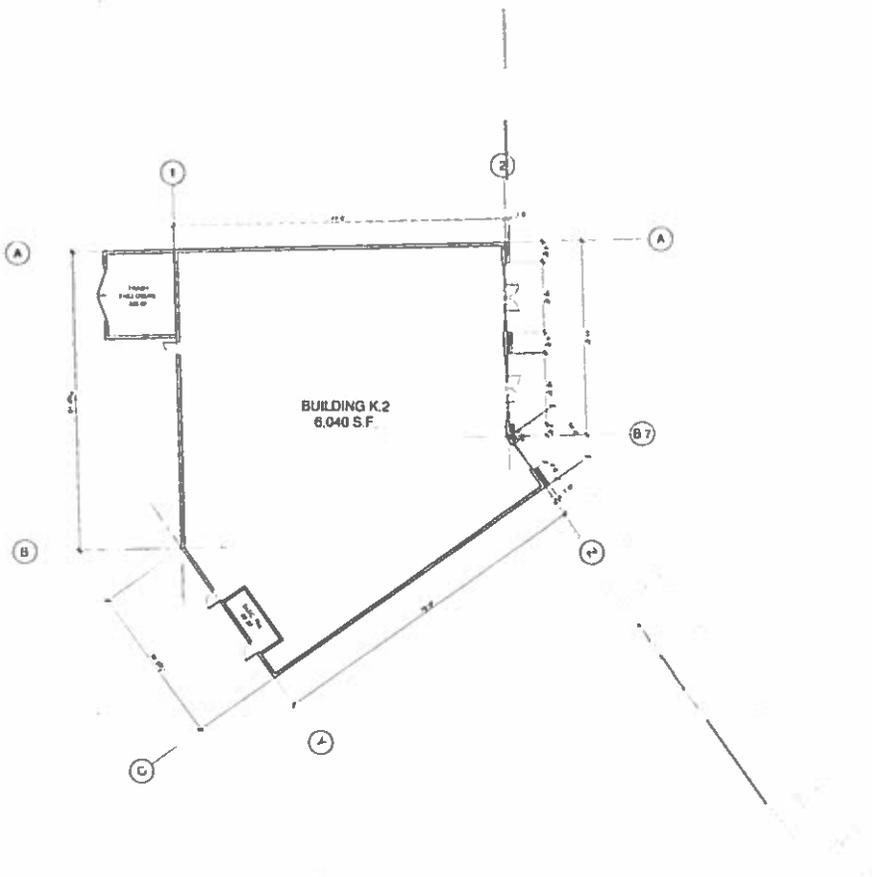


A12

250 Sutter Street, Suite 900
San Francisco, California 94109-4481
D: 415.974.8022 F: 415.974.1556
mcgarchitects.com



ROOF PLAN



FLOOR PLAN

BUILDING K.2
6,040 S.F.

BUILDING K.2

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

FLOOR AND ROOF PLAN



CITY OF MEDFORD
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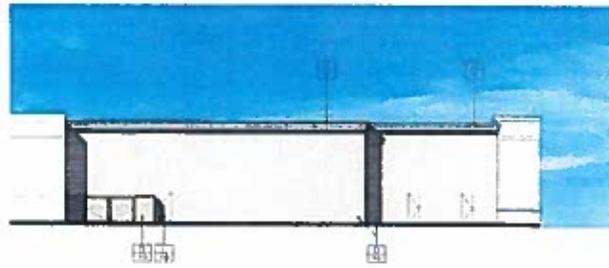
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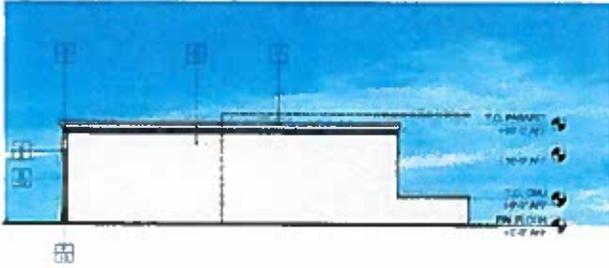
250 Sutter Street, Suite 900
San Francisco, California 94108-4481
T: 415.974.8002 F: 415.974.1928
meyer@medford.com

MATERIAL AND FINISH SCHEDULE

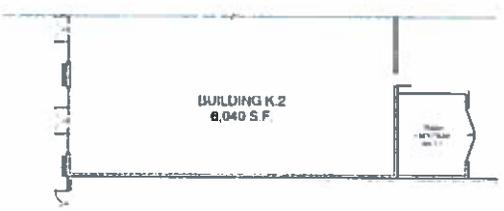
MATERIAL LEGEND	COLOR LEGEND
A BRICK EYESHIM WITH PINK BANDS TYPICAL FINISH	10 PAINT COLOR TO MATCH BENJAMIN MOORE # 612 "TRICK CANDY"
B BRICK VENEER	11 PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP GRAY"
C BRICK - FROTH FACT 6"X6"X16"	12 PAINT COLOR TO MATCH BENJAMIN MOORE # 142 "LUNA TAN"
D BRICK - 12"X8" - 8"X16" FALS 6"X6"X16"	13 PAINT COLOR TO MATCH BENJAMIN MOORE # 1215 "SEATTLE MIST"
E ALUMINUM STOREFRONT SYSTEM WITH 1" (3-PAN) PANELS - 12 ALZING	14 PAINT COLOR TO MATCH BENJAMIN MOORE # 1687 "NORVAL FRODO"
F PRE-CAST CONCRETE BASE	15 PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "DEEP CHARLIE"
G METAL CANOPY WITH LED LIGHTING OF CONCRETE ROOF STRUCTURE	16 PAINT COLOR TO MATCH BENJAMIN MOORE # 1687 "NORVAL FRODO"
H METAL COPING	17 PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "DEEP CHARLIE"
I BRICK VENEER THRU BUILDING	18 PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 "SAGE WIND GRAY"
J CONCRETE CURB	19 METALLIC MATERIAL GRAY & WHITE COLLECTION "ALPINE MIST"
K HOLLOW METAL EXCH AND FRAME	20 COBALT "GRAY" SMOOTH LIMESTONE FINISH
L METAL ROLL-UP LOADING DOOR	21 CLEAR ANODIZED ALUMINUM
M PRE-CAST CONCRETE 12" BUMP CAP LIGHT FIXTURE	22 CLEAR TO MATCH ADU WALL
N BRICK VENEER ALUMINUM JOINT	23 COLOR TO MATCH BACK VENEER
O PRE-CAST CONCRETE WALL CAP	24 PAINT COLOR TO MATCH BENJAMIN MOORE # 1687 "ANTIQUE PARKER HET"
P FUTURE DAMAGE BY TRUCK	25 PAINT COLOR TO MATCH BENJAMIN MOORE # 11
Q ENTRY STRUCTURE AND PIER	26 PAINT COLOR TO MATCH BENJAMIN MOORE # 6 W
R GALVANIZED CHAIN LINK FENCE	27 FABRIC SUNBRELLA @ 6"X6"X12" WITH FRAME 7/8" ALUM TUB COATED TO MATCH FA
S BRICK ACCENTS, TYP	28 DARK BROCADE
T FABRIC LEVNE METAL AWNING	29 FABRIC SUNBRELLA @ 6"X6"X12" WITH FRAME 7/8" ALUM TUB COATED TO MATCH FA
U STONE VENEER	30 INTERPOL MARBLE & L2 GRANITE
	31 METALLIC MATERIAL GR. "GRANITE"



WEST ELEVATION

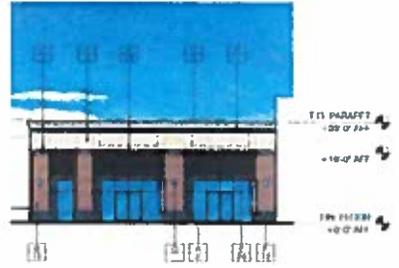


NORTH ELEVATION

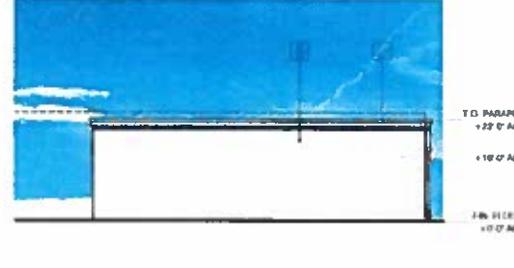
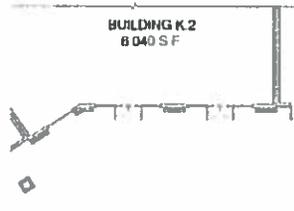


BUILDING K.2

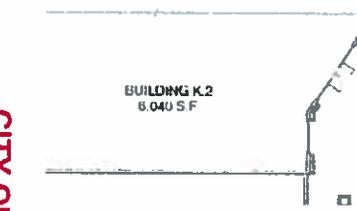
NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501



