

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA APRIL 3, 2015



Commission Members

Jeff Bender, Chair

Jim Quinn, Vice Chair

Daniel Ames

Jim Catt

Bill Chmelir

Mark Dew

Bob Neathamer

Alec Schwimmer

City Council Liaison - Dick Gordon

City Council Liaison Alt. - Eli Matthews

Regular Commission meetings are held on
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380



SITE PLAN AND ARCHITECTURAL COMMISSION

AGENDA
April 3, 2015
12:00 p.m.

Council Chambers, Third Floor, City Hall
411 W. 8th St, Medford OR

10. **Roll Call.**
20. **Consent Calendar.**
- 20.1 AC-15-005 Final Order for consideration of plans to construct a 972 square foot addition to an existing 1,200 square foot shop building, situated at the northwest corner of a 2.09 acre lot, located on the west side of Crater Lake Highway between Commerce Drive and Coker Butte Road, within the I-G (General Industrial) zoning district. Mike Zoller, Applicant/Agent.
30. **Minutes.**
- 30.1 Approval of minutes for the March 20, 2015, meeting.
40. **Oral Requests and Communications.**
50. **Public Hearings.**
- New Business.**
- 50.1 AC-14-129/E-15-025 Consideration of site plan and architectural review and related exception from maximum driveway width requirements for a City of Medford Fire Station facility located on a 1.36 acre parcel south of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C (Community Commercial) zoning district. City of Medford, Greg McKown, Applicant (ORW Architecture, David Wilkerson, Agent).
- 50.2 AC-15-007/E-15-009 Consideration of Plans and associated exception request seeking relief from required parking standards for 26 residential dwelling units upon a deck over the Medford Urban Renewal Agency Parking Lot Facility, located upon .688 acres west of S. Central Avenue and north of E. 10th Street within a C-C/ C-B (Community Commercial – Central Business District Overlay) zone district. Skypark Medford LLC, Applicant (Oregon Architecture, Inc., Mark McKechnie, Agent).
- 50.3 AC-15-013 Consideration of plans for the construction of a 1,850 square foot coffee shop and a 3,285 square foot medical office building on a 1.01 acre parcel located on the south side of Barnett Road between Black Oak Road and Murphy Road, within a C-C (Community Commercial) zoning district. Oregon Architecture, Inc., Mark McKechnie, Applicant/Agent.
60. **Written Communications.** None.
70. **Unfinished Business.** None.
80. **New Business.**
90. **Report from the Planning Department.**
100. **Messages and Papers from the Chair.**
110. **Propositions and Remarks from the Commission.**
120. **City Council Comments.**
130. **Adjournment.**

BEFORE THE MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

STATE OF OREGON, CITY OF MEDFORD

IN THE MATTER OF SITE PLAN AND ARCHITECTURAL COMMISSION)
FILE AC-15-005 APPLICATION FOR PROJECT REVIEW SUBMITTED) **ORDER**
BY MIKE ZOLLER)

AN ORDER granting approval for consideration of plans to construct a 972 square foot addition to an existing 1,200 square foot shop building, situated at the northwest corner of a 2.09 acre lot, located on the west side of Crater Lake Highway between Commerce Drive and Coker Butte Road, within the I-G (General Industrial) zoning district, as provided for in the Medford Land Development Code.

WHEREAS:

1. The Site Plan and Architectural Commission has duly accepted the application filed in accordance with the Land Development Code, Section 10.285.
2. The Site Plan and Architectural Commission has duly held a public hearing on the matter of an application for consideration of plans to construct a 972 square foot addition to an existing 1,200 square foot shop building, situated at the northwest corner of a 2.09 acre lot, located on the west side of Crater Lake Highway between Commerce Drive and Coker Butte Road, within the I-G (General Industrial) zoning district, with a public hearing a matter of record of the Site Plan and Architectural Commission on March 20, 2015.
3. At the public hearing on said application, evidence and recommendations were received and presented by the Planning Department staff; and
4. At the conclusion of said public hearing, after consideration and discussion, the Site Plan and Architectural Commission, upon a motion duly seconded, granted approval and directed staff to prepare a final order with all conditions and findings set forth for the granting of approval.

THEREFORE LET IT BE HEREBY ORDERED that the application of Mike Zoller stands approved subject to compliance with the conditions stated in the Commission Report dated March 20, 2015.

AND LET IT FURTHER BE OF RECORD that the action of the Site Plan and Architectural Commission approving this application is hereafter supported by the following findings:

- (a) That the proposed development, with the conditions of approval, complies with the applicable provisions of all city ordinances as determined by the staff review.
- (b) That the proposed development is compatible with uses and development that exist on adjacent land, based upon information provided in the Applicant's Questionnaire and presented at the public hearing.

FINAL ORDER AC-15-005

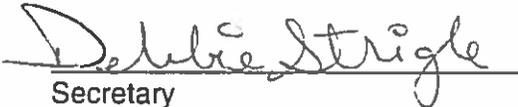
BASED UPON THE ABOVE, it is the finding of the Medford Site Plan and Architectural Commission that the project is in compliance with the criteria of Section 10.290 of the Land Development Code.

Accepted and approved this 3rd day of April, 2015.

MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

Site Plan and Architectural Commission Chair

ATTEST:


Secretary



CITY OF MEDFORD

PLANNING DEPARTMENT

COMMISSION REPORT

Date: March 20, 2015
Subject: Zoller Addition (AC-15-005)
Mike Zoller, Applicant

BACKGROUND

Proposal

Consideration of plans to construct a 972 square foot addition to an existing 1,200 square foot shop building, situated at the northwest corner of a 2.09 acre lot, located on the west side of Crater Lake Highway between Commerce Drive and Coker Butte Road, within the I-G (General Industrial) zoning district.

Subject Site Zoning, GLUP Designation and Existing Uses

Subject Site Zoning: I-G
GLUP Designation: GI (General Industrial)
Existing Uses: Workshop

Surrounding Property Zoning and Uses

North	I-G Metal Masters
South	I-G Vacant
East	I-G Multi-tenant Building: A Plus Pools, Medford Auto Care Center
West	I-G Vacant Industrial Building

Applicable Criteria

10.290 Site Plan and Architectural Commission Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

Corporate Names

The application lists Zoller Properties LLC. as the owner of the subject property. As per the State of Oregon Business Registry, Garrett Zoller is listed as the registered agent.

ISSUES/ANALYSIS

The subject site currently has an existing large industrial building at the eastern (front) end of the site, which was constructed in 1958. At the rear of the property there is a free-standing 1,200 square foot workshop space, originally constructed at approximately the same time as the main building. The applicant is seeking to construct a 972 square foot, single-story addition on the east side of the workshop building.

Building Elevations

The elevations illustrate that the proposed addition will match the existing shop façade. When completed, the new and existing portion of the workshop will appear as a large, unified, single-story structure with two large garage doors.

The highest point of the structure will be 21 feet 10 inches at the peak of the roof. The I-G zoning district allows up to 85 feet in height; this building meets all applicable code requirements. The materials proposed will match the existing building and consist of neutral tone siding and asphalt shingles.

Vehicular Parking

Two vehicular parking spaces are proposed as a part of this project. Medford Land Development Code (MLDC) 10.743 outlines the amount of automobile parking required for various uses. This workshop building does not exactly match any of the uses listed, but can be categorized as *Industrial or Warehouse*. The *Industrial or Warehouse* category requires a minimum of 1.0 space per employee plus 0.2 spaces per 1,000 square feet of gross floor area. At a total of 2,172 square feet with the proposed addition and zero employees (as this

is only a storage/workshop space), the required parking calculates to be 0.43 spaces, which could be rounded up to one space.

The *Industrial or Warehouse* category also regulates parking to a maximum of 1.0 space per employee plus 0.3 spaces per 1,000 square feet of gross floor area. Although there will be no employees, assuming one person would be present using the shop and could be considered like an employee for these purposes, at 2,172 total square feet, the maximum parking allowed calculates to be 1.65 spaces, which equates to two spaces.

The two proposed parking spaces are sufficient to meet the requirements pertaining to this project.

Bicycle Parking

MLDC 10.748 requires bicycle parking to be calculated at twenty percent of the number of spaces provided for automobiles. However, per MLDC 10.751 the approving authority may allow an exception to the standard for *uses that do not generate the need for bicycle parking*. As this small addition to a workshop in the rear of the site is not intended for public access and is very minor in nature, the Commission can find that there is no need for bicycle parking.

Pedestrian Walkways

Sidewalks are already in existence at the eastern most, Crater Lake Highway side of this 2.09 acre site. As this private workshop is not accessible to the general public, not situated adjacent to a public right-of-way, and this rear portion of the site is not intended for pedestrian access or use, the applicant is not proposing any pedestrian walkways. Per MLDC 10.776 walkways are not required on sites which are not intended for or likely to be used by pedestrians.

Concealments

No HVAC equipment or trash receptacles are proposed.

Landscaping

Section 10.797 of the Code requires frontage landscaping along all street frontages outside of the public right-of-way. As the portion of the site pertaining to this project has no direct street frontage and is accessed via a private access easement, there are no applicable requirements for frontage landscaping on the site. No other landscaping is proposed or required.

FINDINGS OF FACT

Staff has reviewed the applicant's narrative and findings and recommends that the Commission adopt them as presented (Exhibit B).

Decision: As discussed in the applicant's narrative and findings of fact, the applicant asked the Commission for relief from a requirement in the Public Works Report for the installation of two street lights along the frontage of Crater Lake Highway. The applicant discussed the issue of proportionality, in that the cost of the street light installation would far exceed the cost of the proposed building addition. In addition, the applicant questioned the rationale for such a requirement on a distant portion of this large site that is otherwise not considered when discussing improvements related to the proposed building addition.

The Commission agreed with the applicant, finding that rough proportionality had not been clearly demonstrated within the Public Works Report. The Commission voted to approve the project, subject to all exhibits and conditions, other than the street light requirement.

ACTION TAKEN

Adopt the findings as recommended by staff and directed staff to prepare the Final Order for approval of AC-15-005 per the Commission Report dated March 20, 2015, including Exhibits A through J, with the exception of the street light requirement in the Public Works Report.

EXHIBITS

- A Conditions of Approval dated March 13, 2015
- B Applicant's Narrative and Findings of Fact received March 10, 2015
- C Site Plan received January 13, 2015
- D Floor Plan and Elevations received January 13, 2015
- E Fire Department Report received February 04, 2015
- F Rogue Valley Sewer Services Letter received February 02, 2015
- G Building Department Memo received February 09, 2015
- H Public Works Report received February 11, 2015
- I Medford Water Commission Memo received February 11, 2015
- J ODOT Letter received February 17, 2015
Vicinity Map

SITE PLAN AND ARCHITECTURAL COMMISSION

Jeff Bender, Chair

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA: MARCH 20, 2015



MINUTES
SITE PLAN AND ARCHITECTURAL COMMISSION MEETING
March 20, 2015

The regular meeting of the Medford Site Plan and Architectural Commission was called to order at approximately 12:00 p.m. in the Council Chambers, City Hall, on the above date with the following members and staff in attendance:

Commissioners Present

Jeff Bender, Chair
Jim Quinn, Vice Chair
Daniel Ames
Jim Catt
Bill Chmelir
Mark Dew
Bob Neathamer
Dick Gordon, City Council Liaison

Staff Present

Jim Huber, Planning Director
Kelly Akin, Principal Planner
Kevin McConnell, Deputy City Attorney
Doug Burroughs, Public Works Department
Jennifer Jones, Planner II
Debbie Strigle, Recording Secretary

Commissioners Absent

Alec Schwimmer - Excused

10. Roll Call.

20. Consent Calendar/Written Communications.

- 20.1 AC-15-002/E-15-003 Final Order for consideration of plans for the construction of a 3,750 square foot In-N-Out restaurant and associated Exception request seeking relief to minimum front yard setback requirements in Medford Land Development Code Section 10.721. The subject site is located on the southeast side of Crater Lake Highway (Highway 62) approximately 500 feet northeast of Route 99 (Pacific Highway) on a 1.04 acre parcel within the C-R/F (Regional Commercial/Freeway Overlay) zone district. In-N-Out Burger, Applicant (Jim Lockington, Agent).

Motion: Adopt the consent calendar.

Moved by: Commissioner Neathamer Seconded by: Commissioner Dew

Roll Call Vote: Motion passed, 7-0

30. Minutes.

- 30.1 The minutes for the March 6, 2015, meeting, were approved as submitted.

40. Oral and Written Requests and Communications. None.

50. Public Hearings.

Kevin McConnell, Deputy City Attorney, read the rules governing the public hearings.

New Business.

50.1 AC-15-005 Consideration of plans to construct a 972 square foot addition to an existing 1,200 square foot shop building, situated at the northwest corner of a 2.09 acre lot, located on the west side of Crater Lake Highway between Commerce Drive and Coker Butte Road, within the I-G (General Industrial) zoning district. Mike Zoller, Applicant/Agent.

Chair Bender asked for any conflicts of interest or ex-parte communications. Commissioner Dew declared that he had consulted with the applicant and recused himself from his seat on the Commission.

Jennifer Jones, Planner II, read the approval criteria, and gave a PowerPoint presentation of the March 13, 2015, Staff Report. Staff recommended approval.

Commissioner Catt wanted to know if there will be an additional holding pond for the storm drainage.

Doug Burroughs, Public Works Department, said the applicant would be required to provide storm water detention and water quality for the addition to the building. He added that if there was already storm water detention at the location the applicant could work with that. He said it would be up to the applicant and his engineer how they would propose to do that.

The public hearing was opened and the following testimony was given:

a) Mike Zoller, applicant, said he was in acceptance with everything from staff except for the Public Works requirement to install three street lights and a base mounted cabinet (BMC). He stated the cost of the street lights and BMC would not be proportional to the amount of money he would be spending on the building addition. He pointed out the distance from the building addition to Crater Lake Highway frontage and did not feel this requirement applied to this project. He is seeking relief from that requirement. He reserved time for rebuttal.

Mr. Burroughs clarified they were asking for two street lights. He said the street frontage is over 200 feet and per Code Section 10.495, anything over 200 feet would require one street light for the first 200 feet and then another street light for anything over that. He said the property is looked at as a whole.

Commissioner Neathamer asked Mr. Burroughs if the street lights are a code requirement. Mr. Burroughs answered yes. Commissioner Neathamer commented there would need to be an exception to meet the applicant's request. Mr. Burroughs answered yes.

After a brief discussion with staff, Kevin McConnell, Deputy City Attorney, stated an exception would not be necessary in this case as the applicant made a Nollan-Dolan argument regarding proportionality. He said there may be a nexus here but not rough proportionality. He added there is no Nollan-Dolan analysis in the Public Works Department Staff Report so based on that this Commission could accept the applicant's request for an exception to the street lights requirement.

Motion: Adopt the Findings as recommended by staff and direct staff to prepare a Final Order for approval of AC-15-005 per the Staff Report dated March 13, 2015, including Exhibits A through J, with the exception of the street light requirement stated in the Public Works Department Staff Report.

Moved by: Commissioner Neathamer

Seconded by: Commissioner Ames

Commissioner Neathamer stated the Commission could find that this project is compatible with the surrounding uses and meets code requirements.

Roll Call Vote: Motion passed, 6-0.

60. **Written Communications.** None.

70. **Unfinished Business.** None.

80. **New Business.** None.

90. **Report from the Planning Department.**

90.1 Ms. Akin stated there is business scheduled for the April 3rd, and April 17th meetings.

90.2 Ms. Akin reported that City Council adopted the Plan Authorization Revision text amendment.

90.3 Ms. Akin said there was still a vacancy for the Landscape position on this Commission.

90.4 Ms. Akin commented she had sent an e-mail out to all the Commissions asking for their input on a new application form for those who would like to serve on a Commission. Any comments and input can be sent to Ms. Akin and she will share those with the City Manager's office.

90.5 Ms. Akin reported there was a Planning Commission hearing regarding the Urban Growth Boundary Expansion project on March 12th. After a lengthy meeting, the public hearing was closed but the meeting was continued to a study session on April 6, 2015, so they could discuss options and deliberate. A new public hearing will be rescheduled at some point in the future.

Commissioner Neathamer noted he would not be able to attend the April 17th meeting.

100. **Messages and Papers from the Chair.** None.

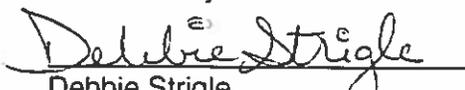
110. **Propositions and Remarks from the Commission.** None.

120. **City Council Comments.** None.

130. **Adjournment.**

130.1 The meeting was adjourned at approximately 12:25 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:


Debbie Strigle
Recording Secretary

Site Plan and Architectural Commission Chair

Approved: April 3, 2015



CITY OF MEDFORD

PLANNING DEPARTMENT

STAFF REPORT

Date: March 27, 2015 for April 3, 2015 Commission Meeting
To: Site Plan and Architectural Commission
From: Desmond McGeough, Planner II *DM*
Reviewed By: Kelly Akin, Principal Planner *KA*
Subject: City of Medford Fire Station # 2 (AC-14-129 E-15-025)
City of Medford, Greg McKown, Applicant
(David Wilkerson, ORW Architecture, Agent)

BACKGROUND

Proposal

Consideration of site plan and architectural review and related exception to maximum driveway width requirements for a City of Medford Fire Station facility located on a 1.36 acre parcel on the south side of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C (Community Commercial) zoning district.

Subject Site Zoning, GLUP Designation, and Existing Uses

Zone: C-C
GLUP Designation: UR (Urban Residential)
Existing Use: Vacant

Surrounding Property Zoning and Uses

North: SFR-6 (Single Family Residential- 6 dwelling units per gross acre)
Use: Single family residential

South: SFR-6
Use: Single family residential

East: C-C
Use: Retail pet supply store

"Working with the Community to Shape a Vibrant and Exceptional City"

West: C-C
Use: Retail shopping center

Related Projects

None

Applicable Criteria

10.290 SITE PLAN AND ARCHITECTURAL REVIEW APPROVAL CRITERIA

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) *The proposed development is compatible with uses and development that exist on adjacent land; and*
- (2) *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

10.253 CRITERIA FOR EXCEPTION

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority (Planning Commission/Site Plan and Architectural Commission) having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

- (1) *The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The Planning Commission/Site Plan and Architectural Commission shall have the authority to impose conditions to assure that this criterion is met.*
- (2) *The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*
- (3) *There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.*

- (4) *The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.*

ISSUES/ANALYSIS

Scope of Project

The subject application is for a new building for the Medford Fire Department, Station No. 2. The proposed facility is located on a 1.36 acre parcel on the south side of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C zoning district. The site is currently vacant. This facility is intended to replace the existing Fire Station No. 2, which is currently located approximately 3,500 feet north of this site at the southeast corner of West 8th Street and Lincoln Street.

The proposed station facility will encompass approximately a total of 9,218 square feet on a two floors. A three vehicle apparatus parking bay encompasses a large portion of the west side of the building. The station also includes offices, multipurpose rooms, kitchen day rooms and bunk rooms, and a secure outdoor patio area on the east corner of the building.

Site Plan

As noted above, the subject site is located on the south side of Stewart Avenue. The proposed fire station is centrally located east to west on the site and located closer toward the Stewart Avenue street frontage. The footprint of the fire station encompasses approximately 15% of the total site area. The office portion of the facility is located approximately 25 feet behind the front property line and the apparatus bays are located approximately 50 feet behind front property line.

Two access points are proposed for the facility. The easterly 11 feet of the property is part of a shared access drive with the adjoining property, having total width of approximately 30 feet. The shared access drive will serve as the ingress to both the fire station and adjoining the pet supply store and serve as egress for the pet supply store. All non-emergency ingress to the fire station site will be from this easterly access. Non-emergency vehicle egress will exit the rear of the site from on west side of the building. An egress of approximately 68 feet is proposed along the northerly portion of the property and designed to provide rapid deployment of fire apparatus from the three vehicle apparatus bay.

A large 60-foot by 180-foot paved area is provided in the rear for vehicular maneuvering and occasional parking of fire emergency vehicles. A total of 10 vehicular parking spaces have been provided by the facility. There is a large landscaped area toward the rear of the site which will serve as the detention area for the new facility and provide a bufferyard area for shade tree

plantings to visually separate the fire station use from the adjoining single family residential units south and southeast of the property.

Exception Request

Also included with the site plan application, the proposal also includes an exception request seeking relief from Section 10.550 (Table 10.550-1) to permit a driveway width greater than 36 feet in width. As demonstrated on the site plan, the proposed driveway width is 68 feet, which is approximately twice the typical width of a commercial driveway. Compliance with the exception request criteria will be discussed in detail further in this report.

Elevations and Materials

The fire station is of a contemporary style with its mass minimized by providing a variety of façade undulations and building heights. The primary exterior façade material consists of red brick which is capped at the top of the building with a band of polished CMU (concrete masonry unit) split face block. Split face block material also provides a background frame which displays the fire station unit number on the front side of the building. A tower feature, encased in high performance glazing window curtain wall, extends from the northeast corner at the main building entrance providing additional aesthetic interest in the structure. The tower feature, along with the architectural alcove projection over the apparatus bay area, is capped with a wide, black metal coping which provides shading over the windows via a significant roof overhang. The patio area on the east side of the building is secured with a metal mesh fencing material that is back in color and wood railing. The overhead vehicle doors on the both the front and back of the facility are fire engine red in color and include window panels to break up the expanse of the metal doors and provide additional architectural interest in the structure. In summary, the blending of architectural materials, projections, recesses and window fenestration harmoniously blend together to achieve a visually pleasing structure.

Landscaping

Per the landscape plan, the proposed development will significantly enhance the street frontage along Stewart Avenue. A densely populated cover of vegetative ground cover as well as street trees will be employed along the Stewart Avenue. Due to the expanse of the proposed driveway apron on the west side, only one half (approximate) of the frontage will have a landscape planter along the street side.

Five street trees are proposed for the site. Two trees are proposed to be located within the planter strip, one tree is located in the front yard setback area at the northwest corner of the site and two additional trees are located in front of the station. Section 10.797 of the MLDC requires a total of 7 street trees based upon the frontage dimension (200 feet). The Parks Department memo provides a condition of approval that two additional trees, chosen from the *City of Medford Selected Street Tree List*, be provided along the street frontage. A condition of approval has been included requiring that the applicant comply with the Parks Department Memo, dated March 11, 2015.

There is a total of 21,456 square feet of landscape area, which is approximately 36% of the site. The landscape plan shows that there is no high water landscaping proposed for the subject development. The development does not provide an amount of parking that triggers internal landscape planter islands to be included along the parking areas, though landscaping areas are placed in close proximity to all parking stalls.

The subject site is subject to landscape bufferyard requirements to provide separation from the single-family neighborhoods on the south side of the site. The bufferyard width requirement is easily met with a large landscaping area that will be also utilized for an on-site detention basin. Along the east west and south property lines within the rear portion of the site, 28 large shade trees have been planted. Within the required buffer yard area over 60% of the trees are Douglas Fir, a non-deciduous tree, whereas 40% of the trees are a large Maple variety. The subject bufferyard landscaping meets the required quantities and tree type specified by the Medford Land Development Code.

Concealments

The equipment penthouse on the mezzanine floor will conceal HVAC equipment. The site plan shows a 6 foot high block wall with steel doors enclosing an emergency generator and trash dumpster, which is located in the southwest portion of the site.

Lighting

The site plan demonstrates the placement of 4 light poles along the south side of the paved area on the rear side of the building and two additional poles along the west property line. While the pole height is not identified on the plan, the photometric diagram displays that the maximum distance behind the pole that a 0.5 footcandle measurement will extend with any of the four displayed heads is 2 to 3 feet when mounted upon a 30 foot pole. Section 10.746 requires that any direct or indirect light from the source shall not exceed 0.5 footcandles upon any property in a residential district. Based on the photometric illustrations, any light at the residential property will be extremely low, if present at all.

Within the Public Works Report (Exhibit C), it is identified that one existing street light on Stewart Avenue be relocated to another location along Stewart Avenue. A condition of approval has been included requiring the applicant to comply with the Public Works Report, dated March 11, 2015 (Exhibit C).

Utilities

The Utility Plan proposes storm drain to run along the east and west sides of the property into a large detention basin located in the southern portion of the property. The Public Works Department Report conditions that the stormwater quality and detention facilities be in accordance with the provisions of MLDC Section 10.481 and 10.792. A condition of approval has been included requiring that the applicant comply with the Public Works Report, dated March 11, 2015 (Exhibit C).

The Medford Water Commission Memo, dated March 9, 2015, notes that the proposed facility will need to construct on-site water facilities and that metered service does not currently exist to this property. MWC has conditioned that the applicant's civil engineer coordinate with MWC Engineering Department. Required engineered plans shall be prepared in accordance with the MWC Standards. A condition of approval has been included requiring that the applicant comply with the MWC Memo, dated March 9, 2015 (Exhibit D).

The subject site lies within the Rogue Valley Sewer Service (RVSS) service area. The memo from RVSS (Exhibit I) notes that an existing 10 inch sewer main transects the property within a 10 foot easement just south of the proposed structure. The proposed site plan includes a steel gate and fence which will restrict 24 hour access to existing RVSS manholes on the subject property. There is an existing sewer service lateral to the property from the above described main line with adequate capacity to serve the proposed development. RVSS has provided two conditions of approval. The first condition notes that they shall have 24 hour unrestricted access inside of the steel gate. Access inside the fenced area shall be available 24 hours per day within 30 minutes' notice to Medford Fire Department. The second pertains to System Development Charges due to RVSS for the proposed structure. A detailed plumbing plan shall be submitted to RVSS for the determination of the SDC fees. A condition of approval has been included requiring that the applicant comply with the RVSS letter, dated March 5, 2015 (Exhibit I).

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS OF FACT

10.290 Site Plan and Architectural Review Criteria.

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

(1) The proposed development is compatible with uses and development that exist on adjacent land; and...

Approval Criterion 1

The Applicant's Site Plan Findings of Fact and Conclusions of Law and Demonstration of Compliance with City Standards (Exhibit B) thoroughly exhibit the proposed structure's compatibility with adjacent uses and development. The Site Plan and Architectural Commission can find that this proposal meets Criterion One and staff recommends that the Site Plan and Architectural Commission adopt the Applicant's findings as presented.

Approval Criterion 2

10.290 Site Plan and Architectural Review Criteria.

(2) *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Applicant's Site Plan Findings of Fact and Conclusions of Law and Demonstration of Compliance with City Standards (Exhibit B) exhibit that the proposal complies with all provisions of the Medford Land Development Code, subject to the approval of the request for exception for commercial driveway width development. The Site Plan and Architectural Commission can find that this proposal meets Criterion Two and recommends that the Site Plan and Architectural Commission adopt the Applicant's findings as presented.

10.253 Criteria for an Exception

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority (Planning Commission/Site Plan and Architectural Commission) having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

(1) *The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The Planning Commission/Site Plan and Architectural Commission shall have the authority to impose conditions to assure that this criterion is met.*

Exception Criteria 1

Regarding the request to establish a driveway width in excess of the maximum permitted width of a commercial driveway entrance (36-feet), the proposed exception has no impact on adjacent sites or the neighborhood as a whole as it affects only the frontage of the subject parcel. The subject driveway will only serve as a point of egress for dispatched fire trucks and to the limited number parking spaces located on the rear side of the facility. Concerns that would typically arise with a wide driveway on a commercial site, such as driver confusion regarding point of entry and exit, are absent in this circumstance since the driveway only serves as a one-way egress, nor is it a driveway that the motoring public would access. While the driveway does have a significant expanse, the applicant has proposed to utilize a patterned concrete across the apron to clearly delineate the sidewalk and provide a higher degree of pedestrian comfort.

While staff supports the basis and need for exception, the Public Works Report notes only a 48 to 56 foot wide driveway width is supported, rather than the 68 foot driveway width shown on

the site plan. It appears that the three apparatus bays and egress drive from the rear of the facility may be served without conflict, even if fire apparatus is parked on the front drive, with a shorter driveway span. From the perspective of a pedestrian, the shortest driveway span across the sidewalk corridor would be the preferable design scenario. From the express language of the preamble of the exception criteria, the power to grant exception shall be sparingly exercised. Staff finds that this language pertains to the not only to the granting of exception requests but to the extent in which particular exception deviates from the code. As such, staff recommends the width of the driveway be limited to the extent feasible while maintaining the necessary mobility needs of fire apparatus and vehicular circulation of the site. Subject to the compliance with the Public Works Report regarding driveway width, staff recommends that the Commission adopt the findings for Criterion 1 as submitted by the applicant.

Criterion 1 is met.

- (2) *The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*

The requested exception pertains to physical improvements to allow a design alternative to the maximum driveway width permitted by the MLDC. Granting of this exception does not establish a use that is not permitted within the zone district. Staff recommends that the Commission adopt the findings for Criterion 2 as submitted by the applicant.

Criterion 2 is met.

- (3) *There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.*

Staff concurs with the applicant's findings in that there are unique circumstances that are particularly applicable to the use rather than the physical site. Public Safety facilities need to be sited and function in a manner that provides maximum speed and efficiency of deployed apparatus to the public. It may be possible there are other sites that could potentially accommodate the driveway width needed to serve a three bay fire station facility. However, such siting would negatively impact response times and thus be detrimental to the community. Not granting an exception to allow a wider driveway width would result in a peculiar, exceptional and undue hardship on the general public. Staff recommends that the Commission adopt the findings for Criterion 3 as submitted by the applicant.

Criterion 3 is met.

- (4) *The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be*

suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.

The need for the Exception is not the result of an illegal act and is suffered by the particular property in question given the specific use. The need for the exception results only from the desire to enhance public safety and welfare. No profit at all will be gained by the Applicant though an approval of the subject exception request. Staff recommends that the Commission adopt the findings for Criterion 4 as submitted by the applicant.

Criterion 4 is met.

RECOMMENDATION

Direct Staff to prepare a final Adopt the Final Order for Approval of AC-14-129/E-15-025, per the Staff Report dated March 27, 2015, including all Exhibits A through I.

EXHIBITS

- A Conditions of Approval dated March 24, 2015;
- B Applicant's Findings of Fact and Conclusions of law including:
 - Demonstration of Compliance with Applicable Standards
 - Site Plan
 - Lighting Plan
 - Floor Plan
 - Storm Drainage Plan
 - Architectural Elevations
 - Landscape Plan & Planting Alternatives
 - Vicinity Map
 - Jackson County Assessor Plat Map
 - City of Medford Zoning Map
 - Map depicting surrounding uses
 - Site Photos
 - Memorandum from Justin Bates, Deputy Fire Chief
- C Public Works Department Staff Report, dated March 11, 2015;
- D Medford Water Commission Memorandum dated March 9, 2015;
- E Fire Department Report, prepared February 27, 2015;
- F Medford Building Department Staff Memo, dated March 11, 2015;
- G Oregon Department of Transportation Email, received March 17, 2015;
- H Parks and Recreation Department Staff Memo, dated March 11, 2015;
- I Rogue Valley Sewer Services Correspondence, dated March 5 2015;
Vicinity Map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA: April 3, 2015

EXHIBIT A

**City of Medford Fire Station No. 2
AC-14-129 E-15-025
Conditions of Approval
March 27, 2014**

CODE CONDITIONS

1. The applicant shall:
 - a. Comply with the Public Works Staff Report dated March 11, 2015 (Exhibit C);
 - b. Comply with the Medford Water Commission Staff Memo dated March 11, 2015 (Exhibit D);
 - c. Comply with the Fire Department Land Development Report prepared February 27, 2015 (Exhibit E).
 - d. Comply with the Medford Parks Department Staff Memo March 11, 2015 (Exhibit H);
 - e. Comply with the letter received from Rogue Valley Sewer Services dated March 5, 2015 (Exhibit I);

CITY OF MEDFORD
EXHIBIT # "A"
File # AC-14-129-E-15-025
1081

II

EVIDENCE SUBMITTED WITH APPLICATION

The following evidence was submitted in support of this application and has been made a part of the record:

- Exhibit 1.** Signed and Completed Application Forms.
- Exhibit 2.** The proposed findings of fact and conclusions of law (this document) which demonstrates how this application complies with the relevant substantive criteria of the City of Medford and State of Oregon
- Exhibit 3.** *Demonstration of Compliance with Applicant Standards* — an analysis of the relevant development standards established in the Medford Land Development Code (“MLDC”)
- Exhibit 4.** Applicant’s Plans including Site Plan; Lighting Plan; Floor Plan; Storm Drainage Plan; and Architectural Profile & Elevations
- Exhibit 5.** Landscape Plan & Planting Alternatives
- Exhibit 6.** Vicinity Map
- Exhibit 7.** Jackson County Assessor plat map 37-2W-36BD which contains and depicts the subject property
- Exhibit 8.** Current City of Medford General Land Use Plan (“GLUP”) on which the subject property is denoted
- Exhibit 9.** Current City of Medford Zoning Map on Aerial on which the subject property is denoted
- Exhibit 10.** Map depicting the surrounding land uses
- Exhibit 11.** Photos depicting the subject and surrounding properties and the development thereon
- Exhibit 12.** Memorandum from Justin Bates, Deputy Fire Chief

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III

APPLICABLE SUBSTANTIVE CRITERIA

The criteria under which a site plan application must be considered are in Section 10.227 of the Medford Land Development Code ("MLDC"). The relevant approval criteria are recited verbatim below:

MLDC 10.337 (SIC 811-972) Uses Permitted in Commercial and Industrial Zoning Districts

92 JUSTICE, PUBLIC ORDER, SAFETY. This major group includes government establishments engaged in justice, public order and safety.

		C-S/P	C-N	C-C	C-R	C-H	I-L	I-G	I-H
921	Courts	P	X	P	P	P	X	X	X
922	Public Order and Safety	P	X	P	P	P	P	P	P

MLDC 10.290 SITE PLAN AND ARCHITECTURAL REVIEW CRITERIA

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land, and
- (2) The proposed development complies with the applicable provisions of all city ordinances, or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC Section 10.253.

MLDC 10.253 Criteria for an Exception

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

- (1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met. (Effective Dec. 1, 2013).
- (2) The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.
- (3) There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.
- (4) The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.



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IV

FINDINGS OF FACT

The Medford Site Plan and Architectural Commission (“SPAC” or “the Commission”) reaches the following facts and find them to be true with respect to this matter:

1. **Property Location:** The subject property is situated on the south side of Stewart Avenue, approximately 400 feet east of its intersection with Columbus Avenue. See, Exhibit 6.
2. **Ownership:** The subject property is owned in fee simple by City of Medford.
3. **Property Description:** The property consists of a single 1.36 acre parcel identified in the records of the Jackson County Assessor as Tax Lot 6200 on assessors plat 37-2W-36BD. See, Exhibit 7.
4. **Existing Land Use:** The property is presently vacant. See, Exhibit 9.
5. **Comprehensive Plan (GLUP):** The subject property’s comprehensive plan map designation (“GLUP”) is Commercial. See, Exhibit 8.
6. **Existing Zoning:** The subject property is zoned Community Commercial (C-C). See, Exhibit 9.
7. **Surrounding Land Uses:** Exhibits 9, 10 and 11 accurately depict the pattern of land partitioning and development in the surrounding area. The surrounding properties include a mix of commercial and residential uses. The land uses which presently surround the property are:

West: Adjacent to the north half of the west property line, along the Stewart Avenue frontage is a small commercial strip center now occupied with a retail commercial businesses including two restaurants and a convenience store. Adjacent to the south half of the west property line is a vacant lot zoned for commercial use.

South: Immediately south of the subject property is a residential subdivision with access from Clearwater Drive.

East: Adjacent to the north half of the east property line, along the Stewart Avenue frontage is a store selling retail pet products — Mini Pet Mart. Adjacent to the south half of the east property line are two existing residences separated from the subject property by an existing fence and three mature trees.

North: Stewart Avenue borders the frontage of the subject property. Across Stewart Avenue to the north is an existing residential subdivision occupied by single family dwellings.

8. **Essential (Category ‘A’) Public Facilities:** The comprehensive plan defines Category ‘A’ public facilities to include sewer, water, storm drainage and transportation. The property fronts and takes access from Stewart Avenue, a city arterial street which carries substantially greater than 1,000 average daily vehicular trips (ADT). Municipal sewer, water and storm drainage facilities presently exist along the property frontage on Stewart Avenue.



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9. **Police and Fire Protection:** Police protection is provided by the Medford Police Department, which is headquartered at Medford City Hall at 8th and Oakdale streets. Fire protection to this property will be supplied by the proposed fire station.
10. **Access and Circulation:** The property fronts and takes access from Stewart Avenue. The proposal is for a fire station which has unique requirements for ingress and egress. Two access points are proposed. The easterly 11 feet of the subject property is part of an existing shared access with the adjacent property to the east, for a combined shared access width of approximately 30 feet. All non-emergency ingress and egress to the site will be from this access with egress on the opposite side of the property adjacent to access from the apparatus bays. An egress of approximately 68 feet is proposed along the northerly portion of the property frontage and has been designed to accommodate the rapid deployment of large fire apparatus during emergencies from the station's three apparatus bays. The rear of the building is separated from the sides and front of the building by fences with gates to accommodate secured vehicular parking for station personnel to the rear and east side of the building, along with space to accommodate the circulation of fire apparatus and other emergency equipment. To the rear of the building is a large 60-foot by approximately 180-foot paved area for vehicular maneuvering and the occasional parking of emergency vehicles.
11. **Architecture and Design Analysis:**
- Design Constraints:** The site is unconstrained by topography or other physical features although it is proximal to existing land uses, including single family dwellings and commercial development.
 - Massing:** The fire station has a footprint slightly smaller than the strip center to its immediate west, although larger than other commercial and residential structures in the surrounding area. See, Exhibits 4 and 9.
 - Architectural Style:** The fire station is of a contemporary style, similar to that of the adjacent strip center and pet supply store, albeit with a clerestory and flat roof. As an institutional structure its style is different from nearby residential structures, all of which have sloped roofs. As shown on the Exhibit 4 plans, the exterior materials of construction consist, predominantly of red brick with a band of polished CMU (concrete masonry unit) and glass. The dumpster enclosure will be of CMU. An example of color palette is provided with Exhibit 4 and is provided on the materials board that was submitted during the public hearing on this matter.
 - Bufferyards:** As illustrated on Exhibit 4, the proposed fire station is to be situated approximately 168 feet from the south property line. The southern-most 68 feet is void of structural improvements and is reserved for storm drain detention swale and vegetation. Along the southerly and a portion of the easterly boundaries of the property is a minimum ten foot wide bufferyard intended to meet the requirements of



MLDC 10.790 including a minor adjustment consistent with MLDC 10.790(E)(6) and the same is explained in Exhibit 3.¹

- e. **Building Height:** The plans in Exhibit 4 depict the height of the proposed fire station measured from grade. The building has a maximum height of 21 feet, 2 inches but ranges downward to only 16 feet, well below the permissible height of buildings in the C-C zoning district and less than other residential and commercial buildings that exist on abutting properties and those in the surrounding area.

12. Fire Station Operational Characteristics: In preparing this application, Applicant's Agent interviewed City of Medford Fire Department Deputy Chief, Justin Bates on November 13, 2014 for the purpose of ascertaining the operational characteristics of this Fire Station #4 and other Medford fire stations to be built or remodeled and Chief Bates followed up with a memorandum (Exhibit 12). Exhibit 12 and the earlier interview yielded the following facts:

- The Medford Fire Department receives approximately 10,000 emergency calls each year.
- When emergency calls are received and fire apparatus are deployed from this and other stations, no sirens are used until the apparatus are two or more blocks from the responding fire station, and then only as needed. During nighttime hours when traffic is comparatively light, sirens are only used when approaching intersections and then only as needed to alert motorists to oncoming fire apparatus.
- Training or other operational matters are typically conducted indoors. However, portions of the property, including the concrete structure immediately to the south and tower have been and will continue to be used for occasional outdoor training. This is an established use that will continue.
- No outdoor recreational facilities are planned for the Fire Station #3 site, although a basketball hoop might later be installed in the rear paved area for daytime use by Fire Department personnel when not responding to emergency calls, inspecting businesses or conducting public information services in the community.

V

CONCLUSIONS OF LAW

The City reaches the following conclusions of law for each of the relevant substantive criteria with respect to Site Plan and Architectural Review and for the granting of exception relief from the standards governing driveway width (and related regulations that go to driveway location and placement as explained in greater detail below:

¹ The bufferyard adjustment involves substituting the existing wood fence for the otherwise required block wall and the additional landscaping and spatial separation afforded on this site. See, Exhibit 5 Landscape Plan.