EAST ELEVATION



SOUTH ELEVATION



ELEVATIONS



Page 188

CITY OF MEDFORD
EXHIBIT # "AA"
File # AC-15-133

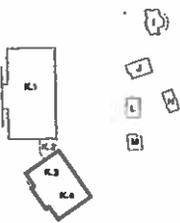
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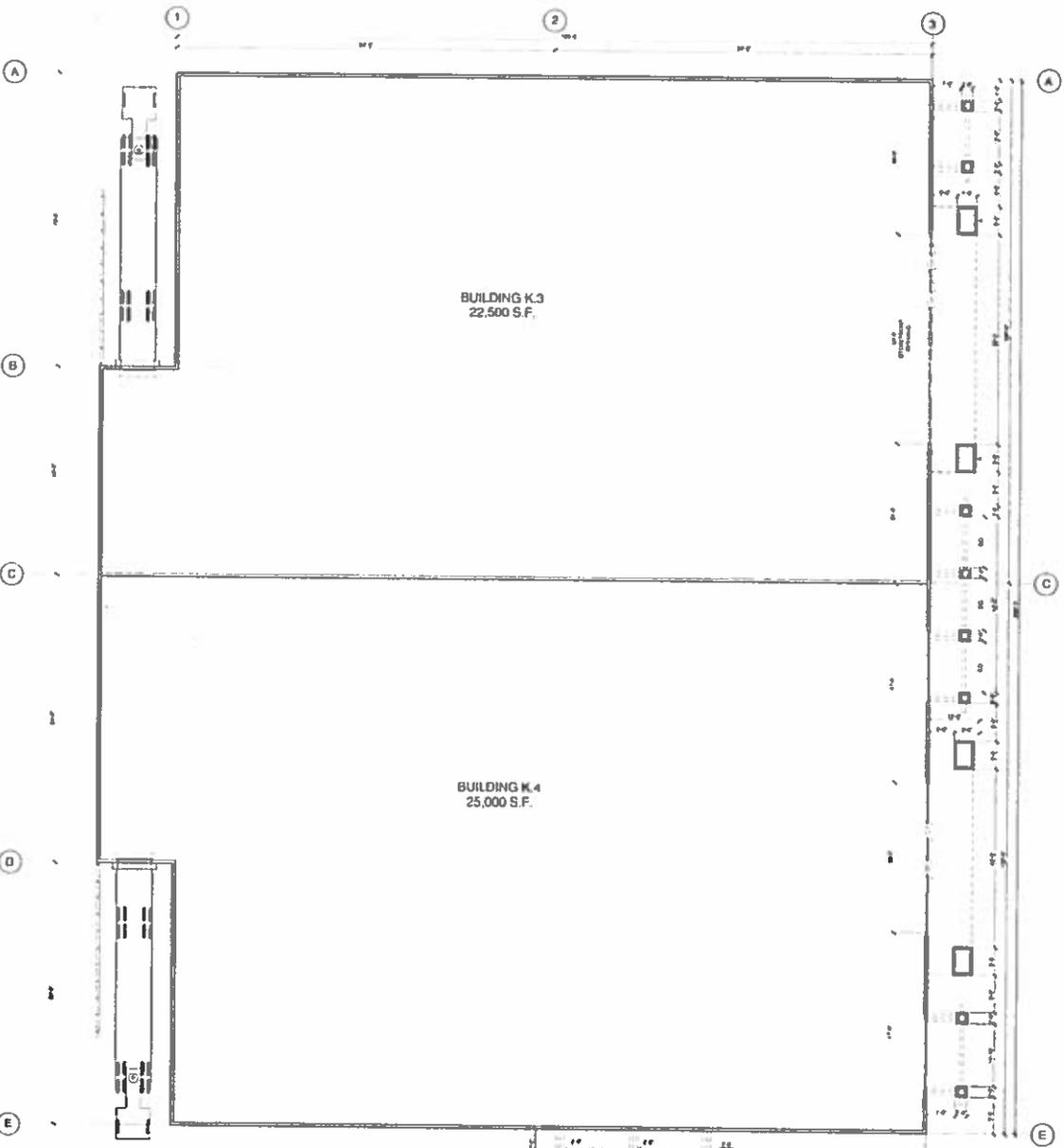


KEY PLAN



A14

225 Sutter Street, Suite 500
San Francisco, California 94102-4491
Tel: 415.674.8002 Fax: 415.674.1650
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BUILDING K.3 & K.4

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

FLOOR PLAN



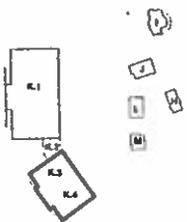
CITY OF MEDFORD
EXHIBIT # BB
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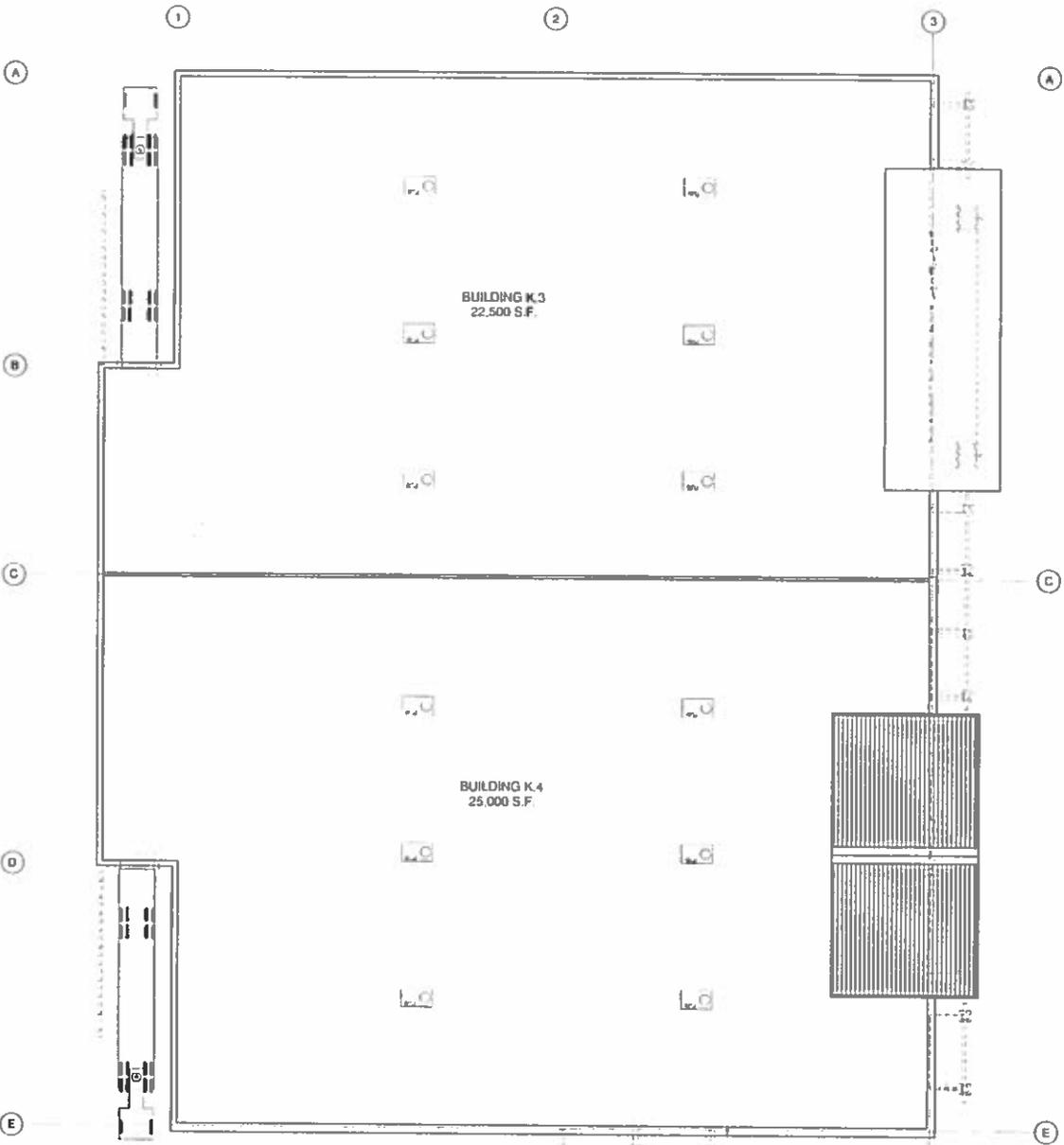
PLANNING DEPARTMENT



KEY PLAN

A14.1

250 Baker Street, Suite 800
 San Francisco, California 94108-4461
 ☎ 415.874.0002 ☎ 415.874.1550
morgenthalers.com



ROOF PLAN

BUILDING K.3 & K.4

NORTHGATE MARKETPLACE
 MEDFORD, OREGON 97501



Scale 3/32" = 1'-0"



CITY OF MEDFORD
 EXHIBIT # AC-15-133
 File # AC-15-133

2012

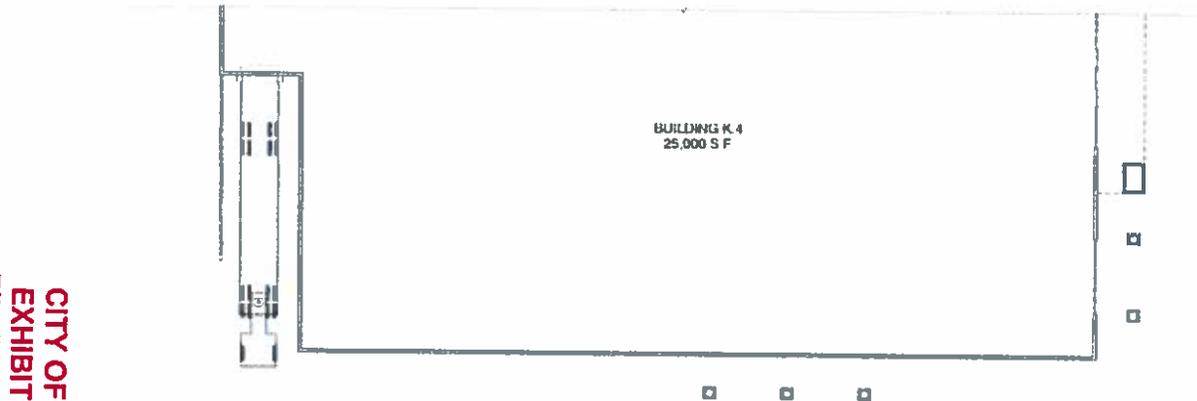
2015



EAST ELEVATION



SOUTH ELEVATION



BUILDING ELEVATIONS



BUILDING K.3 & K.4

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND:	COLOR LEGEND:
1. EIFS SYSTEM WITH FINE SAND 1/8" LUMEN @ 2500	1. PAINT COLOR TO MATCH BENJAMIN MOORE # 837 "ROCK CANDY"
2. BRICK VENEER	2. PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP KISS"
3. GABRIEL BLOCK - MAINTENANCE FACE @ 8" X 8" X 16"	3. PAINT COLOR TO MATCH BENJAMIN MOORE # 1102 "OLIVIA TAN"
4. CMU BLOCK - SPLIT FACE @ 8" X 8" X 16"	4. PAINT COLOR TO MATCH BENJAMIN MOORE # 1534 "REFAITH I MINT"
5. ALUMINA STORE FRONT SYSTEM WITH 1" CLEAR PANEL, GLAZING	5. PAINT COLOR TO MATCH BENJAMIN MOORE # 1467 "THE ROSE @ SLATS"
6. PINK CAST CONCRETE IS BASE	6. PAINT COLOR TO MATCH BENJAMIN MOORE # 1467 "THE ROSE @ SLATS"
7. METAL CANOPY FREESTANDING OR ULTRALIGHT PEAP STRUCTURE	7. PAINT COLOR TO MATCH BENJAMIN MOORE # 1467 "THE ROSE @ SLATS"
8. METAL COPING	8. "HARTFORD GREEN P. 80"
9. EIFS PROCEED TRIANGULAR DIPS	9. BRICK VENEER P525 606 BY DCLDCH (ON PARALLEL EQUIVALENT)
10. CONCRETE CURB	10. PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 "GULL WING GRAY"
11. MELLOW METAL DOOR AND FRAME	11. METAL MATERIAL SPLIT FACE "CASTLE WHITE"
12. METAL ROSS UP LANDING FLOOR	12. METAL MATERIAL GRAYS & WHITES COLLECTION "ALPINE MIST"
13. PCC CAST CONCRETE BASE CAP	13. CDS INC "GRAY"
14. LIGHT FIXTURE	14. SMOOTH LIMESTONE FINISH
15. EIFS REVEAL/CONTROL JOINT	15. CL 1.4M ANODIZED ALUMINUM
16. PINK CAST CONCRETE WALL CAP	16. COLOR TO MATCH ADI WALL
17. FUTURE SIGNAGE BY TO HANT	17. COLOR TO MATCH BRICK W-46 S-4
18. FRUIT STRUCTURE AND PINE	18. PAINT COLOR TO MATCH BENJAMIN MOORE # 868 "ANTIQUE PARCHMENT"
19. GALVANIZED CHAIN LINK FENCE	19. PAINT COLOR TO MATCH BENJAMIN MOORE # 1523 "EMBASSY GREEN"
20. BRICK ALBERTS 11P	20. PAINT COLOR TO MATCH BENJAMIN MOORE # 868 "BERBER WHITE"
21. FABRIC COVERED METAL AWNING	21. FABRIC: SANDRELLA #8 - 3116 (LH-11) REAR
22. STONE VENEER	22. FRAME: 7X2 ALUM TUB COATED TO MATCH FACE
	23. LAMIN MOUNT
	24. FABRIC: SANDRELLA #8 - 3116 (LH-11) REAR
	25. FRAME: 7X2 ALUM TUB COATED TO MATCH FACE
	26. PINK PEO MARBLE & ON GRANITE
	27. METAL MATERIAL GRA "UPONITE"

A15

2000 Center Street, Suite 800
San Francisco, California 94108-4401
T 415.774.0000 F 415.774.1000
www.pulsinone.com

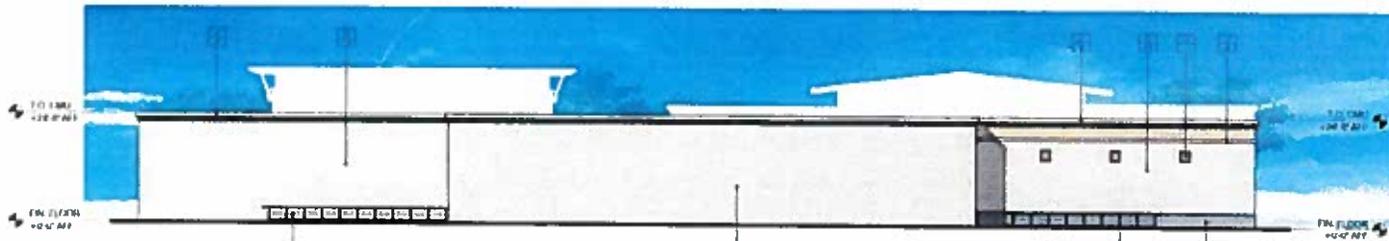
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Page 191

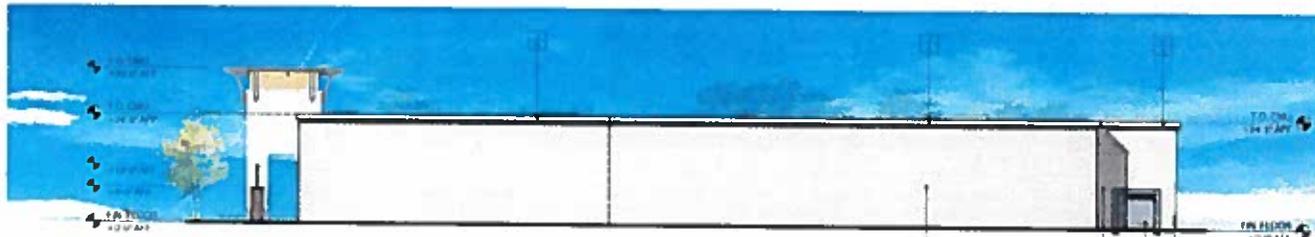
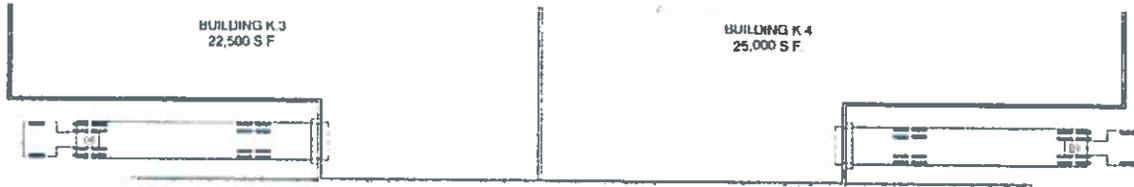
CITY OF MEDFORD
EXHIBIT # CC-
File # AC-15-133