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REQUEST FOR SITE PLAN AND ARCHITECTURAL REVIEW APPROVAL

Applicant City of Medford Fire Department, seeks the approval of its plans in Exhibits 4 and 5 pursuant to the approval criteria set forth in MLDC 10.290 and addressed herein below:

Site Plan and Architectural Review Criterion 1

MLDC 10.290 SITE PLAN AND ARCHITECTURAL REVIEW CRITERIA

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land, and

Discussion; Conclusions of Law: The proposed fire station is located in a Community Commercial (C-C) zoning district in which it is a permitted use. The subject property and proposed fire station is sandwiched between two retail commercial uses — the small commercial strip center to its west and pet supply store to the east, with single family dwellings across Stewart Avenue to the north and dwellings to the south. The fire station is of brick and concrete construction, similar to the mostly concrete block construction of the adjacent commercial uses and will, from an architectural materials standpoint, be compatible therewith. The fire station materials, however, differ from the painted wood siding prevalent on adjacent and nearby dwellings. The roof of the fire station is intended to be flat or very nearly so which differs from the pet supply store building and adjacent dwellings, although the adjacent commercial strip center also has a flat roof, albeit with parapet walls (which the fire station is not intended to have). Instead, the fire station building has a clerestory with windows to let light into the building. The frontage of the pet supply store is attractively landscaped with well-maintained trees, shrubs and groundcover (grass), as are the adjacent dwellings. The commercial strip center has no landscaping along its Stewart Avenue frontage and little landscaping elsewhere. Based upon the Exhibit 5 Landscape Plan, the proposed fire station will be well landscaped along portions of the frontage not devoted to emergency access. Additional landscaping will be supplied adjacent to select portions of the building, where the same will not interfere with fire apparatus maneuvering and vehicular access and more landscaping is intended along the south and portions of the east property boundary to meet municipal standards for bufferyards. Still additional landscaping is intended around the storm water detention pond, as well as to the rear of the building.

As to the use itself in relation to land uses on adjacent properties (including those directly across Stewart Avenue) the adjacent commercial uses to the east and west, are unlikely to experience any issues of incompatibility other than the periodic dispatching of emergency apparatus that might briefly interrupt unfettered access to the commercial uses, a matter of potential incompatibility that the Commission concludes to be slight and largely insignificant. With respect to adjacent residential uses, areas of potential incompatibility also involve the periodic nighttime dispatching of emergency apparatus. However, based upon the findings of fact and Exhibit 12 (which explains the operating standards for Medford fire stations) nighttime impacts will only include the potential for headlights and emergency lights to disrupt the occupants of dwellings across Stewart Avenue; dwelling to the south and



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east of the station should experience no such impacts from headlights or emergency lights, and the only potential impacts to these occupants relates to occasional noise from recreational use by station personnel on rear portions of the site. In this regard, the impacts are not expected to be significant because recreational use will be infrequent and limited to daylight hours and should not be appreciably different from recreational activities carried out by young people in most residential neighborhoods. Moreover, any recreational use will be buffered from residents to the south and east by fencing and screening which will be comprised of the required bufferyard elements, plus a significantly greater spatial separation between the building and boundaries that abut dwelling properties.

Based upon the foregoing, the Commission concludes that this proposed building is attractive and substantial, and while not identical to the architecture or construction materials used on adjacent buildings, is nonetheless compatible. The Commission also concludes that the use itself is rather innocuous but for periods when emergency fire apparatus are deployed from the station, and then the only element of potential incompatibility goes to nighttime deployments where headlights and emergency lights might be seen through the window coverings of dwellings opposite the station on Stewart Avenue. The Commission concludes that such potential impacts on adjacent lands are relatively slight and do not appreciably diminish the fire station's compatibility with the said adjacent uses.

Based upon the findings of fact and evidence and the foregoing discussion and conclusions of law, the Commission concludes that this application is consistent with Site Plan and Architectural Review Criterion 1 because the proposed development is deemed to be compatible with uses and development that exist on adjacent land.

Site Plan and Architectural Review Criterion 2

- (2) The proposed development complies with the applicable provisions of all city ordinances, or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC Section 10.253.

Discussion; Conclusions of Law: Based upon Applicant's plans in Exhibits 4 and 5, and the *Demonstration of Compliance with Applicable Standards* document (Exhibit 3) the Commission concludes that this project complies with all applicable standards, except for the width of the emergency driveway for which exception relief is sought and the approval criteria for which is addressed herein below. The only other issue of compliance regards an adjustment to the required bufferyard standards which involve substituting an existing wood fence for the otherwise required concrete block wall, and is justified by the more substantial amounts of living landscape materials and greater spatial separation between the station and its property boundaries. As such, and based upon the approval for exception relief and bufferyard adjustment, this application is concluded by the City to be consistent with Site Plan and Architectural Review Criterion 2.

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REQUEST FOR EXCEPTION RELIEF

Applicant City of Medford Fire Department seeks the approval of exception relief from the provisions of the MLDC which govern the width of driveways, their spacing and permissible locations. In particular, for uses in commercial zones² driveways along streets having greater than 1,000 ADT (average daily vehicular trips) — like Stewart Avenue — must be no wider than 36 feet. In this instance, Applicant asserts that greater driveway width — in this instance, approximately 68 feet — is required to enable the rapid and proper egress and deployment of emergency apparatus from three equipment bays proposed for this site. This exception is also intended to cover other provisions of MLDC 10.550 which may apply to the proposed additional driveways intended to serve fire station personnel for non-emergency ingress and egress. These provisions include:

- MLDC 10.550(3)(a)(1) Driveways shall be placed adjacent to the property line of a contiguous parcel if possible to do so and meet driveway spacing requirements of this section.
- Table at MLDC 10.550-3 which requires the spacing of driveways further apart than is proposed in this application.
- MLDC 10.550(3)(a)(2) Number of Driveways for each Separately Owned Tract. Within the requirements set forth in a. (1) above, driveway access to an Arterial Street shall be limited to one driveway for each tract of property owned.
- MLDC 10.550(3)(a)(3) Cross-Access Easement Required. Any parcel or tract granted driveway access to an Arterial or Collector Street shall grant cross-access easements to all contiguous parcels or tracts that do not abut a street of a lower order than an Arterial or Collector Street. Site design must accommodate future use of such accesses.

Use of shared driveways on multiple parcels or tracts and cross-access easements shall be required when site and traffic conditions, including projections of future traffic volumes and movements, indicate that such requirements will preserve the capacity and safety of the transportation system.

Exception Criterion 1

MLDC 10.253 CRITERIA FOR AN EXCEPTION

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

- (1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met.

² MLDC 10.550(1) provides that institutional uses shall be considered commercial uses for purposes of that subsection.



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Discussion; Conclusions of Law: Criterion 1 begins with a requirement to evaluate, “the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located.” The subject property is zoned Community Commercial (C-C) and its purpose is set forth briefly in MLDC 10.328:

“The C-C district provides land for the development of commercial facilities servicing the shopping needs of the community and is typically comprised of community shopping centers. All uses, except as noted in section 10.337, do not exceed 50,000 square feet of gross floor area.”

The stated purpose for the C-C zone references the delivery of shopping opportunities in buildings not larger than 50,000 square feet. It contains nothing relevant to the delivery of either emergency services facilities in general or fire stations in particular. As such, the City concludes that the proposed fire station is not inharmonious, and, therefore, the City concludes that it is in harmony with the general purpose and intent of the C-C zone.

Criterion 1 also requires a conclusion that the exception shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. In context of this application the City concludes that it need only show that the greater driveway width (and exceptions from other spacing/location standards in MLDC 10.550) will not injure or otherwise be detrimental to the health, safety, and general welfare or adjacent natural resources. With respect to natural resources, the City concludes that this property is located centrally within incorporated Medford and there are no undeveloped lands containing natural resources within the larger surrounding area (other than the subject property which is being redeveloped). There also are no natural resources located upon the subject property other than existing trees as shown on Exhibit 9, none of which are deemed by the City to be a natural resource under the common meaning of that term.

With respect to whether this exception to driveway width (and other spacing/location standards in MLDC 10.550) will be injurious to the general area, the City concludes that it will not be injurious as the additional expanse of concrete, while perhaps to some unattractive, will nonetheless permit the proper and rapid deployment of emergency apparatus. In reaching this conclusion, the City also concludes that unlike ordinary vehicles that traverse the municipal transportation system, emergency vehicles have the capacity to temporarily alter the traffic flow by use of lights and (when needed) sirens to ensure that fire station egress can and will be safe and not be injurious to the general area. Accordingly, the City concludes that this fire station will serve the general surrounding area and to the extent that the additional expanse of concrete (and nonobservance of other municipal spacing/location standards) may not be as attractive as more narrow driveways (and those which observe otherwise required location and spacing standards) the health, safety, and general welfare of the surrounding area will, in balance, be enhanced by facilitating more rapid and proper deployment of emergency apparatus. Therefore, the City further concludes that the granting of this exception will not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare.

For the reasons above explained, the City concludes that this exception to driveway width is appropriately satisfied under the terms of Exception Criterion 1.



Exception Criterion 2

- (2) The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.

Discussion; Conclusions of Law: The Commission concludes that the proposed exception is specific to driveway width and does not have the effect of permitting the establishment of any use which is not permitted in the Community Commercial (C-C) zone in which the property is located. As such, the Commission concludes that this exception is consistent with and satisfies Exception Criterion 2.

Exception Criterion 3

- (3) There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.

Discussion; Conclusions of Law: The City concludes that any site within incorporated Medford that is proposed to accommodate a new fire station (or to facilitate changes and access improvement to existing stations) will necessarily require an exception to driveway width (and other driveway location and spacing standards) especially for stations that feature multiple emergency apparatus bays. While it might be argued that there are some locations within incorporated Medford where standards related to driveway spacing and location might be attainable, these typically do not exist in developed portions of the city where the subject fire station is to be located. As a matter of emergency services delivery, the City concludes that fire stations are best located where they are able to rapidly and efficiently serve buildings and other improvements within their existing or planned service areas. In this instance, the much larger surrounding area is almost completely occupied by existing urban development which presents no opportunity to locate this fire station where it might comply with the City's driveway spacing and location standards. As to driveway width, there are no potential sites that can accommodate the greater driveway width needed and herein proposed because such width is categorically precluded by the plain and unambiguous terms of the MLDC.

The second portion of Exception Criterion 3 requires a showing that strict application of the standards (in MLDC 10.550) will result in peculiar, exceptional, and undue hardship on the owner. The City concludes that the owner of the subject property is the City of Medford, which in context of this application, operates on behalf of its citizens. If the standards in MLDC 10.550 were strictly applied, the City concludes that the same would reduce the speed and efficiency of emergency apparatus deployment which would produce upon its citizens a peculiar, exceptional, and undue hardship by reducing emergency response times.

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For the reasons established above, the City concludes that this application satisfies the requirements of Exception Criterion 3.

Exception Criterion 4

- (4) The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.

Discussion; Conclusions of Law: Based upon the evidence, the need for this exception does not result from any illegal act and there is no evidence to suggest otherwise. While Applicant City was aware of the standards from which exception relief is sought, it was with the knowledge that *any* fire station site would require similar relief from application of the standards in MLDC 10.550 and, if not approved, compromised emergency apparatus egress/deployment would be suffered directly [by the property in question], the City and its citizens. Therefore, the City concludes that the requirements of Exception Criterion 4 have been satisfied.

VI

ULTIMATE CONCLUSIONS

Based upon the foregoing findings of fact and conclusions of law, the City concludes that these applications for Site Plan and Architectural Review and for Exception relief, is consistent with all of the relevant substantive approval criteria of the City of Medford as set forth and addressed herein above. Therefore, the Site Plan and Architectural Commission on behalf of the City orders that these applications be, and the same hereby are, approved and made subject to the conditions set forth in the Commission Report for this matter.

Respectfully submitted on behalf of applicant:

CSA PLANNING, LTD.

Craig Stone
Consulting Planner

Dated: November 21, 2014



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APPLICANT'S EXHIBIT 3
DEMONSTRATION OF COMPLIANCE WITH APPLICABLE
DEVELOPMENT STANDARDS
FIRE STATION 2

RECEIVED
FEB 05 2015
PLANNING DEPT.

Medford Land Development Code ("MLDC"). The relevant approval standards are recited verbatim below:

10.328 Community Commercial, C-C

The C-C district provides land for the development of commercial facilities servicing the shopping needs of the community and is typically comprised of community shopping centers. All uses, except as noted in section 10.337, do not exceed 50,000 square feet of gross floor area.

10.337 (SIC 01-289) Uses Permitted in Commercial and Industrial Zoning Districts

92 JUSTICE, PUBLIC ORDER, SAFETY. This major group includes government establishments engaged in justice, public order and safety.

		C-S/P	C-N	C-C	C-R	C-H	I-L	I-G	I-H
921	Courts	P	X	P	P	P	X	X	X
922	Public Order and Safety	P	X	P	P	P	P	P	P

Compliance with Standards: The proposed project is a permitted use in the C-C zone. City of Medford uses two and three digit SIC codes. SIC 922 - Public Order and Safety is a 'P' in C-C zone. Pursuant to the US Department of Labor website, Fire Stations have a SIC code of 9224, thereby confirming the use is within the 922 category in the MLDC and fire stations are thereby permitted in C-C zone.

10.550 Access Standards

(1) Driveway Throat Width Standards.

Driveways that connect to a public right-of-way shall be constructed according to the standards in Tables 10.550-1 and 10.550-2. See Figure 10.550-1 for driveway throat, flare and radius definitions.

For the purposes of this section, Minimum Access Easements and Alleys shall be considered driveways.



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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

Street Classification	Land Use on Parcel to be Served by the Driveway*			
	SFR	MFR	COMMERCIAL	INDUSTRIAL
Local (All streets other than Collectors or Arterials.)				
Less than 500 ADT***	12 to 18 ft.****	18 to 24 ft.	24 to 30 ft.	24 to 30 ft.
500 to 1,000 ADT***			24 to 30* ft.	24 to 36 ft.
Greater than 1,000 ADT***			24 to 36* ft.	24 to 36 ft.
Collector	SFR	MFR	COMMERCIAL	INDUSTRIAL
Less than 500 ADT***	12 to 24 ft. (Shared Approach Only)	18 to 36 ft.	18 to 24 ft.	24 to 36 ft.**
500 to 1,000 ADT***			18 to 36 ft.**	24 to 36 ft.**
Greater than 1,000 ADT***			18 to 36 ft.**	24 to 36 ft.**
Arterial	SFR	MFR	COMMERCIAL	INDUSTRIAL
Less than 500 ADT***	No direct access unless no alternative exists, then 12 to 20 ft. (Shared Approach Only)	No direct access unless no alternative exists, then 18 to 36 ft.	18 to 24 ft.	24 to 30 ft.
500 to 1,000 ADT***			18 to 30 ft.	24 to 36 ft.
Greater than 1,000 ADT***			18 to 36 ft.	24 to 36 ft.

*Institutional uses shall be considered commercial uses for the purposes of this subsection.

**Driveway throat widths greater than 24 feet shall be granted only for full movement approaches that warrant a left turn lane.

***ADT = Average Daily Trips using the proposed driveway, determined from the latest version of the Institute of Transportation Engineering handbook based on the expected ultimate use of the site.

****May be up to 24' with approval of Public Works Director or Designee.

Compliance with Standards: The subject property fronts on and takes access from Stewart Avenue, an arterial road with an ADT in excess of 1,000. However, the proposed use is a fire station that requires driveway widths wider than standard in order to accommodate fire apparatus movements. The proposed approach to Stewart Avenue is 68 feet 4.5 inches in width. In addition, the project proposes to share an existing access with the adjacent property to the east for all non-emergency related ingress and egress for passenger vehicles and fire apparatus alike. The shared access width is 30 feet. Of which, eleven feet is on the subject property. Based on a number of factors, one of which being the length of the fire trucks, the driveway throat width warrants a distance greater than 24 feet. See Applicant's Exhibit 2 Findings of Fact and Conclusions of Law including a request for exception to these driveway throat width provisions.

10.550 Access Standards

(2) Driveway Radius Standards

Table 10.550-2 Driveway Radius Standards



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Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

Street Classification	Land Use on Parcel to be Served by the Driveway*			
	SFR	MFR	COMMERCIAL	INDUSTRIAL
Local	Curb Cut	<6 units: curb cut >6 units: 15 ft. radius	Curb cut or 20 ft. radius	Curb cut or 20 ft. radius
Collector	Shared Access Curb cut		20 ft. radius	25 ft. radius
Arterial	No direct access unless no alternative exists, then 15 ft. radius		25 ft. radius	25 ft. radius

Compliance with Standards: The subject property fronts on and takes access from Stewart Avenue, an arterial road. As evidenced by Applicant's site plan, the driveway radiuses are 25 ft, in conformance with this standard.

(3) Driveway Spacing and Locational Standards

a. Arterial and Collector Streets

- (1) Driveway spacing and Location. No driveway access to an Arterial or Collector Street shall be allowed for any parcel that abuts the right-of-way of a lower order street or has legal right of access to any street via any abutting parcel(s).

No driveway access shall be allowed to an Arterial Street within 150 feet of the nearest right-of-way line of an intersection street.

Driveways shall be placed adjacent to the property line of a contiguous parcel if possible to do so and meet driveway spacing requirements of this section.

No driveway access to an Arterial or Collector Street shall be located closer than allowed by the standards in Table 10.550-3, measured from the center of driveway to center of driveway.

SPEED LIMIT (MPH)	25	30	35	40	45
DRIVEWAY SPACING	145 ft.	195 ft.	250 ft.	315 ft.	385 ft.

- (2) Number of Driveways for each Separately Owned Tract. Within the requirements set forth in a. (1) above, driveway access to an Arterial Street shall be limited to one driveway for each tract of property owned.

For purposes of this section, parcels that are contiguous under the same ownership or part of the same application for development shall be considered to be one tract of property.

- (3) Cross-Access Easement Required. Any parcel or tract granted driveway access to an Arterial or Collector Street shall grant cross-access easements to all contiguous parcels or tracts that do not abut a street of a lower order than an Arterial or Collector Street. Site design must accommodate future use of such accesses.

Use of shared driveways on multiple parcels or tracts and cross-access easements shall be required when site and traffic conditions, including projections of future traffic volumes and movements, indicate that such requirements will preserve the capacity and safety of the transportation system.

c. Alternative Access Spacing and Location

The standards set forth in Sections 10.550 (3) (a) and (b) above are the City's basic access standards. The City also recognizes the unique situations and/or complex projects may warrant alternative driveway spacing and locations.

- (1) Approval of Alternative Access Locations: When part of a Class C Plan Authorization, the Public Works Director or designee shall forward a recommendation on alternative access spacing and locations to the approving authority. When not part of a Class C Plan Authorization, the Public Works Director or designee



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may authorize an administrative adjustment to the access spacing and location standards in 10.550 (3) (a) and/or (b) above under one or both of the following circumstances:

- i. The adjustment will align the requested driveway with a driveway across the street and the Public Works Director's (or designee) professional opinion is that this configuration will improve safety and/or operations.
- ii. Where no proposed access location can satisfy all the standards in Section 10.550 (3) (a) and/or (b), the driveway may be located in the location that best meets the City's access management standards in the professional opinion of Public Works Director (or designee).

Compliance with Standards: The subject property has frontage only on Stewart Avenue, an arterial road with a speed limit of 35 mph at the subject site. The proposal includes one fire apparatus-only driveway situated on the property's westerly frontage to be used solely by fire apparatus and emergency vehicles. The primary access to be used for all other ingress and egress is a shared access at the property easterly frontage, including a cross-access easement with the property to the east. This shared access is situated directly across from local road Plum Street.

The proposal includes a fire station. Fire stations have unique access requirements and this application is part of a Class C Plan Authorization. Therefore the proposal is eligible for alternative access and spacing under c above. Applicant requests the Public Works (or designee) recognize the proposed driveways including the separation of emergency egress only driveway from the primary access directly across from local street best meets the City's access management standards.

10.721 Commercial and Industrial Site Development Standards

The following standards apply to commercial and industrial development. See Article III, Sections 10.326 through 10.332 for detailed descriptions of each zoning district, and Section 10.337 for conditional, special, and permitted uses

COMMERCIAL AND INDUSTRIAL DEVELOPMENT		
Standard	C-C	Proposed / *Current Condition
MINIMUM & MAXIMUM AREA FOR ZONING DISTRICT (ACRES)	None	-
MINIMUM LOT AREA (SQ. FEET)	7,000 SF	*1.36 acres (59,242 SF)
MAXIMUM COVERAGE FACTOR	40%	6.7%
MINIMUM LOT WIDTH	70 feet	*200'
MINIMUM LOT DEPTH	100 feet	*297'
MINIMUM LOT FRONTAGE	30 feet	*200'
MINIMUM FRONT & STREET SIDE YARD BUILDING SETBACK	10 feet, EXCEPT 20 feet for vehicular entrances to garages or carports	30'4" front



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Demonstration of Compliance with Applicable Development Standards

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COMMERCIAL AND INDUSTRIAL DEVELOPMENT		
Standard	C-C	Proposed / *Current Condition
MINIMUM SIDE & REAR YARD BUILDING SET BACK	None, EXCEPT ½ foot for each foot in building height over 20 feet	20' west; 48' east; 165' rear
MAXIMUM BUILDING HEIGHT	85 feet, EXCEPT 35 feet if within 150 feet of a residential GLUP or Special Area Plan designation.	22'
MAXIMUM GROSS FLOOR AREA PER BUSINESS (SQ. FEET)	None	9218 SF
PERMITTED OUTDOOR USES	See Note 2	Storage of Emergency Vehicles to rear of buildings within secure fenced and gated area
Note 2: All uses, EXCEPT those customarily conducted outdoors, must be located completely within an enclosed building.		

Compliance with Standards: The proposed project complies with all Commercial and Industrial standards of MLDC 10.721 as indicated by the table above and as evidenced by Applicant's site plan.

10.727 Excavation and Grading Permit

When an excavation and grading permit is required by this Chapter or by Chapter 9, the application for the permit shall be reviewed and approved by the City of Medford Building Safety Director or designee. When a permit is required, no grading or excavation shall occur prior to issuance. The permit shall not be issued until a grading plan pursuant to Section 10.728 has been reviewed and approved. Additional supporting data consisting of a soils engineering report and engineering geology report may also be required. The plans and specifications shall be signed and prepared by a registered engineer.

10.729 Stormwater Quality and Detention Facilities, Private Property

A. Purpose. It is the City's policy to maintain the natural hydrology and preserve water quality by mitigating the direct impacts of new development and preserving the environmental benefits of natural water bodies.

B. Applicability. Stormwater quality and detention facilities shall be required for development and building permits, with the exception of single-family residences and duplexes, which meet any one (1) of the following conditions

- (1) Building permits for development that creates 5,000 square feet or more impervious surface; or...

Compliance with Standards: Applicant's Exhibit 4 includes a stormwater drainage plan demonstrating compliance with relevant stormwater and grading provisions. Should a separate grading permit be found to be required, Applicant will agree to stipulate to filing a grading plan pursuant to MLDC 10.728.

10.732 Fencing of Lots

- (1) Fencing located within the front yard setback area of all zones, except the MFR zone, shall not exceed three (3) feet in height when measured from the grade of the street centerline. When within a MFR zone, a fence shall not



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exceed three (3) feet in height when located within ten (10) feet of a street right-of-way unless otherwise approved by the approving authority. (Effective Dec. 1, 2013.)

- (2) Fencing located in the side or rear yards (when not a through-lot) shall not exceed eight (8) feet in height. Height shall be measured as follows:
 - (a) In required yards abutting a street, it shall be the effective height measured from the finished grade on the side nearest the street.
 - (b) In other required yards, it shall be the total effective height above the finished grade measured on the side nearest the abutting property.
- (3) No fencing shall conflict with the site distance requirements of Section 10.735, Clear View of Intersecting Streets.
- (4) All fencing shall comply with Section 9.560, Hazardous Fences Prohibited, of the City Code.
- (5) Gates on primary vehicle entrances adjacent to public streets shall be set back a minimum of 20 feet from the right of-way. Gates on secondary entrances (e.g., stored vehicles, etc.) may be set back less than 20 feet if approved by the Engineering Division. Gates provided with a remotely operated opening mechanism may be set back 20 feet from the nearest existing or planned vehicular travel lane as determined by the City Engineer.

Compliance with Standards: No fencing is proposed along the street frontages of the property. Applicant's site plan demonstrates compliance with all fencing and gating requirements under MLDC 10.732.

10.735 Clear View of Intersecting Streets

- (1) In order to provide a clear view of intersecting streets, there shall be a triangular area of clear vision formed where a street intersects with another street, driveway, or alley.
- (2) The size of the triangular area is a function of traffic control, volume and speed. See Table 10.735-1 below for posted speeds and site distances.
- (3) On any portion of a lot that lies within the triangular area described and illustrated in Figures 10.735-1 & 2, below, nothing shall be erected, placed, planted or allowed to grow in such a manner as to impede vision between a height of three (3) feet and ten (10) feet above the height of the top of the curb. Where there is no curb, the height shall be measured from the street center lines.
- (4) The triangular area of clear vision shall be determined based upon the type of stop control used at the subject intersection.
 - (a) The clear vision triangular area for an intersection of a street without stop control is shown in Figure 10.735-1. See Table 10.735-2 below for determining all other curb line distances.

Figure 10.735-1 Clear Vision Triangle for an Intersection without Stop Control – SEE BELOW

Table 10.735-1 Intersection Site Distance



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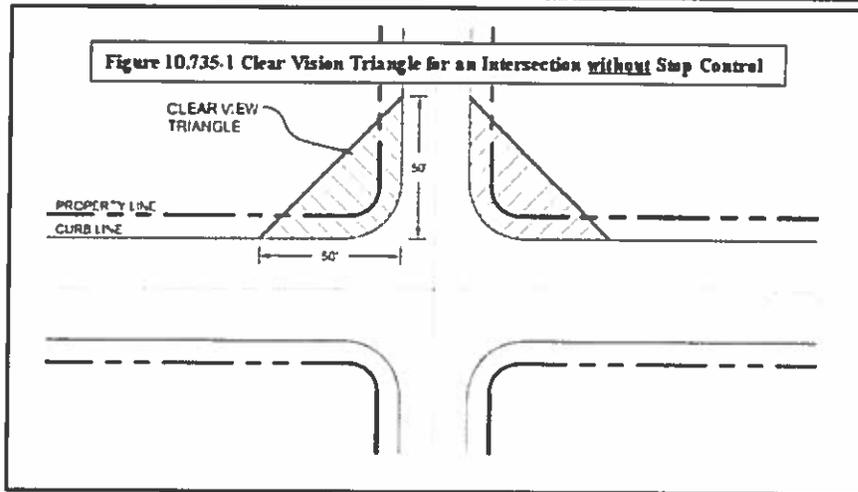
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Posted Speed Limit	Intersection Sight Distance (ft.)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495



Compliance with Standards: Stewart Avenue is a 35 mph street in front of the subject property. The driveway intersections with Stewart Avenue are not stop controlled and therefore require intersection sight distances of at least 250 feet. Stewart Road at the subject property is flat and straight. Based on the horizontal and vertical alignments of Stewart Avenue at the subject property, sight distances at the proposed access points exceed the 250ft minimum required. Applicant's attached site plan illustrates appropriate clear vision triangles for the proposed site. The proposal includes a 6-foot tall sign to be located west of the easterly shared approach, outside the clear vision triangle. Vegetative plantings located at the front of the property, within the clear vision triangle will be low-growing not to exceed 3 feet in height.

10.741 Parking and Loading, General Provisions

A. Off-Street Parking and Loading.

(1) A development permit shall not be issued until plans and evidence are presented to show how the parking and loading requirements are to be fulfilled and that property is and will be available for exclusive use as parking and loading space.

Off-street parking and loading spaces shall be provided and maintained as set forth in Sections 10.741 - 10.745 for all uses in all zoning districts.



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C. Parking and Loading Plan.

A parking plan, drawn to scale, shall accompany development permit applications for all developments excluding those for single and two-family dwellings. The parking plan shall show all those elements necessary to indicate that the requirements of this Code will be fulfilled and shall include, but are not limited to:

- (1) Delineation and typical dimensions of individual parking and loading spaces;
- (2) Dimensions of the circulation and maneuvering areas necessary to serve parking and loading spaces;
- (3) Access to streets, alleys and proper ties to be served;
- (4) Curb cuts;
- (5) Type of fencing or other screening materials;
- (6) Abutting land uses;
- (7) Grading, drainage, surfacing and subgrading details;
- (8) Location, height, and candlepower of lighting fixtures;
- (9) Delineations of all structures and other obstacles to parking and circulation on the site;
- (10) Specifications as to signs and bumper guards, and
- (11) Parking lot landscaping plan.

Compliance with Standards: Applicant's Exhibit 4 accurately depicts items 1 through 5 and 7 through 10. Exhibits 9 through 11 illustrate and Exhibit 1 characterizes abutting land uses (item 6). Exhibits 4 and 11 are consistent with item 11.

10.743 Off-Street Parking Standards

- (1) **Vehicle Parking – Minimum and Maximum Standards by Use.** The number of required off-street vehicle parking spaces shall be determined in accordance with the standards in Table 10.743-1.

Where a use is not specifically listed in table 10.743-1, parking requirements shall be determined by the Planning Director or designee finding that the use is similar to one of those listed in terms of parking needs.

Parking spaces that count toward the minimum requirement are parking spaces meeting minimum dimensional and access standards in garages, carports, parking lots, bays along driveways, and shared parking areas.

- (2) **Number of Required Parking Spaces.** Off-street vehicle parking spaces shall be provided as follows:
 - (a) **Parking Space Calculation.** Parking space ratios are based on spaces per 1,000 square feet of gross floor area, unless otherwise noted.
 - (b) **Parking Categories.**
 - (i) Table 10.743-1 contains parking ratios for minimum required number of parking spaces and maximum permitted number of parking spaces for each land use.
 - A. **Minimum Number of Required Parking Spaces.** For each listed land use, the City shall not require more than the minimum number of parking spaces calculated for each use.
 - B. **Maximum Number of Permitted Parking Spaces.** The number of parking spaces provided shall not exceed the maximum number of parking spaces allowed for each listed land use.

- (3) **Exceptions to Required Off-Street Parking for Non-Residential Uses.** The approving authority may allow exceptions to the number of parking spaces in Table 10.743-1 for specific uses without complying with Section 10.251 if they find that the applicant's detailed description of the proposed use demonstrates that the number of needed parking spaces is less than the minimum required or more than the maximum allowable based upon one or both of the following (effective Dec. 1, 2013):

- (a) An explanation why the characteristics of the proposed use require a different off-street parking standard than what is otherwise required.



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(b) An analysis providing parking data for the same business or a similar use within the city that demonstrates a need for a different off-street parking standard

Table 10.743-1 – City of Medford Minimum and Maximum Parking Standards			
Land Use Category	Parking Standards are based on number of spaces per 1,000 Square Feet of Gross Floor Area (unless otherwise noted)		
	Minimum Number of Required Parking Spaces		Maximum Permitted Parking Spaces
	Central Business District C-B Overlay (outside of Downtown Parking District)**	All Other Zones	All Zones
Public Services (such as utility service yards)	1.0 space per 1.2 employees on the largest work shift, plus 1.0 space per company vehicle normally stored on the premises	1.0 space per 1.1 employees on the largest work shift, plus 1.0 space per company vehicle normally stored on the premises	1.0 space per .8 employees on the largest work shift, plus 1.0 space per company vehicle normally stored on the premises

Compliance with Standards: The proposed project complies with the parking standards of MLDC 10.743. Fire Stations are a public service type of use requiring 1 space per 1.2 employees on largest work shift plus 1.0 space per company vehicle normally stored on the premises. Applicant states there are likely to be 3-4 employees onsite during a typical work shift, with 7 being the maximum number of employees at the station. There are three dedicated bays for fire trucks within the fire apparatus bay. There are five 10' x 18' dedicated parking spaces for employees and visiting personnel at the south side of the building. There is one 10'x18' dedicated car pool space. There are four 9' x 18' parking spaces for visitors at the building northeast corner; one of which is hand-capped space. A concrete paved area 60 feet deep and 185 feet wide is located south of the proposed buildings. This area is to be used for fire apparatus circulation and occasional parking of other emergency related equipment from other stations and public facilities. The south side of the building is fenced / gated from access by the general public.

(4) Reductions in Required Parking Spaces.

The total number of required motor vehicle parking spaces for an existing or new industrial, commercial, or office development may be reduced by five percent (5%) for each of the activities listed below that are provided by the owners or operators in perpetuity, up to a maximum of ten percent (10%) reduction in the total number of motor vehicle spaces required per development.

- (a) Providing showers and lockers for employees who commute by bicycle or foot;
- (b) Providing twice as many covered, secured bicycle parking spaces as required by this code;
- (c) Providing a public plaza as defined herein adjacent to a transit route with transit service currently available, which is within one quarter (1/4) mile of a major transit stop on that route. If there is a bus stop along the site's frontage, the plaza must be adjacent to the bus stop. The plaza must be open to the public, and at least 300 square feet in size exclusive of connecting walkways.
- (d) Providing a transit shelter with landscaping, and trash receptacle;
- (e) Other incentives provided in an approved Transportation Demand Management Plan (TDM) approved by the Approving Authority.

Compliance with Standards: No reduction in parking spaces is being requested.

(5) Two-Wheeled Vehicle Parking.

When over twenty (20) spaces are required, a maximum of five percent (5%) of the required parking may be two-wheeled vehicle spaces (e.g. motorcycle/moped/scooter). Each two-wheeled vehicle space must be at least four (4)



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feet wide and eight (8) feet deep. The two-wheeled vehicle spaces shall be clearly designated by a sign posted or be marked on the pavement within the two-wheeled parking area. Two-wheeled spaces shall be grouped together in designated areas and need not be individually striped. Two-wheeled parking shall be closer to the building than fifty percent (50%) of the vehicle spaces.

Compliance with Standards: The project does not necessitate 20 or more parking space, therefore 2 wheeled spaces are not required and none are proposed.

* * * * *

10.745 Location of Parking Facilities

All parking spaces shall be on the same lot as the main structure it serves or on an abutting lot. However, upon demonstration by the applicant that parking on the same lot or abutting lot is not available, the approving authority may authorize the parking spaces to be on any lot within 250 feet walking distance of the structure being served upon written findings of compliance with the following provisions:

- (1) There is a safe, direct, attractive, lighted and convenient pedestrian route between the parking area and the use being served;
- (2) There is assurance in the form of deed, lease, contract or other similarly recorded document that the required spaces will continue to be available for off-street parking use according to the required standards.

Compliance with Standards: All proposed parking spaces are located on the same parcel as the main structure. Applicant's site plan demonstrates compliance with safety and aesthetic standards required herein.

* * * * *

10.748 Bicycle Parking Standards

Bicycle parking spaces shall be provided in accordance with the following:

Residential, retirement or congregate housing: .05 space per unit; 4 space minimum, regardless of number of units.

Compliance with Standards: Bicycle parking is not required per this provision and none are proposed.

* * * * *

10.763 Particulate Matter (Windborne Dust)

The release of windborne dust across lot lines shall be considered a nuisance. Open storage piles, unpaved roads, open yards and the like shall be wetted, oiled, planted or covered to eliminate or minimize windborne dust. Fugitive sources of windborne dust, such as from windows, ventilators, conveyors and the like shall be controlled by closure, collection, or other acceptable methods of good practice.

Compliance with Standards: As evidenced by Applicant's site plan, the site is proposed to be paved and there are no open storage piles, open yards or unpaved roads. The proposed use is not one that is typically associated with generating high levels of windborne dust, however in the event potential sources are found to occur, Applicant will control windborne dust with closure, collection or other acceptable methods of good practice.

10.764 Glare



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In all districts, any operation or activity producing glare shall be so conducted that direct or indirect light from the source shall not have a maximum permitted illumination in excess of 0.5 foot candles on any property in a residential district, other than the lot on which the glare is generated. This section is not intended to apply to public street lighting.

(1) Definitions.

Candlepower: The amount of light that will illuminate a surface one (1) foot distant from a light source to an intensity of one (1) footcandle. Maximum (peak) candlepower is the largest amount of candlepower emitted by any lamp, light source, or luminaire.

Footcandle: A unit of illumination produced on a surface, all points of which are one (1) foot from a uniform point source of one (1) candle.

Glare: The brightness of a light source which causes eye discomfort.

Maximum Permitted Illumination: The maximum illumination measured in footcandles at the interior buffer-yard line at ground level in accordance with the standards of Subsection 4 below.

Post Height: The distance measured from the grade at the base of the light post to the top of the light fixture.

(2) Exemption for Specified Outdoor Recreational Uses.

Because of their unique requirements for nighttime visibility and their limited hours of operation, lighting for ball diamonds, playing fields, and tennis courts is exempted from the exterior lighting standards of Subsection (4) below. Exterior lighting for outdoor recreational uses must meet all other applicable requirements of this section and of this code.

(3) Maximum Lighting Height for Specified Outdoor Recreational Uses.

Notwithstanding height limitations elsewhere in this code, exterior lighting for the outdoor recreational uses specified in (2) above shall be permitted a maximum post height of ninety (90) feet. When a Conditional Use Permit is required for the specified outdoor recreational uses, additional height limitations may be imposed to meet approval criteria.

(4) Additional Regulations. Notwithstanding any other provision of this section to the contrary:

(a) No flickering or flashing lights shall be permitted.

(b) Light sources or luminaires shall not be located within bufferyard areas except on pedestrian walkways.

(5) Measurement: When required, the measurement of lighting levels shall be conducted by the developer and certified by a licensed engineer that the measurements have been conducted as per the following:

(a) Metering Equipment. Lighting levels shall be measured in footcandles with a direct-reading, portable light meter. The meter shall have a color and cosine-corrected sensor with multiple scales and shall read within an accuracy of plus or minus five (5) percent. It shall have been tested, calibrated, and certified by an independent commercial photometric laboratory or the manufacturer within one (1) year of the date of its use.

(b) Method of Measurement. The meter sensor shall be mounted not more than six (6) inches above ground level in a horizontal position. Readings shall be taken by qualified personnel only after the cell has been exposed long enough to provide a constant reading. Measurements shall be made after dark with the light sources in question on, then with the same sources off. The difference between the two readings shall be compared to the maximum permitted illumination and property line at ground level. This procedure eliminates the effects of moonlight and other ambient light.



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(5) Exterior Lighting Plan. At the time any exterior lighting is installed or substantially modified an exterior lighting plan shall be submitted in order to determine whether the requirements of this section have been met. The lighting plan shall identify:

- (a) Location of light fixtures
- (b) Type of luminaire
- (c) Height of luminaire
- (d) Maximum illumination
- (e) Cut-off angle

10.767 Oxidizing Materials

The manufacture of oxidizing materials is prohibited. The storage or utilization of oxidizing materials in excess of 500 pounds is prohibited.

10.768 Flammable Gases

The storage, utilization or manufacture of flammable gases shall not exceed 250,000 standard cubic feet, exclusive of finished products in sealed portable containers.

Compliance with Standards: Applicant's Exhibit 4 includes a Lighting Plan with details and specifications on all proposed lighting fixtures consistent with this provision. Applicant's proposal does not include the manufacture of oxidizing materials. The site can and will comply with the flammable gases provisions of MLDC 10.768 and herewith agrees to stipulate to the same.

10.772 Pedestrian Walkway General Provisions

These walkway provisions are intended to provide safe, direct and convenient pedestrian access in new office and commercial developments.

Internal pedestrian circulation shall be provided in new office and commercial developments through clustering of buildings, construction of pedestrian ways, skywalks, where appropriate, and similar techniques.

10.773 Pedestrian Walkway Connections and Routing

A pedestrian walkway shall be provided:

- (1) To each street abutting the property, and for every 300 feet of street frontage not including limited access freeways.
- (2) To connect with walkways, sidewalks, bikepaths, and other bicycle or pedestrian connections on adjacent properties.
- (3) To connect building entrances to one another, to existing or planned transit stops and to connect the pedestrian circulation system to other areas of the site such as parking lots, children's play areas, required outdoor areas and any pedestrian amenities such as plazas, resting areas and viewpoints.

Compliance with Standards: Walkways on the proposed site plan meet the above standards. See, Applicant's Exhibit 4.



~B''
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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

10.780 Landscape and Irrigation Requirements

B. General Provisions.

(1) Landscaping shall provide a variety of plant sizes, shapes, texture and color while being horticulturally compatible and minimizing irrigation reliance, thus conserving the public water supply.

(2) Landscaping shall be maintained in good health by the property owner in conformance with approved landscaping plans and shall not be reduced in area or number.

E. Landscape Plan and Irrigation Plan Review.

All development and building permit applications that include landscaping areas, as defined herein, with the exception of development types provided within Section 10.780 C.(1)a., shall include a Landscape Plan and Irrigation Plan in accordance with the provisions of this section. Landscaping and irrigation improvements shall be installed in accordance with approved plans.

Compliance with Standards: Applicant's detailed landscape plan (Exhibit 5) demonstrates consistency with MLDC 10.780.

* * * * *

10.781 Concealment of Trash Receptacles

Trash receptacles shall be kept in enclosures made of solid wood, metal, brick, or masonry block which conceal them from view. Access doors shall be made of solid wood or metal. When possible, enclosures should be incorporated into the building (i.e., wing walls, alcoves). If free standing, the enclosures shall be constructed of materials which are compatible with other structures on the site. Chain link fencing, with or without slats, for this purpose is prohibited. [Added, Sec. 2, Ord. No. 1999-211, Dec. 16, 1999.]