1052

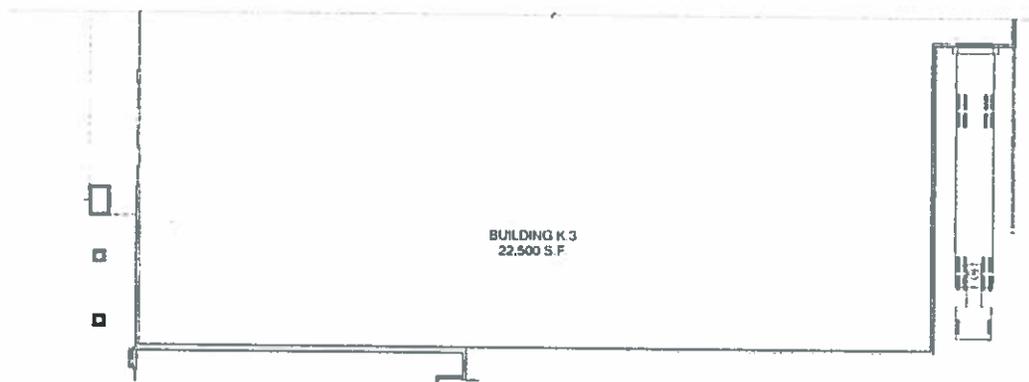
8



WEST ELEVATION



NORTH ELEVATION



BUILDING ELEVATIONS

BUILDING K.3 & K.4

NORTHGATE MARKETPLACE
MDFORD, OREGON 97501

MATERIAL AND FINISH SCHEDULE

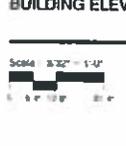
MATERIAL LEGEND	COLOR LEGEND
1 EIFS SYSTEM WITH FINE SAND TRISULFATE FINISH	1 PAINT COLOR TO MATCH BENJAMIN MOORE # 837 "NOCK CANYON"
2 GRICK VENEER	2 PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP RUMBLE"
3 LAMB MILDLY SMOOTH FACE 8"x8"x16"	3 PAINT COLOR TO MATCH BENJAMIN MOORE # 142 "SOLIDITY TAN"
4 CHALK DOLOR SPLIT FACE 8"x8"x16"	4 PAINT COLOR TO MATCH BENJAMIN MOORE # 1554 "SUNSET F. BAY"
5 ALUMINUM SCREW FRONT SYSTEM WITH 1" CLEAN FRAME, 64 ALFIN	5 PAINT COLOR TO MATCH BENJAMIN MOORE # 1467 "SABAL FLAMINGO"
6 PHS CAST CONCRETE BASE	6 PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "SILVER LAKELAKE"
7 METAL CANOPY/TRELLIS/BARBOR URCENATIVE HOLD & HULL LINE	7 "HARTFORD GREEN P. 80"
8 METAL COPING	8 BRICK VENEER #543 605 BY DELDEN ON PITCH 1/2 RISE/12 IN
9 EIFS PRECAST TRIM/ROLLERS	9 PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 "SILVER WING GRAY"
10 CONCRETE CURB	10 MUTUAL MATERIAL SPLIT FACE "CASTLE WHITE"
11 HOLLOW METAL DOOR AND FRAME	11 MUTUAL MATERIAL GRAYS & WHITES COLLECTION "TALKING MIST"
12 METAL ROLL UP LOADING DOOR	12 COBALT "GLAY" SMOOTH LIMESTONE FINISH
13 PRC CAST CONCRETE DALL CAP	13 CLEAN ANCHOR DALL LAMINATE
14 LIGHT FIXTURE	14 COLOR TO MATCH ADU WALL
15 EIFS REVEAL/CONTROL JOINT	15 COBALT MATCH BRICK VENEER
16 PHS CAST CONCRETE WALL CAP	16 PAINT COLOR TO MATCH BENJAMIN MOORE # 809 "ANTIQUE PARCHMENT"
17 FUTURE SIGNAGE BY TENANT	17 PAINT COLOR TO MATCH BENJAMIN MOORE # 1523 "EMERALD GREEN"
18 ENTRY STRUCTURE AND PHS	18 PAINT COLOR "BENJAMIN MOORE"
19 GALVANIZED CHAIN LINK FENCE	19 FABRIC SLING 1/2" X 1/2" X 1/2" FRAME 2X2 IN COATED TO MATCH
20 BRICK ACACIA 1/2" X 1/2"	20 1/2" X 1/2" X 1/2"
21 FABRIC COVERED METAL AWNING	21 FABRIC SLING 1/2" X 1/2" X 1/2" FRAME 2X2 IN COATED TO MATCH
22 STONE VENEER	22 1/2" X 1/2" X 1/2"
	23 FABRIC SLING 1/2" X 1/2" X 1/2" FRAME 2X2 IN COATED TO MATCH
	24 1/2" X 1/2" X 1/2"
	25 FABRIC SLING 1/2" X 1/2" X 1/2" FRAME 2X2 IN COATED TO MATCH
	26 1/2" X 1/2" X 1/2"
	27 1/2" X 1/2" X 1/2"
	28 1/2" X 1/2" X 1/2"
	29 MUTUAL GATE "GRANITE"

A16

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BUILDING K.1, K.2, K.3 & K.4

NORTHGATE MARKETPLACE
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BUILDING ELEVATIONS

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4/15/15
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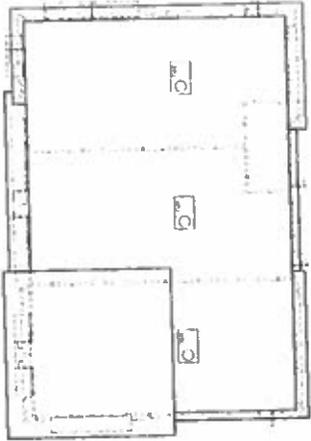
PLANNING DEPARTMENT



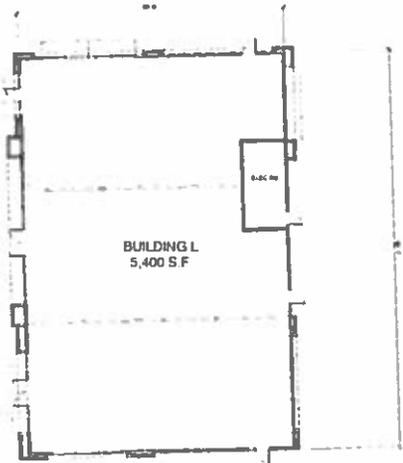
KEY PLAN

A18

250 Butler Street, Suite 200
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D 415.874.8822 F 415.874.1536
mgsarchitecture.com



ROOF PLAN



FLOOR PLAN

BUILDING L

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

FLOOR AND ROOF PLAN



Scale 1/32" = 1'-0"

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A19

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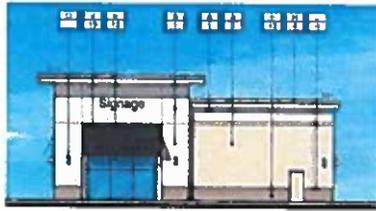
MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND:

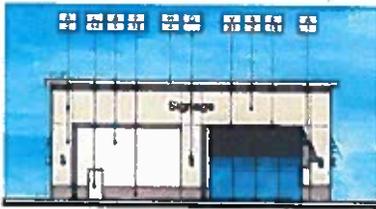
- 1 EPS SYSTEM WITH FINE SAND TEXTURE (8/24)
- 2 BRICK VENEER
- 3 CURU BLOCK - SMOOTH FACE 8"x8"x16"
- 4 CURU BLOCK - SPLIT FACE 8"x8"x16"
- 5 ALUMINUM STORE FRONT SYSTEM WITH 1" CLEAR PANEL, OR ADP3
- 6 PRE-CAST CONCRETE BASE
- 7 METAL CARRY-TYRELLS-AMBUR (EXTERIOR ROOF STRUCTION)
- 8 METAL CLAPPING
- 9 EPS PROF & D'S TRIM MOLDING
- 10 CONCRETE CURB
- 11 HELIXON METAL BRUSH ANET FRAM-
- 12 METAL ROLL-UP LOADING DOOR
- 13 PRE-CAST CONCRETE WASH CAP
- 14 LIGHT FEATURE
- 15 EPS REVEAL CONTROL JOINT
- 16 PRE-CAST-CONCRETE WALL CAP
- 17 FUTURE SIGNAGE BY TENANT
- 18 ENTRY STRUCTURE AND PIERS
- 19 GALVANIZED CHANNEL FENCE
- 20 BRICK ACCENTS, EYP
- 21 FABRIC SUNSHIELD (SEE ANNOTS)
- 22 STONE VENEER

COLOR LEGEND:

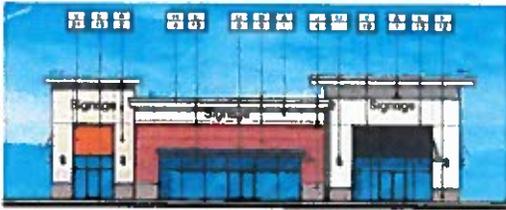
- 1 PAINT COLOR TO MATCH BENJAMIN MOORE # 937 "ROCK CANDY"
- 2 PAINT COLOR TO MATCH BENJAMIN MOORE # 234 "CRISP ROAST"
- 3 PAINT COLOR TO MATCH BENJAMIN MOORE # HC 25 "JOURNEY TAK"
- 4 PAINT COLOR TO MATCH BENJAMIN MOORE # 1535 "SEATTLE MIST"
- 5 PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "HERBAL ESCAPE"
- 6 PAINT COLOR TO MATCH BENJAMIN MOORE # 1477 "DEEP CREEK"
- 7 "MARTINDALE GREEN P 50"
- 8 BRICK VENEER ACCENTS BY BRIDEN OR INECCOT EQUIVALENT
- 9 PAINT COLOR TO MATCH BENJAMIN MOORE # 2134 "SAGE FLANKERWAY"
- 10 NATURAL MATERIAL SPLIT FACE "CASTLE WALL"
- 11 INTERIOR MATERIAL GRAYS & WHITE'S COLLECTION "A19 PINE 1057"
- 12 CEDRIC "LHAY"
- 13 SMOOTH LIMESTONE FINISH
- 14 CLEAR ANODIZED ALUMINUM
- 15 COLOR TO MATCH ADU WALL
- 16 COLOR TO MATCH BRICK VENEER
- 17 PAINT COLOR TO MATCH BENJAMIN MOORE # 805 "ANTIQUE PARCHMENT"
- 18 PAINT COLOR TO MATCH BENJAMIN MOORE # 1523 "TENNESSEE GREEN"
- 19 PAINT COLOR TO MATCH BENJAMIN MOORE # 903 "BERGER WHITE"
- 20 FABRIC SUNSHIELD 60008 "BLACK" STRETCHED TIGHT FRAME 2X2 ALUM TUBE POWDER COATED TO MATCH FABRIC
- 21 DARK BRONZE
- 22 FABRIC SUNSHIELD 60457 "TURCAN" STRETCHED TIGHT FRAME 2X2 ALUM TUBE POWDER COATED TO MATCH FABRIC
- 23 PINK PIG MARBLE & GRANITE (SEE GRANITE)
- 24 NATURAL MATERIAL GRAYS & WHITE'S "GRANITE"



SOUTH ELEVATION



NORTH ELEVATION



WEST ELEVATION



EAST ELEVATION



BUILDING L

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

BUILDING ELEVATIONS



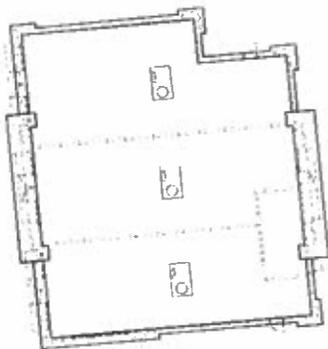
1/8" = 1'-0"
1/4" = 2'-0"
1/2" = 4'-0"
3/4" = 6'-0"
1" = 8'-0"

CITY OF MEDFORD
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File # AC-15-133

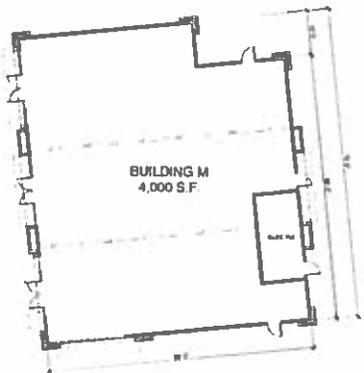
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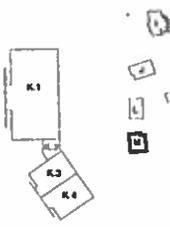
PLANNING DEPARTMENT



ROOF PLAN



FLOOR PLAN



KEY PLAN

A20

250 Market Street, Suite 420
San Francisco, California 94102-4481
☎ 415.874.6262 ☎ 415.874.1558
meyerchick.com

BUILDING M
NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

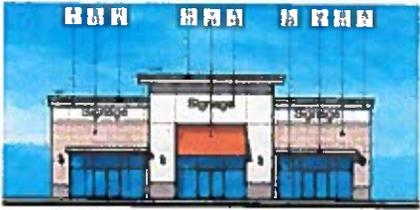
FLOOR AND ROOF PLAN

Scale 3/32" = 1'-0"

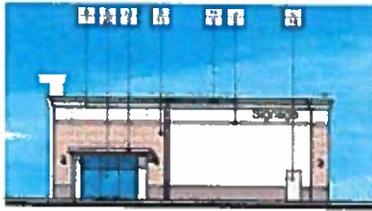


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File # AC-15-133

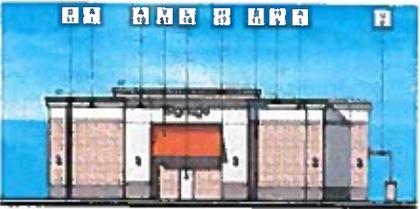
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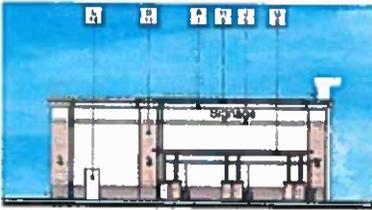
WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION



MATERIAL AND FINISH SCHEDULE

MATERIAL LEGEND	COLOR LEGEND
A AIR SYSTEM WITH FINE SAND TEXTURE FINISH	1 PANT. COLOR TO MATCH BENJAMIN MOORE # 837 "ROCK CANDY"
B BRICK VENEER	2 PANT. COLOR TO MATCH BENJAMIN MOORE # 231 "CRISP KHAKI"
C BRICK BLOCK BAYBOTH FACE EXPOSED	3 PANT. COLOR TO MATCH BENJAMIN MOORE # MC 25 "COURCY TAN"
D BRICK BLOCK SPLIT FACE EXPOSED	4 PANT. COLOR TO MATCH BENJAMIN MOORE # 1535 "SEATTLE MIST"
E ALUMINUM STOREFRONT SYSTEM WITH 1" CLEAR INSUL. GLAZING	5 PANT. COLOR TO MATCH BENJAMIN MOORE # 1877 "VERMILION ESCAPE"
F PRE-CAST CONCRETE BASE	6 PANT. COLOR TO MATCH BENJAMIN MOORE # 1917 "DEEP CAMEL"
G METAL CANOPY/TIMBER/BRUSH IN COGNITIVE ROOF STRUCTURE	7 "HARTFORD GREY PL-30"
H METAL CLIPPING	8 BRICK VENEER #603 500 BY BILSDEN OR EQUIVALENT
I 6" B PHOENIX TERRAZZO CLING	9 PANT. COLOR TO MATCH BENJAMIN MOORE # 7134 50 "LACE WIND GRAY"
J CONCRETE CURB	10 MUTUAL MATERIAL SPLIT FACE "CASTLE WHITE"
K HOLLOW METAL UPRIGHT AND FRAME	11 MUTUAL MATERIAL GRAYS & WHITES COLLECTION "ALPINE MIST"
L METAL ROLL-UP LOADING DOOR	12 CDI PNC "GRAY"
M PRE-CAST CONCRETE BASE CAP	13 SMOOTH LIMESTONE FINISH
N LIGHT FIXTURE	14 CLEAR ANODIZED ALUMINUM
O PFS REVEAL CENTER JOINT	15 COLOR TO MATCH ADJ WALL
P PRE-CAST CONCRETE WALL CAP	16 COLOR TO MATCH BRICK VENEER
Q FUTURE SIGNAGE BY PLANT	17 PANT. COLOR TO MATCH BENJAMIN MOORE # 808 "AN HOUR PANCHRANT"
R ENTRY STRUCTURE AND PIERS	18 PANT. COLOR TO MATCH BENJAMIN MOORE # 1523 "EMERALD GREEN"
S GALVANIZED CHAIN LINK FENCE	19 PANT. COLOR TO MATCH BENJAMIN MOORE # 855 "ORDER WHITE"
T BRICK ACCENTS TYP	20 FABRIC SUNBELLER #808 "BLACK" STRETCHED TIGHT
U FABRIC COATED METAL AWNING	21 FRAME 242 ALUM. TUBE POWDER COATED TO MATCH FABRIC
V STONE VENEER	22 DARK BRONZE
	23 FABRIC SUNBELLER #8077 "TUSCAN" STRETCHED TIGHT
	24 FRAME 242 ALUM. TUBE POWDER COATED TO MATCH FABRIC
	25 PRESHIPED MARBLE & GRANITE GRALLO VERMILION GRANITE
	26 MUTUAL MATERIAL GRAYS & WHITES "GRANITE"

BUILDING M

NORTHGATE MARKETPLACE
MEDFORD, OREGON 97501

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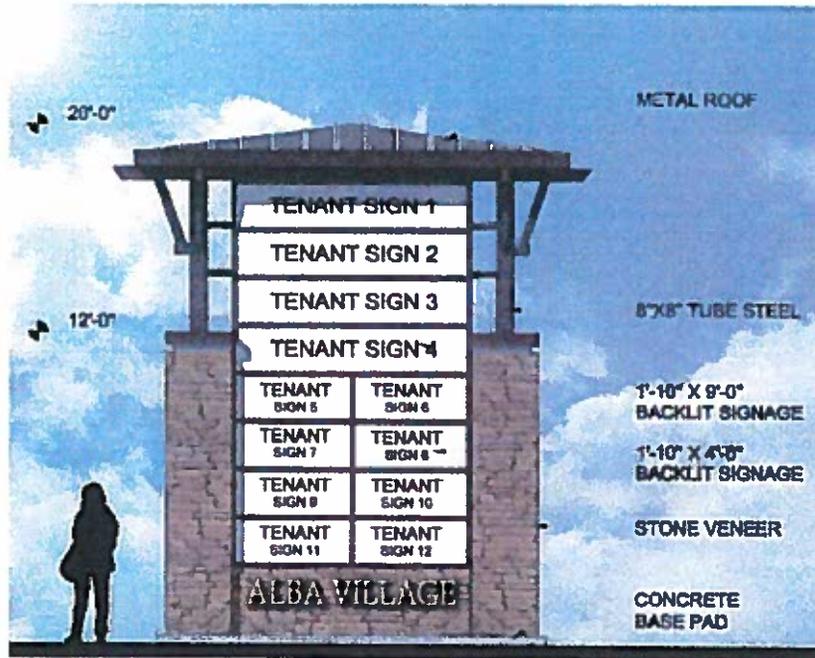
CITY OF MEDFORD
EXHIBIT #
File # AC-15-133

AC-14-0716 Appendix

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JUN 13 2011
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SIDE ELEVATION



FRONT / BACK ELEVATION
Sign Area: 150 SF

Pylon Sign
Tenant Identification Sign

regency centers
1000 West Main Street
Medford, Oregon 97504
Tel: 531.477.2000

Perkins + East
1000 West Main Street
Medford, Oregon 97504
Tel: 531.477.2000

HBH
Consulting
Engineers
2000 Old Pacific Highway, Suite 100
Medford, Oregon 97504
Tel: 531.477.2000

galbraith
2000 Old Pacific Highway
Medford, Oregon 97504
Tel: 531.477.2000

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ALBA VILLAGE
Retail Shopping Center
West of Main Highway 201, Corvallis Avenue
Medford, Oregon 97504

100% Full-time 22 Hours/Day

A4.0
Date: 06.10.2011
Project No.: 10737-11



CITY OF MEDFORD
EXHIBIT # 
File # AC-15-133

MACKENZIE.

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OCTOBER 1, 2015
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September 23, 2015

City of Medford
Attention: Alex Georgevitch
200 S Ivy Street, 2nd Floor
Medford, OR 97501

Re: **Alba Village Traffic**
Phase 2 Trip Generation Accounting
Project Number 2110022.03

Dear Mr. Georgevitch:

This letter details the attached summary of the PM peak hour trip generation accounting for the existing Phase 1 development plus the proposed Phase 2 development at Alba Village. A total of 720 PM peak hour new trips would be generated by the uses, compared to the vested 1,882 trips from the July 7, 2006, Northgate Center TIA.

The existing Phase 1 development consists of 80,753 sf of shopping center. No other development has occurred at the site at this time.