Compliance with Standards: Applicant's site plan depicts the free standing trash enclosure consistent with this standard.

10.782 Concealment of Heating, Ventilation, Air Conditioning (HVAC) Equipment and Roof-Mounted Wireless Communication Facilities

All HVAC equipment and roof-mounted wireless communication facilities shall be concealed from view. Where possible, such concealment should be accomplished using the architectural elements of the buildings (i.e., roof forms, parapets, wing walls, alcoves, etc.). Free standing walls or fences may also serve as sight-obscuring concealment devices. Chain link fencing, with or without slats, for this purpose is prohibited.

10.783 Location of Wall Mounted Utility Services

Wall mounted utility devices, such as meters and television cable boxes, shall be mounted on the side of the building not facing a street unless they are concealed from view. Where possible, concealment should be accomplished using architectural elements of the building (i.e., wing walls, alcoves, etc.).



~ 8"
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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

Compliance with Standards: The proposed trash receptacles, HVAC equipment, and Roof-Mounted Wireless Communication Facilities and all wall-mounted utility services can and will comply with this standard.

10.790 Bufferyards

A. Purpose.

Bufferyards are utilized in order to minimize potential conflicts caused by in types and intensity of uses on adjacent properties. Factors to be mitigated include nuisances, such as visual impacts of buildings or parking areas, glare, views from upper story windows, dirt, litter, noise and signs.

B. Location.

Bufferyards shall be located along property lines which define the boundary between one zoning district and another, or along the boundary between a zoning district and a General Land Use Plan Map (GLUP) designation where there is not yet city zoning. The specific location of the bufferyard, relative to the property line, is governed by Subsections C-E. Bufferyards are not required along any portion of a public right-of-way or private street.

Subject Site Zoning	Zoning on Abutting Land to South and South Half of East Line
	SFR
C-C	A

E. Bufferyard Standards.

(1) This Subsection provides the width of the bufferyard, type of fence or wall required, and the required planting scheme to provide effective screening between adjacent properties having dissimilar land use. For an administratively approved bufferyard, the Standard Planting Scheme as required by 10.790 (E)(1)(b¹) shall be used unless the applicant wishes to submit a Site Plan and Architectural Review application to have the Commission consider modifying the requirement.

(a) **Planting Scheme:** In addition to compliance with other landscaping provisions in this chapter, bufferyards shall include a variety of plant sizes and shapes and provide effective visual screening between the adjacent properties having dissimilar land uses. The bufferyard shall be planted with trees and shrubs of the appropriate size, shape and spacing to provide a continuous canopy between the top of the fence/wall and a height of 20 feet within ten (10) years. A minimum of 60 percent of the trees used to provide visual screening shall be non-deciduous species. The planting plan shall take into account the nature of the impacts specific to the two sites, particularly building height and locations of windows and lighting.

Table 10.790-3. Bufferyard Types

Type	Width	Wall / Fence
------	-------	--------------

¹ This reference is likely intended to read 10.790(E)(1)(a) titled "Planting Scheme", for there is no subsection (b).



"B"
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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

A	10 feet	Six (6) foot concrete or masonry wall.
---	---------	--

- (2) The fence or wall shall typically be placed on the property line between the two uses; however, the approving authority may authorize its location anywhere within the bufferyard. Fences and walls shall be constructed of a material and design that is sight-obstructing, compatible with adjacent uses, and accepted by the approving authority.
- (3) Any part of the bufferyard may be located on the adjoining property provided it is planted with a proportionate share of the required plants and, for any part located outside of the standard setback, a perpetual bufferyard easement is recorded by the property owner. The easement shall allow for the installation and perpetual maintenance of the bufferyard and restrict use of the area to only the bufferyard.
- (4) Encroachments into bufferyards: The bufferyard is intended to provide a minimum amount of space for the required plants to grow and for aesthetic separation between uses. Therefore, this area shall be reserved exclusively for such use. Encroachment of driveways, parking and maneuvering areas, sidewalks, patios, or structures (other than the required fence or wall) are prohibited in the bufferyard area.
- (5) Bufferyard credits: Existing plant materials within the bufferyard area may be counted toward the bufferyard requirement.
- (6) Adjustments to bufferyards: The approving authority shall have the discretion to make adjustments to the bufferyard requirements if an unusual circumstance exists and a finding is made that adequate buffering will be provided to avoid significant adverse impacts to the livability or value of the adjoining properties. Adjustments shall not be made simply for the convenience of site design. Adjustments to the bufferyard requirements may include, but are not limited to, the following:
 - (a) Where a building wall with no openings below eight (8) feet abuts the bufferyard, the building wall may be counted in place of a required wall or fence.
 - (b) Where there is existing development on the site, such as paving or a building, which affects or precludes implementation of the bufferyard standard.
 - (c) Where a proposed project abuts existing development, and the adjacent uses are the same (i.e., apartment parking lot adjacent to commercial parking lot) or are sufficiently compatible that the full buffering, otherwise required, is not necessary and the uses are not expected to change significantly over time.
 - (d) Where a project abuts an irrigation canal, natural waterway, railroad right-of-way, or other such element.

Compliance with Standards: Applicant's site plan (Exhibit 4) and landscape plan (Exhibit 5) illustrate the proposed buffering on the south and southeast portions of the property, where the property is adjacent to residential zones and development. Applicant seeks a minor adjustment to the bufferyard provisions as allowed under subsection 6 herein, requesting that the proposed separation of improvements, abundance of proposed vegetation and existing 6 foot-fence be considered a sufficient buffer. Applicant proposes that the unique situation is that the layout of the proposed project is such that there will be a 168-foot separation between the proposed building and the rear property line. Of which, some 68 feet will be comprised of vegetation including the detention swale proposed to be heavily landscaped. Considering the significant separation of proposed improvements, the extent of proposed vegetation and the existence of a 6-foot tall sight-obscuring fence in good condition, Applicant proposes the fire station and adjacent residential development will be sufficiently compatible that the installation of an additional wall is not necessary. The proposed fire station is not expected to change significantly over time.



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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

A. This section establishes the minimum landscaping requirements along all street frontages outside of the public right-of-way. Plans submitted to comply with this section shall be approved by the approving authority.

(1) For land divisions with houses that do not face an arterial street, an arterial street frontage landscape plan shall be submitted showing a vertical separation feature that is a minimum of eight (8) feet in height. The separation feature shall create a solid visual screen. A fence or wall shall be engineered to stand straight. The separation feature shall be reduced in height where otherwise required in a front or side yard or clear vision triangle. The Planning Commission may allow adjustments to the above requirement in response to topography.

(2) For all other street frontages the number of plants required for distances above or below one hundred (100) feet shall be prorated with the resulting numbers of plants rounded so that one-half or more shall be deemed to require a full plant. All required planting shall be located in the required yard area adjacent to the street unless otherwise approved by the approving authority.

(3) Existing plant materials which meet the requirements of this code may be counted as contributing to the total landscaping required by this section. Interstate 5 and other highway frontages shall be treated the same as city street frontage.

(4) The following table specifies the type and number of plants required by this section.

Table 797-1. Frontage Landscaping - Materials and Quantities

Zoning District	Number of plants required per 100 feet of street frontage less driveways	
	Trees	Shrubs
SFR (Nonresidential uses), MFR, C-N, C-S/P, C-C, C-R, C-H	4	25
I-L, I-G, I-H	3	15

10.798 General Exceptions to Yard Requirements

The following projections shall be permitted within the required yard area:

(1) Cornices, eaves, fireplaces, canopies, mechanical (heating and cooling equipment), or other similar architectural features may extend a maximum of one (1) foot into a required yard.

(2) Open uncovered accessory structures such as fire escapes, patios, porches, balconies, or outside stairways may extend a maximum of one (1) foot into the required side and rear yard and a maximum of six feet into a required front yard. Porches, decks or stoops which are open and uncovered and not exceeding 18 inches in height may be located within 18 inches of any lot line.

(3) Within the commercial districts awnings shall be allowed to extend a maximum of six (6) feet into the required front yard.

(4) Established Neighborhoods: On a parcel where the abutting lots, adjoining the same street, contain legally constructed buildings whose setbacks are equal to or less than that required by the underlying zone, the front yard setback may be reduced to a distance equal to the average setback of the abutting parcels. If one of the adjoining lots is vacant, the minimum setback of the underlying zone shall be utilized to determine the average.

Compliance with Standards: See, Applicant's Exhibit 5 landscape plan demonstrating conformance with the standards provided herein above.

10.808 New Commercial and Institutional Development



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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

All new commercial, office and institutional buildings on parcels within 600 feet of an existing or planned major transit stop, as designated by the City of Medford Transportation System Plan (TSP), shall provide the following:

- (1) Building entrances. All such uses shall provide a main entrance on the facade of a building nearest to and facing a street with a designated "transit route", hereinafter "transit street". A building may have more than one main entrance. If the lot has frontage on more than one transit street, the building need only have one main entrance oriented to a transit street or to the corner where two transit streets intersect.
- (2) Setbacks. Buildings shall be set back no more than 20 feet from the designated transit stop or transit street. Where the site is adjacent to more than one designated transit street, a building is required to meet the maximum setback standard on only one of the streets.
- (3) Parking lots. No automobile parking shall be permitted between the building and the designated transit street.
- (4) An exception to one or more of the requirements in this section may be granted if the approving authority determines that:
 - (a) Strict compliance with the standard is not possible because of terrain or other physical conditions beyond the control of the applicant, or,
 - (b) Direct, convenient and safe access to transit is otherwise adequately provided for by other measures.

Compliance with Standards: The nearest RVTD stop is across the street, at approximately 350 feet from the subject property. That stop is bus stop number 310 on Route 2 that goes from east to west. Said stop is not a 'major' stop as defined by the Medford TSP (See, Medford TSP page 4-12) and there are no other stops within 600 feet. Therefore, this provision is met by manner of not being relevant.

10.809 Vanpool and Carpool Preferential Parking Requirements

All new industrial, commercial and institutional development shall provide preferential parking for vanpools and carpools as follows:

- (1) Number: Industrial, commercial, institutional and office developments shall designate at least 10% of the employee parking spaces for vanpool or carpool parking.
- (2) Marking: The vanpool/carpool spaces shall be clearly marked "Reserved - Vanpool/Carpool Only".
- (3) Location: Designated vanpool/carpool spaces shall be the closest employee parking spaces to the building entrance normally used by employees except for any accessible spaces provided.

Compliance with Standards: Applicant's site plan provides a dedicated car pool space in conformance with these provisions.

10 Article 6 – Signage

10.1030 Application



1'-0"
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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

Application for a permit shall be made to the Planning Department upon a form provided by the City and shall include the following:

- (1) Two sets of drawings to scale, including the following information with regard to the signs:
 - (a) The written or graphic content.
 - (b) Location of the sign on the building or building site.
 - (c) Dimensions of the sign.
 - (d) Construction materials.
 - (e) Method of attachment and character of structural members to which attachment is to be made.
 - (f) Electrical wiring and components or U.L. approved number.
 - (g) If an electronic message sign, the frequency at which the text changes.
- (2) The Building Safety Official may also require that a licensed engineer furnish information concerning structural design and proposed attachments.

10.1700 Signs in Community Commercial District (C-C) and Heavy Commercial District (C-H, and Regional Commercial District (C-R): Basic Regulations

Signs shall be permitted as follows in the C-C, C-R, and C-H districts:

- (1) Ground Signs: Each parcel of land is permitted one ground sign per street frontage, subject to the following limitations:
 - (a) Maximum Height: 20 feet
 - (b) Maximum Square Footage: 150 square feet per sign
 - (c) Minimum Setback: Shall not project into public right-of-way.
 - (d) Electronic Message Signs are permitted as a ground sign subject to the following limitations:
 - (i) Each parcel of land is permitted one (1) electronic message sign if the sign is 150 feet or farther from any residential zoning district or GLUP Map designation.
 - (ii) All text displayed on an electronic message sign must be static for a minimum of two (2) seconds. The continuous scrolling of text is prohibited. This restriction shall not apply to animated images and images which move, or give the appearance of movement.
 - (iii) All electronic message signs shall have automatic dimming capabilities that adjust the brightness to the ambient light at all times of day and night.
 - (iv) The conversion of an existing, conforming ground sign to an electronic message sign is permitted.
 - (v) The conversion of an existing, nonconforming ground sign to an electronic message sign is prohibited.



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APPLICANT'S EXHIBIT 2

Demonstration of Compliance with Applicable Development Standards

Site Plan and Architectural Review Fire Station 2 – Steward Avenue

Applicant: City of Medford

(2) Wall Signs: Wall signs are permitted, subject to the following limitations:

(a) Principal Facade: The aggregate area of all signs shall not exceed one and one-half square feet for each linear foot of business frontage, except if the building is set back more than 20 feet from the right-of-way, in which case the aggregate area of all signs shall not exceed two square feet for each linear foot of business frontage. No part of any sign shall be higher than the building height as defined in Section 10.705.

(b) Secondary Facade: The aggregate area of all signs shall be limited in area to two square feet for each linear foot of business frontage and shall be placed flat against the building supporting the sign. No part of any sign shall be higher than the building height as defined in Section 10.705.

(c) Electronic Message Signs are permitted as a primary or secondary facade wall sign subject to the following limitations:

(i) The electronic message sign must be 150 feet, or farther, from any residential zoning district or GLUP Map designation.

(ii) All text displayed on an electronic message sign must be static for a minimum of two (2) seconds. The continuous scrolling of text is prohibited. This restriction shall not apply to animated images and images which move, or give the appearance of movement.

(iii) All electronic message signs shall have automatic dimming capabilities that adjust the brightness to the ambient light at all times of day and night.

(iv) The conversion of an existing, conforming wall sign to an electronic message sign is permitted.

(v) The conversion of an existing, nonconforming wall sign to an electronic message sign is prohibited.

(3) Projecting Signs: No sign shall project more than 18 inches into the public right-of-way except under the following conditions:

(a) The building is set back less than two feet from public right-of-way, and

(b) The sign is less than 25 square feet in area.

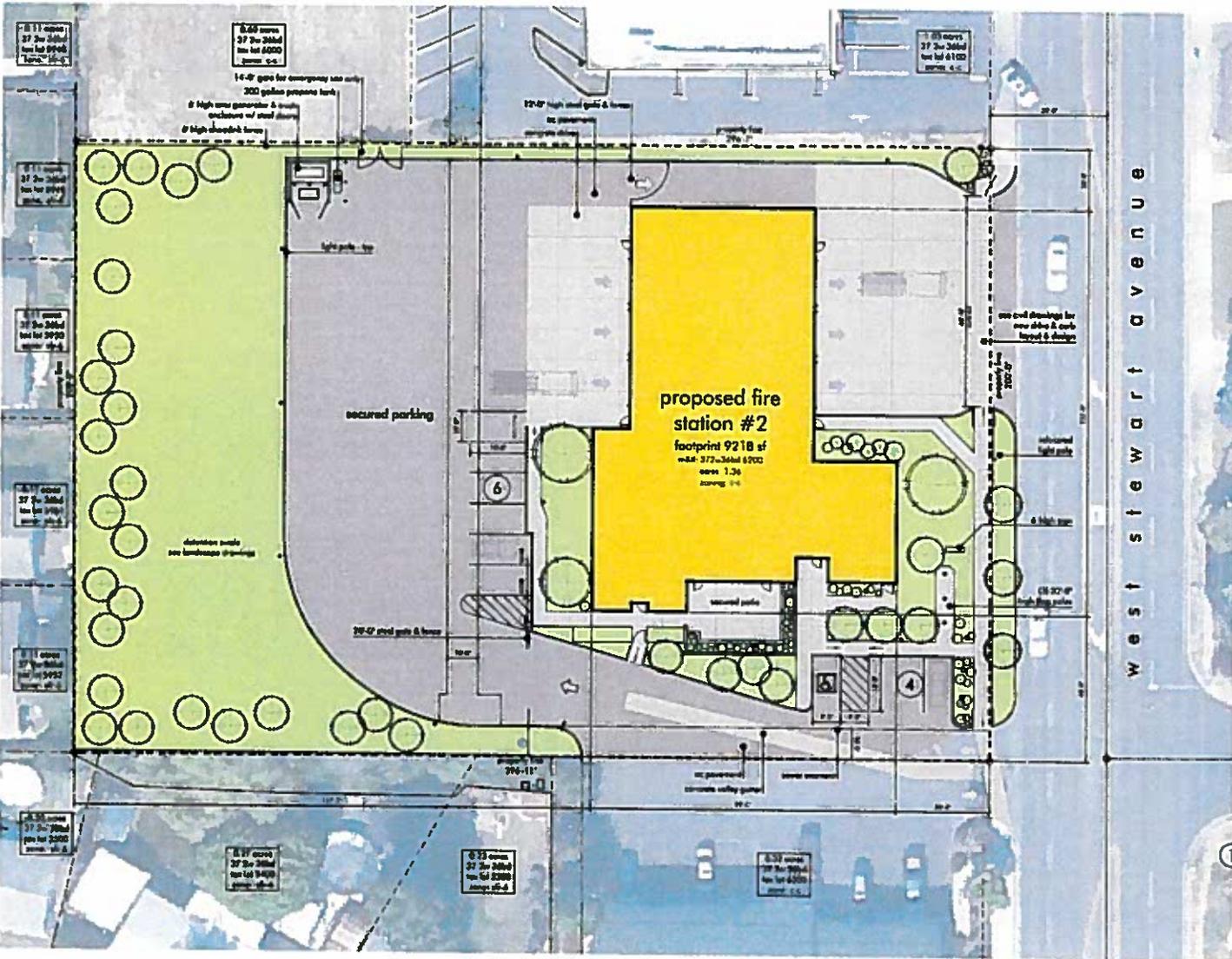
(c) No part of any sign shall be higher than the building height as defined in section 10.705.

(4) Awning/Canopy/Marquee Signs: All such signs shall be assessed against the aggregate wall signage for the facade.

Compliance with Standards: Applicant will adhere to all sign permit standards and permit application requirements. As evidenced by Applicant's Site Plan, the proposed free-standing sign will be 6 feet tall with a sign surface area of no greater than 150 square feet.



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project summary:
see site story for details for City of Medford fire department.

statistics:

- map & fee lot # 372+ 346d 4300
- site area 1.36 acres
- zoning R-4
- area by (sq)

building area calculations

- total area of 9,218 sf
- total of 9,218 sf

setbacks

- front yard 10'
- side yards (2) per lot on building height above 20' 1'-0"
- back yard (2) per lot on building height above 20' 1'-0"

materials
brick masonry exterior, ground floor area, aluminum storefront doors & windows with high performance glazing

building height

- maximum 25'-0"
- proposed 22'-0"

parking (10.743)

- public services 7 employees (single shift)
- maximum parking (1) 0 spaces per 1 sq ft of area 0
- minimum parking (1) 0 spaces per 1 sq ft of area 0
- accessible parking spaces (1) required, 1 provided

total parking for

- provided 0
- site 0
- required 0

general parking for

- provided 0
- site 0
- required 0

total parking 11

legend

- landscape, parking
- landscape
- asphalt pavement
- concrete sidewalk

general notes
• in coordination with city of medford planning, design committee with street department, engineering, and code options per CRS 455 447 & 455 010-455 230

1 site plan
scale: 1" = 30'-0"

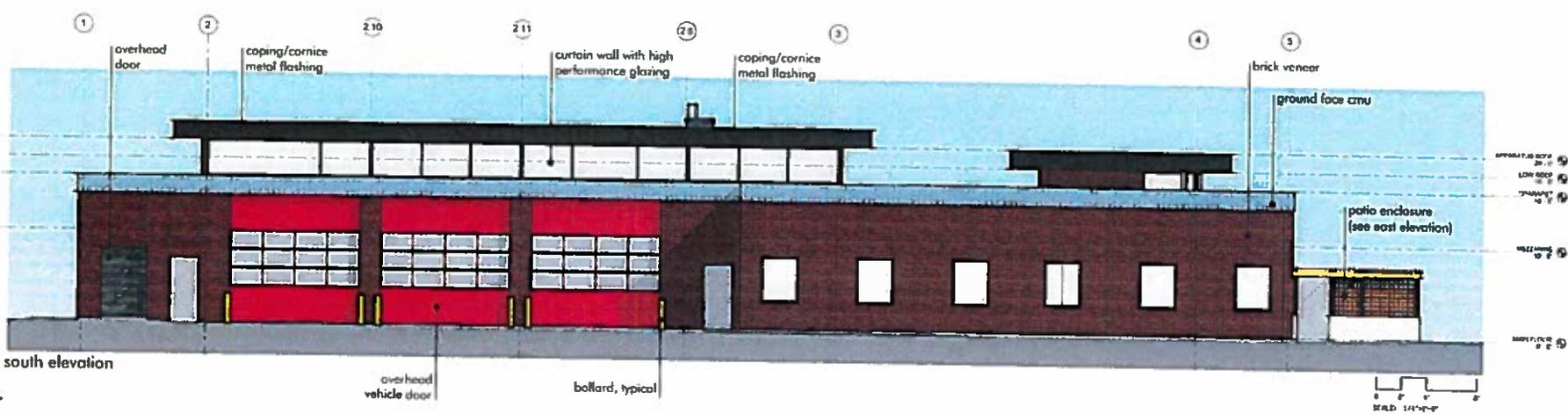
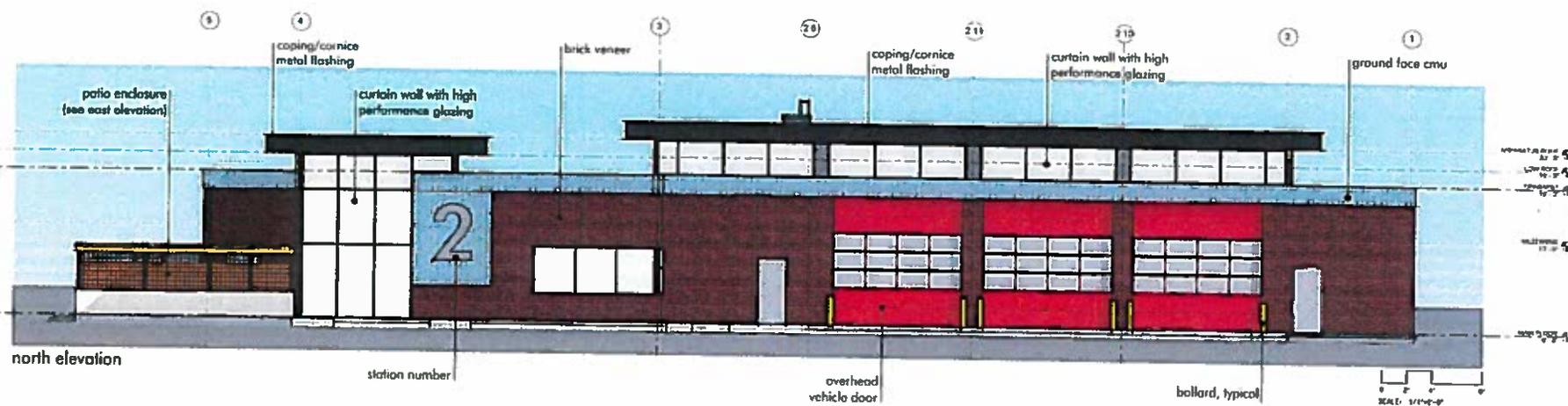


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city of medford fire station #2
1107 Stewart Avenue Medford, Oregon 97501
19 January 2015



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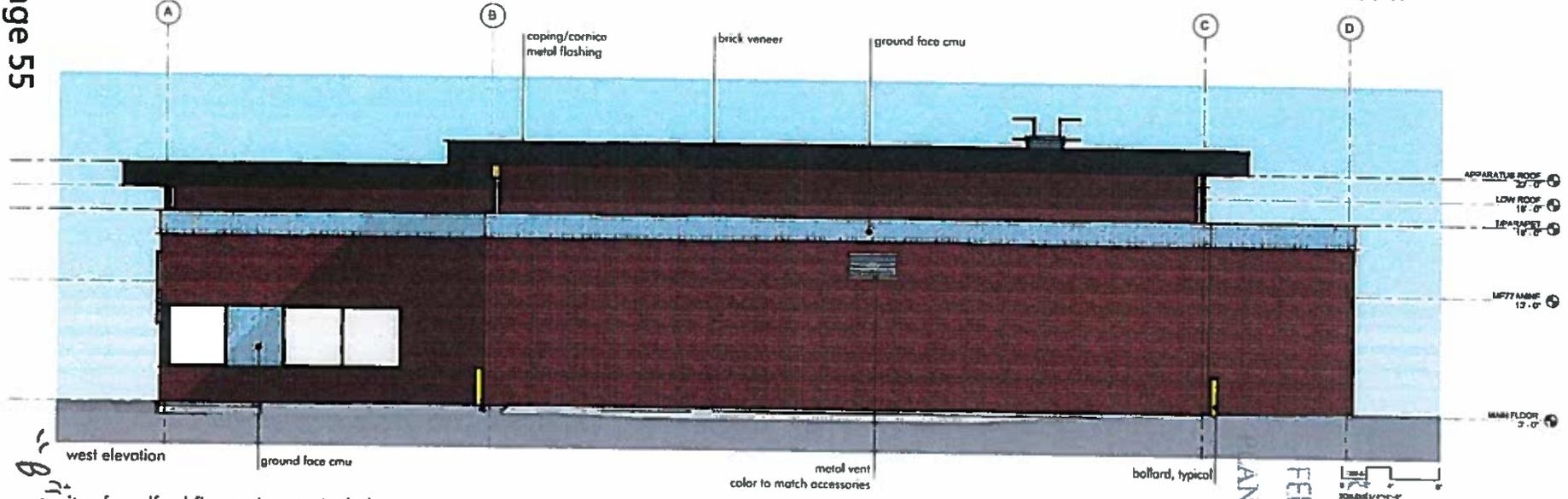
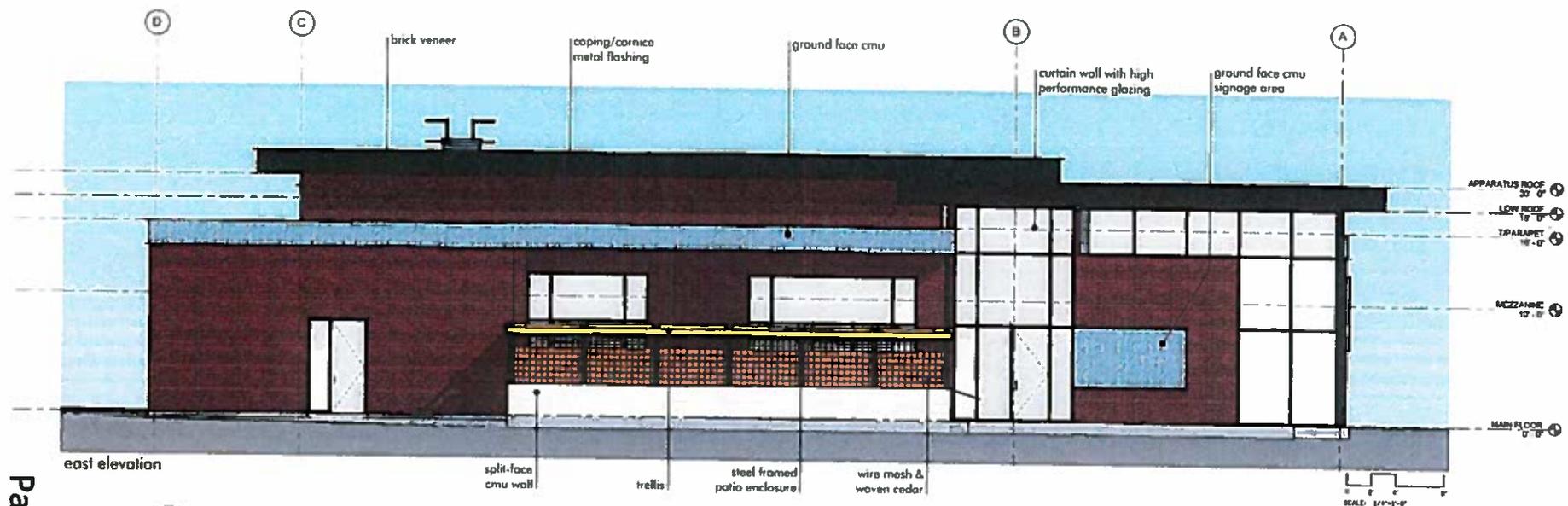
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city of medford fire station, typical elevations

30 january 2015

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city of medford fire station, typical elevations

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30 JANUARY 2015

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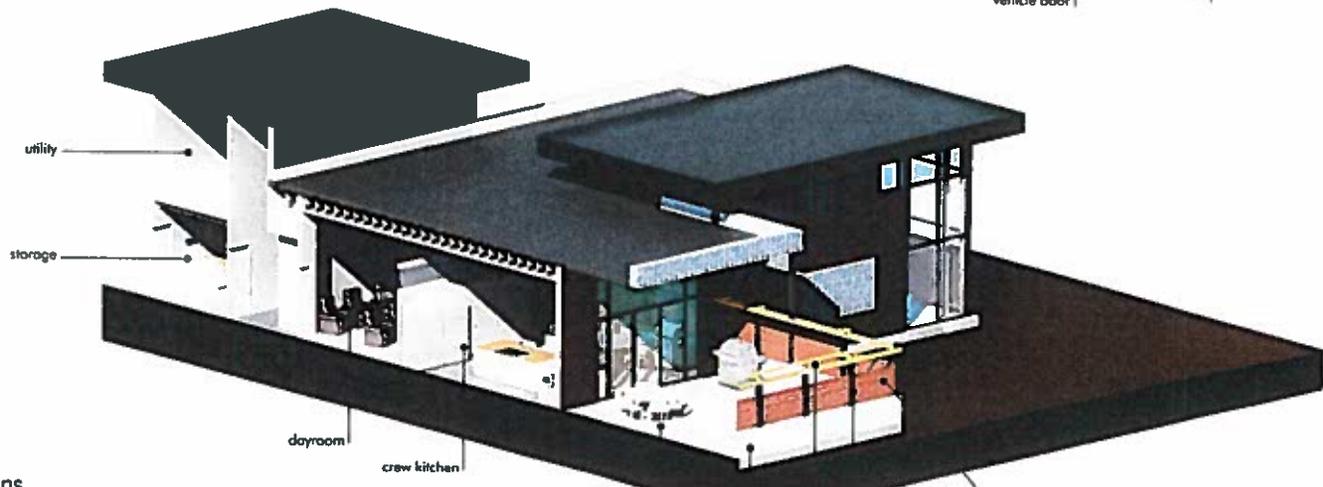


view from street, north-east

station number

curtain wall with high performance glazing
 coping/cornice metal flashing
 ground face block
 brick veneer

overhead vehicle door
 bollard, typical



utility
 storage

dayroom
 crew kitchen

view of secure patio & public entrance

secure patio
 split-face CMU wall
 trellis
 steel framed patio enclosure
 wire mesh & woven cedar

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city of medford fire station, typical elevations

30 January 2015



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MEDFORD FIRE DEPARTMENT
MEDFORD, OR 97501

Medford Fire Station 2
Hessert Avenue
Medford, OR 97501

M
Architecture • Interiors
Planning • Engineering

Medford, OR 97501
www.mackenzie.com

MACKENZIE

Professional Services
333 SE Second Ave., Suite 101
Medford, OR 97504

NOTES:
1. ALL NOTES APPLY TO THIS SET OF DRAWINGS UNLESS OTHERWISE NOTED.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY UTILITIES INFORMATION.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY SURVEY DATA.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY RECORD DRAWINGS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY RECORD DRAWINGS.
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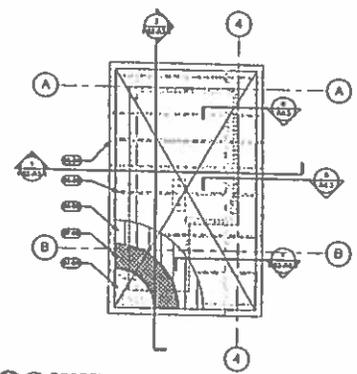
STATION 02
ROOF PLAN

DESIGNED BY GFC
CHECKED BY JAF
DATE

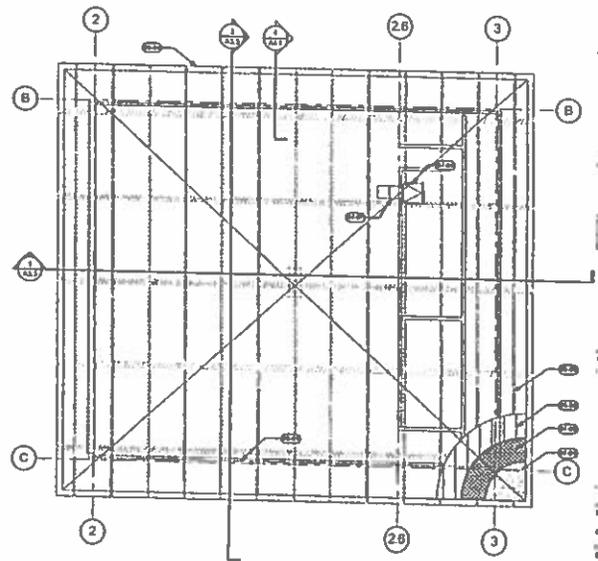
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30% CONSTRUCTION SET: 10/08/2014



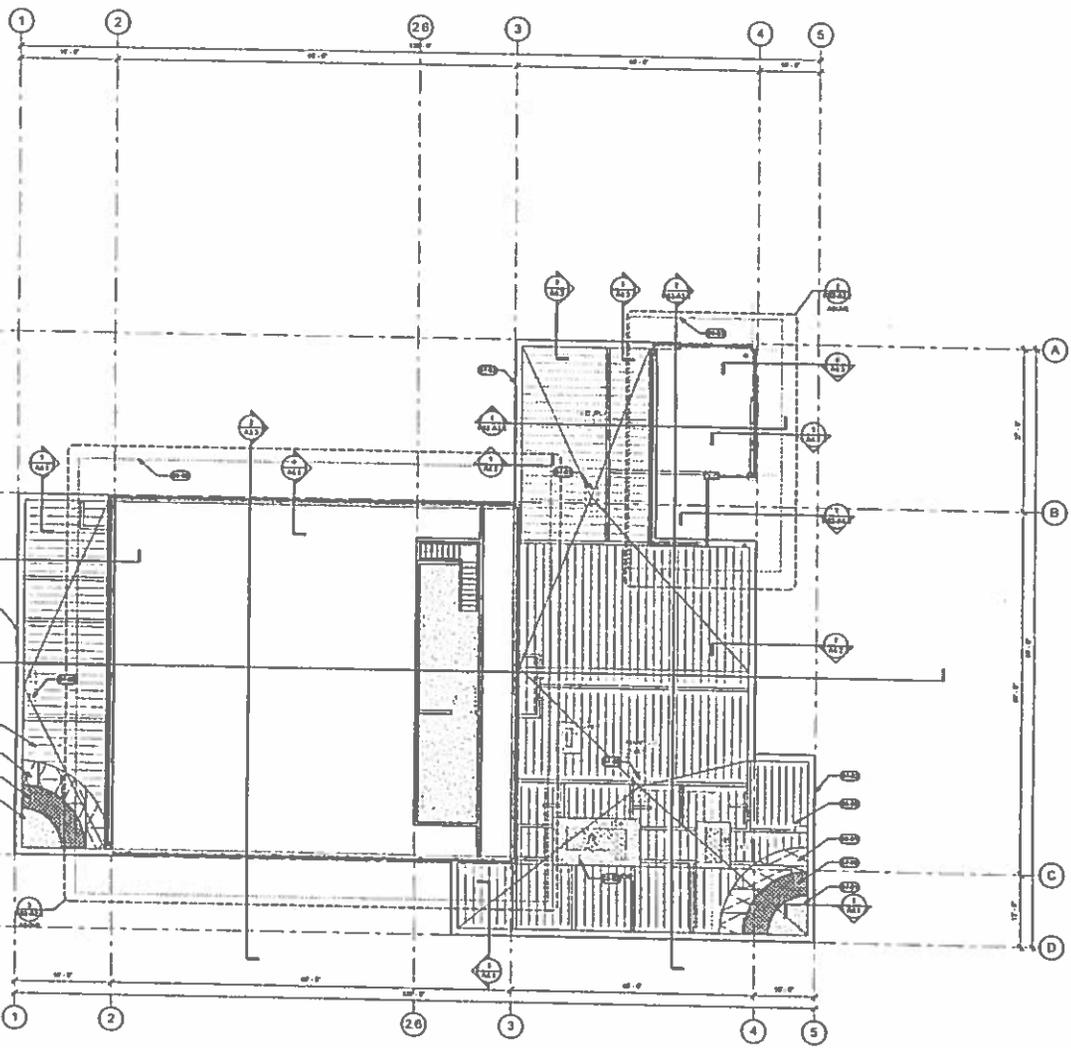
ROOF PLAN
1/4" = 1'-0"



ROOF PLAN (DEORD)
1/4" = 1'-0"

EXHIBIT #

File #



ROOF PLAN
1/4" = 1'-0"

LEGEND

- ARROW - INDICATES DIRECTION OF FLASH TURNED ROOF SHEET
- --- --- - INDICATES LINE OF SLOPE
- --- --- - CONCRETE CURB UP INSULATION AREA
- --- --- - FINISH
- --- --- - UNFINISH TO FACE OF STRUCTURALLY INSULATED PANEL JOINT

GENERAL NOTES

- A. STRUCTURAL BEARING @ DESIGN FOR REPEATED ONLY - CONCRETE
- B. EXACT LOCATION OF ROOFTOP AIR CONDITIONER UNITS
- C. FOR PIPE PENETRATION SEE MECHANICAL DRAWINGS AND NOTES, AND 1 DETAIL 204.1
- D. REFER TO SECTION 1401 FOR FINISH SCHEDULES
- E. PLACE CURBETS AT UP SLOPE END OF ALL ROOFTOP EQUIPMENT ROOFS TO DRAIN, ETC. AND ENSURE DRAINAGE - VENTURE UP AND FROM INSULATION SLOPE TO ENSURE PROPER DRAINAGE AT ALL PORTIONS OF ROOF

KEY NOTES

- 01-00 - CLASH/STAY STRAP/STAY SYSTEM BELOW
- 02-00 - LINE OF ROOF SLOPE
- 03-00 - CONCRETE MECHANICAL SLAB - SEE STRUCTURAL
- 04-00 - GUTTER DETAIL AS SHOWN
- 05-00 - ROOF FINISHING - SEE STRUCTURAL
- 06-00 - 1" WF METAL DECK - SEE STRUCTURAL
- 07-00 - 1/2" INSULATION - SEE STRUCTURAL
- 08-00 - INSULATE ALL ROOFS AND SYSTEMS
- 09-00 - FINISH TO FACE OF STRUCTURALLY INSULATED PANEL JOINT
- 10-00 - CONCRETE CURB UP INSULATION
- 11-00 - CONCRETE CURB UP INSULATION
- 12-00 - CONCRETE CURB UP INSULATION
- 13-00 - CONCRETE CURB UP INSULATION
- 14-00 - CONCRETE CURB UP INSULATION
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- 19-00 - CONCRETE CURB UP INSULATION
- 20-00 - CONCRETE CURB UP INSULATION

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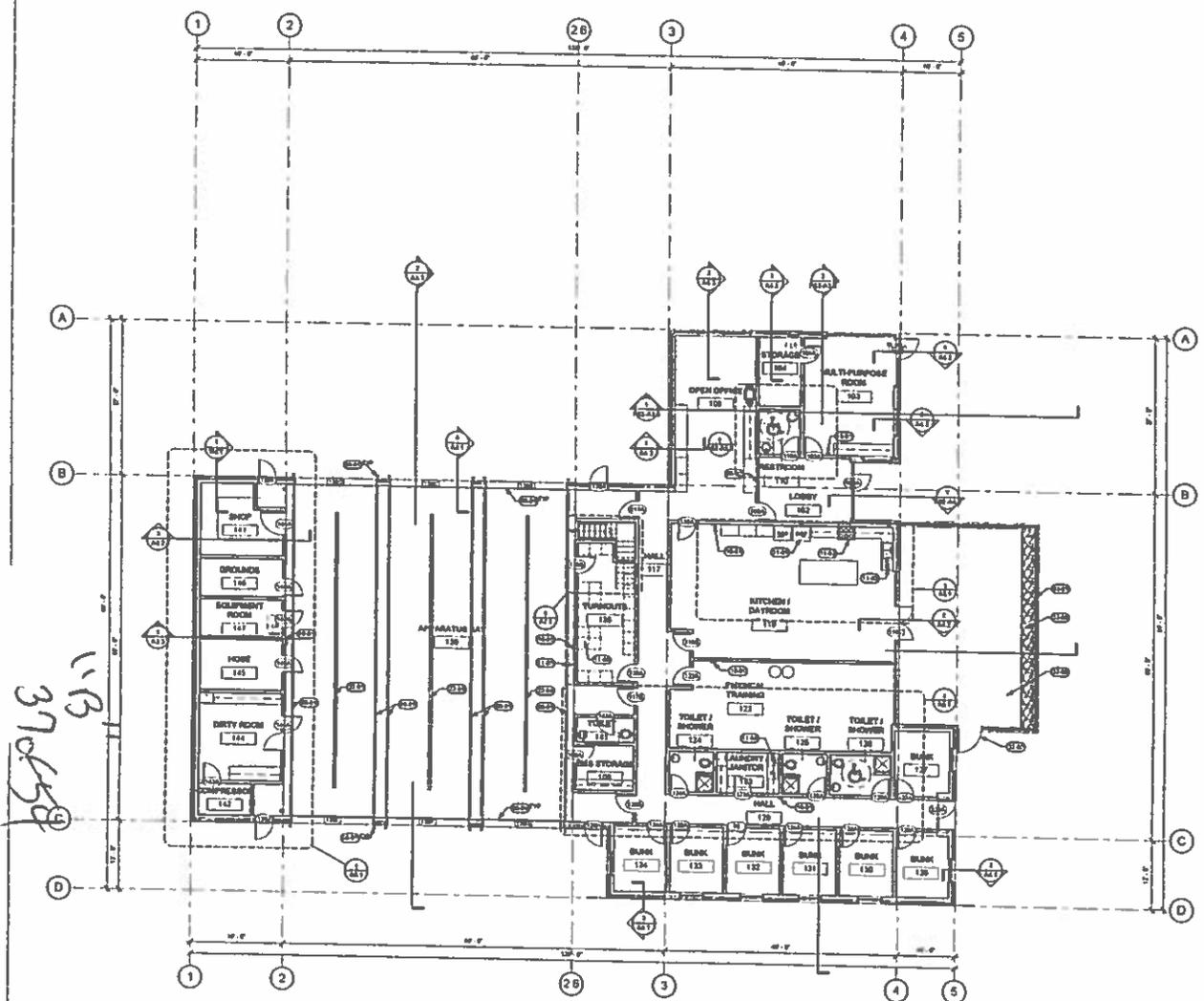
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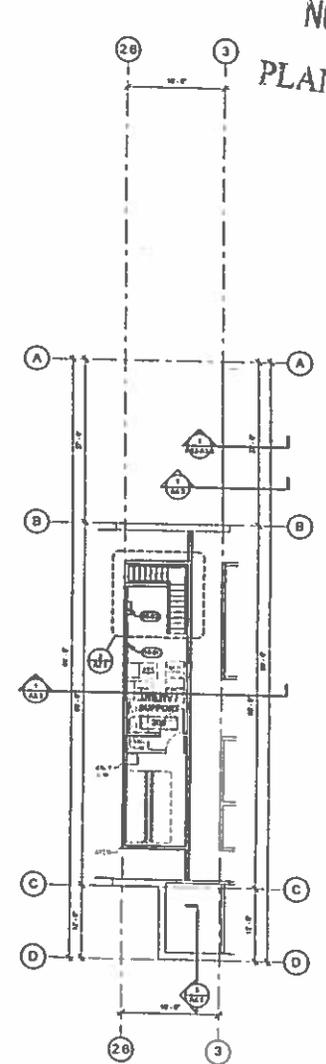
FS2-A2.1 FIRST FLOOR PLAN
10/1/14

- LEGEND**
- TYPICAL OVERHEAD DUCT - SEE 11.6 FOR ADDITIONAL INFORMATION
 - TYPICAL EXTERIOR WALL - MASONRY MASONRY, SEE REVISIONS, SECTIONS AND 11.6
 - STRUCTURAL BEAM WALL/LOADING WALL - SEE STRUCTURAL AND WALL TYPES ON SHEET 11.6
 - FLOOR BEAM - SEE PLUMBING
 - FIRE EXTINGUISHER LOCATION - SEE 11.1.1
 - CEILING JOINT
 - NETWORK
 - MOLDING
 - BRASS PLATE (SEE 11.6.1)
 - NEW CONSTRUCTION JOINT

- GENERAL NOTES**
1. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR EXTERIOR MASONRY
 2. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
 3. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
 4. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
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 10. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
 11. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
 12. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
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 19. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY
 20. SEE 11.6 FOR TYPICAL AND ALTERNATE SCHEDULES FOR INTERIOR MASONRY

- KEY NOTES**
1001. 2" P-8 REINFORCED CONCRETE SECURITY WALL IN RETAIN SCREEN WALL TO 2'-0"
 1002. PAINTED STEEL DOOR/LAND
 1003. VERTICAL ROOF ACCESS LADDER - SEE 11.6 FOR SCHEDULE
 1004. INTERIOR SECTIONAL OVERHEAD BEAM - SEE BEAM SCHEDULES
 1005. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1006. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1007. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1008. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1009. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1010. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1011. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1012. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1013. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1014. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1015. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1016. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1017. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1018. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1019. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM
 1020. 2" P-8 1'-0" DIA. HOLLOW CORED CONCRETE BEAM

MEZZANINE FLOOR PLAN
10/1/14



MEZZANINE FLOOR PLAN
10/1/14

CITY OF MEDFORD
EXHIBIT # B
File # 37

MEDFORD FIRE DEPARTMENT
1400 W. 10TH ST.
MEDFORD, OR 97504

Medford Fire Station 2
1400 W. 10TH ST.
MEDFORD, OR 97504

M.
Architecture • Interiors
Planning • Engineering

Architect: Dan Mackenzie
Interior Designer: Dan Mackenzie
Planner: Dan Mackenzie
Engineer: Dan Mackenzie

MACKENZIE.
ARCHITECTURE • INTERIORS
PLANNING • ENGINEERING

1400 W. 10TH ST.
MEDFORD, OR 97504
503.754.1100
www.mackenzie.com

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Revision Schedule
Revision # | Description | Date

STATION 02
FIRST FLOOR
AND
MEZZANINE
PLAN

DESIGNED BY: LSC
CHECKED BY: LSC
DATE:

FS2-A2.1

30% CONSTRUCTION SET: 10/08/2014

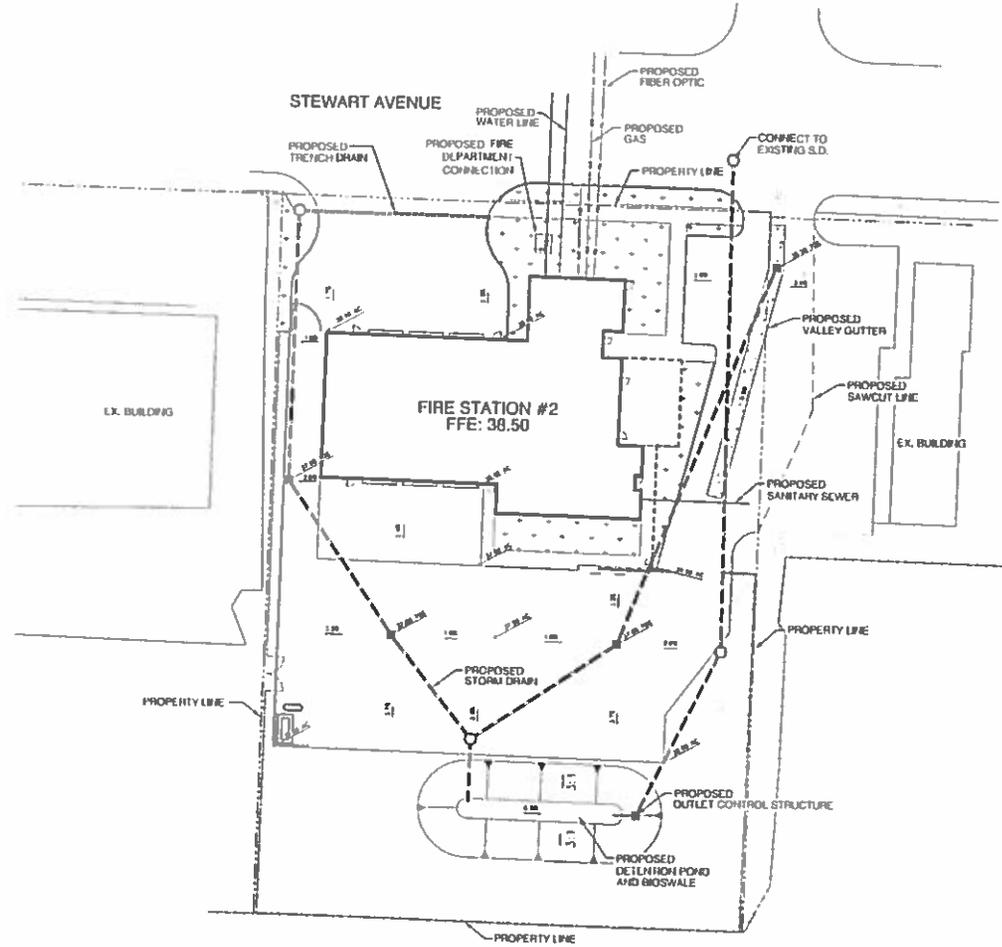
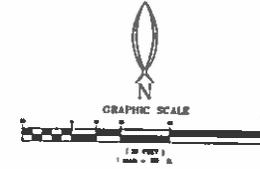
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NOV 25 2014

PLANNING DEPT.



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Medford Fire Department
Medford, OR 97504

FEA
Harley Engineering & Associates, Inc.
P.O. Box 1422
Medford, OR 97501
Phone: 541-771-6888
Fax: 541-771-0511
Email: info@fea-inc.com

STANDARD SPECIFICATIONS
REPORT
SHEET NO.

SPAC EXHIBIT
DRAWN BY: JH
CHECKED BY: JH
SHEET

CITY OF MEDFORD
EXHIBIT # _____
File # AR-14-129
11/3

JOB NO 214033.00

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NOV 25 2014

CSX2 LED LED Area Luminaire

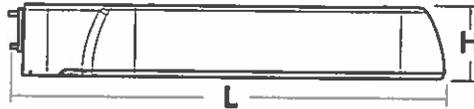


Catalog Number	
Notes	Medford Fire - Rescue
Type	SD - Option

PLANNING DEPT

Specifications

EPA:	1.2 ft ² (0.11 m ²)
Length:	35-3/8" (89.7 cm)
Width:	18-1/2" (46.9 cm)
Height:	5-7/8" (14.9 cm)
Weight (max):	59 lbs (26.8 kg)



Introduction

The Contour® Series luminaires offer traditional square dayforms with softened edges for a versatile look that complements many applications.

The CSX2 combines the latest in LED technology with the familiar aesthetic of the Contour® Series for stylish, high-performance illumination that lasts. It is ideal for replacing traditional metal halide in area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

Ordering Information

EXAMPLE: CSX2 LED 120C 1000 40K T5M MVOLT SPA DDBXD

CSX2 LED 120C

Series	LEDs	Drive current	Color temperature	Distribution	Voltage	Mounting	Options	Finish (dependent)
CSX2 LED	120C 120 LEDs	700 700 mA	40K 4000K	T2M Type II	MVOLT ¹	Shipped included SPA Square pole mounting RPA Round pole mounting Shipped Separately² SPUMBA Square pole universal mounting adaptor RPUMBA Round pole universal mounting adaptor	Shipped installed PER NEMA twist-lock receptacle only (no controls) DCR Dimmable and controllable via ROAM® (no controls) ³ DMG 0-10V dimming driver (no controls) ⁴ HS House-side shield ² SF Single fuse (120, 277, 347V) ⁵ DF Double fuse (208, 240, 480V) ⁵ DS Dual switching ^{6,7} WTB Utility terminal block ⁸ UT Utility terminal block and closing screws ⁸ BL30 Bi-level switched dimming, 30% ^{7,9} BL50 Bi-level switched dimming, 50% ^{7,9} Shipped separately⁷ VG Vandal guard BS Bird-deterrent spikes	DDBXD Dark bronze DBLXD Black DNAXD Natural aluminum DWHXD White DDBTXD Textured dark bronze DBLBXD Textured black DNATXD Textured natural aluminum DWHGXD Textured white
		1000 1000 mA (1A)	50K 5000K	T3M Type III	120 ¹			
				T4M Type IV	208 ¹			
				T5M Type V	240 ¹			
				TFTM Forward throw	277 ¹			
					347			
					480			

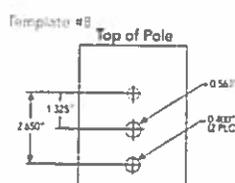
Accessories

Ordered and shipped separately.

DLL127F 1.5 JU	Photocell - SSL twist-lock (120-277V) ¹⁰
DLL347F 1.5 CUL JU	Photocell - SSL twist-lock (347V) ¹⁰
DLL480F 1.5 CUL JU	Photocell - SSL twist-lock (480V) ¹⁰
SC U	Shorting cap ¹⁰
SPUMBA DDBXD U	Square pole universal mounting bracket adaptor (specify finish)
RPUMBA DDBXD U*	Round pole universal mounting bracket adaptor (specify finish)
CSX2HS U	House-side shields (includes 4 shields)
CSX2VG U	Vandal guard accessory
CSX2BS U	Bird-deterrent spikes accessory

For more control options, visit [DTL](#) and [ROAM](#)

Drilling



CSX2 shares a unique drilling pattern with the AERIS™ family. Specify this drilling pattern when specifying poles.

DM18AS	Single unit
DM28AS	2 at 90°
DM28AS	2 at 180°
DM38AS	3 at 90°
DM48AS	4 at 90°
DM32AS	3 at 120°

Example: SSA 20 4C DM18AS DDBXD

* Round pole requires 3.25" O.D. minimum.
** For round pole mounting (RPA) only

Tenon Mounting Slipfitter**

Tenon O.D.	Slipfitter	2 at 180°	2 at 90°	3 at 120°	3 at 90°	4 at 90°
2 3/8"	AST20-190	AST20-280	N/A	N/A	N/A	N/A
2 7/8"	AST25-190	AST25-280	N/A	AST25-320	N/A	N/A
4"	AST35-190	AST35-280	AST35-290	AST35-320	AST35-330	AST35-430

NOTES

- MVOLT driver operates on any line voltage from 120-277V (50/60 Hz). Specify 120, 208, 240 or 277 options only when ordering with fusing (SF, DF options).
- Also available as a separate accessory; see Accessories information at left.
- Specifies a ROAM® enabled luminaire with 0-10V dimming capability; PER option required. Not available with 347 or 480V. Additional hardware and services required for ROAM® deployment; must be purchased separately. Call 1-800-442-6745 or email: sales@roamservices.net.
- Not available with 347 or 480V.
- Single fuse (SF) requires 120, 277 or 347 voltage option. Double fuse (DF) requires 208, 240 or 480 voltage option.
- Provides 50% dimming capability via two independent drivers, each operating half the luminaire. N/A with PER, DCR, DMG, WTB or UT.
- Requires an additional switched line.
- Not available with BL30 or BL50.
- Dimming driver standard. MVOLT only. Not available with WTB, UT or DCR.
- Requires luminaire to be specified with PER option. Ordered and shipped as a separate line item. For 1000mA 347V or 480V PER, must order DLL127F 1.5 JU Photocell - SSL twist-lock (120-277V) as the receptacle is wired after the stepdown transformer.



CITY OF MEDFORD

EXHIBIT # "B"

File # AC-14-129

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Performance Data

Lumen Output

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Actual performance may differ as a result of end-user environment and application. Actual wattage may differ by +/- 8% when operating between 120-480V +/- 10%. Contact factory for performance data on any configurations not shown here.

LEDs	Drive Current (mA)	Performance Package	System Watts	Dist. Type	40K (4000K, 70 CRI)					50K (5000K, 67 CRI)				
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW
					120C (120 LEDs)	700 mA	120C 700-K	268W	T2M	26,094	3	0	4	97
				T3M	27,757	3	0	4	104	29,897	3	0	4	112
				T4M	27,658	3	0	4	103	29,792	3	0	5	111
				T5M	28,025	5	0	4	105	30,186	5	0	4	113
				TFTM	28,304	3	0	4	106	30,487	3	0	4	114
	1000 mA	120C 1000-K	416W	T2M	34,700	4	0	4	83	37,406	4	0	5	90
				T3M	36,910	4	0	5	89	39,789	4	0	5	96
				T4M	36,780	3	0	5	88	39,649	4	0	5	95
				T5M	37,267	5	0	4	90	40,174	5	0	5	97
				TFTM	37,638	3	0	5	90	40,574	3	0	5	98

Lumen Ambient Temperature (LAT) Multipliers

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient	Lumen Multiplier
0°C	1.02
10°C	1.01
20°C	1.00
25°C	1.00
30°C	1.00
40°C	0.99

Projected LED Lumen Maintenance

Data references the extrapolated performance projections for the CSX2 LED 120C platform in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	0	25,000	50,000	100,000
Lumen Maintenance Factor	1.0	0.94	0.90	0.83

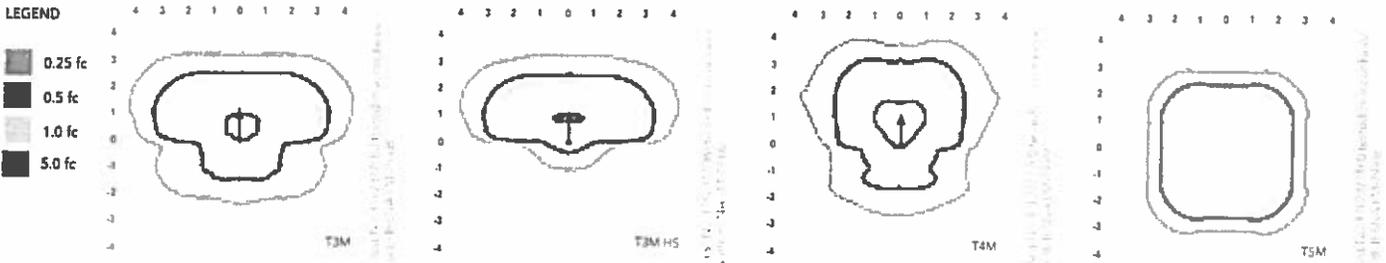
Electrical Load

Number of LEDs	Drive Current (mA)	System Watts	Current (A)					
			120	208	240	277	347	480
120C	700	268W	2.641	1.511	1.318	1.159	0.923	0.674
	1000	416W	4.135	2.397	2.111	1.886	1.527	1.210

Photometric Diagrams

To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's CSX2 homepage.

Isofootcandle plots for the CSX2 LED 120C 1000 40K. Distances are in units of mounting height (30').



FEATURES & SPECIFICATIONS

INTENDED USE

The Contour Series LED area luminaire is ideal for streets, walkways, parking lots, and surrounding areas that call for high-performance LED lighting in a transitional dayform.

CONSTRUCTION

Single-piece die cast housing has a unique flow-through design that allows for optimized thermal management through convective cooling. A metallic screen covers the top of the housing, preventing debris build-up while allowing natural cleaning of the heat sinks. Modular design allows for ease of maintenance and future light engine upgrades. The LED driver(s) and electronics are thermally isolated from the light engines, ensuring long life. Housing is completely sealed against moisture and environmental contaminants.

FINISH

Exterior parts are protected by a zinc-infused Super Durable TGIC thermoseal powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling.

OPTICS

Precision-molded acrylic lenses provide optimal luminaire spacing and improved uniformity. Lenses are indexed to the circuit board to ensure consistent optical alignment and delivering repeatable photometric performance. Light engines are available in standard 4000K (70 CRI) or optional 5000K (67 CRI) configurations. The CSX2 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

ELECTRICAL

Light engines consist of 120 high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (100,000 hrs at 40°C, L70). Class 1 electronic driver designed to have a power factor >90%, THD <20%, with an expected life of 100,000 hours with <1% failure rate. Easily-serviceable surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

INSTALLATION

Integral arm provides easy installation to a pole and assists in alignment and leveling. Secure connection withstands up to 2.0 G vibration load rating per ANSI C136.31. The CSX2 utilizes the AERIS™ series pole drilling pattern for SPA and RPA options.

LISTINGS

CSA Certified to U.S. and Canadian standards. Light engines are IP66 rated. Luminaire is IP65 rated. U.S. Patent No. D632830. U.S. Patent No. D653,382 S.

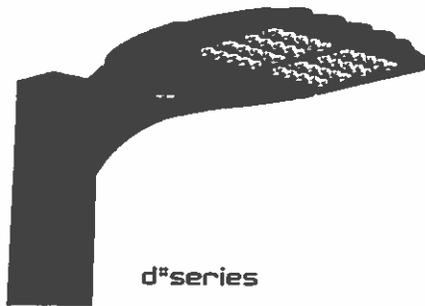
DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at www.designlights.org to confirm which versions are qualified.

WARRANTY

Five year limited warranty. Full warranty terms located at www.acuitybrands.com/CustomerResources/terms_and_conditions.aspx.

Note: Specifications subject to change without notice.





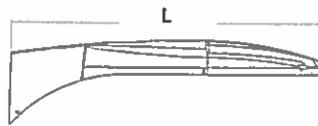
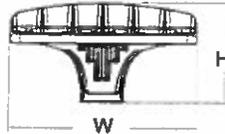
D-Series Size 0 LED Area Luminaire



d^{series}

Specifications

EPA:	0.8 ft ² (.07 m ²)
Length:	26" (66 cm)
Width:	13" (33.0 cm)
Height:	7" (17.3 cm)
Weight (max):	16 lbs (7.25 kg)



Catalog Number	
Notes	Medford Fire - Rescue
Type	SD - w/ Motion Sensor

Introduction

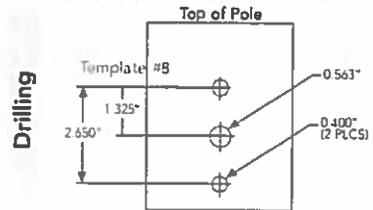
The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment.

The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire. The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 400W metal halide with typical energy savings of 65% and expected service life of over 100,000 hours.

Ordering Information

EXAMPLE: DSX0 LED 40C 1000 400K T3M MVOLT SPA DDBXD

DSX0 LED	40C	1000	4000K	MV		DDBXD							
Series	LEDs	Drive current	Color temperature	Distribution	Voltage	Mounting	Control options	Other options	Finish (optional)				
DSX0 LED	Forward optics	530 530 mA	30K 3000 K (80 CRI min.)	T1S Type I short	MVOLT ⁴	Shipped included	Shipped installed	Shipped installed	DDBXD Dark bronze				
		700 700 mA	40K 4000 K (70 CRI min.)	T2S Type II short	120 ⁴					SPA Square pole mounting	PER NEMA twist-lock receptacle only (no controls) ⁴	HS House-side shield ¹¹	DBLXD Black
		1000 1000 mA (1 A) ²	50K 5000 K (70 CRI)	T2M Type II medium	208 ⁴					RPA Round pole mounting	DMG 0-10V dimming driver (no controls) ⁹	SF Single fuse (120, 277, 347V) ¹⁴	DNAXD Natural aluminum
	20C 20 LEDs (one engine)	40C 40 LEDs (two engines)	Rotated optics ¹	30C 30 LEDs (one engine)	T3S Type III short	240 ⁴	WBA Wall bracket	DCR Dimmable and controllable via ROAM [®] (no controls) ¹⁰	DF Double fuse (208, 240, 480V) ¹⁴	DWHXD White			
					T3M Type III medium	277 ⁴					SPUMBA Square pole universal mounting adaptor ⁶	L90 Left rotated optics ¹	DDBTXD Textured dark bronze
				AMBPC Amber phosphor converted ¹	T4M Type IV medium	347 ⁴	RPUMBA Round pole universal mounting adaptor ⁶	PIR Motion sensor, 8-15' mounting height ¹¹	R90 Right rotated optics ¹	DDBLXD Textured black			
					T4M Type IV medium	480 ⁴					SPUMBA Square pole universal mounting adaptor ⁶	PIRH Motion sensor, 15-30' mounting height ¹¹	DDNATXD Textured natural aluminum
					TFTM Forward throw medium		Shipped separately ⁷	BL30 Bi-level switched dimming, 30% ¹²	DDL Diffused drop lens ¹³	DWHGXD Textured white			
					TSSV Type V very short						KMAB Mast arm mounting bracket adaptor (specify finish)	BL50 Bi-level switched dimming, 50% ¹²	
					TSS Type V short						DDBXD U Mast arm mounting bracket adaptor (specify finish)		
				TSM Type V medium									
				TSW Type V wide									



DSX0 shares a unique drilling pattern with the AERIS™ family. Specify this drilling pattern when specifying poles, per the table below.

DM19AS	Single unit	DM29AS	2 at 90° *
DM28AS	2 at 180°	DM39AS	3 at 90° **
DM49AS	4 at 90° *	DM32AS	3 at 120° **

Example: SSA 20 4C DM19AS DDBXD
Visit Lithonia Lighting's POLES (LITHONIA) to see our wide selection of poles, accessories and educational book.
*Round pole top must be 3.25" O.D. minimum
**For round pole mounting (IPK) only.

Tenon Mounting Slipfitter **

Tenon O.D.	Single Unit	2 at 180°	2 at 90°	3 at 120°	3 at 90°	4 at 90°
2-3/8"	AST20-190	AST20-280	AST20-290	AST20-320	AST20-390	AST20-490
2-7/8"	AST25-190	AST25-280	AST25-290	AST25-320	AST25-390	AST25-490
4"	AST35-190	AST35-280	AST35-290	AST35-320	AST35-390	AST35-490

- NOTES**
- 30 LEDs (30C option) and rotated options (L90 or R90) only available together.
 - 1000mA is not available with AMBPC.
 - AMBPC only available with 530mA or 700mA.
 - MVOLT driver operates on any line voltage from 120-277V (50/60 Hz). Specify 120, 208, 240 or 277 options only when ordering with fusing (SF, DF options).
 - Not available with single-board, 530 mA product (20C 530 or 30C 530). Not available with DCR, BL30, or BL50.
 - Available as a separate combination accessory: PUMBA (fin.sh) U.
 - Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" mast arm (not included).
 - Photocell ordered and shipped as a separate line item from Acuity Brands Controls. See accessories.
 - DMG option for 347v or 480v requires 1000mA.
 - Specifies a ROAM[®] enabled luminaire with 0-10V dimming capability. PER option required. Not available with 347 or 480V. Additional hardware and services required for ROAM[®] deployment; must be purchased separately. Call 1-800-442-6745 or email: sales@roamservices.net. N/A BL30, BL50, PIR, or PIRH.
 - PIR specifies the SensorSwitch SBGR-10-ODP control. PIRH specifies the SensorSwitch SBGR-4-ODP control; see Motion Sensor Guide for details. Dimming driver standard. Not available with DCR.
 - Requires an additional switched circuit. Dimming driver standard. MVOLT only. Not available with DCR.
 - Also available as a separate accessory; see Accessories information. HS and DDL are not available together.
 - Single fuse (SF) requires 120, 277 or 347 voltage option. Double fuse (DF) requires 208, 240 or 480 voltage option.
 - Requires luminaire to be specified with PER option. Ordered and shipped as a separate line item from Acuity Brands Controls.

Accessories

- DL127F 1.5 IU Photocell SSL twist-lock (120-277V)¹¹
- DL137F 1.5 CUL IU Photocell SSL twist-lock (347V)¹¹
- DL140F 1.5 CUL IU Photocell - SSL twist-lock (480V)¹¹
- SC U Shorting cap¹¹
- DSXHS 20C U House-side shield for 20 LED unit¹¹
- DSXHS 30C U House-side shield for 30 LED unit¹¹
- DSXHS 40C U House-side shield for 40 LED unit¹¹
- DSXDDL U Diffused drop lens (polycarbonate)¹¹
- PUMBA DDBXD U* Square and round pole universal mounting bracket adaptor (specify finish)
- KMAB DDBXD U Mast arm mounting bracket adaptor (specify finish)⁷

For more control options, visit DTL and KAMM online



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Performance Data

Lumen Output

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Actual performance may differ as a result of end-user environment and application. Actual wattage may differ by +/- 8% when operating between 120-480V +/-10%. Contact factory for performance data on any configurations not shown here.