Phase 2 is proposed to include a total of 178,990 sf of retail space, which falls within the shopping center rate in accordance with the original TIA.

The trip generation estimates are based on the current 9th Edition of the Trip Generation Manual published by the Institute of Transportation Engineers. Use of the rates allows for consistency in trip estimates as each update is prepared.

A 25% pass-by reduction is applied, as allowed by the City of Medford, for Shopping Center trips. A 10% reduction is allowed for the entire center, consistent with the original TIA, but will not be applied until a use other than shopping center is proposed.

Without the internal trip reduction, a total of 723 new trips would be generated by the Phase 1 and Phase 2 development. Once other uses develop at the center, and the internal reduction is applied, these two phases will account for 651 new trips.

Please let us know if you have any questions regarding the trip accounting update for the proposed Phase 2 development.

Sincerely,

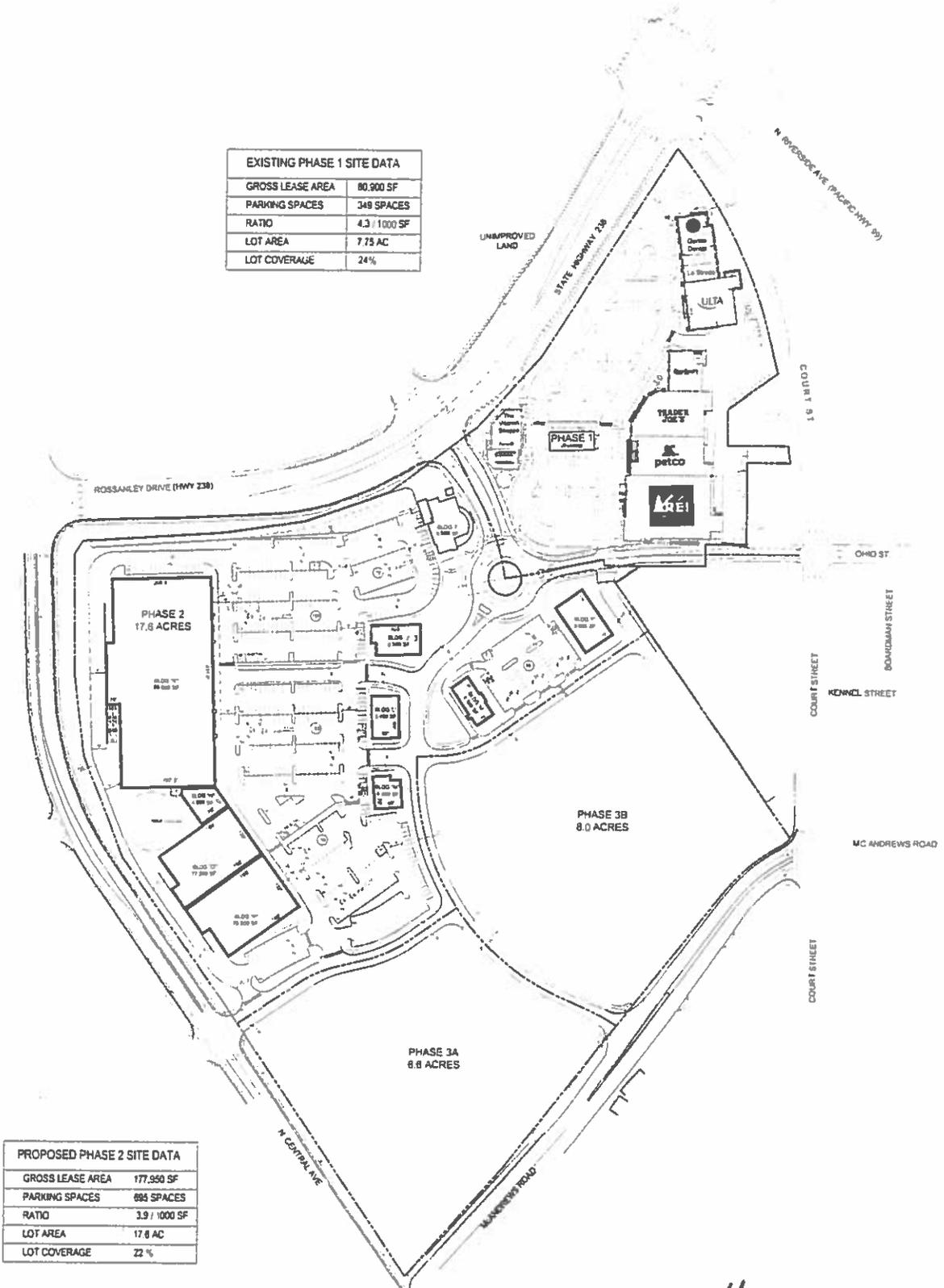


Brent Ahrend, PE
Senior Associate | Traffic Engineer

Enclosures: Phase 2 Site Plan, Trip Generation Accounting Summaries

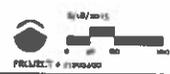
c: Craig Ramey - Regency Centers
Jennifer Danziger - Mackenzie

EXISTING PHASE 1 SITE DATA	
GROSS LEASE AREA	80,900 SF
PARKING SPACES	349 SPACES
RATIO	4.3 / 1000 SF
LOT AREA	7.75 AC
LOT COVERAGE	24%



PROPOSED PHASE 2 SITE DATA	
GROSS LEASE AREA	177,950 SF
PARKING SPACES	686 SPACES
RATIO	3.9 / 1000 SF
LOT AREA	17.8 AC
LOT COVERAGE	22 %

1142'
284'



Northgate Trip Accounting - Northgate Marketplace Phases 1 & 2				
Land Use	Code	Size (1000 square Feet)	PM Trip Rate	PM Trips
Shopping Center	820	259.743	3.71	964
Supermarket	850	0.000	9.48	0
Office Park	750	0.000	1.48	0
Business Park	770	0.000	1.26	0
Park-and-Ride	90	0	0.62	0
Subtotal				964
10% Reduction for Internal Trips				0
25% Reduction for Pass-by-Shopping Center				(241)
25% Reduction for Pass-by-Supermarket				0
New Trips				723
Trip Cap				1,882

* Based off 9th Edition ITE Trip Generation Manual

no internal trips are included in this update due to all uses being Shopping Center

"K12"
3 of 4

Northgate Trip Accounting - Northgate Marketplace Phases 1 & 2				
Land Use	Code	Size (1000 square Feet)	PM Trip Rate	PM Trips
Shopping Center	820	259.743	3.71	964
Supermarket	850	0.000	9.48	0
Office Park	750	0.000	1.48	0
Business Park	770	0.000	1.26	0
Park-and-Ride	90	0	0.62	0
Subtotal				964
10% Reduction for Internal Trips				(96)
25% Reduction for Pass-by-Shopping Center				(217)
25% Reduction for Pass-by-Supermarket				0
New Trips				651
Trip Cap				1,882

* Based off 9th Edition ITE Trip Generation Manual

Internal trips are shown for reference and will be applied when uses other than Shopping Center are added

"KK"
4 of 4



Continuous Improvement Customer Service

CITY OF MEDFORD

LD Date: 11/4/2015
File Number: AC-15-133

PUBLIC WORKS DEPARTMENT STAFF REPORT Northgate Market Place, Phase 2

Project: Consideration of a Final Site Plan for the future development Northgate Market Place, Phase 2, (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site, within a C-R (Regional Commercial) zoning district. Generally located north of East McAndrews Road, east of N. Central Avenue, west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238); Regency Centers, L.P., Applicant (Cardno, Inc., Agent)

NOTE: Existing Requirements and Agreements: Developer shall meet the conditions of approval for the Northgate Development CP-06-065, ZC-06-066, and Council Ordinance 2006-264 as modified by application CP-11-041, ZC-11-042, and Council Ordinance 2011-122, and Public Works memo dated October 25, 2006 as well as the amended Disposition and Development Agreement (DDA) dated August 8, 2011 (Council Ordinance 2011-163) and the approved Alba Village Mater Plan (AC-11-076) dated September 2, 2011.

The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of building permits. Construction plans for the improvements would need to be approved by the Public Works Engineering Department prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

A. STREETS

1. Dedications

North Central Avenue - No additional right-of-way or public utility easement (PUE) dedications are required along this developments frontage.

Highway 238 (Rossanley Drive) – Please contact Oregon Department of Transportation (ODOT) for any requirements.

Phase 3 – There may be additional right-of-way dedication required on East McAndrews Road for development in Phase 3.

2. Public Improvements

a. Public Streets

North Central Avenue - All standard street section improvements have been completed, including pavement, curb and gutter, street lights, and sidewalk. **No additional street improvements are required.**

Highway 238 (Rossanley Drive) – Please contact Oregon Department of Transportation (ODOT) for any requirements.

All off-site Improvements, per Exhibit 3 of the amended Disposition and Development Agreement (DDA), have been satisfied for this phase of development.

b. Street Lights and Signing

No additional street lights are required.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer's contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along the frontage of North Central Avenue.

3. Access and Circulation

The Applicant has provided a summary of the trip generation accounting for Phase 1 and 2, which shows the new trip generation to be below the current trip cap.

Access to this site will be restricted to the existing driveways currently serving this development, as shown on the site plan. No other accesses from North Central Avenue or Highway 238 "u"

(Rossanley Drive) will be allowed for this phase of the development.

The “Truck Route” access located near Building”K4” intersects with the private road too close to the North Central Avenue driveway. This intersections shall be relocated a minimum of 100-feet east of the east right-of-way line of North Central Avenue or a raised concrete median shall be installed to only allow for a right-in/right-out traffic movement to the truck route.