LEDs	Drive Current (mA)	System Watts	Dist. Type	30K (3000 K, 85 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																														
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				20C (20 LEDs) <tr> <td rowspan="18">20C (20 LEDs)</td> <td rowspan="9">530 mA</td> <td rowspan="9">35W</td> <td>T1S</td><td>2,904</td><td>1</td><td>0</td><td>1</td><td>83</td><td>3,655</td><td>1</td><td>0</td><td>1</td><td>104</td><td>3,941</td><td>1</td><td>0</td><td>1</td><td>113</td> </tr> <tr> <td>T2M</td><td>2,902</td><td>1</td><td>0</td><td>1</td><td>83</td><td>3,652</td><td>1</td><td>0</td><td>1</td><td>104</td><td>3,937</td><td>1</td><td>0</td><td>1</td><td>112</td> </tr> <tr> <td>T2S</td><td>2,959</td><td>1</td><td>0</td><td>1</td><td>85</td><td>3,723</td><td>1</td><td>0</td><td>1</td><td>106</td><td>4,014</td><td>1</td><td>0</td><td>1</td><td>115</td> </tr> <tr> <td>T3M</td><td>2,952</td><td>1</td><td>0</td><td>1</td><td>84</td><td>3,715</td><td>1</td><td>0</td><td>1</td><td>106</td><td>4,005</td><td>1</td><td>0</td><td>1</td><td>114</td> </tr> <tr> <td>T3S</td><td>2,923</td><td>1</td><td>0</td><td>1</td><td>84</td><td>3,679</td><td>1</td><td>0</td><td>1</td><td>105</td><td>3,966</td><td>1</td><td>0</td><td>1</td><td>113</td> </tr> <tr> <td>T4M</td><td>2,937</td><td>1</td><td>0</td><td>1</td><td>84</td><td>3,696</td><td>1</td><td>0</td><td>1</td><td>106</td><td>3,984</td><td>1</td><td>0</td><td>1</td><td>114</td> </tr> <tr> <td>T5M</td><td>3,037</td><td>2</td><td>0</td><td>1</td><td>87</td><td>3,823</td><td>2</td><td>0</td><td>1</td><td>109</td><td>4,121</td><td>3</td><td>0</td><td>1</td><td>118</td> </tr> <tr> <td>T5S</td><td>3,074</td><td>2</td><td>0</td><td>0</td><td>88</td><td>3,869</td><td>2</td><td>0</td><td>0</td><td>111</td><td>4,171</td><td>2</td><td>0</td><td>0</td><td>119</td> </tr> <tr> <td>TSVS</td><td>3,028</td><td>2</td><td>0</td><td>0</td><td>87</td><td>3,811</td><td>2</td><td>0</td><td>0</td><td>109</td><td>4,109</td><td>2</td><td>0</td><td>0</td><td>117</td> </tr> <tr> <td>TSW</td><td>3,044</td><td>2</td><td>0</td><td>1</td><td>87</td><td>3,831</td><td>3</td><td>0</td><td>1</td><td>109</td><td>4,130</td><td>3</td><td>0</td><td>1</td><td>118</td> </tr> <tr> <td>TFTM</td><td>2,903</td><td>1</td><td>0</td><td>1</td><td>83</td><td>3,653</td><td>1</td><td>0</td><td>1</td><td>104</td><td>3,939</td><td>1</td><td>0</td><td>2</td><td>113</td> </tr> <tr> <td rowspan="9">700 mA</td> <td rowspan="9">45W</td> <td>T1S</td><td>3,599</td><td>1</td><td>0</td><td>1</td><td>80</td><td>4,524</td><td>1</td><td>0</td><td>1</td><td>101</td><td>4,902</td><td>1</td><td>0</td><td>1</td><td>109</td> </tr> <tr> <td>T2M</td><td>3,596</td><td>1</td><td>0</td><td>1</td><td>80</td><td>4,520</td><td>1</td><td>0</td><td>1</td><td>100</td><td>4,898</td><td>1</td><td>0</td><td>1</td><td>109</td> </tr> <tr> <td>T2S</td><td>3,667</td><td>1</td><td>0</td><td>1</td><td>81</td><td>4,608</td><td>1</td><td>0</td><td>1</td><td>102</td><td>4,994</td><td>1</td><td>0</td><td>1</td><td>111</td> </tr> <tr> <td>T3M</td><td>3,658</td><td>1</td><td>0</td><td>1</td><td>81</td><td>4,598</td><td>1</td><td>0</td><td>1</td><td>102</td><td>4,983</td><td>1</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td>T3S</td><td>3,623</td><td>1</td><td>0</td><td>1</td><td>81</td><td>4,554</td><td>1</td><td>0</td><td>1</td><td>101</td><td>4,935</td><td>1</td><td>0</td><td>1</td><td>110</td> </tr> <tr> <td>T4M</td><td>3,639</td><td>1</td><td>0</td><td>1</td><td>81</td><td>4,574</td><td>1</td><td>0</td><td>2</td><td>102</td><td>4,957</td><td>1</td><td>0</td><td>2</td><td>110</td> </tr> <tr> <td>T5M</td><td>3,764</td><td>2</td><td>0</td><td>1</td><td>84</td><td>4,731</td><td>3</td><td>0</td><td>1</td><td>105</td><td>5,127</td><td>3</td><td>0</td><td>1</td><td>114</td> </tr> <tr> <td>T5S</td><td>3,810</td><td>2</td><td>0</td><td>0</td><td>85</td><td>4,788</td><td>2</td><td>0</td><td>0</td><td>106</td><td>5,189</td><td>2</td><td>0</td><td>0</td><td>115</td> </tr> <tr> <td>TSVS</td><td>3,753</td><td>2</td><td>0</td><td>0</td><td>83</td><td>4,717</td><td>2</td><td>0</td><td>0</td><td>105</td><td>5,112</td><td>2</td><td>0</td><td>0</td><td>114</td> </tr> <tr> <td>TSW</td><td>3,772</td><td>3</td><td>0</td><td>1</td><td>84</td><td>4,741</td><td>3</td><td>0</td><td>1</td><td>105</td><td>5,138</td><td>3</td><td>0</td><td>1</td><td>114</td> </tr> <tr> <td>TFTM</td><td>3,598</td><td>1</td><td>0</td><td>1</td><td>80</td><td>4,522</td><td>1</td><td>0</td><td>2</td><td>100</td><td>4,900</td><td>1</td><td>0</td><td>2</td><td>109</td> </tr> <tr> <td rowspan="18">1000 mA</td> <td rowspan="18">72W</td> <td>T1S</td><td>4,654</td><td>1</td><td>0</td><td>1</td><td>65</td><td>6,206</td><td>2</td><td>0</td><td>2</td><td>86</td><td>6,640</td><td>2</td><td>0</td><td>2</td><td>92</td> </tr> <tr> <td>T2M</td><td>4,650</td><td>1</td><td>0</td><td>1</td><td>65</td><td>6,200</td><td>2</td><td>0</td><td>2</td><td>86</td><td>6,634</td><td>2</td><td>0</td><td>2</td><td>92</td> </tr> <tr> <td>T2S</td><td>4,741</td><td>1</td><td>0</td><td>1</td><td>66</td><td>6,322</td><td>2</td><td>0</td><td>2</td><td>88</td><td>6,764</td><td>2</td><td>0</td><td>2</td><td>94</td> </tr> <tr> <td>T3M</td><td>4,730</td><td>1</td><td>0</td><td>2</td><td>66</td><td>6,307</td><td>2</td><td>0</td><td>2</td><td>88</td><td>6,749</td><td>2</td><td>0</td><td>2</td><td>94</td> </tr> <tr> <td>T3S</td><td>4,685</td><td>1</td><td>0</td><td>1</td><td>65</td><td>6,246</td><td>1</td><td>0</td><td>2</td><td>87</td><td>6,684</td><td>2</td><td>0</td><td>2</td><td>93</td> </tr> <tr> <td>T4M</td><td>4,706</td><td>1</td><td>0</td><td>2</td><td>65</td><td>6,275</td><td>1</td><td>0</td><td>2</td><td>87</td><td>6,714</td><td>2</td><td>0</td><td>2</td><td>93</td> </tr> <tr> <td>T5M</td><td>4,868</td><td>3</td><td>0</td><td>1</td><td>68</td><td>6,490</td><td>3</td><td>0</td><td>1</td><td>90</td><td>6,945</td><td>3</td><td>0</td><td>1</td><td>96</td> </tr> <tr> <td>T5S</td><td>4,926</td><td>2</td><td>0</td><td>0</td><td>68</td><td>6,568</td><td>2</td><td>0</td><td>0</td><td>91</td><td>7,028</td><td>2</td><td>0</td><td>0</td><td>98</td> </tr> <tr> <td>TSVS</td><td>4,853</td><td>2</td><td>0</td><td>0</td><td>67</td><td>6,471</td><td>2</td><td>0</td><td>0</td><td>90</td><td>6,924</td><td>3</td><td>0</td><td>0</td><td>96</td> </tr> <tr> <td>TSW</td><td>4,878</td><td>3</td><td>0</td><td>1</td><td>68</td><td>6,504</td><td>3</td><td>0</td><td>2</td><td>90</td><td>6,959</td><td>3</td><td>0</td><td>2</td><td>97</td> </tr> <tr> <td>TFTM</td><td>4,652</td><td>1</td><td>0</td><td>2</td><td>65</td><td>6,203</td><td>1</td><td>0</td><td>2</td><td>86</td><td>6,637</td><td>1</td><td>0</td><td>2</td><td>92</td> </tr> <tr> <td rowspan="18">530 mA</td> <td rowspan="18">68W</td> <td>T1S</td><td>5,579</td><td>1</td><td>0</td><td>1</td><td>82</td><td>7,019</td><td>2</td><td>0</td><td>2</td><td>103</td><td>7,565</td><td>2</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td>T2M</td><td>5,574</td><td>2</td><td>0</td><td>2</td><td>82</td><td>7,012</td><td>2</td><td>0</td><td>2</td><td>103</td><td>7,558</td><td>2</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td>T2S</td><td>5,683</td><td>1</td><td>0</td><td>1</td><td>84</td><td>7,150</td><td>2</td><td>0</td><td>2</td><td>105</td><td>7,706</td><td>2</td><td>0</td><td>2</td><td>113</td> </tr> <tr> <td>T3M</td><td>5,670</td><td>1</td><td>0</td><td>2</td><td>83</td><td>7,133</td><td>2</td><td>0</td><td>2</td><td>105</td><td>7,688</td><td>2</td><td>0</td><td>2</td><td>113</td> </tr> <tr> <td>T3S</td><td>5,615</td><td>1</td><td>0</td><td>2</td><td>83</td><td>7,065</td><td>2</td><td>0</td><td>2</td><td>104</td><td>7,614</td><td>2</td><td>0</td><td>2</td><td>112</td> </tr> <tr> <td>T4M</td><td>5,641</td><td>1</td><td>0</td><td>2</td><td>83</td><td>7,097</td><td>2</td><td>0</td><td>2</td><td>104</td><td>7,649</td><td>2</td><td>0</td><td>2</td><td>112</td> </tr> <tr> <td>T5M</td><td>5,835</td><td>3</td><td>0</td><td>1</td><td>86</td><td>7,340</td><td>3</td><td>0</td><td>1</td><td>108</td><td>7,912</td><td>3</td><td>0</td><td>2</td><td>116</td> </tr> <tr> <td>T5S</td><td>5,905</td><td>2</td><td>0</td><td>0</td><td>87</td><td>7,429</td><td>3</td><td>0</td><td>0</td><td>109</td><td>8,007</td><td>3</td><td>0</td><td>1</td><td>118</td> </tr> <tr> <td>TSVS</td><td>5,817</td><td>2</td><td>0</td><td>0</td><td>86</td><td>7,318</td><td>3</td><td>0</td><td>0</td><td>108</td><td>7,888</td><td>1</td><td>0</td><td>2</td><td>116</td> </tr> <tr> <td>TSW</td><td>5,847</td><td>3</td><td>0</td><td>1</td><td>86</td><td>7,355</td><td>3</td><td>0</td><td>2</td><td>108</td><td>7,928</td><td>3</td><td>0</td><td>2</td><td>117</td> </tr> <tr> <td>TFTM</td><td>5,576</td><td>1</td><td>0</td><td>2</td><td>82</td><td>7,015</td><td>1</td><td>0</td><td>2</td><td>103</td><td>7,561</td><td>2</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td rowspan="18">700 mA</td> <td rowspan="18">91W</td> <td>T1S</td><td>7,074</td><td>2</td><td>0</td><td>2</td><td>78</td><td>8,930</td><td>2</td><td>0</td><td>2</td><td>98</td><td>9,619</td><td>2</td><td>0</td><td>2</td><td>106</td> </tr> <tr> <td>T2M</td><td>7,068</td><td>2</td><td>0</td><td>2</td><td>78</td><td>8,922</td><td>2</td><td>0</td><td>2</td><td>98</td><td>9,610</td><td>2</td><td>0</td><td>2</td><td>106</td> </tr> <tr> <td>T2S</td><td>7,207</td><td>2</td><td>0</td><td>2</td><td>79</td><td>9,097</td><td>2</td><td>0</td><td>2</td><td>100</td><td>9,798</td><td>2</td><td>0</td><td>2</td><td>108</td> </tr> <tr> <td>T3M</td><td>7,190</td><td>2</td><td>0</td><td>2</td><td>79</td><td>9,076</td><td>2</td><td>0</td><td>2</td><td>100</td><td>9,776</td><td>2</td><td>0</td><td>2</td><td>107</td> </tr> <tr> <td>T3S</td><td>7,121</td><td>2</td><td>0</td><td>2</td><td>78</td><td>8,988</td><td>2</td><td>0</td><td>2</td><td>99</td><td>9,682</td><td>2</td><td>0</td><td>2</td><td>106</td> </tr> <tr> <td>T4M</td><td>7,153</td><td>2</td><td>0</td><td>2</td><td>79</td><td>9,029</td><td>2</td><td>0</td><td>2</td><td>99</td><td>9,726</td><td>2</td><td>0</td><td>2</td><td>107</td> </tr> <tr> <td>T5M</td><td>7,399</td><td>3</td><td>0</td><td>2</td><td>81</td><td>9,339</td><td>3</td><td>0</td><td>2</td><td>103</td><td>10,060</td><td>3</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td>T5S</td><td>7,488</td><td>3</td><td>0</td><td>0</td><td>82</td><td>9,452</td><td>3</td><td>0</td><td>1</td><td>104</td><td>10,181</td><td>3</td><td>0</td><td>1</td><td>112</td> </tr> <tr> <td>TSVS</td><td>7,377</td><td>3</td><td>0</td><td>0</td><td>81</td><td>9,311</td><td>3</td><td>0</td><td>1</td><td>102</td><td>10,030</td><td>3</td><td>0</td><td>1</td><td>110</td> </tr> <tr> <td>TSW</td><td>7,414</td><td>3</td><td>0</td><td>2</td><td>81</td><td>9,359</td><td>4</td><td>0</td><td>2</td><td>103</td><td>10,080</td><td>4</td><td>0</td><td>2</td><td>111</td> </tr> <tr> <td>TFTM</td><td>7,071</td><td>1</td><td>0</td><td>2</td><td>78</td><td>8,926</td><td>2</td><td>0</td><td>3</td><td>98</td><td>9,614</td><td>2</td><td>0</td><td>3</td><td>106</td> </tr> <tr> <td rowspan="18">1000 mA</td> <td rowspan="18">138W</td> <td>T1S</td><td>9,557</td><td>2</td><td>0</td><td>2</td><td>69</td><td>12,020</td><td>2</td><td>0</td><td>2</td><td>87</td><td>12,957</td><td>3</td><td>0</td><td>3</td><td>94</td> </tr> <tr> <td>T2M</td><td>9,548</td><td>2</td><td>0</td><td>2</td><td>69</td><td>12,009</td><td>3</td><td>0</td><td>3</td><td>87</td><td>12,946</td><td>3</td><td>0</td><td>3</td><td>94</td> </tr> <tr> <td>T2S</td><td>9,735</td><td>2</td><td>0</td><td>2</td><td>71</td><td>12,245</td><td>3</td><td>0</td><td>3</td><td>89</td><td>13,199</td><td>3</td><td>0</td><td>3</td><td>96</td> </tr> <tr> <td>T3M</td><td>9,713</td><td>2</td><td>0</td><td>2</td><td>70</td><td>12,217</td><td>2</td><td>0</td><td>3</td><td>89</td><td>13,169</td><td>3</td><td>0</td><td>3</td><td>95</td> </tr> <tr> <td>T3S</td><td>9,619</td><td>2</td><td>0</td><td>2</td><td>70</td><td>12,099</td><td>2</td><td>0</td><td>2</td><td>88</td><td>13,042</td><td>2</td><td>0</td><td>2</td><td>95</td> </tr> <tr> <td>T4M</td><td>9,663</td><td>2</td><td>0</td><td>2</td><td>70</td><td>12,154</td><td>2</td><td>0</td><td>3</td><td>88</td><td>13,102</td><td>2</td><td>0</td><td>3</td><td>95</td> </tr> <tr> <td>T5M</td><td>9,995</td><td>3</td><td>0</td><td>2</td><td>72</td><td>12,571</td><td>4</td><td>0</td><td>2</td><td>91</td><td>13,552</td><td>4</td><td>0</td><td>2</td><td>98</td> </tr> <tr> <td>T5S</td><td>10,115</td><td>3</td><td>0</td><td>1</td><td>73</td><td>12,723</td><td>3</td><td>0</td><td>1</td><td>92</td><td>13,715</td><td>3</td><td>0</td><td>1</td><td>99</td> </tr> <tr> <td>TSVS</td><td>9,965</td><td>3</td><td>0</td><td>1</td><td>72</td><td>12,534</td><td>3</td><td>0</td><td>1</td><td>91</td><td>13,511</td><td>3</td><td>0</td><td>1</td><td>98</td> </tr> <tr> <td>TSW</td><td>10,015</td><td>4</td><td>0</td><td>2</td><td>73</td><td>12,597</td><td>4</td><td>0</td><td>2</td><td>91</td><td>13,579</td><td>4</td><td>0</td><td>2</td><td>98</td> </tr> <tr> <td>TFTM</td><td>9,552</td><td>2</td><td>0</td><td>3</td><td>69</td><td>12,015</td><td>2</td><td>0</td><td>3</td><td>87</td><td>12,951</td><td>1</td><td>0</td><td>2</td><td>94</td> </tr>																20C (20 LEDs)	530 mA	35W	T1S	2,904	1	0	1	83	3,655	1	0	1	104	3,941	1	0	1	113	T2M	2,902	1	0	1	83	3,652	1	0	1	104	3,937	1	0	1	112	T2S	2,959	1	0	1	85	3,723	1	0	1	106	4,014	1	0	1	115	T3M	2,952	1	0	1	84	3,715	1	0	1	106	4,005	1	0	1	114	T3S	2,923	1	0	1	84	3,679	1	0	1	105	3,966	1	0	1	113	T4M	2,937	1	0	1	84	3,696	1	0	1	106	3,984	1	0	1	114	T5M	3,037	2	0	1	87	3,823	2	0	1	109	4,121	3	0	1	118	T5S	3,074	2	0	0	88	3,869	2	0	0	111	4,171	2	0	0	119	TSVS	3,028	2	0	0	87	3,811	2	0	0	109	4,109	2	0	0	117	TSW	3,044	2	0	1	87	3,831	3	0	1	109	4,130	3	0	1	118	TFTM	2,903	1	0	1	83	3,653	1	0	1	104	3,939	1	0	2	113	700 mA	45W	T1S	3,599	1	0	1	80	4,524	1	0	1	101	4,902	1	0	1	109	T2M	3,596	1	0	1	80	4,520	1	0	1	100	4,898	1	0	1	109	T2S	3,667	1	0	1	81	4,608	1	0	1	102	4,994	1	0	1	111	T3M	3,658	1	0	1	81	4,598	1	0	1	102	4,983	1	0	2	111	T3S	3,623	1	0	1	81	4,554	1	0	1	101	4,935	1	0	1	110	T4M	3,639	1	0	1	81	4,574	1	0	2	102	4,957	1	0	2	110	T5M	3,764	2	0	1	84	4,731	3	0	1	105	5,127	3	0	1	114	T5S	3,810	2	0	0	85	4,788	2	0	0	106	5,189	2	0	0	115	TSVS	3,753	2	0	0	83	4,717	2	0	0	105	5,112	2	0	0	114	TSW	3,772	3	0	1	84	4,741	3	0	1	105	5,138	3	0	1	114	TFTM	3,598	1	0	1	80	4,522	1	0	2	100	4,900	1	0	2	109	1000 mA	72W	T1S	4,654	1	0	1	65	6,206	2	0	2	86	6,640	2	0	2	92	T2M	4,650	1	0	1	65	6,200	2	0	2	86	6,634	2	0	2	92	T2S	4,741	1	0	1	66	6,322	2	0	2	88	6,764	2	0	2	94	T3M	4,730	1	0	2	66	6,307	2	0	2	88	6,749	2	0	2	94	T3S	4,685	1	0	1	65	6,246	1	0	2	87	6,684	2	0	2	93	T4M	4,706	1	0	2	65	6,275	1	0	2	87	6,714	2	0	2	93	T5M	4,868	3	0	1	68	6,490	3	0	1	90	6,945	3	0	1	96	T5S	4,926	2	0	0	68	6,568	2	0	0	91	7,028	2	0	0	98	TSVS	4,853	2	0	0	67	6,471	2	0	0	90	6,924	3	0	0	96	TSW	4,878	3	0	1	68	6,504	3	0	2	90	6,959	3	0	2	97	TFTM	4,652	1	0	2	65	6,203	1	0	2	86	6,637	1	0	2	92	530 mA	68W	T1S	5,579	1	0	1	82	7,019	2	0	2	103	7,565	2	0	2	111	T2M	5,574	2	0	2	82	7,012	2	0	2	103	7,558	2	0	2	111	T2S	5,683	1	0	1	84	7,150	2	0	2	105	7,706	2	0	2	113	T3M	5,670	1	0	2	83	7,133	2	0	2	105	7,688	2	0	2	113	T3S	5,615	1	0	2	83	7,065	2	0	2	104	7,614	2	0	2	112	T4M	5,641	1	0	2	83	7,097	2	0	2	104	7,649	2	0	2	112	T5M	5,835	3	0	1	86	7,340	3	0	1	108	7,912	3	0	2	116	T5S	5,905	2	0	0	87	7,429	3	0	0	109	8,007	3	0	1	118	TSVS	5,817	2	0	0	86	7,318	3	0	0	108	7,888	1	0	2	116	TSW	5,847	3	0	1	86	7,355	3	0	2	108	7,928	3	0	2	117	TFTM	5,576	1	0	2	82	7,015	1	0	2	103	7,561	2	0	2	111	700 mA	91W	T1S	7,074	2	0	2	78	8,930	2	0	2	98	9,619	2	0	2	106	T2M	7,068	2	0	2	78	8,922	2	0	2	98	9,610	2	0	2	106	T2S	7,207	2	0	2	79	9,097	2	0	2	100	9,798	2	0	2	108	T3M	7,190	2	0	2	79	9,076	2	0	2	100	9,776	2	0	2	107	T3S	7,121	2	0	2	78	8,988	2	0	2	99	9,682	2	0	2	106	T4M	7,153	2	0	2	79	9,029	2	0	2	99	9,726	2	0	2	107	T5M	7,399	3	0	2	81	9,339	3	0	2	103	10,060	3	0	2	111	T5S	7,488	3	0	0	82	9,452	3	0	1	104	10,181	3	0	1	112	TSVS	7,377	3	0	0	81	9,311	3	0	1	102	10,030	3	0	1	110	TSW	7,414	3	0	2	81	9,359	4	0	2	103	10,080	4	0	2	111	TFTM	7,071	1	0	2	78	8,926	2	0	3	98	9,614	2	0	3	106	1000 mA	138W	T1S	9,557	2	0	2	69	12,020	2	0	2	87	12,957	3	0	3	94	T2M	9,548	2	0	2	69	12,009	3	0	3	87	12,946	3	0	3	94	T2S	9,735	2	0	2	71	12,245	3	0	3	89	13,199	3	0	3	96	T3M	9,713	2	0	2	70	12,217	2	0	3	89	13,169	3	0	3	95	T3S	9,619	2	0	2	70	12,099	2	0	2	88	13,042	2	0	2	95	T4M	9,663	2	0	2	70	12,154	2	0	3	88	13,102	2	0	3	95	T5M	9,995	3	0	2	72	12,571	4	0	2	91	13,552	4	0	2	98	T5S	10,115	3	0	1	73	12,723	3	0	1	92	13,715	3	0	1	99	TSVS	9,965	3	0	1	72	12,534	3	0	1	91	13,511	3	0	1	98	TSW	10,015	4	0	2	73	12,597	4	0	2	91	13,579	4	0	2	98	TFTM	9,552	2	0	3	69	12,015	2	0	3	87	12,951
20C (20 LEDs)	530 mA	35W	T1S	2,904	1	0	1	83	3,655	1	0	1	104	3,941	1	0	1	113																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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		T2S	4,741	1	0	1	66	6,322	2	0	2	88	6,764	2	0	2	94																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		T3M	4,730	1	0	2	66	6,307	2	0	2	88	6,749	2	0	2	94																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		T3S	4,685	1	0	1	65	6,246	1	0	2	87	6,684	2	0	2	93																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		T4M	4,706	1	0	2	65	6,275	1	0	2	87	6,714	2	0	2	93																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		T5M	4,868	3	0	1	68	6,490	3	0	1	90	6,945	3	0	1	96																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
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		TFTM	4,652	1	0	2	65	6,203	1	0	2	86	6,637	1	0	2	92																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		530 mA	68W	T1S	5,579	1	0	1	82	7,019	2	0	2	103	7,565	2	0	2	111																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T2M	5,574	2	0	2	82	7,012	2	0	2	103	7,558	2	0	2	111																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T2S	5,683	1	0	1	84	7,150	2	0	2	105	7,706	2	0	2	113																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T3M	5,670	1	0	2	83	7,133	2	0	2	105	7,688	2	0	2	113																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T3S	5,615	1	0	2	83	7,065	2	0	2	104	7,614	2	0	2	112																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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				T5M	5,835	3	0	1	86	7,340	3	0	1	108	7,912	3	0	2	116																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
T5S	5,905			2	0	0	87	7,429	3	0	0	109	8,007	3	0	1	118																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
TSVS	5,817			2	0	0	86	7,318	3	0	0	108	7,888	1	0	2	116																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
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				T2M	7,068	2	0	2	78	8,922	2	0	2	98	9,610	2	0	2	106																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T2S	7,207	2	0	2	79	9,097	2	0	2	100	9,798	2	0	2	108																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T3M	7,190	2	0	2	79	9,076	2	0	2	100	9,776	2	0	2	107																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T3S	7,121	2	0	2	78	8,988	2	0	2	99	9,682	2	0	2	106																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T4M	7,153	2	0	2	79	9,029	2	0	2	99	9,726	2	0	2	107																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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		T5S	7,488	3	0	0	82	9,452	3	0	1	104	10,181	3	0	1	112																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		TSVS	7,377	3	0	0	81	9,311	3	0	1	102	10,030	3	0	1	110																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		TSW	7,414	3	0	2	81	9,359	4	0	2	103	10,080	4	0	2	111																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		TFTM	7,071	1	0	2	78	8,926	2	0	3	98	9,614	2	0	3	106																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
		1000 mA	138W	T1S	9,557	2	0	2	69	12,020	2	0	2	87	12,957	3	0	3	94																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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				T2S	9,735	2	0	2	71	12,245	3	0	3	89	13,199	3	0	3	96																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T3M	9,713	2	0	2	70	12,217	2	0	3	89	13,169	3	0	3	95																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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				T4M	9,663	2	0	2	70	12,154	2	0	3	88	13,102	2	0	3	95																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
				T5M	9,995	3	0	2	72	12,571	4	0	2	91	13,552	4	0	2	98																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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TSW	10,015			4	0	2	73	12,597	4	0	2	91	13,579	4	0	2	98																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
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Note: Available with phosphor-converted amber LED's (nomenclature AMBPC). These LED's produce light with 97+% >530 nm. Output can be calculated by applying a 0.7 factor to 4000 K lumen values and photometric files.



One Lithonia Way • Conyers, Georgia 30012 • Phone 800 279 8041 • Fax 770 918 1209 • www.lithonia.com
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DSX0-LED
Rev 10/07/14

Handwritten: "B" 4/2 of 54

Performance Data

Lumen Ambient Temperature (LAT) Multipliers

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient	Lumen Multiplier
0°C	1.02
10°C	1.01
20°C	1.00
25°C	1.00
30°C	1.00
40°C	0.99

Projected LED Lumen Maintenance

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	0	25,000	50,000	100,000
Lumen Maintenance Factor	DSXO LED 20C 1000			
	1	0.97	0.94	0.90
	DSXO LED 40C 1000			
	1	0.94	0.90	0.84
	DSXO LED 40C 700			
	1	0.99	0.98	0.96

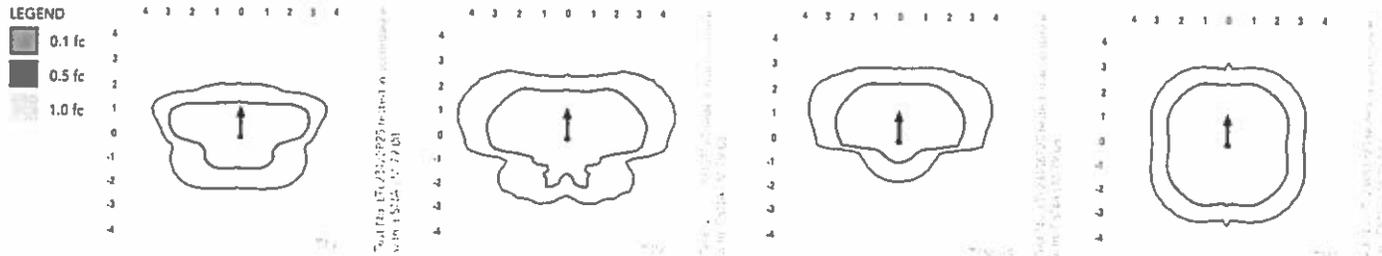
Electrical Load

Number of LEDs	Drive Current (mA)	System Watts	Current (A)					
			120	208	240	277	347	480
20C	530	35	0.34	0.22	0.21	0.20	--	--
	700	45	0.47	0.28	0.24	0.22	0.18	0.14
	1000	72	0.76	0.45	0.39	0.36	0.36	0.26
30C	530	52	0.51	0.31	0.28	0.25	--	--
	700	70	0.72	0.43	0.37	0.34	0.25	0.19
	1000	104	1.11	0.64	0.56	0.49	0.47	0.34
40C	530	68	0.71	0.41	0.36	0.33	0.25	0.19
	700	91	0.94	0.55	0.48	0.42	0.33	0.24
	1000	138	1.45	0.84	0.73	0.64	0.69	0.50

Photometric Diagrams

To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's D-Series Area homepage.

Isofootcandle plots for the DSXO LED 40C 1000 40K. Distances are in units of mounting height (20').



FEATURES & SPECIFICATIONS

INTENDED USE

The sleek design of the D-Series Size 0 reflects the embedded high performance LED technology. It is ideal for many commercial and municipal applications, such as parking lots, plazas, campuses, and streetscapes.

CONSTRUCTION

Single-piece die-cast aluminum housing has integral heat sink fins to optimize thermal management through conductive and convective cooling. Modular design allows for ease of maintenance and future light engine upgrades. The LED driver is mounted in direct contact with the casting to promote low operating temperature and long life. Housing is completely sealed against moisture and environmental contaminants (IP65). Low EPA (0.8 ft²) for optimized pole wind loading.

FINISH

Exterior parts are protected by a zinc-infused Super Durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Available in both textured and non-textured finishes.

OPTICS

Precision-molded proprietary acrylic lenses are engineered for superior area lighting distribution, uniformity, and pole spacing. Light engines are available in standard 4000 K (70 minimum CRI) or optional 3000 K (80 minimum CRI) or 5000 K (70 CRI) configurations. The D-Series Size 0 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

ELECTRICAL

Light engine(s) configurations consist of high-efficiency LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L96/100,000 hours at 25°C). Class 1 electronic drivers are designed to have a power factor >90%, THD <20%, and an expected life of

100,000 hours with <1% failure rate. Easily serviceable 10kV or 6kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

INSTALLATION

Included mounting block and integral arm facilitate quick and easy installation. Stainless steel bolts fasten the mounting block securely to poles and walls, enabling the D-Series Size 0 to withstand up to a 3.0 G vibration load rating per ANSI C136.31. The D-Series Size 0 utilizes the AERIS™ series pole drilling pattern. Optional terminal block, tool-less entry, and NEMA photocontrol receptacle are also available.

LISTINGS

UL Listed for wet locations. Light engines are IP66 rated, luminaire is IP65 rated. Rated for -40°C minimum ambient. U.S. Patent No. D672,492.5. International patent pending.

DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at www.designlights.org to confirm which versions are qualified.

WARRANTY

Five-year limited warranty. Full warranty terms located at: www.acuitybrands.com/CustomerResources/Terms_and_conditions.aspx

Note: Specifications subject to change without notice.



One Lithonia Way • Conyers, Georgia 30012 • Phone 800.279.8041 • Fax 770.918.1209 • www.lithonia.com

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DSXO-LED
Rev 10/07/14

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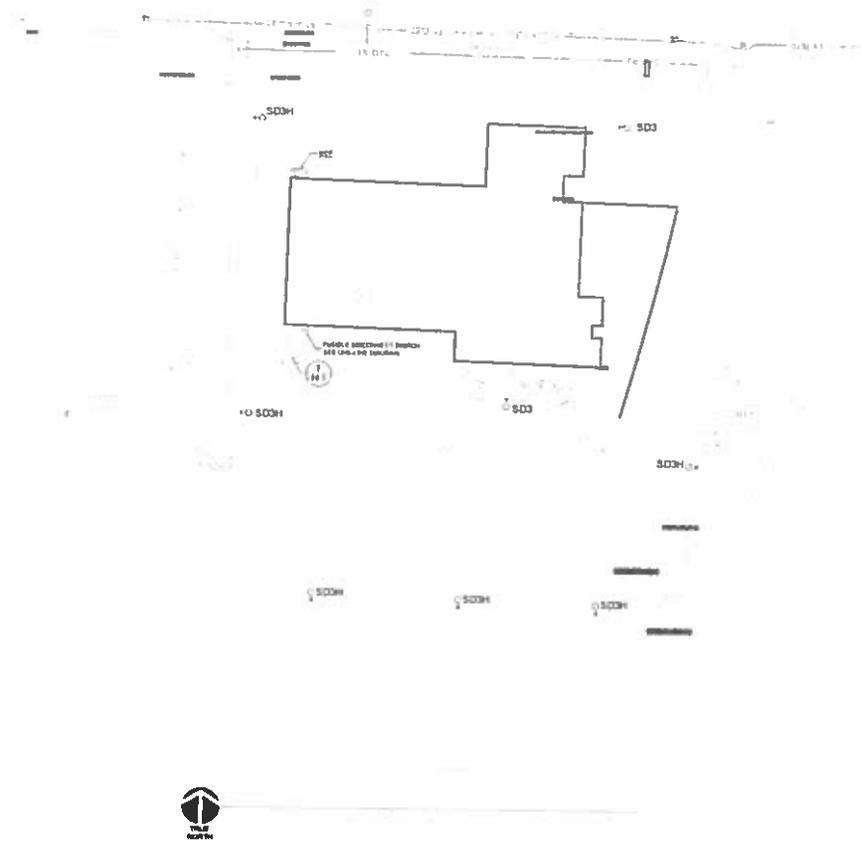
NOV 25 2014

PLANNING DEPT

NEW FIRE STATION #2

City of Medford Fire Department
1107 Stewart Ave Medford OR 97501

Handwritten: "B" 44 of 54



MACKELE
ARCHITECTURE + INTERIORS
PLANNING + ENGINEERING

Page 65

CONSTRUCTION

ELECTRICAL
SITE PLAN

GENERAL NOTES

KEY NOTES

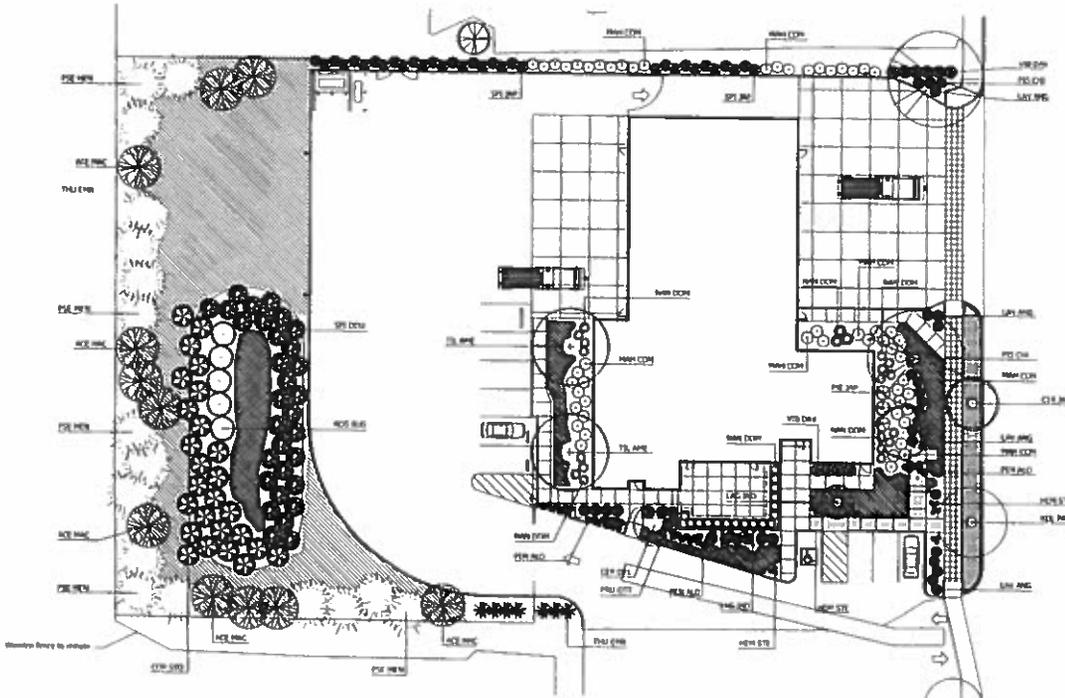
CITY OF MEDFORD

EXHIBIT # _____

File # AC-14-129
"B" 44

E1.0

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LANDSCAPE NOTES

- The irrigation system shall be automatic and have a top loading double check valve installed at the Irrigation Point of Connection. The system shall provide irrigation to 100% of the public and community landscaped area.
- 12"-3/4" bark which shall be applied in all shrub planting areas to a depth of 3 inches. Place clean 1-1/2" round river rock in swale and pond area to a depth of 3". Prior to placing bark and river rock, pre-emergent herbicide shall be applied to all planting areas.
- Preferred Topsoil: Minimum of 18" depth of Topsoil to be imported to the site and applied in all shrub areas.
- Per the City of Medford code 10.780 G.10 a "Soil Volume" each new tree will have a minimum (1) cubic foot of soil volume for each inch. R. of tree canopy at the established mature canopy. Structural Soil will be used in paved areas where topsoil volume is not sufficient.
- Finish topsoil grades at paved surfaces shall be 3" below top of paving. Parking islands shall be slightly crowned to match height of adjacent curbs. Swale and basin finish grades are to relate to Civil Engineer's drawings.
- Per City of Medford Code 10.780 G.8 a "Soil Quality/Restoration Preparation"
 - a. Required Organic Content:
 - Mature compost, or shelled horse, shall be added to the soil of landscape areas at a rate of three cubic yards of compost per 1,000 square feet of landscaping area to be planted. This requirement is not applicable in the following circumstances:
 - In areas fully secured by fencing for the purpose of protecting undisturbed soil from damage and compaction during construction
 - In stormwater quality and detention facilities constructed in accordance with the current adopted version of the Rogue Valley Stormwater Quality Design Manual.
 - In landscaped areas where a soil test demonstrates an organic content of at least three percent based upon core samples taken at one foot per 20,000 square feet, with a minimum of three samples per foot. Samples shall be taken at least 40 feet apart to a depth of six (6) inches following completion of rough grading.
 - Required Soil Preparation Procedures:
 - All areas that soil is substandard dry before beginning.
 - Spread desired amount (Equivalent)
 - Open up soil to a depth of eight (8) inches.
 - Allow organic content as required by Subsection 10.780 G.8(b).
 - Cultivate soil and amendments to a depth of four (4) to six (6) inches.

galbraith
LANDSCAPE ARCHITECTS

LANDSCAPE ARCHITECTS
6000 W. 10TH STREET
MEDFORD, OREGON 97504
PHONE: 541-754-1234
FAX: 541-754-1234

REGISTERED
LANDSCAPE ARCHITECT
254
JAMES L. GALBRAITH
MEDFORD
04/07/00

Handwritten notes: "54", "11' 5 1/2\"", "45' 5 1/2\"", "9' 1/2\"/>

PLANT MATERIAL AND INSTALLATION

- All plant material shall be number 1 grade stock and typical of their location and variety. They shall have well-developed branches, be densely branched when in leaf, and have a well-developed root system. Submit to Owner's Representative, certification of confirmed plant order prior to shipment of plant material. Submit submission requests to Landscape Architect. Botanical names take precedence over common names.
- The numerical plant quantities shown on the drawing are for the Contractor's convenience only. The Contractor is responsible for the quantity of plant material as is graphically shown. Quantities and sizes of plants shall not be altered without written permission from the Owner's Representative.
- Shrub and tree fertilizer shall be a complete, slow release fertilizer with a nitrogen content between 10 and 15; a phosphorus content between 10 and 15; and a potassium content between 8 and 12. Pre-emergent herbicide shall be applied at the manufacturer's recommended rate in all shrub planting areas.
- Each plant shall receive Mycorrhizae inoculant of choice brand such as "Fungi Plus" which is available at: Mycorrhizal Applications, Inc. (541) 475-3065. Apply at manufacturer's recommended rates.
- Planting holes shall be made as wide as rootball size and no deeper than the rootball. Set plants with top of rootball no lower than finish grade after planting. Do not pull bark from under plants, but cut bark and loosen bark from around top of roots.
- All public street trees within 3' of public impervious surface shall have "Shrub" or root control barriers installed per city standard.

PLANT MATERIAL WARRANTY

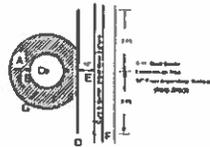
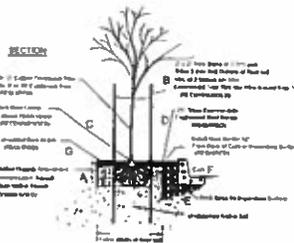
- All plant material and workmanship shall be warranted for one year (12 months) from the date of project final acceptance.
- Plant material under warranty shall be in healthy condition at the end of the warranty period. Any dead or dying plant material within that warranty period shall be replaced by plant of the same species and size.
- The Contractor shall not be responsible for any plant material damaged by vandalism or theft during the warranty period.

PLANT LIST

SYM	BOTANICAL	COMMON	SIZE	QTY.
TREES				
ACE MAC	Acer macrophylla	Big Leaf Maple	2"	11
CER DFL	Cornus Florida	Flowering Dogwood	2"	9
CER JAP	Camellia japonica	Japanese Camellia	2"	2
FDL PAM	Fuchsia paniculata	Garden Bush Tree	2"	1
LAD BUD	Ligustrum lucidum	Shiny Leaf Dogwood	2"	3
PIS CH	Platanus occidentalis	American Sycamore	2"	2
PIS SF N	Platanus sp. Florida	Florida Sycamore	2"	17
TE. GAE	Tilia cordata	American Linden	2"	3
SHRUBS / PERENNIALS				
COH BER	Cornus amomum	Sweet Spice Dogwood	10"	26
HEM STE	Hemerocallis stricta	Star Flower	10"	12
LAV ANG	Lavandula angustifolia	English Lavender	10"	15
MOR CDM	Morone chrysantha	Orange Blossom	10"	24
NAN DOM	Nandina domestica	Sacred Fire	10"	24
PEA BL D	Penstemon digitalis	Blue Bell	10"	20
PEA JAP	Penstemon japonicus	Japanese Bell	10"	13
PRU OTT	Prunella verticillata	Blackberry	10"	13
RUG ICE	Rumex crispus	Winged Rumex	10"	8
SPR DCL	Sparganium angustifolium	Water Plantain	10"	27
SPR JAP	Sparganium japonicum	Japanese Water Plantain	10"	23
THU G BR	Thuja occidentalis	Green Cedar	10"	15
VIB DAY	Viburnum dentatum	Blackhaw	10"	10

GROUNDCOVER

Symbol	Arctostaphylos uva-ursi	Common Laurel	30" plant
Symbol	Rubus coccineus	Strawberry	30" plant
Symbol	Adiantum pedatum	White Fern	18" plant
Symbol	Thymus serpyllifolius	Creeping Thyme	18" plant
Symbol	Thymus serpyllifolius	Creeping Thyme	18" plant



City of Medford Tree Planting Standard - N.T.S. Medford Code 10.780

Planting Plan - Fire Station 2

RECEIVED

FEB 05 2015

PLANNING DEPT.

SCALE: 1"=20'-0"

FIRE STATION #2
1107 Stewart Avenue
Medford, Oregon 97501

8/1/2014

DATE	1470
DRAWN BY	07/20/14
CHECKED BY	08/07/14
DATE	08/07/14
DATE	08/07/14

L1

FIRE STATION 2
Medford, Oregon

RECEIVED
FEB 03 2015
PLANNING DEPT.

ALTERNATE PLANT LIST
(January 27, 2015)

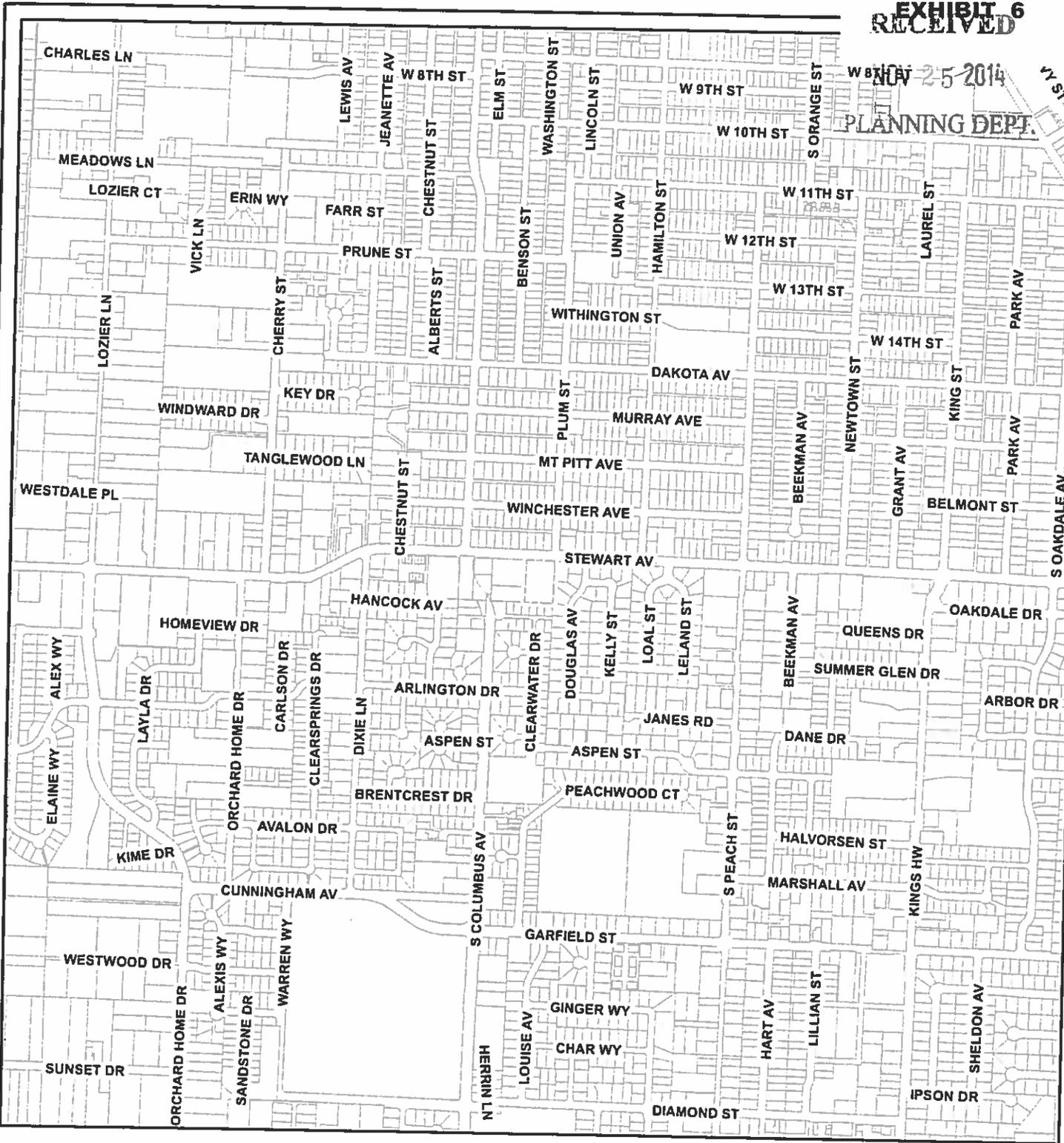
ORIGINAL PLANT	ALTERNATE PLANT
TREES	
Acer macrophylla	Platanus acerifolia 'Bloodgood'
Cercis 'Oklahoma'	Cercis 'Canadensis'
Cercidiphyllum japonicum	Cercis 'Oklahoma'
Koelreuteria paniculata	Pistachia chinensis 'Keith Davies'
Lagerstoemia indica 'Watermelon Red'	Cercis 'Oklahoma'
Pistacia chinensis 'Keith Davies'	Zelkova serrata
Pseudotsuga menzeseii	Pinus ponderosa
Tillia americana	Tillia cordata
SHRUBS / PERENNIALS / VINES	
Cornus sericea 'Isanti'	Cornus sericea 'Flaviramea'
Hemerocallis 'Stella d'Oro'	Hemerocallis 'Happy Returns'
Lavandula angustifolia 'Munstead'	Lavandula angustifolia 'Hidcote'
Mahonia repens	Nandina 'Harbour Dwarf'
Nandina domestica 'Moyers Red'	Nandina domestica 'Plum Passion'
Pennisetum alopecuroides 'Little Bunny'	Iberis sempervirens
Pieris japonica 'Valley Valentine'	Pieris japonica 'Compacta'
Prunus laurocerasus 'Otto Luyken'	Ilex cornuta 'Carissa'
Rosa rugosa	Juncus patens 'Elk Blue'
Spiraea douglasii	Cornus sericea 'Kelseyi'
Spiraea japonica 'Magic Carpet'	Spiraea japonica 'Limemound'
Thuja occidentalis 'Emerald Green'	Prunus lusitanica
Viburnum davidii	Rhododendron 'Dora Amateis'
GROUNDCOVERS / VINES	
Arctostaphylos 'Emerald Carpet'	Arctostaphylos uva ursi 'Massachusetts'
Rubus calycinoides	Arctostaphylos uva ursi
Teucrium chamaedrys 'Prostratum'	Ceanothus gloriosus
Juncus patens 'Elk Blue'	Juncus effusus

(*) – Galbraith and Associates, Inc. does not condone this list exclusively as a source for replacement plant material. The Landscape Architect should be contacted prior to making any plant material substitutions.

"B" 460654

NOV 25 2014

PLANNING DEPT.



Subject FS 2
 Tax Lots

CITY OF MEDFORD

EXHIBIT # 18

Vicinity Map File # 22-14-129
470654

37-2W-36BD-6200

CSA Planning, Ltd.

11-18-2014 Source: CSA Planning, Ltd. Jackson County GIS

1 inch = 1,000 feet

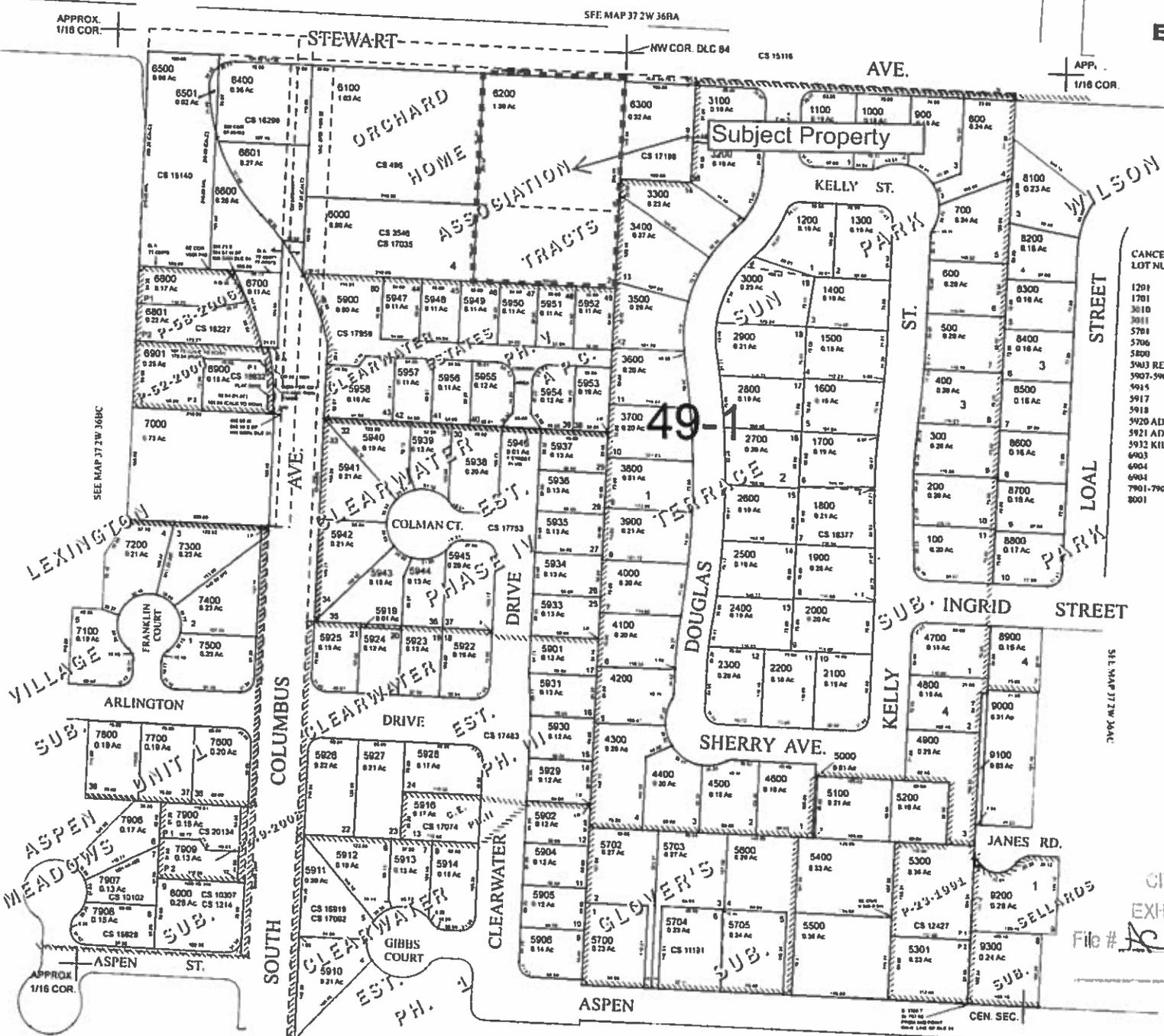
3

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NOV 25 2014

PLANNING DEPT

- CANCELLED TAX LOT NUMBERS:
- 1201
 - 1701
 - 3010
 - 3011
 - 5701
 - 5706
 - 5800
 - 5903 REMAPPED TO 372W36CA
 - 5907-5909 REMAPPED TO 372W36CA
 - 5915
 - 5917
 - 5918
 - 5920 ADDED TO 5923
 - 5921 ADDED TO 5922
 - 5932 KILLED TO STREET
 - 6003
 - 6004
 - 6004
 - 7901-7905 REMAPPED TO 372W36BC
 - 8001

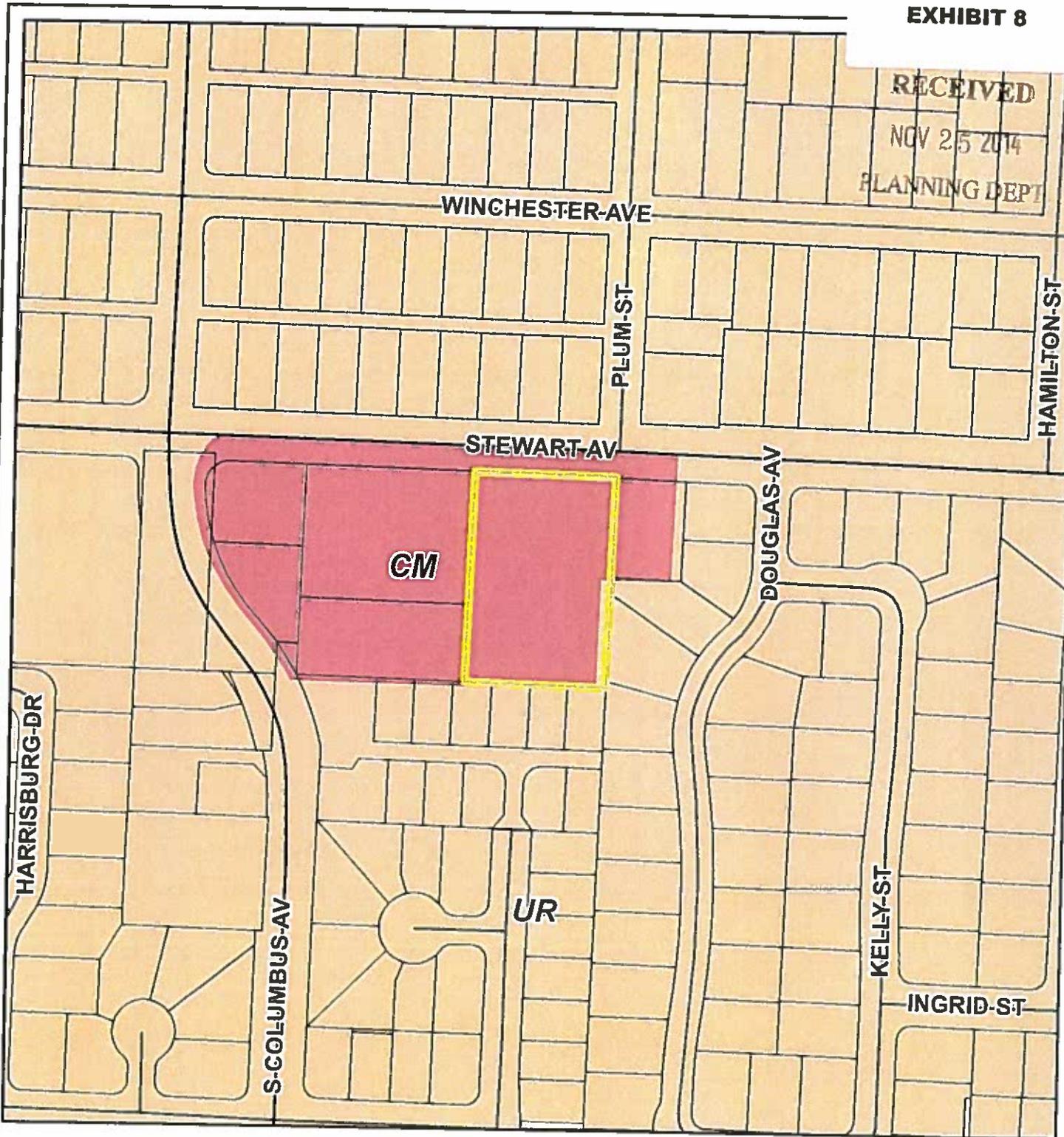


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CITY OF MEDFORD EXHIBIT # _____ File # AC-14-029

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NOV 25 2014
PLANNING DEPT



	Subject Property		Medford GLUP CM
	Tax Lots		UR

General Land Use Plan (GLUP)
 CITY OF MEDFORD
 File # AC-14-129
 49.85/1N
 37-2W-36BD-6200

CSA Planning, Ltd.
 07-30-2014 Source: CSA Planning, Ltd. Jackson County GIS

1 inch = 200 feet



5

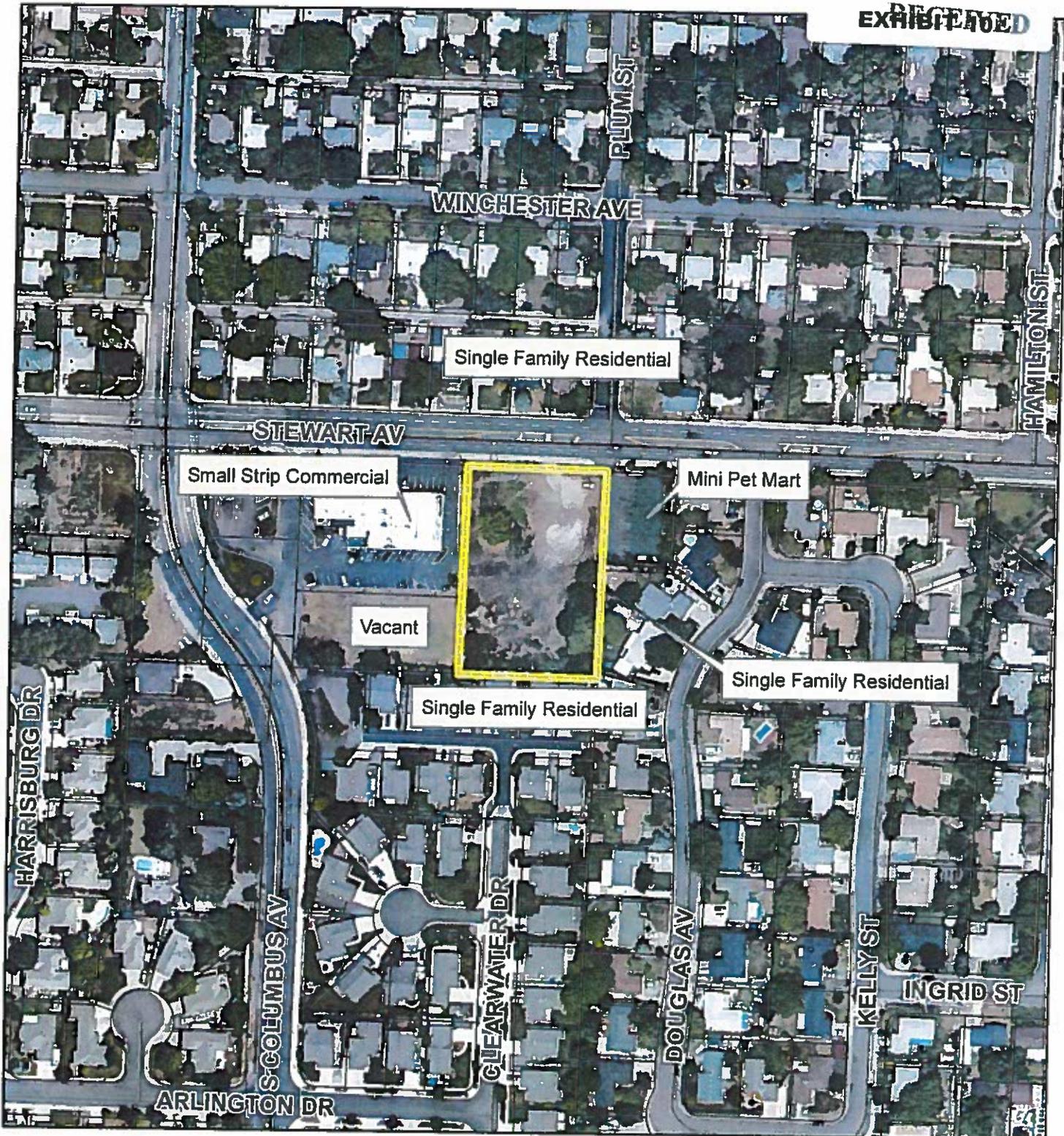


 Subject FS 2
 Tax Lots
 Medford Zoning

Zoning
 37-2W-36BD-6200
 CITY OF MEDFORD
 EXHIBIT # "B"
 File # AC-14-129
 50 of 54

 1 inch = 200 feet
 CSA Planning, Ltd.
 07-30-2014 Source: CSA Planning, Ltd. Jackson County GIS

16



Legend

- Subject Property
- Tax Lots

2012 Aerial

Surrounding Uses

City of Medford
 Proposed Fire Station 2
 37-2W-36BD-6200

CITY OF MEDFORD
 EXHIBIT # 102
 File # AC-14-129
51054



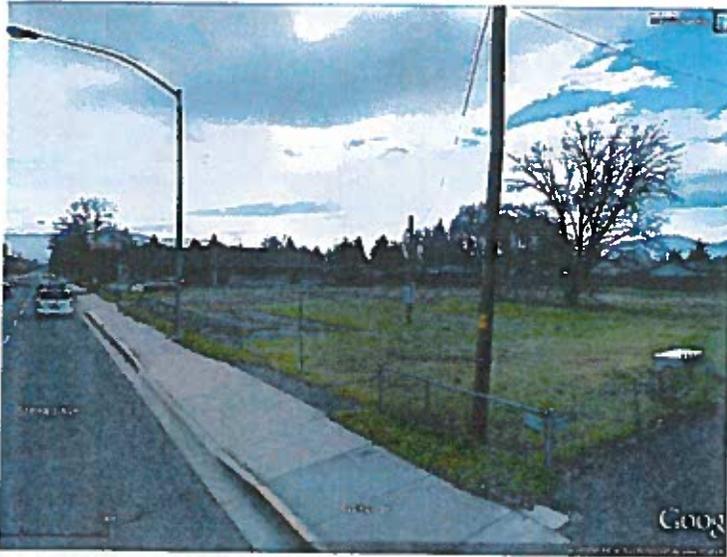
CSA Planning, Ltd.

200 100 0 200 Feet

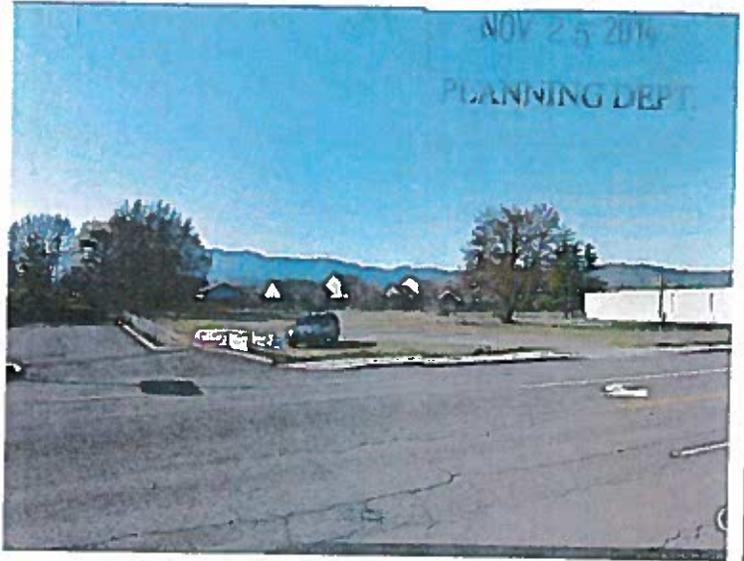


Site Photographs Proposed Fire Station 2

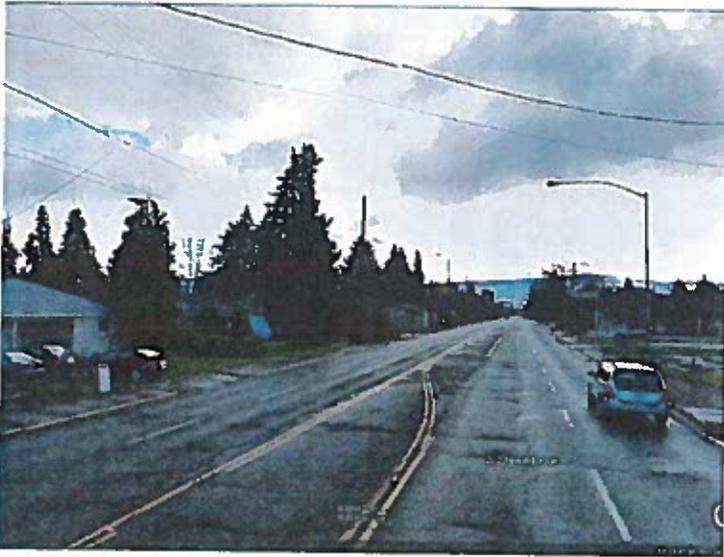
EXHIBIT 11
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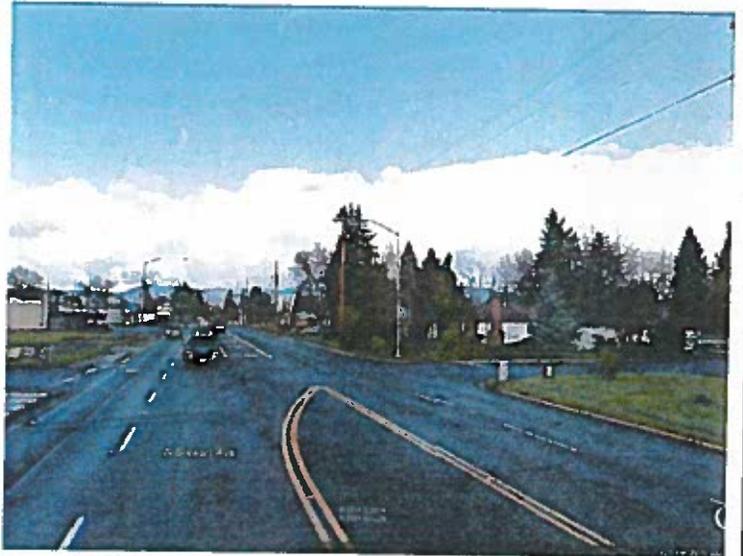
01 Looking Southwest to Site From Stewart Ave.



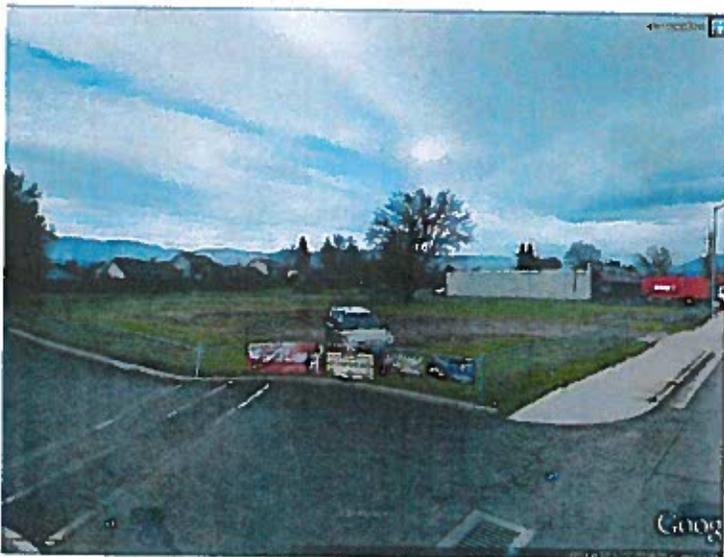
02 Looking Southeast to Site From Stewart Ave.



03 Looking East Down Stewart Ave.; Subject on Right



04 Looking West Down Stewart Ave.; Subject on Left



05 Looking Southwest to Site From Mini Pet Mart



Looking Southwest to Site

AC-11-129 118"
520/91 18



MEDFORD FIRE-RESCUE
INTER-OFFICE MEMORANDUM

RECEIVED

NOV 25 2014

PLANNING DEPT.

TO: Dave Straus
FROM: Justin Bates, Deputy Fire Chief
DATE: November 6, 2014
RE: Fire Department Information Request

Per your request, attached are some answers to questions regarding fire department activities within the new fire stations.

- Description/Explanation of practices and how we respond to emergencies:

Medford Fire-Rescue is a fully career department that has a minimum staffing of 3 firefighters on duty 24 hours a day, 365 days a year. Fire stations are staffed using a rotating 3 shift system (A shift, B shift, C shift) who work 24 hour days. We respond to a variety of emergency calls for service including fire, emergency medical, natural disasters, motor vehicle collisions, etc. Each year we respond to approximately 10,000 calls for service out of our 5 fire stations. We respond to calls for service in full size fire apparatus (fire engines).

- How/When do we use lights/siren:

When responding to calls for service, we typically utilize lights & siren. Normally, personnel are sensitive to siren use and will delay the use of the siren immediately adjacent to the fire station. They will delay the use of the siren until they have moved beyond the immediate area of the fire station.

- How many projected number of employees:

The minimum number of employees at a single fire station is 3 firefighters. The maximum number of employees at a single fire station is 7 firefighters. On average, the typical number of employees expected to be working at a fire station is 3-4 firefighters.



MEDFORD FIRE-RESCUE
INTER-OFFICE MEMORANDUM

- Typical vehicle types & numbers:

Typically we respond to emergencies in a full size fire engine. These apparatus are approximately 35 feet long and weigh 50,000 lbs. Each fire station typically has one primary fire engine responding to emergencies. Occasionally we will have 2 fire engines responding from a single station based on call volume. Each station will also contain one or two reserve fire engines housed in the apparatus bay for back up purposes. These pieces of equipment do not respond to calls for service out of these stations.

I hope this answers all your questions. Let me know if you need any further information.

"B"
54 of 54



Continuous Improvement Customer Service

CITY OF MEDFORD

Date: 3/11/2015
File Number: AC-14-129/E-15-025

PUBLIC WORKS DEPARTMENT STAFF REPORT FIRE STATION #2

Project: Consideration of plans for the construction of a City of Medford Fire Station and associated Exception request seeking relief to maximum driveway width requirements in Medford Land Development Code (MLDC) 10.550.

Location: South side of Stewart Avenue, 450 feet east of Columbus Avenue on a 1.36 acre parcel. 372W36BD TL6200, also known as 1107 Stewart Ave.

Zoning: C-C (Community Commercial) District.

Applicant: City of Medford: Fire-Rescue Department (Greg McKown)

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements. The applicant may provide security for 100% of the improvements prior to issuance of building permits. Construction plans for the improvements would need to be approved by the Public Works Engineering Department prior to acceptance of security.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.

A. STREETS

1. Dedications

Stewart Avenue is classified as a major arterial street, and in accordance with Medford Land Development Code (MLDC) Section 10.428, it requires a total right-of-way width of 100 feet. Prior to issuance of any permit for construction, the developer shall dedicate for public right-of-way, sufficient width of land along the Stewart Avenue frontage of this development to comply with the half width of right-of-way for a major arterial street, which is 50-feet. Based on the Developers Site Plan and other records, it appears that the dedication width to meet this requirement will be 12-feet. (MLDC 10.451).

The developer will receive S.S.D.C. (Street System Development Charge) credits for the public right-of-way dedication per the methodology established by the MLDC 3.815. **Should the developer elect to have the value of the land be determined by an appraisal, a letter to that effect must be submitted to the City Engineer within sixty (60) calendar days of the date of the Final Order of the Site Plan and Architectural Commission. The City will then select an appraiser, and a cash deposit will be required, from the Developer, as stated in Section 3.815.**

In accordance with MLDC, Section 10.471, the property owner shall dedicate a 10 foot Public Utility Easement (PUE) adjacent to the right-of-way line on Stewart Avenue.

The right-of-way and easement dedication shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: the right-of-way and easement dedication; a copy of a current Lot Book Report, Preliminary Title Report, or Title Policy; a mathematical closure report (if applicable), and the Planning Department File Number; for review and City Engineer acceptance signature prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the right-of-way and PUE area.

2. Public Improvements

a. Public Streets

Stewart Avenue is currently constructed in close conformance to major arterial standards with pavement, curb, gutter, sidewalk and street lights. The Developer shall remove the curb, gutter, and sidewalk for the existing driveway and replace it with full height curb, gutter and new sidewalk constructed to City of Medford Standards. If the Developer elects to construct new curb, gutter and sidewalk along the entire street frontage, then a 5 ft. wide pedestrian sidewalk with a 10 foot planter strip is required in accordance to MLDC 10.428. Public improvement plans for this work shall be submitted directly to the Public Works Engineering Department separately from plans submitted for the private improvements to the Building Department. See General Conditions Section 'D' for additional information.

11-11
2 of 7

The Preliminary Utility Plan shows a proposed curb inlet near the east driveway. This will be a public structure and shall be connected to a public main and shown on the public improvement plans.

b. Street Lights

The Developer is proposing to relocate the existing street light. Public improvement plans for this work shall be submitted directly to the Public Works Engineering Department.

The street light is required to be completed and turned on prior to first building permit, or provide security for 100% of construction costs and sign a Land Development Agreement, if a building permit is desired prior to constructing the light.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer shall re-install new signs provided by Medford Public Works Department and paid for by Developer.

c. Pavement Moratoriums

There is a pavement cutting moratorium currently in effect on this section of Stewart Avenue until July 26, 2018.

3. Access and Circulation

Driveway access to the proposed development site shall be limited to two proposed driveways on Stewart Avenue. Public Works supports an Exception for a 48-56 foot wide driveway at the west end of the site on Stewart Avenue. The extra wide driveway is needed to accommodate the three doors, which are in close proximity to each other. The driveway approaches on Stewart Avenue shall have 20 foot curb radii and concrete valley gutters at the throat of each driveway.

This Development shall obtain or provide proof a cross access easement granted from the adjacent parcel to the east. (MLDC 10.550)

If the Developer wants to consider constructing an emergency pre-emption signal at this location, Public Works Department would be supportive. Please coordinate with the Traffic Division.

B. SANITARY SEWERS

This site lies within the Medford sewer service area. However, the Utility Plan indicates that the sanitary sewer (SS) lateral has a proposed connection to the existing SS main on the south side of the building located within an easement on the subject property. This SS main is part of the

11' C'
3' 6" F

Rogue Valley Sewer Services (RVSS) sanitary sewer system. This connection is allowed by the City. Please contact RVSS for information regarding this connection.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development shall be submitted with the building permit application for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

The detention facility for the adjacent property to the east will be impacted and is to be redirected to the proposed stormwater detention and water quality facility constructed with this development. This facility shall be sized accordingly.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater

shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and

11/6"
5067

maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges

Buildings in this development are subject to street, storm drain, sanitary sewer collection and treatment system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Doug Burroughs

*11-11-17
leaf 7*

SUMMARY CONDITIONS OF APPROVAL
City of Medford, Fire Station #2
AC-14-129/E-15-025

A. Streets

1. Street Dedications to the Public:

Dedicate 12' of additional public right-of-way on south side of Stewart Avenue.

2. Improvements:

No additional street improvements are required for this development.
If frontage improvements are elected, public improvement plans for this work shall be submitted directly to the Public Works Engineering Department.

Street light relocation requires public improvement plans submittal.

Pavement moratoriums currently in effect.

Access driveway approach Exception allowed up to 56 feet wide.

B. Sanitary Sewer:

Contact RVSS for information on sanitary sewer connections.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.

11/11
7 of 7



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-14-129 & E-15-025

PARCEL ID: 372W36DB TL 6200

PROJECT: Consideration of site plan and architectural review and related exception from maximum driveway width requirements for a City of Medford Fire Station facility located on a 1.36 acre parcel south of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C (Community Commercial) zoning district; City of Medford Fire-Rescue Department/Parks & Recreation Department, Applicants (Ogden Roemer Wilkerson Architecture, Agent). Desmond McGeough, Planner.

DATE: March 9, 2015

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

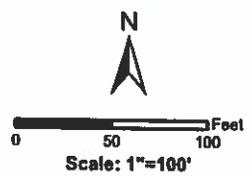
CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Applicants' civil engineer shall coordinate with MWC Engineering Department for required engineered plans for the proposed fire service vault and domestic water meter locations.
4. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required. (See Condition 3)
3. MWC-metered water service does not exist to this property. (See Condition 2 & 3 above)
4. Static water pressure is approximately 65 psi.
5. Access to MWC water lines is available. There is a 14-inch water line located in Stewart Avenue, located on north side of turn median.

CITY OF MEDFORD
EXHIBIT # "9"
File # AC-14-129/E-15-025
10-2



Water Facility Map for CUP-14-129 & E-15-025

"New Fire Dist. No.2 Station"

Legend

- Air Valve
- Sample Station
- Fire Service
- ⊕ Hydrant
- ▲ Reducer
- ⊠ Blow Off
- ⊕ Plug-Caps
- Water Meters:**
 - Active Meter
 - On Well
 - Unknown
 - Vacant
- Water Valves:**
 - Butterfly Valve
 - Gate Valve
 - Tapping Valve
- Water Mains:**
 - Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line
- Boundaries:**
 - ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots
- MWC Facilities:**
 - C Control Station
 - P Pump Station
 - R Reservoir



This map is based on a digital dataset provided by Medford Water Commission. It is not a substitute for a field inspection. Medford Water Commission is not responsible for any errors or omissions in this map. For more information, contact Medford Water Commission at 541-426-2200.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - PLANNING

To: Desmond McGeough

LD Meeting Date: 03/11/2015

From: Greg Kleinberg

Report Prepared: 02/27/2015

Applicant: Medford Fire-Rescue, Parks & Rec (Ogden Roemer Wilkerson Architecture,

File #: AC - 14 - 129

Associated File #'s: E - 15 - 25

Site Name/Description: New Fire Station 2

Consideration of site plan and architectural review and related exception from maximum driveway width requirements for a City of Medford Fire Station facility located on a 1.36 acre parcel south of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C (Community Commercial) zoning district; City of Medford Fire-Rescue Department/Parks & Recreation Department, Applicants (Ogden Roemer Wilkerson Architecture, Agent). Desmond McGeough, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
----------------------------	-----------

Requirement FIRE HYDRANTS

OFC 508.5

Fire hydrants with reflectors will be required for this project.

Hydrant locations shall be as follows: One fire hydrant is required at the West corner of the property on Stewart Ave.

The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Plans and specifications for fire hydrant system shall be submitted to Medford Fire Department for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).

Requirement PARKING RESTRICT.-COMMERCIAL AREAS (PAINTED CURBS)

OFC D103.6

For the driveway entrance into the back of the fire station: Curbs shall be painted red along the entire distance of the fire department access. Minimum 4" white letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals.

Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12).

This restriction shall be recorded on the property deed as a requirement for future construction.

CITY OF MEDFORD
EXHIBIT # WE
File # AC-14-129/E-15-025
1052

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

"E"
2022



Memo

To: Desmond McGeough, Planner, Planning Department
From: Tanner Fairington, Building Department
CC: City of Medford Fire-Rescue Department / Parks & Recreation Department, Applicant
Ogden Roemer Wilkerson Architecture, Agent
Date: March 11, 2015
Re: March 11, 2015 LDC Meeting: AC-14-129/E-15-025

Please Note:

This is not a plan review. These are general notes based on general information provided. Plans need to be submitted and will be reviewed by a commercial plans examiner to determine if there are any other requirements for this occupancy type.

Fees are based on valuation. Please contact the front counter for estimated fees.

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Code and Design Information" on left side of screen; click on "Design Criteria"; and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" on left side of screen for information.

General Comments:

3. The comments below are based on the site plans and elevations submitted. When final plans are submitted, and building information is provided, a more thorough review can be provided.

Site Plan

4. Accessible parking spaces to be per section 1106.7 of the 2014 OSSC, including figures referenced (see Building Codes Division for 2014 OSSC Errata which included these figures, dated October 16, 2014). Locations appear to be acceptable.
5. Please provide sufficient curb and sidewalk spot elevations, and sidewalk slopes, to review accessible route from accessible parking locations to main entrance.

CITY OF MEDFORD
EXHIBIT # "F"
File # AC-14-129/E-15-025
1 of 1

Desmond M. McGeough

From: MOREHOUSE Donald <Donald.MOREHOUSE@odot.state.or.us>
Sent: Tuesday, March 17, 2015 12:38 PM
To: Desmond M. McGeough
Subject: AC-14-129/E-15-025

Desmond,

Thank you for sending agency notice of an application for a site plan and architectural review and related exception from maximum driveway width requirements for a City of Medford Fire Station facility located on a 1.36 acre parcel south of Stewart Avenue, approximately 450 feet east of Columbus Avenue, within a C-C (Community Commercial) zoning district. We reviewed this and determined that it would not significantly affect state transportation facilities under the State Transportation Planning Rule (OAR 660-012-0060) or State Access Management Rule (OAR 734-051-000). We have no further comments at this time.

Don Morehouse
Senior Transportation Planner
ODOT Region 3, District 8 (Rogue Valley Tech Center)
Ph: (541) 774-6399
Fax: (541) 774-6349
Donald.Morehouse@odot.state.or.us

CITY OF MEDFORD
EXHIBIT # 4-C
FILE # AC-14-129 E-15-025
1 of 1

MEDFORD PARKS & RECREATION

HEALTHY LIVES. HAPPY PEOPLE. STRONG COMMUNITY.



CITY OF MEDFORD Interoffice Memo

TO: Planning Department

FROM: Tim Stevens- Park Maintenance Supervisor

SUBJECT: LANDSCAPE REVIEW OF FILE AC-14-129, MEDFORD FIRE-RESCUE DEPT./PARKS AND RECREATION DEPT.

DATE: March 11, 2015

As per City of Medford Code 10.797 Street Frontage Landscaping Requirements, two additional trees are needed along Steward Ave. to meet requirements. Varieties of these trees should be chosen from the City of Medford Selected Street Tree List.

This report addresses horticultural concerns only. Applicant shall comply with all aspects of Medford Code 10.780 Interpretation of the Medford Code will be per the Planning Department. Aesthetic considerations will be per the Site Plan and Architectural Review Commission or Planning Commission upon their review.

CITY OF MEDFORD
EXHIBIT # "H"
File # AC-15



CONTINUOUS IMPROVEMENT | CUSTOMER SERVICE

701 N COLUMBUS AVE | MEDFORD, OR 97501 | 541.774.2400
WWW.PLAYMEDFORD.COM | PARKS@CITYOFMEDFORD.COM



COMMUNITY ENRICHMENT EXCELLENCE EXCEPTIONAL CUSTOMER SERVICE INNOVATION



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 7502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

March 5, 2015

City of Medford Planning Department
200 South Ivy Street, Lausman
Annex, Room 240
Medford, OR 97501

RE: AC-14-129, Medford Fire, Ref E-15-025 (372W36BD, TL 6200)
ATTN: Desmond McGeough,

An existing 10 inch sewer main transects the property within a 10 foot easement just south of the proposed structure. The proposed site plan includes a steel gate and fence which will restrict RVSS 24 hour access to our existing manholes on the subject property.

There is an existing sewer service lateral to the property from the above described main line with adequate capacity to serve the proposed development. Connection to this lateral will require a permit from the City of Medford.

Rogue Valley Sewer Services requests the following conditions apply to this planning action:

- RVSS shall have 24 hour unrestricted access inside of the steel gate. Access inside the fenced area shall be available 24 hours per day within 30 minutes' notice to Medford Fire.
- System Development Charges will be due to RVSS for the proposed structure. Submit a detailed plumbing plan to RVSS for the determination of the SDC fees owed for this development.

Sincerely,

Wade Denny, PE

Digitally signed by Wade Denny, PE
DN: cn=Wade Denny, PE, o=Rogue Valley Sewer Services,
ou=District Engineer, email=wadenny@rvss.us, c=US
Date: 2015.03.05 13:02:41 -0800

Wade Denny, P.E.
District Engineer

CITY OF MEDFORD
EXHIBIT # 188
File # AC-14-129/E-15-025
188

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Vicinity Map

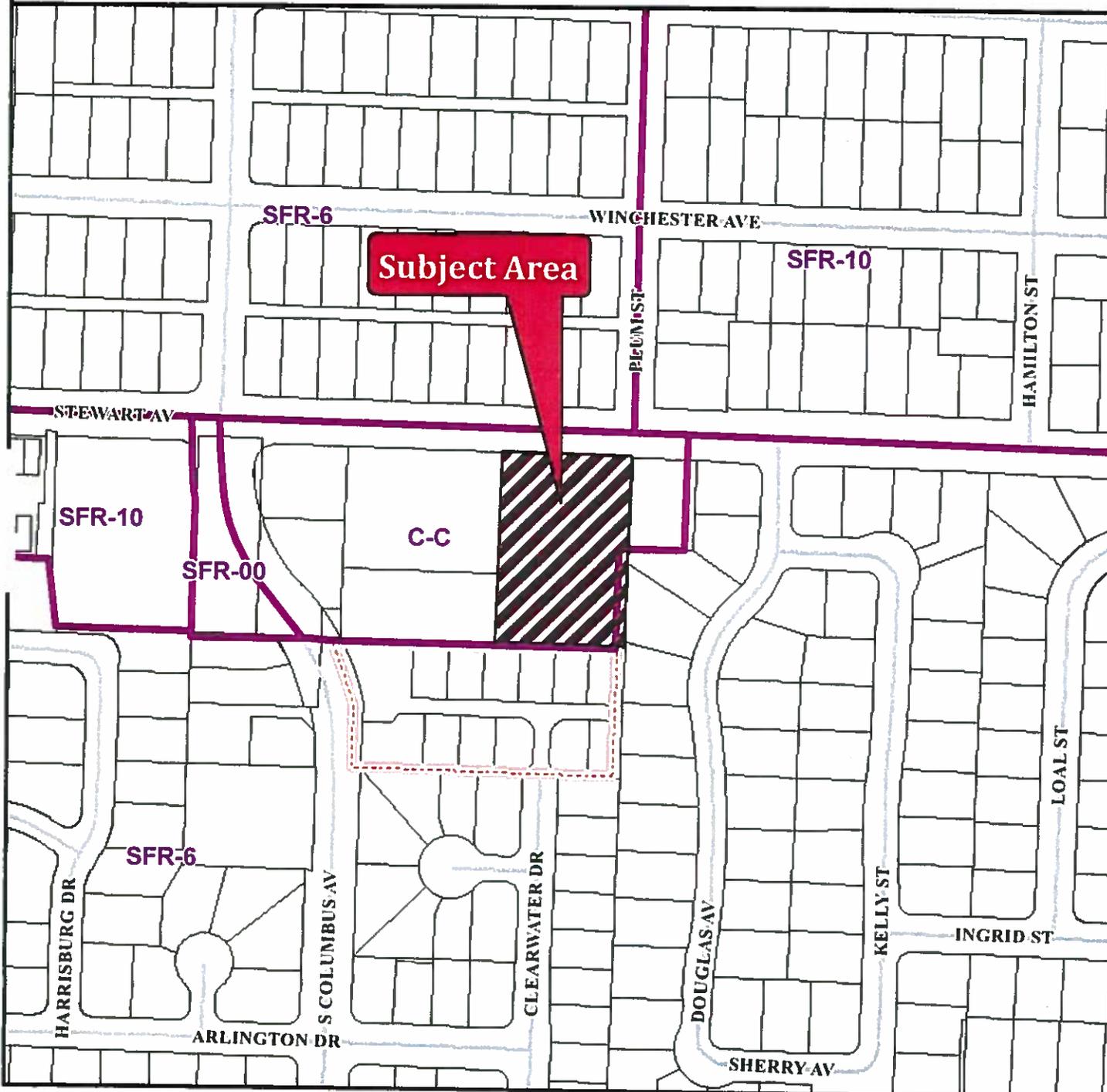
Application Name/Description:
Fire Station 2

Proposal:
New fire station in C-C zone & exception for relief from max driveway width

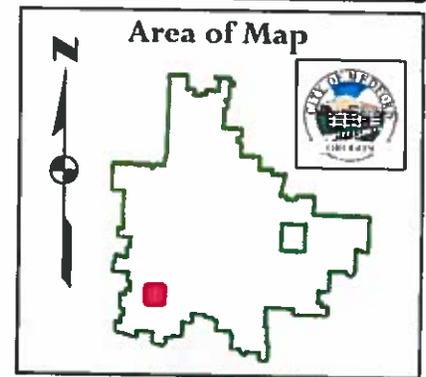
File Numbers:
AC-14-129 & E-15-025

Applicant:
Greg McKown / City of Medford: Fire-Rescue Department

Map/Taxlot:
372W36BD TL 6200



-  Subject Area
-  Medford Zoning
-  UGB
-  Tax Lots
-  City Limits
-  PUD



02/23/2015



CITY OF MEDFORD

PLANNING DEPARTMENT

STAFF REPORT

Date: March 27, 2015 for April 3, 2015 Commission Meeting

To: Site Plan and Architectural Commission

From: Desmond McGeough, Planner II *[Signature]*

Reviewed By: Kelly Akin, Principal Planner *[Signature]*

Subject: Sky Park Residential Development (AC-15-007/ E-15-009)
Sky Park Medford LLC, Applicant
(Mark McKechnie, Oregon Architecture, Inc., Agent)

BACKGROUND

Proposal

Consideration of plans and associated Exception request seeking relief from required parking standards for 26 residential dwelling units upon a deck over a 0.688 acre portion of the Medford Urban Renewal Agency parking lot facility located on the northwest corner of S. Central Avenue and E. 10th Street within a C-C/C-B (Community Commercial – Central Business District Overlay) zone district.

Subject Site Zoning, GLUP Designation, and Existing Uses

Zone: C-C / CB
GLUP Designation: CC (City Center)
Existing Use: MURA Parking Lot

Surrounding Property Zoning and Uses

North: C-C / CB
Use: MURA Parking Lot / Retail

South: C-C / CB
Use: Retail

East: C-C / CB
Use: Library

"Working with the Community to Shape a Vibrant and Exceptional City"

West: C-H /CB (Heavy Commercial with Central Business District Overlay)
Use: Dance Studio, Auto Glass Repair, MURA Parking Lot

Applicable Criteria

10.290 SITE PLAN AND ARCHITECTURAL REVIEW APPROVAL CRITERIA

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and*
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

10.253 CRITERIA FOR EXCEPTION

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority (Planning Commission/Site Plan and Architectural Commission) having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

- (1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The Planning Commission/Site Plan and Architectural Commission shall have the authority to impose conditions to assure that this criterion is met.*
- (2) The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*
- (3) There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.*
- (4) The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be*

suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.

ISSUES/ANALYSIS

Scope of Project/ Background

The subject application is for a new building for the Sky Park residential development. The applicant seeks approval to construct a total of 26 one, two and three bedroom residential dwelling units. The proposed development is located upon a raised deck that spans over a Medford Urban Renewal Agency parking lot facility, located on northwest corner of 10th Street and Central Avenue, across the street from the public library. The parking lot, known as "Central A" and is managed by the City of Medford, currently provides public parking for 94 vehicles. Seventy-Five of spaces located in "Central A" are located under the proposed structure.