Driveway access to the proposed development site shall comply with MLDC 10.550. The driveway approaches shall have curb radii, concrete valley gutters and ADA ramps at the throat of each driveway.

The Developer shall submit evidence of or provide cross-access easements for the property along its boundaries in accordance with MLDC 10.550. The easements shall be described to include the proposed driveways for this Development.

B. SANITARY SEWERS

The proposed development is primarily situated within the Medford sewer service area. The Developer shall provide one separate individual service lateral to the each tax lot or ensure that each tax lot is served by an individual service lateral.

Connection to City of Medford sanitary sewer extension from Ohio Street is acceptable. If this line is extended further, then the Developer shall submit Public Improvement Plans accordance to Section ‘D’ of this report.

All public sanitary sewer mains shall be located in paved public streets or within easements. Easements shall have a minimum width of 10-feet or a width of two (2) times the depth of the main if greater than 5-feet deep to the invert of the main. All manholes shall be accessible by paved, all-weather roads. All easements shall be shown on the public improvement plans.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first vertical building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development shall be submitted with the building permit application for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Wetlands

The Developer shall contact the Division of State Lands for the approval or clearance of the subject property with regards to wetlands and/or waterways.

6. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Approval shall be obtained prior to

beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans.

Contractors proposing to do work on public streets, sewers, or storm drains shall 'prequalify' with the Engineering Division prior to starting work. Contractors shall work off a set of public improvement drawings that have been approved by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and storm drain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges

Buildings in this development are subject to street, sanitary sewer collection and treatment, and Storm Drain system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

Northgate Market Place, Phase 2

AC-15-133

A. Streets

1. Street Dedications to the Public:

No dedications are required with this phase.

2. Improvements:

No public improvements are required with this phase.

3. Access and Circulation:

Relocate Truck Route intersection or provide raised median.

Provide cross access easements.

B. Sanitary Sewer:

Ensure or construct separate individual sanitary sewer connection.

Provide Public Improvement Plans and easements if the public main is extended.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Locate all private storm drain lines outside of the right-of-way and PUE.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

The developer shall contact Division of State Lands for approval and/or clearance of the development with regards to wetlands.

Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-15-133

PARCEL ID: 372W24 TL 700 & 800

PROJECT: Consideration of a Final Site Plan for the future development Northgate Market Place, Phase 2, (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site, within a C-R (Regional Commercial) zoning district. Generally located north of West McAndrews Road, east of N. Central Avenue, west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238); Regency Centers, L.P., Applicant (Cardno, Inc., Agent). Desmond McGeough, Planner.

DATE: October 28, 2015

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of "On-Site" 8-inch and 12-inch water lines is required. Applicants' civil engineer shall coordinate with MWC engineering department staff for water facility layout.
4. Dedication of a 10 foot wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement to be submitted to MWC for review and recordation prior to construction.
5. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.
6. A pre-design meeting between the applicants Civil Engineer and MWC Engineering staff is required prior to plan review submittal to MWC.

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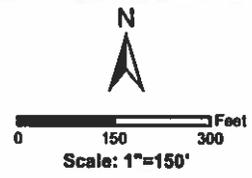
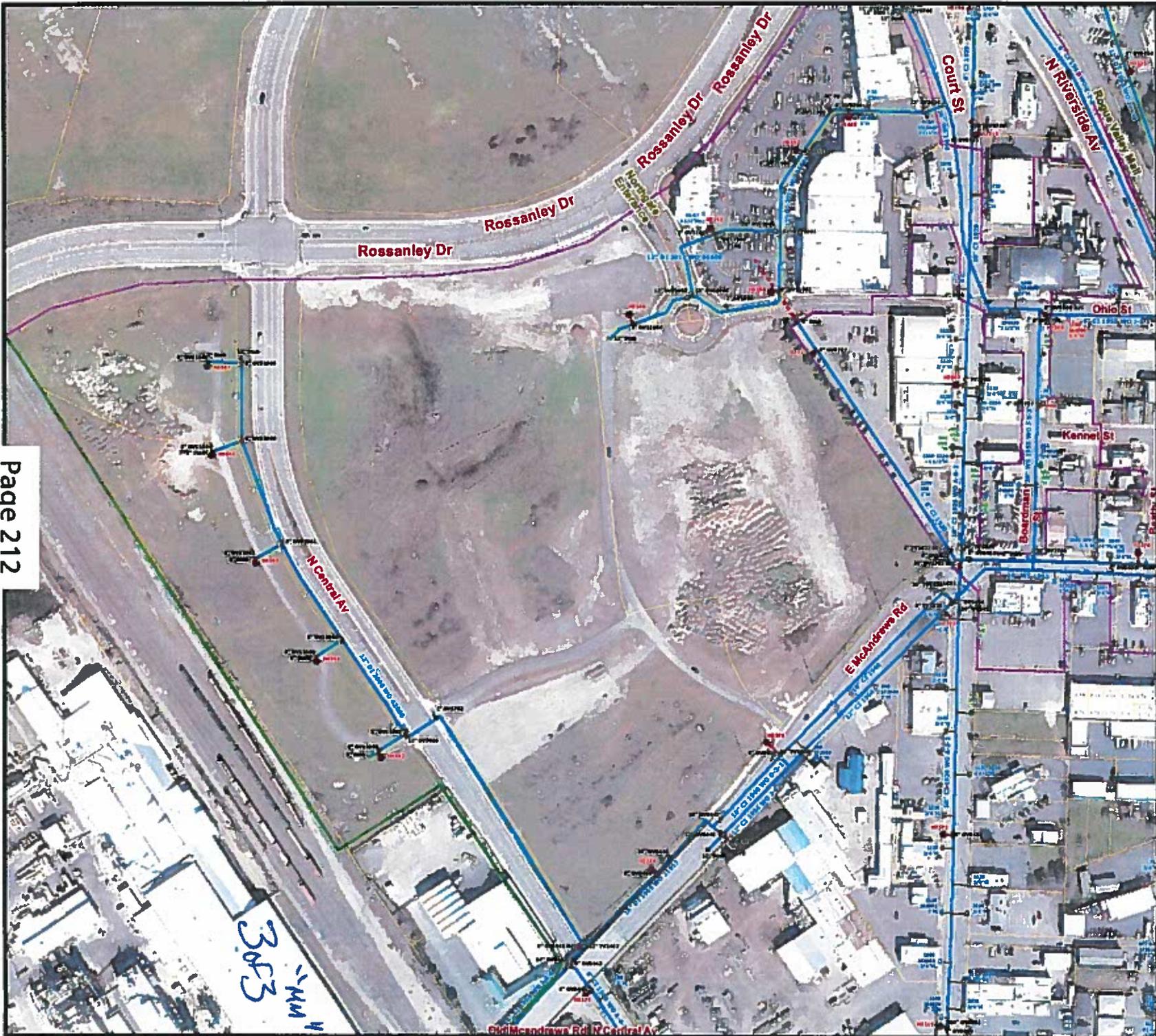
CITY OF MEDFORD
EXHIBIT # MM'
File # AC-15-133



Continued from previous page

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required. (See Condition 3 above)
3. MWC-metered water service does not exist to this property.
4. Static water pressure is expected to be between 100 and 105 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
5. Access to MWC water lines is available. There is an existing 12-inch water line stubbed for extension to the west near the existing round-a-bout located in Phase 1.



**Water Facility Map
for
AC-15-133**

Legend

- Air Valve
- Sample Station
- Fire Service
- ◆ Hydrant
- ▲ Reducer
- Blow Off
- ◆ Plug-Caps

- Water Meters:**
- Active Meter
 - On Well
 - Unknown
 - Vacant

- Water Valves:**
- Butterfly Valve
 - Gate Valve
 - Tapping Valve

- Water Mains:**
- Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line

- Boundaries:**
- Urban Growth Boundary
 - City Limits
 - Tax Lots

- MWC Facilities:**
- C Control Station
 - P Pump Station
 - R Reservoir



This map is based on data furnished by the Medford Water Commission. It is not a warranty, representation, or endorsement of any product, service, or company. It is for informational purposes only. Medford Water Commission, 2015. All rights reserved.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: Desmond McGeough

LD Meeting Date: 11/04/2015

From: Greg Kleinberg

Report Prepared: 10/23/2015

Applicant: Regency Centers, L.P., Applicant (Cardno, Inc., Agent)

File #: AC - 15 - 133

Site Name/Description: Northgate Market Place, Phase 2

Consideration of a Final Site Plan for the future development Northgate Market Place, Phase 2, (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site, within a C-R (Regional Commercial) zoning district. Generally located north of West McAndrews Road, east of N. Central Avenue, west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238); Regency Centers, L.P., Applicant (Cardno, Inc., Agent). Desmond McGeough, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
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Requirement FIRE HYDRANTS

OFC 508.5

Fire hydrants with reflectors will be required for this project.

Hydrant locations shall be as follows: Approved as submitted.

Additional hydrants may be required to comply with the requirement of proximity to fire department connections (for fire sprinkler and standpipe systems, the fire department connection shall be located at an approved location away from the building and within 75' of a fire hydrant. The fire department connection shall be located on the same side as the fire department access route.).

The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Plans and specifications for fire hydrant system shall be submitted to Medford Fire Department for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).

Requirement FD ACCESS TO BUILDINGS AND FACILITIES

OFC 503.1.1

Fire apparatus access roads shall be provided and maintained in accordance with Sections 503.1.1 through 503.1.3 (See Appendix D).