On September 23, 2014, the Developer, the City of Medford and Medford Urban Renewal Agency entered into an Agreement for Disposition and Development of Property (DDA). Within the agreement, MURA has agreed to sell the air rights above the entire property area. The developer will lease, for a period of 50 years, the ground floor area required for the elevator lobby, egress, stairs, trash enclosure, utility metering room and deck support columns. Per the DDA, the developer will submit application for building permit within six months after receiving a final order for approval and commence construction within one year after receiving a building permit.

As part of a Lease Agreement between the Developer and MURA, approved by the City Council on January 15, 2015, the City has agreed to provide 26 reserved spaces within the "Central A" lot under the building. These spaces will be available for the exclusive lease to owners of Sky Park dwelling units. The remaining 47 spaces under the building will be maintained as parking spaces for general public use. A total of 19 spaces in the portion of the "Central A" parking facility not under the structure to the west will be available for tenants and guest. Permits for these spaces may be purchased for a one week period, however a person can purchase up to four weeks of permits in advance.

Overlays

The subject parcel is within the Central Business District Overlay Zone. Section 10.358 of the Medford Land Development Code notes that the purpose of the overlay zone is to "*recognize the unique and historic character of the downtown area as an asset to the community and provide standards necessary for continued development and redevelopment.*" The section further identifies that all commercial development standards, with exception of parking provisions on vacant sites, are waived. Therefore, site plan conformance to MLDC provisions typically presented in staff review, such as setbacks, lot coverage and building height, do not apply to the subject proposal.

Site Plan (Exhibit B)

The subject site is bounded by 10th Street on trash enclosure. Existing the south, Riverside Avenue on the east, 9th Street on the north, and a public alley on the west. The footprint of deck area encompasses 30,000 square feet, which is 100% of the parcel. The parking garage below will generally remain in its current configuration with slight changes to accommodate a landscape buffers between the sidewalk and parking lot area will also remain in place as will sidewalk furniture and street trees. A staircase and elevator are proposed for the northeast corner of the site and a utility room and staircase are proposed for the southeast corner of the site. Both of these ground floor additions are outside of the current parking lot area in an existing sidewalk plaza area.

Exception Request/Parking

The applicant has requested an exception to required parking for the residential units. The MLDC typically requires two parking spaces for each single-family unit. Parking provisions are applicable to residential development in the CB Zoning District Overlay, though there are special provisions pertaining commercial and industrial development located within the Parking Overlay District.

The MLDC table for required parking (Table 10.743-1) contains a column that provides parking standards that are particular to the CB Overlay Zone District. The column heading notes that the standards are applicable to the CB Overlay, "*outside of the Downtown Parking District*". The double asterisks in the column provides reference to the note at the bottom of the table defining the physical boundaries of the Parking District. The subject site is in the Parking District.

When considering the text of Section 10.358 and 10.743 as a whole, it is evident that off-street parking must be provided for residential uses in the CB zoning overlay district, even when such development is located within the parking district. Section 10.358(1)(a) provides off-street parking and loading standards for Commercial and Industrial Development located within the CB Overlay District and notes that development of vacant parcels are subject to off street parking requirements, "*except when located in the parking district.*" However, this section is only applicable commercial and industrial development within the CB Zoning District Overlay, residential development is not discussed within this section, it is identified in a subsequent subsection.

When considering section 10.743(1) of the MLDC (Off-Street Parking Standards), it specifically excludes residential use from the parking district provision of having no requirement for minimum number of off street parking spaces. Section 10.731(1) provides the following:

"For non-residential uses, there is no minimum number of off-street parking spaces required in the Downtown Parking District, per Section 10.358(1)(a); ..."

Based on the express language provided in the MLDC, staff finds that off-street parking requirements of the CB Overlay District are full in effect when considering residential development within the Parking District Overlay. Even though the subject units are all attached, based on the code definition of Multi-Family Residential, the dwelling units would be considered single family residential units. Therefore, the number of off-street parking spaces required for each unit is two, thus the total number of parking spaces needed for the project is 52 stalls.

Per the applicant's narrative, an owner of unit within the development may opt to lease one of the 26 "reserved" spaces under the structure, on a per annual basis. A permit may also be obtained for any of the 19 spaces located to the west of the structure within the "Central A" parking lot. Permits for this area may only be purchased only for a one week period; however a person can purchase a permit up to four weeks of permits in advance. Thus, 26 of the 52 required off-street parking spaces are available for the exclusive use to residents of the development. As such, the development seeks relief from the parking section of the Land Development Code as to provide one exclusive space per unit within the parking lot rather than two.

Staff would like to note, per the definition of the provided in the MLDC, these units are considered single-family dwellings since each one of is considered a separate structure. However, this development of one, two and three bedroom units functions no differently than a garden style apartment building that has multiple units in one single structure. The required parking for a multi-family unit is 1.5 spaces per unit. It could be reasonably argued that required parking for this development should be considered under the multi-family dwelling standard rather than a single family standard. If this project was considered under the multi-family dwelling unit standard, the required parking for this development would only be 39 parking spaces.

The applicant's Exception findings provide rationale for the parking reduction. The applicant notes that one reserved space is sufficient for the development, given its central location within walking distance to most downtown features and nearby transit routes. Additionally, like commercial uses within the Downtown Parking District, owners in the development will also be contributing to the maintenance of the MURA parking lots and structures within the district. Residents of the development will have access to any space within the district and will be able to reserve a space on an annual basis for a fee under the structure. Given the amount of parking in the area, the applicant contends there is adequate parking for the development. Given the lease agreement with MURA secures and makes available the 26 parking spaces for lease exclusively for this development, staff concurs with the Applicant's findings that parking needs for the development are adequately met. Discussion regarding the Exception approval criteria will be discussed further below in this report.

Elevations and Materials

The townhouse structure building mass has been designed to provide depth and visual variety through offsetting layers and varying roof lines. The structure includes a variety of facade treatments including Craftsman style, Spanish mission style and traditional brownstone character. Several architectural materials are proposed which are consistent with these varying archetypes including: hardie board siding, eldorado stone, brick and stucco. Many of the units also include a small standing terrace extension to provide a small outdoor space overlooking the streetside. The structure has been designed to maintain a 12-foot clearance for the parking lot should MURA seek to redevelop ground floor area as retail or office space in the future by replacing the existing parking stalls. The proposed structure is enhanced by a monumental stair case located at the corner of Riverside Avenue and 9th Street which terminates within a tower feature and entry provides entry onto the second story courtyard deck.

Discussion

During the Land Development Committee Meeting with the applicant, staff commended the overall architectural direction of the Sky Park development and the project as a whole. The perspective rendering provided for the Riverside Avenue side gives a strong sense of the architectural layering and diversity of elements. Staff has encouraged the applicant to provide additional architectural elements on the courtyard living side of development to create the same sense of architectural flavor and liveliness as the street frontage and requested additional consideration to architectural elements on the alley side.

Architectural enhancements suggested by staff that can enhance the thematic character of the development upon the interior living side included:

- Window awnings
- Spanish style wrought iron railings
- Divided light windows consistent with the architectural style
- Top and bottom window lintels
- Full (four sided) window architectural popouts
- Enhancements to the porch support posts
- Railings defining personal outdoor space
- Cornice molding at roof edge

On the rear of the building, the building plane undulates, which provides an element of depth to the structure: however, staff has encouraged the Applicant to incorporate the use scoring articulation around windows to provide additional interest on the rear side. Awnings and other architectural projections, other than where currently shown on the plans, cannot be added as certain sections of the structure already approach the property line at the alleyway.

On the front side of the building, staff has suggested that one of the unit walls extend down to grade as to further ground the structure and provide additional mass toward the foundation.

The applicant's architect has informed staff that they are considering recommendations provided and look to submit an update to the application elevations. However, with a short turnaround time between the Land Development Committee meeting and publication, to date staff has not received revised elevations. Should updated drawings be provided prior to the meeting, staff will forward those to the Commission.

Open Space/Landscaping

The open courtyard garden is a private out door area proposed to have potted trees and street furniture to provide an open space area for the occupants of the building. There are no additional landscape plantings or landscape area proposed for this development. However, there is an existing five foot landscaped buffer located between the existing parking lot and urban cross section sidewalk along Riverside Avenue, 9th Street and 10th Street. All landscaping within the landscape planter buffer, along with the existing street trees within the side walk are proposed to remain.

Signage

Proposed signage is not a part of this review. Staff notes that the project name is proposed to be located on the tower element at the corner of 9th Street and Riverside.

Concealments

It is not clear from the elevations or narrative of where the HVAC equipment will be placed or its method of concealment. A box structure appears on the roof of each unit but it is unclear if this is equipment, a parapet wall or some other screening device, or perhaps fireplace ventilation. Thus, staff has included a Code condition of approval citing Section 10.782, Concealment of HVAC Equipment and Roof Mounted Wireless Communication Facilities. Such concealment should be accomplished using architectural elements of the building.

The subject site plan identifies a trash enclosure on the north side of the parking area under the building with swinging doors. It is not clear from the elevations or narrative what the materials are utilized for the enclosure or door. Based on the line weight, it appears that it is CMU block construction. As such, staff has included a Code condition of approval citing Section 10.781, Concealment of Trash Receptacles. The enclosure shall be made of solid wood, metal, brick or masonry block with solid wood or metal doors.

Lighting

Per the Public Works Report (Exhibit M), the developer shall protect and preserve all existing street lighting, power supply and appurtenances. No additional street lighting is required for this project. The applicant's narrative notes exterior lighting on the deck will be of fixtures that cast no light above the horizontal plane. Fixtures will not extend above roof ridges of the units so no glare will be produced outward onto adjacent properties.

Utilities

The applicant's Site Plan Narrative notes that the parking lot was constructed, a storm water system was developed, which included on-site drainage basins. The applicant proposes to collect the same water on the roof and use it for irrigation of deck plants and direct the balance into the existing system. The Public Works Report notes that a comprehensive drainage plan will be needed and that storm water detention facilities in accordance with Section 10.481 and 10.729 will be required. A condition of approval has been included requiring the applicant to comply with the Public Works Report, dated March 18, 2015. (Exhibit M)

The site is located within the City of Medford sewer service area. The Public Works report indicates that the plan does not clearly indicate the location of building connections to the existing sanitary sewer main. The developer shall ensure that the connection is to an existing sewer main as a separate individual lateral or make a new connection to an existing main with a separate lateral. A condition of approval has been included requiring the applicant to comply with the Public Works Report, dated March 18, 2015. (Exhibit M)

The Medford Water Commission Report notes that off-site or on site water line installation is not required. The water facility design and construction will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices." All parcels/lots of proposed property divisions will be required to have metered water service. Installation of an MWC approved backflow device is required for this development. A condition of approval has been included requiring the applicant to comply with the Medford Water Commission Report, dated March 16, 2015. (Exhibit N)

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS OF FACT

10.290 Site Plan and Architectural Review Criteria.

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

(1) The proposed development is compatible with uses and development that exist on adjacent land; and...

Approval Criterion 1

The Applicant's Site Plan Narrative (Exhibit C) and site plan exhibits thoroughly document the structure's compatibility with adjacent uses and development, and with the general purpose and intent pertaining to the Central Business District Zoning Overlay. Section 10.358 of the Municipal Code notes that intent of the C-B district is to "recognize the unique and historic character of the downtown area as an asset to the community". It further recognizes that the C-B overlay is intended to provide standards and criteria necessary for continued development and redevelopment as a vital part of the community. The Site Plan and Architectural Commission can find that this proposal is consistent with these objectives and meets Criterion One.

Approval Criterion 2

10.290 Site Plan and Architectural Review Criteria.

(2) *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

Exhibit A contains Conditions of Approval which the applicant shall satisfy. Subject to the Commission approval of the requested exception regarding the request to reduce the number of parking spaces that are of the exclusive use of the development, the proposed development complies with all applicable provisions of the Code. The Site Plan and Architectural Commission can find that the proposal meets Approval Criterion Two.

Pertaining to the applicant's requests for exception, Staff recommends that the Commission adopt the applicant's findings (Exhibit D) as presented and the findings provided by staff below.

10.253 Criteria for an Exception

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority (Planning Commission/Site Plan and Architectural Commission) having jurisdiction over the plan authorization unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

(1) *The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The Planning Commission/Site Plan and Architectural Commission shall have the authority to impose conditions to assure that this criterion is met.*

Exception Criteria 1

Regarding the request to reduce off-street parking, the existing parking lot facility was constructed as part of the City's efforts to revitalize downtown. The subject request to reduce parking should have limited impact to the Parking District area as a whole. If considered as multi-family residential the project would require 39 spaces. If considered single family dwellings, the project needs 52 spaces. The applicant's findings note that one reserved space per unit is sufficient for the development being within walking distance to most downtown features and nearby transit hub. Additionally, like commercial uses within the Downtown Parking District, owners in the development will also be contributing to the maintenance of the MURA parking lots and structures within the district. A resident of the development will be able to reserve a one space on an annual basis for a fee under the structure. Additional parking needs of a tenant can be accommodated with purchase of weekly permits upon the 19 spaces west of the structure or through paid meter parking within other nearby MURA parking facilities. Given the amount of off-street parking available in the in the area, the applicant contends that the proposed number of spaces for the development is adequate. The Commission can find that Exception Criteria 1 is met.

Criterion 1 is met.

- (2) *The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*

The requested exceptions pertain to parking requirements of the Medford Land Development Code. Additionally it can be found this proposal supports continued development and redevelopment of downtown as a vital part of the overall community. Granting of this exception does not establish a use that is not permitted in the zone district. The Commission can find that Exception Criterion 2 is met.

Criterion 2 is met.

- (3) *There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.*

Staff concurs with the applicant's findings in that there are unique circumstances in that apply to the subject property given its central location to many vital resources and transit lines. Given the abundant amount of available off-street downtown parking that may be accessed through either weekly parking permit or parking meter, one exclusive off street parking space per unit should be sufficient to meet parking demands of the development. Strict application of the standard would result peculiar hardship on the development. The project seeks to further downtown redevelopment efforts and enhance the vitality of the central business district through the infusion of residential dwelling units. However, strict application of the parking standard for this development would result in the reduction of spaces available to patrons of

downtown businesses. The applicant's narrative indicates the concern to parking spaces available to business patrons was a significant factor in the development of the agreement between the MURA and the developer. The Commission can find that Exception Criterion 3 is met.

Criterion 3 is met.

- (4) *The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.*

The need for the Exception is not the result of an illegal act and is suffered by the particular property in question given the specific use. The need for the exception results only from the MURAs desire to enhance redevelopment efforts downtown though the infusion of new residential development. Approval of the exception request will not result in a profit by the developer of the proposed project. The Commission can find that Exception Criterion 4 is met.

Criterion 4 is met.

RECOMMENDATION

Direct Staff to prepare a final Adopt the Final Order for Approval of AC-15-007/E-15-009, per the Staff Report dated March 27, 2015, including all Exhibits A through Q.

EXHIBITS

- A Conditions of Approval dated March 27, 2015;
- B Site Plan, received March 23, 2015;
- C Applicant's Site Plan Narrative & Code Compliance Form, received January 21 2015;
- D Applicant's Findings of Fact for Exception Request, received January 27, 2015;
- E Floor Plan, received January 21, 2015;
- F Roof Plan, received January 21, 2015;
- G Exterior Elevations, received January 21, 2015;
- H Interior (Courtyard) Elevations, received January 21, 2015;
- I Exterior Elevation Perspective, received January 21, 2015;
- J Renderings of Exterior Elevations, received January 21, 2015; ;
- K Existing Landscape Plan, received January 21, 2015;
- L Conceptual Site Drainage Plan, received January 21, 2015;
- M Public Works Department Staff Report, dated March 18, 2015;
- N Medford Water Commission Memorandum dated March 16, 2015;
- O Fire Department Report, prepared March 10, 2015;
- P Medford Building Department Staff Memo, dated March 17, 2015;

- Q Lease Agreement between Medford Urban Renewal Agency and Sky Park Medford LLC., approved January 15, 2015.
Vicinity Map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA: April 3, 2015

EXHIBIT A

Sky Park Site Plan
AC-15-007/ E-15-009
Conditions of Approval
March 27, 2015

CODE CONDITIONS

1. The applicant shall:
 - a. Comply with the Public Works Staff Report dated March 18, 2015(Exhibit M);
 - b. Comply with the Medford Water Commission Staff Memo dated March 16, 2015(Exhibit N).
 - c. Comply with the Medford Fire Department Report, prepared March 10, 2015. (Exhibit O).

2. On the site plan submitted for building permit, the applicant shall provide a detail of the trash receptacle enclosure that demonstrates compliance with Section 10.781 of the Medford Land Development Code.

3. On the site plan submitted for building permit, the applicant shall demonstrate compliance with Section 10.782 of the Medford Land Development Code. All HVAC equipment and roof-mounted wireless communication facilities shall be concealed from view through architectural elements or free standing site obscuring walls.

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EXHIBIT # A
File # AC-15-007 | E-15-009
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PROJECT NARRATIVE FOR THE SKYPARK MEDFORD DEVELOPMENT

January 20, 2015

PROJECT DESCRIPTION:

The City of Medford Urban Renewal Agency has agreed to sell the all airspace from 12 feet above finish grade above its Central A parking lot to Sky Park Medford, LLC. It has also agreed to lease the ground area of that same parking lot required to support a structure to Sky Park Medford LLC. The Sky Park Medford project will occupy the half city block bounded by 9th Street on the north, 10th Street on the south, Central on the east and the mid-block alley on the west, which is slightly smaller than the Central A parking lot.

The proposed building will feature an internal open air court reminiscent of the garden apartments popular in the 1930s and 1940s. The court will have hard surface paving, as well as areas for trees and plantings. All of this will be on a solid deck 12 feet above the parking lot surface.

Sky Park proposes to construct a concrete deck supported on steel beams and posts and then construct 26 one and two story dwelling units on top of the deck. The deck level will be accessed via elevator and stairs. Access will be via secure card key. The parking lot will remain as a public parking lot owned and operated by the City of Medford once the project is complete. Except for the grand public entry stair, the ground floor lobby area, a trash enclosure, an enclosure to house utility meters and support columns, the ground level is generally expected to remain as it is, although the parking spaces will be reconfigured. In all, Sky Park will occupy approximately 1,000 square feet of the ground surface.

When MURA constructed the parking lot in 2001 it created a 5 foot landscape buffer area around the perimeter of the lot, within the lot area itself (i.e., inside the 300' x 100' lot) and adjacent to the public sidewalk. The public sidewalk was also widened. The installed landscaping has matured nicely and Sky Park proposes to maintain that landscaping. No additional landscaping is proposed at the ground level, and any landscape materials along the perimeter damaged during the construction process will be replaced. When MURA constructed the lot it also upgraded the sidewalks and installed tree wells and street lighting. None of that is proposed to be removed or replaced. In addition, there is landscaping and site lighting within the lot itself and all of that is scheduled to be removed.

The Sky Park plan proposes balconies and units that will overhang the main building structure. While these elements will overhang the existing landscaping and sidewalks, they will be within the existing lot area (300' x 100'), so no encroachment permit will be required. The project proposing to leave the existing expanded sidewalks and landscape planters as is, and is neither expecting nor proposing to dedicate any additional public ROW. Indeed, MURA and/or the City of Medford will continue to retain ownership of the street level property of this site. Sky Park LLC will only be leasing approximately 1,000 square feet in a long term lease.

Sky Park will provide new lighting within the parking lot attached to the deck structure. In addition, the parking lot will have a fire suppression system, as will the residential units above. No additional street or alley lighting is to be provided.

CITY OF MEDFORD
EXHIBIT # 4
File # AC-15-007
1 of 6

The new units will be 1, 2 and 3 bedroom units of one and two story construction. Most will feature a deck facing a public ROW. The units will feature a variety of building designs and exterior materials, all of which are described in greater detail elsewhere in this application.

PROJECT COMPATIBILITY WITH THE NEIGHBORHOOD

- A. List the existing uses and development adjacent to your project site. Describe the architecture, age, and condition of the adjacent buildings. Along with this list describe the architecture, age and condition of the adjacent buildings.**

The development will occupy the half block between 9th and 10th Streets, Central Avenue on the east and the midblock alley on the west. Across Central Avenue is the recently constructed main branch of the Jackson County Library. Between the Library and the main campus building of Rogue Community College to the north along Central is a row of eclectic single-story shops. On the north is more parking. On the west is more parking, Farrell's Glass and a recently opened dance studio in a renovated building. These two buildings are single-story structures sheathed in stucco and in decent shape. To the south is Medford Mattress, which is a collection of several small single-story structures that are well maintained.

- B. Describe building architecture and exterior treatments in this proposal and how they fit with and complement adjacent buildings and development.**

The proposed structure has been designed to do several things simultaneously. Sky Park LLC agreed to maintain a 12 foot clearance for the parking area in case MURA ever decided to abandon the parking and create retail space in the ground floor area. The balance of the building is primarily single-story in nature in keeping with the generally low-rise nature of the surrounding neighborhood.

- C. Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass.**

The façade of the structure utilizes different materials and has façade changes both vertically and horizontally to give the impression of a collection of urban residential units that break up large expanses of façade and give relief to building mass.

- D. Describe how the placement and orientation of the proposed building relates to the street facilities and how this orientation promotes a more pedestrian friendly site design.**

The units have been placed to the perimeter of the site to reinforce the urban grid. To that we have added balconies and overhanging units to provide visual interest and the opportunity for vistas up and down Central and the mid-block alley. The relatively small size of the individual units provides the opportunity to create a central garden space in the center of the development – a nice amenity for the use of occupants.

- E. If the site lies within 600-feet of an existing transit stop describe compliance with the standards of Section 10.808.**

The central hub of the Rogue Valley transit system is a half block away on Front and 9th Streets. We have located the entrance to the development on 9th Street to take advantage of that proximity.

- F. Describe pedestrian facilities and amenities on your site and how they will function for pedestrians.**

Technically, the proposed site is one level above the street level. The open court/garden will have trees and street furniture and open space available for occupants of the building. When MURA constructed the parking lot in 2001 they installed benches and other street furniture on the corners of 9th and 10th Streets and Central Avenue. That street furniture will be retained and relocated as appropriate around the perimeter of the project. In addition, the Sky Park project will have a monumental stair that can serve as a gathering space for citizens, similar to other cities around the world.

u/c Zelle

- G. Describe vehicle and pedestrian access to the site and how it relates internally on the site and to adjacent sites.**

While parking is not required for this site, as it is within the Central Parking District, Sky Park LLC has an agreement with the City to allow one reserved parking space within the Central A parking lot for each unit. Access for unit occupants and deliveries will be by means of a secure elevator that will have direct access from the parking lot and 9th Street.

H. Describe if and how the proposed plan is sensitive to retaining any existing streets or significant native vegetation on the site.

There are currently no trees within the parking lot. There are existing street trees along both 9th and Central, which will remain. The site has no native vegetation, but there is mature landscape materials along the perimeter of the three streets, which will be retained.

I. Describe stormwater detention facilities on the site. If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.

When the parking lot was constructed a stormwater system was developed, which included on-site drainage into catch basins. The site was essentially 100% developed as impervious. We are proposing to collect that same rain water, utilize it as needed for irrigation of deck plants and trees and then direct the balance into the system that was originally designed for the parking lot, essentially without providing a lot of additional piping. The net result is the amount of runoff will be close to what now enters the system, but it will be cleaner because it is not picking up contaminants from the parking lot.

J. Describe how your proposed landscaping design will enhance the building and other functions on the site.

The existing landscaping along the street level exterior will remain as a buffer to the parked cars. The trees to be introduced on the deck will provide significant shading to the open court.

K. Describe how your exterior lighting illuminates the site, and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of section 10.764.

Exterior lighting on the deck will be by means of fixtures that cast no light above the horizontal plan. Also the fixtures will not extend above the roof ridges of the units, so no light or glare will be produced that will affect adjacent properties.

L. Describe any proposed signage and how it will identify the location of the occupant and serve as an attractive complement to the site.

Only one building sign is proposed – see elevation sheet

M. Explain any proposed fencing, including its purpose, and how it has been incorporated as a functional, attractive component of the development.

No fencing is proposed at the street level. We propose to use the existing landscaping around the perimeter as a vegetative buffer between parked cars and pedestrians using the public sidewalks. There may be some additional screening along the tops of the openings underneath the units, but as of yet it is undefined.

N. Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of Section 10.752-10.761.

We expect the occupants of this development to be very quiet overall. No loud noise generation is expected to occur as it will remain a residential project for at least the next 100 years.

O. Explain anything else about your project that adds to the compatibility of the project with adjacent development and uses.

No new market rate housing has been developed within the downtown core of the City of Medford for a long time, and Sky Park LLC believes commercial core housing is both needed and desired at this moment in time. Providing 26 single family residential units within the commercial core will provide a dramatic boost for existing service and retail businesses. We think this project will have a benefit for the city and existing businesses. 2nd 3046

P. List and explain any exceptions or modifications requested and provide reasons for such.
Sky Park LLC is not aware of any exceptions to the City of Medford development standards that are needed for this project and is not requesting any at this time.

Q. List any petition for relief of landscaping standards. Provide rationale for requested deviation from standard.

There are no minimum landscaping standards for this site. The project is proposing to retain the existing landscaping and planters at street level, as well as the expanded site walks. No relief from landscaping standards is being requested at this time.

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40/40

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

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Section II – Code Compliance: Criterion No. 2

PROJECT SITE		
	PROPOSED	REQUIRED
• Zoning District	<u>CC</u>	
• Overlay District(s)	<u>DOWNTOWN PARKING DISTRICT</u>	
• Proposed Use	<u>RESIDENTIAL</u>	
• Project Site Acreage	<u>30,000 SF</u>	
• Site Acreage (+ right-of-way)	<u>46800 SF</u>	
• Proposed Density (10.708)	<u>240/AC</u>	
• # Dwelling Units	<u>26</u>	
• # Employees	<u>N/A</u>	

STRUCTURES		
	EXISTING	PROPOSED
• # Structures	<u>Ø</u>	
• Structure Square Footage (10.710-10.721)	<u>Ø</u>	

SITE DESIGN STANDARDS		
	PROPOSED	REQUIRED
• Front Yard Setback (10.710-721)	<u>VARIES 3-5'</u>	<u>Ø FT</u>
• Side Yard Setback (10.710-721)	<u>1'</u>	<u>Ø FT</u>
• Side Yard Setback (10.710-721)	<u>1'</u>	<u>Ø FT</u>
• Rear Yard Setback (10.710-721)	<u>Ø</u>	<u>Ø FT</u>
• Lot Coverage (10.710-721)	<u>64%</u>	

PARKING		
	PROPOSED	REQUIRED
• <u>IN LOT-GROUND FLOOR</u> Regular Vehicular Spaces (10.743)	<u>78</u>	<u>Ø</u>
• Disable Person Vehicular Spaces (10.746[8])	<u>4</u>	<u>Ø</u>
• Carpool/Vanpool Spaces (10.809)	<u>—</u>	<u>Ø</u>
• Total Spaces (10.743)	<u>82</u>	<u>Ø</u>
• Bicycle Spaces (10.748) <u>IN UNITS</u>		<u>Ø</u>
• Loading Berths (10.742)	<u>Ø</u>	<u>Ø</u>

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 AC-15-007
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SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

LANDSCAPING	PROPOSED	REQUIRED
• Total Landscape Area (square feet)	<u>EXISTING</u>	_____
• Total Landscape Area in High Water Use Landscaping (square feet)	_____	_____
• Total Landscape Area in High Water Use Landscaping (percentage)	_____	_____
• Total % Landscape Coverage	_____	_____
• Required Organic Content (cu.yd.)	_____	_____
• Frontage Landscaping (10.797)		
• Street:	<u>EXISTING</u>	_____
• Feet:	<u>5</u>	_____
• # Trees:	_____	_____
• # Shrubs:	_____	_____
• Street:	_____	_____
• Feet:	_____	_____
• # Trees:	_____	_____
• # Shrubs:	_____	_____
• Bufferyard Landscaping (10.790)		
• Type:	_____	_____
• Distance (ft):	_____	_____
• # Canopy Trees:	_____	_____
• # Shrubs:	_____	_____
• Fence/Wall:	_____	_____
• Parking Area Planter Bays (10.746)		
• Type:	<u>NONE</u>	_____
• # Bays:	_____	_____
• Area:	_____	_____
• # Trees:	_____	_____
• # Shrubs:	_____	_____

STRUCTURE

	PROPOSED
• Materials	<u>STUCCO BRICK SIDING, BLOCK</u>
• Colors	<u>SEE ELEVATIONS</u>

Please remember that the information you provide in response to the questionnaire must be included with your SPAR application submittal. Remember to sign and date your written response.

"c" leafle

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NARRATIVE ON EXCEPTIONS TO THE FOLLOWING MEDFORD CITY LAND DEVELOPMENT CODE SECTIONS FOR THE SKYPARK DEVELOPMENT

January 26, 2015

Description of Project:

The Developer is seeking approval to construct 26 one, two and three bedroom units on a deck over the parking lot owned by the Medford Urban Renewal Agency and operated by the City of Medford, known as "Central A". The units will be marketed by the Developer as townhouses and offered for sale to the public as single units. The site is located at 206 S Central Avenue.

Please see the SPAC application for additional information on this project. The important information is that SkyPark Medford LLC will be purchasing the entire area from 12 feet above the ground up and leasing only parts of the site from 12 feet up and lower. SkyPark has no control over changes on the ground.

The project is located between 9th and 10 Streets, Central Avenue South and the mid-block alley to the west. It is within the Downtown Parking District (DPD).

After months of negotiation, the City of Medford agreed to provide 26 reserved spaces within the "Central A" lot under the building. These spaces will be available for the exclusive use of owners of SkyPark units, with certain limitations. The spaces will be reserved as a block, rather than one space for one specific unit. That will leave 55 spaces under the building, plus an additional 19 spaces in the portion of Central A not under the building that are available for tenants and guests. The main difference between the "reserved" spaces and the open spaces is that permits for the reserved spaces can be purchased on an annual basis. Permits for the open spaces can only be purchased a week at a time, although a person can purchase up to 4 weeks of permits in advance.

When you read Section 10.743 of the Medford Land Development Code, single family residential within the CBD is required to have 2 parking spaces per unit. The Table in the Code has an asterisk which indicates this requirement applies only to areas outside the Downtown Parking District. The Table pretty clearly indicates there is no parking requirement for any use within the DPD. Frankly, here planning staff and I disagree as to whether any parking is required specifically for residential uses within the DPD. My reading of the Code leads me to believe it is not. Planning staff's interpretation of the LDC is that parking is required for Residential Uses within the DPD. If SPAC wishes to offer an opinion on parking for specific uses within the DPD, I think it would be greatly appreciated by both parties.

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File # E-15-009
10/24

In the meantime, based on Planning Staff's interpretation of the LDC, SkyPark LLC is requesting an exception to the two spaces per dwelling unit requirement for single family residential units within the CBD.

Below are the Sections of the LDC that deal with parking for residential uses in the Central Business District.

SECTION 10.741 (1)

...plans and evidence are presented to show... that property is and will be available for exclusive use as parking space.

The Development Code requires that property for parking be available for the exclusive use of the project tenants. In this case, there is adequate parking in the lot underneath the building, and 26 spaces have been reserved for the exclusive use of Sky Park residents, but the additional required spaces while available, are not for the exclusive use of SkyPark residents.

There will be 74 additional spaces available in this lot for residents that may have second vehicles, and the spaces can be reserved for up to a month in advance. In addition, there are other locations in other garages within the DPD a few blocks away where residents can park vehicles for extended periods of time and reserve spaces for up to a year. To manage the parking needs of the development, the Homeowners Association will be responsible for parking permits for residents.

SkyPark LLC is looking for relief from this section of the Land Development Code.

SECTION 10.743 - Table 743-1

For single family residential uses within the Central Business District, but outside the Downtown Parking District, 2 off-street parking spaces per dwelling unit are required.

SkyPark LLC believes one reserved parking space will be sufficient for this development given its central location in the heart of Medford within walking distance to most downtown features. Further, like all commercial uses within the DPD, unit owners will be contributing to the maintenance of all the lots and parking structures within the District. As with commercial users and residents from around the City SkyPark residents will have access to any space within the District and they will be able to reserve a space for a fee. SkyPark LLC believes there is adequate parking available for residents within close proximity to the units.

The Central A parking lot gets its heaviest use during the day by students at RVCC, and the lot is not enforced during evening and nighttime hours. Residents who work outside the downtown area should be able to use any of the non-reserved spaces after hours.

SkyPark LLC is seeking relief from the requirement to provide two dedicated off-street parking spaces on-site for this development.

SECTION 10.744

This section establishes criteria on which to base shared parking.

The Central A Parking Lot is a shared facility available for all retail businesses, offices, residents and visitors to the central business district. The difficulty arises in meeting the criteria

"P"
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established by this Section of the Development Code in that it is impossible to accurately ascribe the parking requirements of various users within the Downtown Parking District with the available spaces in lots and garages. We know parking is currently available and we know more is coming on line, but we cannot define who gets assigned what parking.

SkyPark LLC is seeking relief from meeting the requirements of this Section.

SECTION 10.744

This section establishes criteria on where parking can be located to qualify as parking for a given project.

There is enough parking directly under the project to satisfy the requirements of this section of the Code. The issue is that it is not dedicated parking reserved for the residential use.

SkyPark LLC is seeking relief from meeting the requirements of this Section.

EXCEPTION APPROVAL CRITERIA

- 1. The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met.*

The granting of the exception is in harmony with the general purpose and intent of the regulations. There is, in fact, plenty of parking on the site. The parking need can be met by individual residents according to their individual needs. And it can even be reserved parking, within certain limitations. Residents just won't be able to point to a parking spot and say "that is mine". Granting an exception will have no impact on the health, safety and general welfare of the community at large, or any impact on natural resources.

- 2. The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*

The proposed use for this project is one permitted by the Development Code, and frankly, highly desired by planning officials, and the MURA Board of Directors; and one we believe will be good for the City of Medford and can serve as a model for other communities looking to revitalize their central business districts.

- 3. There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standards for which an exception is being requested would result in peculiar, exceptional and undue hardship to the owner.*

The existing Central A Parking Lot is one of the two busiest hourly pay lots within the city of Medford. City of Medford staff was very concerned that reserving more than the absolute minimum number of spaces required would send the wrong message to those that rely on the availability of public pay spaces within the CBD to conduct their day-to-day business. SkyPark LLC, for its part, felt that given the location and the expected owners of the units that one

LD
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parking space per unit should be sufficient for most residents. And if additional spaces are necessary, weekly permits are readily available.

In this case, strict adherence to the standard will impose exceptional and undue hardship on MURA, which has agreed to lease the parking to SkyPark LLC, as well as SkyPark LLC, which will not be able to lease additional parking anywhere that will meet the criteria established in the Development Code for residential parking.

- 4. The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.***

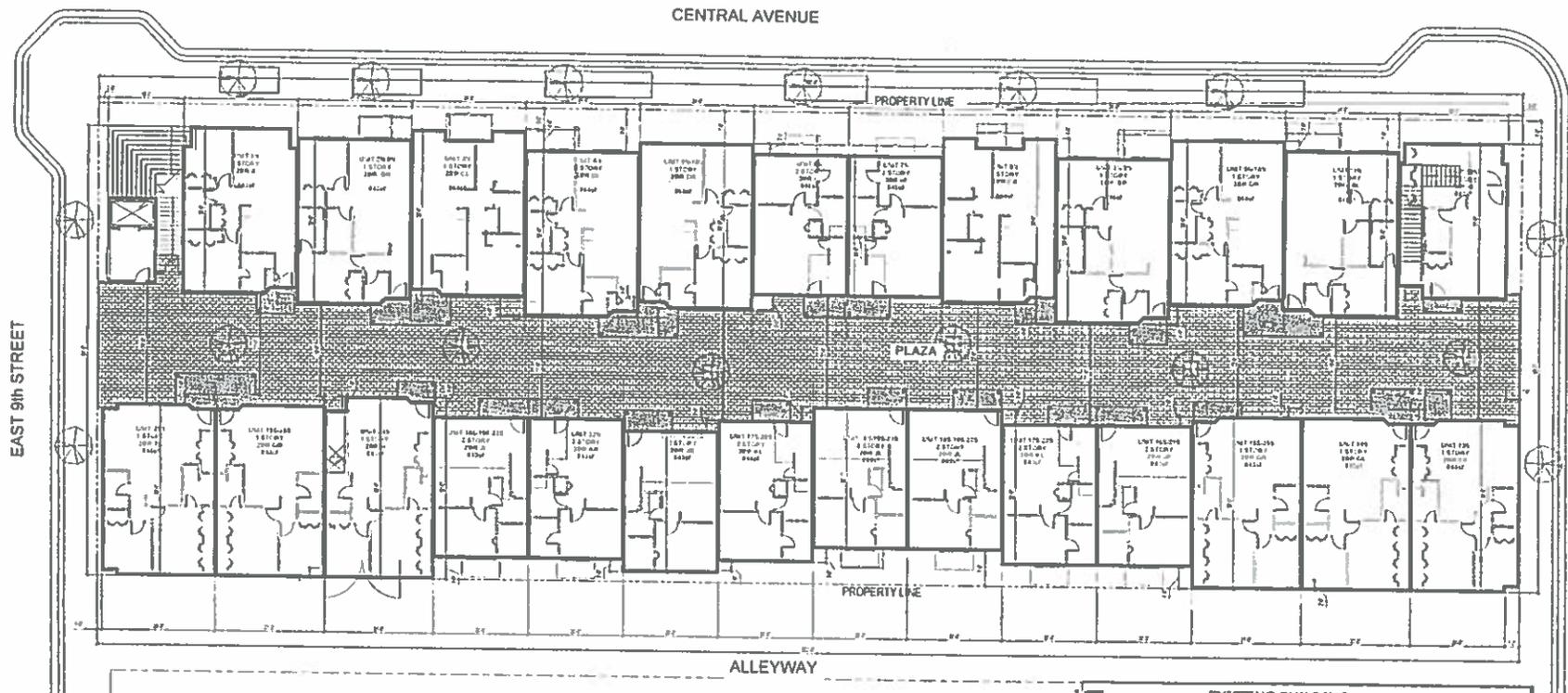
The need for the exception is not the result of an illegal act. The need for the exception is simply the result of the project being constructed over an existing parking lot, and that the spaces within the lot cannot be ascribed to specific units of the development, as required by the Land Development Code.

Submitted by

Mark McKechnie
For the Developer
SkyPark LLC

"D"
4/10/14

RECEIVED
 JAN 21 2015
 PLANNING DEPT.



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1 FINISHED FLOOR LEVEL

10 VERT SCALE 1" = 10'-0" 1/4" HORIZ SCALE 1" = 40'-0" DIMENSIONS SHOWN BY ONE DOTTED LINE

CITY OF MEDFORD
 EXHIBIT # AC-15-007E-15001
 File # _____

EXISTING BUILDING

RECORD
 AC-15-007

CITY PLAN REVIEW

 ARCHITECTURE 221 West Lomb Street Medford, OR 97501 Phone 541.772.4372 Fax 541.490.0230										
SKYPARK MEDFORD INTERSECTION OF CENTRAL AVENUE AND EAST 10th STREET										
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>Approved for the Code Book</td> <td>_____</td> </tr> <tr> <td>City Engineer</td> <td>_____</td> </tr> <tr> <td>City Clerk</td> <td>_____</td> </tr> <tr> <td>City Manager</td> <td>_____</td> </tr> <tr> <td>City Attorney</td> <td>_____</td> </tr> </table>	Approved for the Code Book	_____	City Engineer	_____	City Clerk	_____	City Manager	_____	City Attorney	_____
Approved for the Code Book	_____									
City Engineer	_____									
City Clerk	_____									
City Manager	_____									
City Attorney	_____									
A1.2										



② -9th st low



① -9th st high

CITY OF MEDFORD
 EXHIBIT A
 File # PC-15-007
 141

PLANNING DEPT.
 JAN 21 2015
 APPROVED

223 West 10th Street Medford, Oregon 97504 Phone: 541.772.4372 Fax: 541.409.6329	
SKYPARK MEDFORD INTERSECTION OF CENTRAL AVENUE AND EAST 10th STREET	
PREPARED FOR THE CLIENT BY FILE NO. 15-007	DATE 1/21/15
ISSUE DATE 1/21/15	DRAWN BY P. MEDFORD
CHECKED BY P. MEDFORD	SCALE AS SHOWN
A2.2	



Continuous Improvement Customer Service

CITY OF MEDFORD

Date: 3/18/2015
File Number: AC-15-007/E-15-009

**PUBLIC WORKS DEPARTMENT STAFF REPORT
SKYPARK RESIDENTIAL BUILDING**

Project: Consideration of Plans and associated exception request seeking relief from required parking standards for 26 residential dwelling units upon a deck over the Medford Urban Renewal Parking Lot Facility, 0.688 acres.