Approved fire apparatus access roads shall be provided for every facility, building or portion of a building hereafter constructed or moved into or within the jurisdiction. The fire apparatus access road shall comply with the requirements of this section and shall extend to within 150 feet (45 720 mm) of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility.

CITY OF MEDFORD
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10/23



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
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LAND DEVELOPMENT REPORT - PLANNING

To: Desmond McGeough

LD Meeting Date: 11/04/2015

From: Greg Kleinberg

Report Prepared: 10/23/2015

Applicant: Regency Centers, L.P., Applicant (Cardno, Inc., Agent)

File #: AC - 15 - 133

Site Name/Description: Northgate Market Place, Phase 2

Exception: The fire code official is authorized to modify Sections 503.1 and 503.2 where any of the following applies:

1. The building is equipped throughout with an approved automatic sprinkler system installed in accordance with Section 903.3.1.1, 903.3.1.2 or 903.3.1.3.
2. Fire apparatus access roads cannot be installed because of location on property, topography, waterways, nonnegotiable grades or other similar conditions, and an approved alternative means of fire protection is provided.
3. There are not more than two Group R-3 or Group U occupancies (OFC 503.1.1).

Requirement FD APPARATUS ACCESS ROAD DESIGN

OFC 503.2.1

Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 60,000 pounds.

(See also OFC 503.4; D102.1)

The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).

Requirement PRIVATE FIRE DEPARTMENT ACCESS PARKING RESTRICTION

OFC 503.4

Curbs are required to be painted/stenciled along the entire distance of fire lanes as stated below.

Fire apparatus access roads 20-26' wide shall be posted on both sides as a fire lane. Fire apparatus access roads more than 26' to 32' wide shall be posted on one side as a fire lane (OFC D103.6.1).

Where parking is prohibited for fire department vehicle access purposes, NO PARKING signs shall be spaced at 50' intervals along the fire lane and at fire department designated turn-around's. The signs shall have red letters on a white background stating "NO PARKING FIRE LANE TOW AWAY ZONE ORS 98.810 to 98.812" (See handout).

For privately owned properties, posting/marketing of fire lanes may be accomplished by any of the following alternatives to the above requirement (consult with the Fire Department for the best option):

Handwritten signature: WNN 11 2015



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: Desmond McGeough

LD Meeting Date: 11/04/2015

From: Greg Kleinberg

Report Prepared: 10/23/2015

Applicant: Regency Centers, L.P., Applicant (Cardno, Inc., Agent)

File #: AC - 15 - 133

Site Name/Description: Northgate Market Place, Phase 2

Alternative #1:

Curbs shall be painted red along the entire distance of the fire department access. Minimum 4" white letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals.

Alternative #2:

Curbs shall be painted yellow along the entire distance of the fire department access. Minimum 4" black letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals.

Alternative #3:

Asphalt shall be striped yellow or red along the entire distance of the fire department access. The stripes shall be at least 6" wide, be a minimum 24" apart, be placed at a minimum 30-60 degree angle to the perimeter stripes, and run parallel to each other. Letters stating "NO PARKING-FIRE LANE" shall be stenciled on the asphalt at 25-foot intervals.

Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12).

This restriction shall be recorded on the property deed as a requirement for future construction.

Development shall comply with access and water supply requirements in accordance with the Fire Code in effect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

UNN¹¹
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MEDFORD PARKS & RECREATION

HEALTHY LIVES. HAPPY PEOPLE. STRONG COMMUNITY.



CITY OF MEDFORD Interoffice Memo

TO: Planning Department
FROM: Tim Stevens- Park Maintenance Supervisor
SUBJECT: LANDSCAPE REVIEW OF FILE AC-15-133, Northgate
DATE: October 29, 2015

Staff recommends changing selected parking lot trees of *Fraxinus Americana* 'Junginger' to a different species. Threat of Emerald Ash Borer would decimate the parking lot canopy and selection of a different species would be more suitable for long term viability.

Per City of Medford code 10.780 G.(10.) 2. Plan should adequately depict where structural soil is, within parking lot, in relation to tree canopy area to meet requirements for soil volume.

This report addresses horticultural concerns only. Applicant shall comply with all aspects of Medford Code 10.780 Interpretation of the Medford Code will be per the Planning Department. Aesthetic considerations will be per the Site Plan and Architectural Review Commission or Planning Commission upon their review.

CITY OF MEDFORD
EXHIBIT # 00

File # AC-15-133

1 of 1



CONTINUOUS IMPROVEMENT | CUSTOMER SERVICE
701 N COLUMBUS AVE | MEDFORD OR 97501 | 541.774.2400
WWW.PLAYMEDFORD.COM | PARKS@CITYOFMEDFORD.COM



COMMUNITY ENRICHMENT EXCELLENCE EXCEPTIONAL CUSTOMER SERVICE INNOVATION



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

October 22, 2015

City of Medford Planning Department
411 West 8th Street
Medford, Oregon 97501

Re: AC-15-133, Northgate Marketplace, Phase 2, (372W24- 700, 800)

ATTN: Desmond,

The proposed development is located within the City of Medford's sewer service area.

Rogue Valley Sewer Services does have an 8 inch sewer main on N. Central Avenue which could serve the proposed development if connection to the City of Medford sewer is impractical. This would require a service boundary adjustment by both Medford and RVSS.

Feel free to contact me with any questions.

Sincerely,

Carl Tappert

Carl Tappert, PE
Manager

CITY OF MEDFORD
EXHIBIT # "PP"
File # AC-15-133

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Desmond M. McGeough

From: MOREHOUSE Donald <Donald.MOREHOUSE@odot.state.or.us>
Sent: Thursday, November 05, 2015 11:50 AM
To: Desmond M. McGeough
Cc: HARSHMAN Cathaleen A * Cathy; ALLEMAND Roger B; WANG Wei * Michael
Subject: AC-15-133

Desmond,

Thank you for sending agency notice of a consideration of a Final Site Plan for the future development Northgate Market Place, Phase 2, (Formally Alba Village), consisting of 178,990 square feet of commercial retail development located on a 17.81 acre site, within a C-R (Regional Commercial) zoning district. Generally located north of West McAndrews Road, east of N. Central Avenue, west of Court Street (State Highway 99) and south of Rossanley Drive (State Highway 238). We reviewed this and determined that it would not significantly affect state transportation facilities under the State Transportation Planning Rule (OAR 660-012-0060). Comments pertaining to the Access Management Rule (OAR 734-051-000) are as follows:

- The developer is prohibited from using ODOT right of way to meet City of Medford landscaping standards.
- ODOT requests that the applicant contact Roger Allemand (Permit Specialist) at 541-774-6360 to request the miscellaneous permits that will be needed for work that occurs within the ODOT right of way (OR 238).

Thank you,

Don Morehouse
Senior Transportation Planner
ODOT Region 3, District 8 (Rogue Valley Tech Center)
Ph: (541) 774-6399
Fax: (541) 774-6349
Donald.Morehouse@odot.state.or.us

CITY OF MEDFORD
EXHIBIT # 620
File # AC-15-133

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Exhibit 3

Off-site Improvement Timing Plan

The Developers shall, at their sole expense, cause to be constructed the following off-site transportation improvements. The Developers will provide evidence of compliance with these transportation improvement requirements.

Location	Description	Square Feet when Triggered
Biddle Rd @ McAndrews Rd *	Turn lane improvement	1
Highway 238 @ Ross Ln	Construct a signal	1
Highway 238 @ Sage Rd NB (Timber Products Frontage) *	Add a NB to EB exclusive right turn lane and half street improvements for future 5 lane section on Sage Rd	1
South side of McAndrews Rd between Central and Court * (on-site improvement)	Add sidewalk along south side of McAndrews Rd between Central Ave and Court St where it does not currently exist	1
NB Central @ Highway 99 *	Add a right turn lane NB Central to EB/SB Highway 99	300,000
I-5 South @ Highway 62	Add a right turn lane from I-5 SB off-ramp to WB Highway 62	475,000
Highway 62 @ Highway 99 (Big X)	Add a 2 nd WB to NB exclusive right turn lane	675,000
Highway 99 @ Lynn St	Contribute \$300,000 to ODOT for future signal	725,000
Highway 62	Contribute to Highway 62 Access Management Improvements	775,000
	Contribute to flyover	823,800
Highway 238 @ Sage Rd SB **	Add a SB to WB exclusive right turn lane pending future traffic analysis	Deferred for future analysis

*Qualifies for Street System Development Credits pursuant to Section 3.1.1 of this Agreement.

**Developers shall provide additional traffic analysis to the City at the time of development of Phase III (Business Park) of Northgate Centre. The purpose of the additional analysis will be to re-evaluate the need for the SB right turn lane based on traffic counts that are the result of Phases I and II of Northgate Centre being in place and functioning. If the additional analysis continues

CITY OF MEDFORD,
EXHIBIT # RA
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to support the need for the SB right turn lane, Developers shall be responsible for the design and construction of the improvement prior to or in conjunction with the issuance of development permits for Phase III. If the additional analysis shows the SB right turn lane is not needed, the City will release Developers from this condition.

"RB"
20f2



City of Medford
Planning Department

Vicinity
Map

File Number:

AC-15-133



Project Name:

**Northgate Marketplace -
Phase 2**

Map/Taxlot:

372W24 TL 700, 800



10/12/2015



Subject Area



Medford Zoning

City Limits



Tax Lots



PUD

Medford UGB with Wards