Location: Located at the intersection of Central Avenue and East 10th Street; 371W30BC TL 11600.

Zoning: C-C/C-B (Community Commercial Central Business District Overlay)

Applicant: Skypark Medford LLC

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage
- Submittal and approval of plans for site public improvements and work within the public right-of-way, if required.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.

CITY OF MEDFORD
 EXHIBIT # M
 File # AC 15-007/E-15-009
1025

A. STREETS

1. Dedications

No additional right of way on Central Avenue is required.

No additional right-of-way on 9th Street is required.

No additional right-of-way on 10th Street is required.

No additional right-of-way is required in the public alley on the westerly edge of the site.

2. Public Improvements

a. Public Streets

All standard street section improvements have been completed on streets surrounding the proposed development, including pavement, curb and gutter and sidewalk. **No additional street improvements will be required.**

b. Street Lights

The Developer shall protect and preserve all existing street lighting, power supply, and appurtenances.

c. Pavement Moratoriums

There is a pavement cutting moratorium on South Central Avenue at this frontage effective until 7/19/2017 No pavement cuts are allowed in South Central Avenue except as provided in Medford Municipal Code (MMC) 3.070.

At this frontage on 10th street, an overlay is scheduled for summer, 2015. After work is complete, no pavement cuts will be allowed for five (5) years in 10th Street except as provided in MMC 3.070.

3. Access and Circulation

Access to this site shall be restricted to the public alley on the westerly frontage of the site.

B. SANITARY SEWERS

This site lies within the Medford sewer service area. The proposed site plan does not clearly indicate the location of building connections to the existing sanitary sewer main. The Developer shall ensure that this connection is to an existing main as a separate individual lateral or make a new connection to an existing main with a separate lateral.

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C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development shall be submitted with the building permit application for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Storm water quality and detention facilities shall be required in accordance with Medford Land Development Code Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

Any required public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

P:\Staff Reports\AC\2015\AC-15-007_E-15-009 Skypark Medford LLC\AC-15-007 E-15-009 Skypark Staff Report - DB.docx Page 3

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges

Buildings in this development are subject to street, sanitary sewer collection and treatment system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Kris Lillie, RH2 Engineering

"M"
4 of 5

P:\Staff Reports\AC\2015\AC-15-007_E-15-009 Skypark Medford LLC\AC-15-007 E-15-009 Skypark Staff Report - DB.docx Page 4

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

SUMMARY CONDITIONS OF APPROVAL

Skypark Residential Building

AC-15-007 / E-15-009

A. Streets

1. Street Dedications to the Public:

No street dedications are required for this development.

2. Improvements:

No additional street improvements are required for this development.

Pavement moratoriums are currently in effect.

B. Sanitary Sewer:

Provide separate individual sanitary sewer lateral to main.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.

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PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-15-007 & E-15-009

PARCEL ID: 371W30BC TL 11600

PROJECT: Consideration of Plans and associated exception request seeking relief from required parking standards for 26 residential dwelling units upon a deck over the Medford Urban Renewal Agency Parking Lot Facility, located upon .688 acres west of S. Central Avenue and north of E. 10th Street, within a C-C/C-B (Community Commercial – Central Business District Overlay) zone district; Skypark Medford LLC., Applicant (Oregon Architecture, Inc. Agent). Desmond McGeough, Planner.

DATE: March 16, 2015

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Applicants' civil engineer shall coordinate with MWC engineering staff for domestic water service and fire protection facility connections to support the proposed 26 residential dwelling units.
4. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is not required.

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CITY OF MEDFORD
EXHIBIT # "N"
File # AC-15-007/E-15-009
102

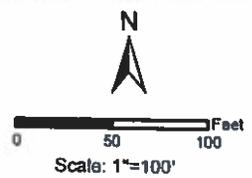
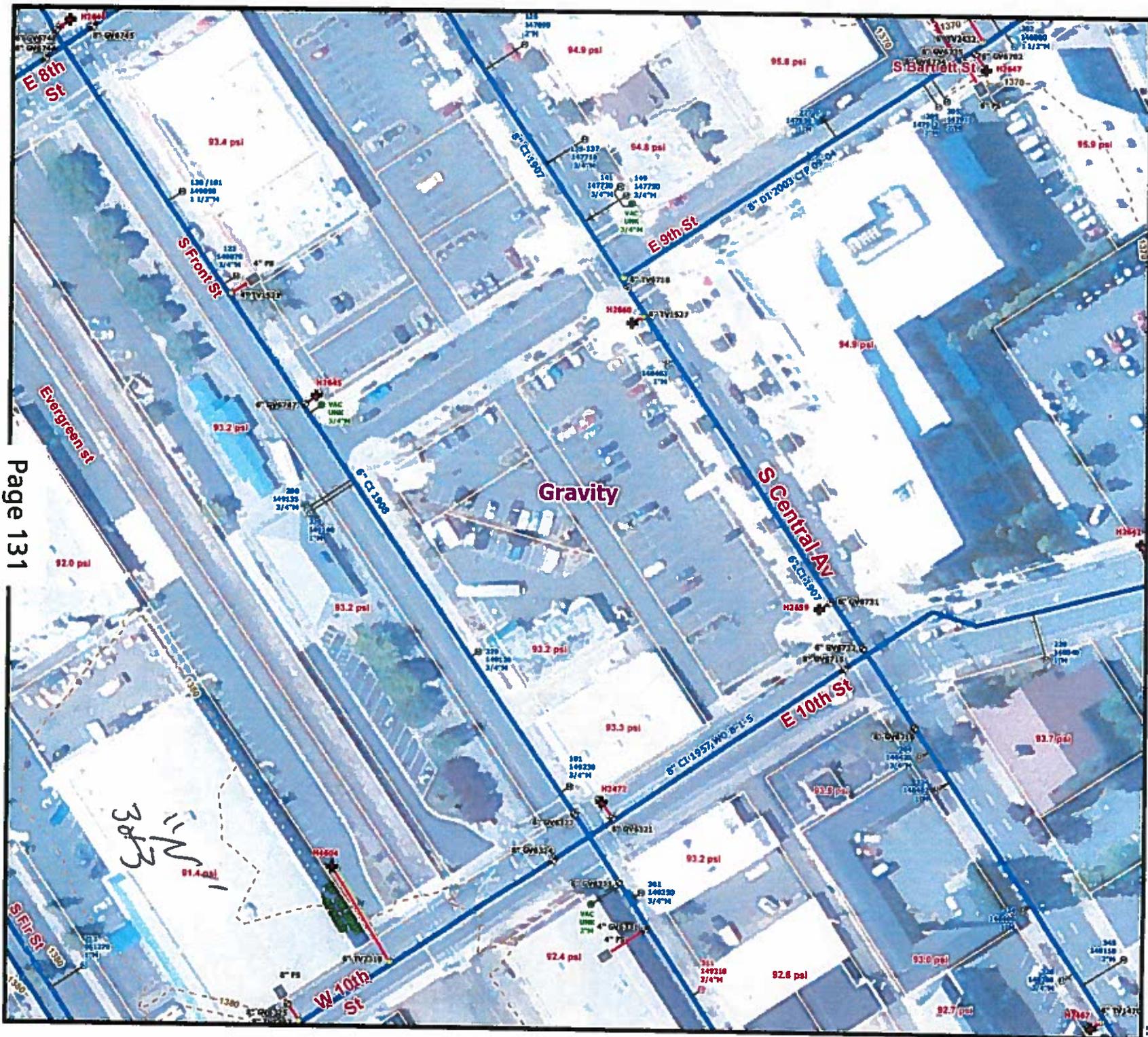


BOARD OF WATER COMMISSIONERS
Staff Memo

Continued from previous page

3. Static water pressure is approximately 95 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
4. MWC-metered water service does exist to this property. There is a City of Medford 1-inch irrigation water meter for this parking lot landscaping.
5. Access to MWC water lines is available. There is a 6-inch cast iron water line in Central Avenue, and an 8-inch cast iron water line in East 10th Street.

"N"
2023



Water Facility Map for AC-15-007 & E-15-009

Legend

- Air Valve
- Sample Station
- Fire Service
- ⊕ Hydrant
- ▲ Reducer
- Blow Off
- ◆ Plugs-Caps
- Water Meters:**
 - Active Meter
 - On Well
 - Unknown
 - Vacant
- Water Valves:**
 - ⌵ Butterfly Valve
 - ⊕ Gate Valve
 - Tapping Valve
- Water Mains:**
 - Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line
- Boundaries:**
 - ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots
- MWC Facilities:**
 - C** Control Station
 - P** Pump Station
 - R** Reservoir



This project is a public improvement project funded by the City of Medford. The City of Medford is not responsible for the accuracy of the information shown on this map. The City of Medford is not responsible for the accuracy of the information shown on this map. The City of Medford is not responsible for the accuracy of the information shown on this map.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
www.medfordfirerescue.org

LAND DEVELOPMENT REPORT - APPLICANT

To: Desmond McGeough

LD Meeting Date: 03/18/2015

From: Greg Kleinberg

Report Prepared: 03/10/2015

Applicant: Applicant (Oregon Architecture, Inc. Agent)

File #: AC - 15 - 7

Associated File #'s: E - 15 - 9

Site Name/Description: Medford Urban Renewal Agency Parking Lot Facility

Consideration of Plans and associated exception request seeking relief from required parking standards for 26 residential dwelling units upon a deck over the Medford Urban Renewal Agency Parking Lot Facility, located upon .688 acres west of S. Central Avenue and north of E. 10th Street, within a C-C/C-B (Community Commercial - Central Business District Overlay) zone district; Skypark Medford LLC., Applicant (Oregon Architecture, Inc. Agent). Desmond McGeough, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
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Requirement FIRE SPRINKLER SYSTEM

OFC

903

A NFPA 13 fire sprinkler system will be required by code for this occupancy (both the parking garage and the residences).

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.
Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.
Specific fire protection systems may be required in accordance with the Oregon Fire Code.
This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.
Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

CITY OF MEDFORD
EXHIBIT # "0"
File # AC-15-007/E-15-09
10F1

Memo



To: Desmond McGeough, Planner, Planning Department
From: Tanner Fairington, Building Department
CC: Oregon Architecture, Applicant / Agent
Date: March 17, 2015
Re: March 18, 2015 LDC Meeting: AC-15-007 / E-15-009 – Item #3

Please Note:

This is not a plan review. These are general notes based on general information provided. Plans need to be submitted and will be reviewed by a commercial plans examiner to determine if there are any other requirements for this occupancy type.

Fees are based on valuation. Please contact the front counter for estimated fees.

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Code and Design Information" on left side of screen; click on "Design Criteria"; and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" on left side of screen for information.

General Comments:

3. The comments below are based on the site plans and elevations submitted. When final plans are submitted, and building information is provided, a more thorough review can be provided.

Site Plan

The comments below are based on the 2014 OSSC, unless noted otherwise.

4. Accessible parking spaces to be per section 1106.7 of the 2014 OSSC, including figures referenced (see Building Codes Division for 2014 OSSC Errata which included these figures, dated October 16, 2014). Locations appear to be acceptable.
5. Per 903.3.1.1 and 903.2.8, an NFPA 13 automatic sprinkler system is required throughout the building, including the parking structure.
6. Per 1008.1.2, the means of egress door leading in to the south stairway shall swing in the direction of egress travel since the occupant load for the second story will exceed 50.
7. Exit signs shall be provided per Section 1011
8. Please evaluate whether the following Sections of the 2014 OSSC apply prior to submitting plans for review:
 - a. 510.7 – Fire Barrier and means of egress requirements
 - b. 705.2 – Overhangs
 - c. Table 508.4, footnote c – See 406.3.4.

CITY OF MEDFORD
EXHIBIT # "P"
File # AC-15-007/E-15-009
1051

3. Term Of Lease

(a) The Lease Term shall commence on January 15, 2015, and shall terminate on January 15, 2065, unless extended or sooner terminated as provided in this Agreement. Lessor and Lessee shall both work together in good faith to mitigate any disruption of parking lot operation, and both agree that work shall be done at night when feasible during the period when Rogue Community College and/or Southern Oregon University are in session, starting on or about June 1, 2015, subject to financial timing and city approvals relating to the project.

(b) At any time within one year prior to the lease expiration date Lessor will either 1) sell the Leased Property to Lessee at an agreed upon price, or 2) execute a lease of the Leased Property for another 50 years at the price of \$1 (One Dollar).

(c) Terms specific to Lessee's use of parking spaces at the Central A parking lot are attached hereto as Exhibit A.

4. Rental Payments Required. Lessee shall pay to Lessor a rental amount of One Dollar (\$1.00) for the 50 year lease term. Rent may be pre-paid up to the entire term of the lease without penalty.

(a) All rents payable under this Agreement shall be payable in advance on the first day of each Lease Year, at whatever address Lessor may specify in writing from time to time. Lessor will provide Lessee prior reasonable written notice of changes to the address for payment of rent hereunder.

(b) All amounts which Lessee is required to pay under this Agreement (including but not limited to taxes, utility costs and maintenance expenses) shall be payable as additional rent, and shall be paid promptly when due.

5. Use Of Leased Property. Lessee shall use the Leased Property solely for all SPM Development-associated design elements, supporting columns, utility facilities and utility pathways, stairways, elevators, access elements, lobby, mail room, and trash area. Lessee agrees to make a reasonable, good faith effort to construct and operate the SPM Development in a manner which will minimize the area of the Leased Property and maximize the usability of the MURA Property as a public parking lot.

(a) Requirements Of MURA Property To Have Priority. Lessee acknowledges and agrees that Lessee's ability to make beneficial use of the Leased Property shall at all times be subject to the reasonable requirements of the Lessor to make use of, and have access to and across, MURA Property.

(b) Changes In Use. At no time during the Lease Term shall Lessee make, cause, allow, suffer or otherwise permit any material change in the use of any portion of the Leased Property without obtaining the prior written consent of Lessor.

(c) Prohibited Uses. Lessee shall not use, or cause, allow, suffer or otherwise permit any other person or entity to use, the Leased Property in any manner which would or might reasonably be expected to: (1) create or tend to create waste or a nuisance, or (2) materially interfere with the maintenance, operation and/or beneficial use of the MURA Property, or (3) have any adverse visual, auditory or olfactory impact on any portion of the MURA Property, or (4) be unreasonably offensive to Lessor or users of any portion of the MURA Property, or (5) make it impossible or more costly for Lessor to insure against loss or damage to the MURA Property, or against personal injury or property damage occurring on or to any portion of the MURA Property, or (6) unreasonably restrict access to or from the MURA Property.

(d) Compliance. Lessee shall promptly obtain all permits, licenses and permissions which shall be required to perform Lessee's responsibilities under this Agreement, and shall comply with all statutes, laws, ordinances, orders, judgments, decrees, injunctions, rules, regulations, licenses, directives and requirements

of all federal, state, county, municipal and other governments, commissions, boards, courts, authorities and officials, together with companies or associations insuring the Leased Property, which now or at any time hereafter may be applicable to the Leased Property or any part thereof, or to any use of or condition of the Leased Property or any part thereof. Lessee shall remedy at Lessee's expense any failure of compliance created through Lessee's fault or by reason of Lessee's use.

(e) Security. Lessee shall be solely responsible for ensuring the safety and security of persons using the Leased Property and property located above the Leased Property.

6. Utilities.

(a) Sprinkler Fixtures. In connection with the construction of the SPM Development, Lessee shall install a fire sprinkler system above the Leased Property which meets the requirements of applicable construction codes. Lessee shall be responsible for providing and paying the cost of all water used in connection with that sprinkler system. Lessee shall be responsible for, and shall bear the cost of, operating and maintaining that sprinkler system.

(b) Lighting Fixtures.

(1) At the commencement of the SPM Construction Period, Lessee shall remove Lessor's existing lighting fixtures (and other parking equipment) from the SPM Real Property and deliver those items to Lessor.

(2) In connection with the construction of the SPM Development, Lessee shall install a lighting system above the Leased Property which meets the requirements of applicable construction codes and is compatible with the lighting system currently installed in MURA's parking structures at One West Main and the Middleford garage. Lessee also will install a new 400 amp single phase electrical panel, service entrance and meter, and will connect the lighting system to the panel. Subsequent to Lessee's installation of those fixtures, Lessor shall be responsible for, and shall bear the cost of, operating and maintaining those fixtures.

(c) Utilities. Lessor shall have no responsibility to arrange for or provide any utility services to the Leased Property. Lessee shall be responsible for obtaining, and shall pay the cost of, all water, electricity, natural gas, heating oil, telephone service, refuse collection, sewage and other utilities and services provided to the Leased Property, or used on or in connection with the Leased Property, during the Lease Term. Lessee shall make payment for all such utilities and services directly to the providers of those utilities and services. Lessor shall not be liable to Lessee in the event of any interruption in the supply of any utility or service to the Leased Property (other than an interruption caused by the Lessor). Lessee shall not install any additional facilities utility fixtures on the Leased Property which might reasonably be expected to materially interfere with the maintenance, operation and/or beneficial use of the MURA Property.

7. Taxes On Real And Personal Property.

(a) Lessee shall pay all real property taxes, general and special assessments, and other taxes and charges which are levied on or assessed during the Lease Term against the Leased Property or improvements located on the Leased Property, as those taxes become due and payable, and before delinquency.

(b) Lessee shall pay all personal property taxes and other taxes and charges which are levied on or assessed against personal property, leasehold improvements, fixtures, equipment, furniture, inventories, merchandise and any other personal property installed or located on the Leased Property during the Lease Term (the "Personal Property Taxes"), as those taxes become due and payable, and before delinquency, and regardless of whether such levy or assessment is made against Lessee or against Lessor, and regardless of whether such property has been installed by Lessee or by Lessor. Lessee shall make all tax payments directly to the taxing authorities.

(c) If any tax is permitted by a taxing authority to be paid in installments, Lessee may elect to

do so as long as each installment (together with any interest charged) is paid before it becomes delinquent. Lessee may contest in good faith the validity or amount of any tax, assessment or charge in accordance with the procedures established by applicable statute or administrative rule, as long as the Leased Property is not subjected to any lien as a result of the contest. Lessee shall furnish to Lessor receipts or other proof of payment of all taxes, assessments and charges payable by Lessee hereunder, within ten (10) days after Lessor's written request for such proof.

8. **Repairs And Maintenance.**

(a) Lessee shall be responsible for maintaining the SPM Development in a manner which will not materially interfere with the beneficial use of the MURA Property by Lessor.

(b) Lessee shall maintain in safe, workable and neat condition all elements and aspects of the Leased Property. Lessee shall be responsible for maintaining the Leased Property in a manner which will not materially interfere with the beneficial use of the MURA Property by Lessor. Lessor shall have no responsibility to perform any repairs or maintenance with respect to the Leased Property.

(c) Neither party shall be obligated to resurface any portions of the existing parking lot located on the MURA Property which remain after completion of construction of the SPM Development. Lessee shall be obligated to patch and return to useable condition any portions of that remaining parking lot which are damaged during the construction of the SPM Development.

(d) If any failure by Lessee to keep and preserve the Leased Property in the state or condition required under this Section 8 causes any material interference with the beneficial use of the MURA Property, then Lessor may, after ten (10) days written notice to Lessee, make whatever repairs are necessary to place or return the Leased Property to eliminate that material interference, without liability to Lessee for any loss or damage which may result to Lessee's business by reason of those repairs. In the event of such repairs by Lessor, Lessee shall be obligated to pay to Lessor an amount equal to the total costs and expenses incurred by Lessor in making those repairs, plus 15%, which total amount shall be promptly due and owing as additional rent.

9. **No Warranties By Lessor.** Lessor makes no warranty, either express or implied, as to the condition of the Leased Property, or the suitability or fitness of the Leased Property for any purpose. Lessee agrees that neither Lessor nor any agent of Lessor has made any representations or warranties as to any of the following: (i) the suitability or fitness of the Leased Property for Lessee's permitted use(s) (as identified in Section 5), (ii) the physical condition of the Leased Property, (iii) the expenses of operation of the Leased Property, or (iv) any other matter affecting or relating to the Leased Property except as expressly set forth in this Agreement and in the Disposition and Development Agreement.

(a) Prior to executing this Agreement, Lessee has inspected the Leased Property and has become thoroughly acquainted with the condition of the Leased Property. Lessee agrees to take and accept the Leased Property "AS IS". The taking of possession of the Leased Property by Lessee shall be a conclusive acknowledgment by Lessee that the Leased Property is in good and satisfactory condition as of the date when possession is taken, and that Lessee has determined the Leased Property to be suitable for Lessee's permitted use(s) (as identified in Section 5). Lessor shall not be required to make any alterations or improvements of any kind to the Leased Property.

(b) Although it is the expectation of the parties that Lessee shall use the Leased Property for the permitted use(s) identified in Section 5, it is expressly agreed that the obligations of Lessee under this Agreement shall not be abated, diminished or in any other manner affected by the inability of Lessee for any reason whatsoever to use all or any portion of the Leased Property for those purposes, except as otherwise expressly provided herein.

10. **No Liens.** Lessee shall not allow the Leased Property to be subjected to any mortgage or other lien as security for a loan or other obligation of Lessee, without first obtaining the express written consent of Lessor, which consent may be subject to any terms or conditions, or withheld by Lessor for any reason or for no reason in Lessor's absolute discretion. Lessee shall keep the Leased Property free and clear of all personal

property tax liens and encumbrances. Lessee shall pay as due all claims for labor or work done on, and for services rendered or material furnished to, the Leased Property, and Lessee shall keep the Leased Property free from any mechanic's, workman's or materials lien of any kind. If Lessee receives notice of the filing of any claim or lien against the Leased Property or the commencement of any action which might affect the title to the Leased Property, Lessee shall give prompt written notice thereof to Lessor.

11. Insurance.

(a) Lessee shall maintain and shall pay all premiums with respect to insurance protecting Lessor and Lessee as the named insureds against loss or liabilities arising from personal injury or death or damage to property caused by any accident or occurrence in connection with the use, operation or condition of the Leased Property and the SPM Development, with limits deemed appropriate from time to time by Lessor in the reasonable exercise of Lessor's discretion. Any proceeds of the insurance referred to in this subsection shall be applied towards extinguishment or satisfaction of the liabilities with respect to which those insurance proceeds are paid.

(b) Lessee shall maintain, and shall pay all premiums for, insurance against loss or damage to the improvements located on the Leased Property and the SPM Development by fire, lightning, vandalism, malicious mischief, sprinkler leakage, breakage of plate glass, or other perils or casualties, with an all risk endorsement. All such insurance shall be for the benefit of Lessee only. All such insurance also shall cover incidental loss or damage to the MURA Property (arising by reason on fire or other perils or casualties on or about the Leased Property and the SPM Development), with limits deemed appropriate from time to time by Lessor in the reasonable exercise of Lessor's discretion, and with proceeds attributable to such incidental loss or damage being for the benefit of Lessor only.

(c) Lessee hereby releases Lessor and Lessor's agents and employees from responsibility and liability for loss or damage occurring to, or in connection with the use of, the Leased Property and the SPM Development, and Lessee waives all right of recovery against Lessor and Lessor's agents and employees for such loss or damage. Lessee agrees to: (i) notify Lessee's insurance carrier(s) of the release and waiver set forth in the preceding sentence, and (ii) obtain from Lessee's insurance carrier(s), at Lessee's sole cost, a written waiver of all subrogation rights against Lessor and Lessor's agents and employees.

(d) All insurance required to be carried by Lessee under this Section 11 shall be issued by responsible insurance companies, qualified to do business in the state of Oregon, and reasonably acceptable to Lessor. Each such insurance policy shall name Lessor as the sole insured or as an additional insured. No such insurance policy shall be subject to cancellation or material modification except after ten (10) days prior written notice to Lessor. Within three (3) business days after receipt by Lessee of any written request from Lessor for proof that Lessee has obtained the insurance coverage required of Lessee under this Section 11, Lessee shall be obligated to provide to Lessor a binder or other such proof. At least ten (10) days prior to the expiration of any insurance policy required of Lessee under this Section 11, Lessee shall provide to Lessor copies of renewals or binders for the issuance of one or more replacement insurance policies.

12. Destruction Of Improvements. Except as specifically provided in this Section 12, no other obligations of Lessee under this Agreement shall be altered, affected, discharged or released by reason of any damage to or destruction of any improvements on the Leased Property and SPM Development.

13. Eminent Domain. If, during the Lease Term, there shall be a total or partial taking of the Leased Property by any public authority under the power of eminent domain, then the leasehold estate of Lessee in and to the Leased Property shall cease and terminate as of the date when the condemning authority takes possession of or title to (whichever occurs first) all or any portion of the Leased Property.

(a) Sale of all or part of the Leased Property to a purchaser with power of eminent domain, in the face of the threat or probability of the exercise of the power of eminent domain, shall be treated for purposes of this Agreement as a taking by condemnation. All compensation and damages awarded for the taking of all or any portion of the Leased Property shall be equitably apportioned between Lessor and Lessee as their interests may then appear.

(b) Lessee shall have the right, at its sole cost and expense, to assert a separate claim in any condemnation proceedings for the value of Lessee's leasehold interest. Whenever notice of a taking of all or any portion of the Leased Property is received by either party, that party shall notify the other party thereof, and Lessor and Lessee thereafter shall jointly negotiate with the taking authority as to the value of their respective interests in the Leased Property or the improvements located thereon to the end of being fairly compensated therefor.

14. **Alterations.** Lessee shall not make any construction, reconstruction, improvement, change, modification, utility installation or other alteration (collectively "Alterations") in, on or to all or any portion of the Leased Property without first providing to Lessor detailed plans, specifications and explanations relating to the proposed Alterations and obtaining Lessor's express written approval to those Alterations (which approval may be subject to any terms or conditions reasonably determined by Lessor, and may be withheld by Lessor in the reasonable exercise of Lessor's discretion). Any Alteration which has been approved by Lessor in accordance with the preceding sentence is referred to in this Paragraph 14 as an "Approved Alteration").

(a) Any Approved Alteration shall be done at the cost of Lessee, and shall become the property of Lessor and shall remain on the Leased Property and be surrendered to Lessor upon termination of the Lease Term.

(b) Lessee and Lessor shall cooperate to ensure that all work with respect to any Approved Alteration is done in a good and workmanlike manner and diligently prosecuted to completion. Any Approved Alteration shall be performed and done strictly in accordance with the plans and specifications approved in writing by Lessor and in accordance with all applicable laws, ordinances and regulations, and the requirements of all insurance carriers and fire rating bureaus with respect to the Leased Property.

15. **Indemnification Against Damage Or Injury.** Sky Park agrees to indemnify, defend and hold harmless the MURA and its officers, agents and employees against all liability, loss, and costs arising from actions, suits, claims or demands attributable solely and exclusively to acts or omissions of Sky Park, and Sky Park's officers, agents, and employees, arising from the possession or use of the leased property while this lease is in effect.

Subject to the limitations of the Oregon Tort Claims Act and the Oregon Constitution, MURA agrees to indemnify, defend and hold harmless Sky Park and its officers, agents, and employees against all liability, loss, and costs arising from actions, suits, claims, or demands attributable solely and exclusively to acts or omissions of MURA, and MURA's officers, agents, and employees, arising from the possession or use of the parking lot known as "Central A" while this lease is in effect.

16. **Good Title.** Lessor warrants that it has good right to lease the Leased Property and will defend Lessee's right to quiet enjoyment of the Leased Property against the lawful claims of all persons during the Lease Term.

17. **Sale Of Lessor's Interest.** Lessor may sell all or any portion of the Leased Property during the Lease Term. Any such sale shall be subject to the terms of this lease.

18. **Limitation On Assignment Or Sublease By Lessee.** Except for subleases expressly permitted under Section 5 of this Agreement, Lessee shall not voluntarily or by operation of law assign this Lease or sublease any portion of the Leased Property, or enter into any license agreement, franchise agreement, or concession agreement with respect to the Leased Property, or mortgage, hypothecate or otherwise encumber all or any portion of Lessee's interest in this Agreement or in the Leased Property, or in any other manner permit the occupation of or shared possession of all or any portion of the Leased Property, without obtaining in each instance the written consent and approval in advance of Lessor, subject to any reasonable terms or conditions determined by Lessor, which consent may be not be unreasonably withheld. Any purported assignment, sublease, encumbrance or other conveyance of any interest in this Agreement or in the Leased Property without the written consent of Lessor shall be void and of no effect. Consent by Lessor in any one instance shall not constitute a waiver or consent to any subsequent instance. The consent by Lessor to any assignment, sublease, encumbrance or other conveyance shall not relieve or otherwise affect the continuing primary liability of Lessee under this Agreement, and Lessee shall not be released from performing any of the terms, covenants and conditions of this Agreement.

19. **Landlord's Lien.** Lessee hereby grants to Lessor a lien upon the improvements, trade fixtures and furnishings of Lessee to secure full and faithful performance of all of the terms of this Agreement.

20. **Lessee's Default.**

(a) The following shall be "events of default" under this Agreement, and the terms "event of default" or "default" shall mean, whenever used in this Agreement, any one or more of the following events:

(1) The failure by Lessee to pay or cause to be paid the full amount of any rent or other charge specified in this Agreement, within ten (10) days after the date when due. Lessor shall not be required to provide any notice to Lessee before declaring a default arising out of Lessee's failure to make any payment required under this Agreement, but no default shall be declared until ten (10) days after that payment is due.

(2) The failure by Lessee to comply with any term or condition, or fulfill any obligation of this Agreement (other than the payment of rent or other charge) within twenty (20) days after written notice by Lessor specifying the nature of the default with reasonable particularity and requesting that the default be remedied. If the default is of such a nature that it cannot be completely remedied within the 20-day period, this provision shall be complied with if Lessee begins correction of the default within the 20-day period and thereafter proceeds with reasonable diligence and good faith to affect the remedy as soon as possible.

(3) A breach of any provision of Section 7 of this Agreement, in which event Lessee shall be entitled to no notice of default and no opportunity to cure.

(b) Whenever any event of default shall have occurred, Lessor may declare, by written notice to Lessee, that all unpaid and delinquent installments of rent, and all other unpaid and delinquent charges and payments due under this Agreement shall be immediately due and payable, whereupon those amounts shall become immediately due and payable.

(c) No remedy conferred upon or reserved to Lessor under this Agreement is intended to be exclusive of any other available remedy, but each and every remedy shall be cumulative and in addition to every other remedy given under this Agreement or existing at law or in equity. No delay or omission to exercise any right or power accruing upon any default shall impair any such right or power or shall be construed to be a waiver thereof, and any such right or power may be exercised from time to time and as often as deemed expedient by Lessor. In order to entitle Lessor to exercise any remedy reserved to Lessor, it shall not be necessary to give any notice other than a notice which is expressly required in this Agreement.

21. **Lessor's Default; Lessee's Remedies.** If Lessor fails to observe or perform any obligation required to be observed or performed by Lessor under the terms of this Agreement, and Lessor fails to cure that breach within thirty (30) days after written notice thereof from Lessee, then Lessor shall be deemed in default under this Agreement. Upon Lessor's default, Lessee, at its option, may exercise any one or more of the following remedies, which shall be Lessee's sole and exclusive remedies:

(a) Lessee may cure the default by performing Lessor's obligation, in which case Lessor shall reimburse Lessee for all costs and expenses reasonable incurred by Lessee in making that cure, or

(b) Lessee may terminate this Lease immediately upon written notice to Lessor.

22. **Lessee's Responsibility For Contamination By Hazardous Substances**

(a) Lessee shall not intentionally or unintentionally cause or permit any Hazardous Material to spill, leak or be discharged onto the soil or other surface of the Leased Property or be discharged into any storm drain, sewer or other waste disposal system located on the Leased Property which is not specifically designed for, and intended to be used solely for, the retention and disposal of that Hazardous Material. In the event of any such spill, leak or discharge, Lessee shall file all reports, take all remedial actions and pay all fines and other levies as shall be required by applicable federal, state and local statute, ordinance, regulation and order.

(b) Lessee shall at all times use, sell, store, transport, dispose of and treat hazardous materials (as defined in subsection 1(f) of this Agreement) in strict accordance with all applicable federal, state and local laws and regulations (collectively referred to in this Section 23 as the "Laws"). If, prior to termination of the Lease and completion by Lessee of the obligations imposed under Section 16, there occurs upon the Leased Property any release, spill, leak or discharge of hazardous materials which is in violation of any of the Laws and is caused by any activity or activities of Lessee on or with respect to the Leased Property, then Lessee shall be obligated to cause and complete the repair, cleanup, detoxification and/or decontamination of the Leased Property, and the preparation and implementation of any closure, remedial action or other required plan or plans in connection therewith, all as required by the Laws.

(c) Lessee shall indemnify, defend, protect and hold harmless Lessor and each of Lessor's partners, employees, agents, successors and assigns (collectively referred to in this Section 23 as "Lessor"), from and against any and all criminal and civil claims and causes of action (including but not limited to claims resulting from, or causes of action incurred in connection with, the death of or injury to any person, or damage to any property), liabilities (including but not limited to liabilities arising by reason of actions taken by any governmental agency), penalties, forfeitures, prosecutions, losses and expenses (including reasonable attorney fees) which directly or indirectly arise from or are caused by either: (i) the presence in, on or about the Leased Property of any hazardous materials which result from any activity or activities of Lessee on or with respect to the Leased Property, or (ii) the Lessee's use, sale, storage, transportation, disposal, release, threatened release, discharge or generation of hazardous materials to, in, on, under, about or from the Leased Property. Lessee's obligations under this subsection 23(c) shall include, but not be limited to, the obligation to bear the expense of any and all costs, whether foreseeable or unforeseeable, of any necessary (as required by the Laws) repair, cleanup, detoxification or decontamination of all or any portion of the Leased Property, and the preparation and implementation of any closure, remedial action or other required plan or plans in connection therewith.

(d) Notwithstanding any other provision of this Agreement, the obligations of Lessee pursuant to this Section 23 shall remain in full force and effect after the termination of the Lease Term and until the expiration of the latest period stated in any applicable statute of limitations during which a claim, cause of action or prosecution relating to the matters described herein may be brought, and until payment in full or satisfaction of any and all losses, claims, causes of action, damages, liabilities, charges, costs and expenses for which Lessee is liable hereunder shall have been accomplished.

(e) For purposes of subsections 23(a), 23(b) and 23(c), any acts or omissions of or by any one or more employees, agents, assignees, sublessees, franchisees, licensees, permittees, customers, invitees, contractors, successors-in-interest or other persons permitted by Lessee to have access to the Leased Property or acting for or on behalf of Lessee (whether or not the actions of such persons are negligent, intentional, willful or unlawful) shall be strictly attributable to Lessee.

(f) If any claim, demand, action or proceeding is brought against Lessor which is or may be subject to Lessee's obligation to indemnify Lessor as set forth under this Section 23, Lessor shall provide to Lessee immediate notice of that claim, demand, action or proceeding, and Lessee thereafter shall defend Lessor at Lessee's expense using attorneys and other counsel selected by Lessee and reasonably acceptable to Lessor. Lessor agrees to cooperate with Lessee in Lessee's defense of Lessor.

23. **Expenses.** Each of the parties shall pay its own expenses incidental to the preparation and consummation of this Agreement, including but not limited to the attorney fees and expenses.

24. **Notices.** Any notice required or permitted under this Agreement shall be deemed to have been duly given when actually delivered or when deposited in the United States mail, certified and return receipt requested, postage prepaid, addressed to the addresses specified on page 1 of this Agreement or such other addresses as may be specified from time to time by the parties in writing.

25. **Time Of Essence.** Time is of the essence in the performance of all obligations of Lessor and/or Lessee under this Agreement.

26. **Attornment And Subordination.**

(a) Lessee shall execute at Lessor's request, without further consideration, any and all instruments subordinating this Agreement to the lien of any mortgage, deed of trust or other encumbrance which may now or hereafter affect the Leased Property, together with all renewals, modifications, consolidations, replacements or extensions thereof; provided, however, that: (i) any encumbrancer relying on that subordination or those instruments will covenant with Lessee that Lessee's leasehold interests hereunder shall remain in full force and effect, and (ii) Lessee shall not be disturbed in the event of sale, foreclosure or other action so long as Lessee is not in default hereunder, and (iii) condemnation and insurance proceeds shall be paid to Lessee in accordance with this Agreement notwithstanding the subordination of the Lease to the lien of the mortgage or deed of trust. Upon Lessor's request, Lessee shall furnish to Lessor a copy of Lessee's financial statement; Lessor shall not disclose Lessee's financial statement to any person or entity other than to a potential lender or purchaser of the Leased Property.

(b) If Lessor's interest is transferred to and owned by any lender of Lessor as a result of a foreclosure or other proceeding brought by the lender in lieu of or pursuant to a foreclosure or in any other manner, and if the lender thereby succeeds to the interest of Lessor hereunder, then, subject to the non-disturbance agreement referred to in subparagraph (a), Lessee shall be bound to the lender under all of the terms, covenants and conditions hereof for the balance of the remaining Lease Term, with the same force and effect as if the lender was the original Lessor hereunder. Lessee hereby attorns to any such lender, with the attornment to be effective and self-operative immediately upon the lender succeeding to the interest of Lessor, and without the necessity of the execution of any further instrument. If a lender shall succeed to the interest of Lessor, the lender shall not be liable for any act or omission of Lessor, and shall not be subject to any offsets or defenses which Lessee might assert against Lessor and which arise prior to the date of that attornment.

27. **Estoppel Certificates.** Within twenty-one (21) business days after request by Lessor, Lessee shall execute and deliver to Lessor an estoppel certificate in such form as Lessor may reasonably request, or as a prospective purchaser or encumbrancer of the Leased Property may reasonably request, relating to the then current status of the lease and stating any claims, offsets or defenses asserted by Lessee with respect to the lease. Any such estoppel certificate may be conclusively relied upon by any prospective purchaser or encumbrancer of the Leased Property. If Lessee fails to deliver a requested estoppel certificate within twenty-one (21) business days after Lessor's written request therefor, Lessee shall be deemed conclusively to have agreed that: (i) this Agreement is in full force and effect, without modification except as may be represented by Lessor, (ii) there are no uncured defaults in Lessor's performance under this Agreement, (iii) not more than one monthly installment of the rental due under this Agreement has been paid in advance, and (iv) any terms or conditions of an estoppel certificate required by a prospective purchaser or encumbrances of the Leased Property are satisfied and agreed to by Lessee. Any failure by Lessee to deliver an estoppel statement (showing any exceptions to any of the statements of fact required thereby) shall be a material breach of this Agreement.

28. **Miscellaneous.** This Agreement shall be governed and performed in accordance with the laws of the state of Oregon. In the event of any inconsistent or incompatible provisions between this lease agreement and the Disposition and Development Agreement, the Disposition and Development Agreement shall take precedence. Each of the parties hereby irrevocably submits to the jurisdiction of the courts of Jackson County, Oregon, and agrees that any legal proceedings with respect to this Agreement shall be filed and heard in the Circuit Court of Jackson County, Oregon. The paragraph headings set forth in this Agreement are set forth for convenience purposes only, and do not in any way define, limit or construe the contents of this Agreement. If any provision of this Agreement shall be determined to be void by any court of competent jurisdiction, then that determination shall not affect any other provisions of this Agreement, and all such other provisions shall remain in full force and effect. It is the intention of the parties that if any provision of this Agreement is capable of two constructions, only one of which would render the provision valid, then the provision shall have the meaning which renders it valid. If suit or action is instituted in connection with any controversy arising out of this Agreement, the prevailing party in that suit or action or any appeal therefrom shall be entitled to recover, in addition to any other relief, the sum which the court may judge to be reasonable attorney fees. This Agreement shall inure to the benefit of and shall be binding upon the successors, assigns, heirs and personal representatives of the parties. This Agreement may be executed in multiple counterparts, each of which shall be an original, and all of which shall constitute a single instrument, when signed by the parties. There are no oral agreements or representations between the parties

hereto which affect this Agreement, and this Agreement supersedes and cancels any and all previous negotiations, arrangements, agreements, warranties, representations and understandings, if any, between the parties. Waiver by either party of strict performance of any of the provisions of this Agreement shall not be a waiver of, and shall not prejudice the party's right to subsequently require strict performance of, the same provision or any other provision, and no delay or omission to exercise any right or power accruing upon any breach shall impair any such right or power or shall be construed to be a waiver thereof. The consent or approval of either party to any act by the other party of a nature requiring consent or approval shall not be deemed to waive or render unnecessary the consent to or approval of any subsequent similar act. No remedy conferred upon or reserved to either party under this Agreement is intended to be exclusive of any other remedy available to that party by reason of the other party's breach, but each and every remedy shall be cumulative and in addition to every other remedy given under this Agreement or existing at law or in equity.

IN WITNESS WHEREOF, the parties have executed this Agreement effective JAN 15, 2015.

LESSOR: MEDFORD URBAN RENEWAL AGENCY

Chris Coakley
Chairman

1-15-15
Dated

LESSEE: SKY PARK MEDFORD LLC

Mark McKechnie
Mark McKechnie, Authorized Member

1-2-15
Dated

Exhibit A

Agreement between Medford Urban Renewal Agency and Sky Park Medford LLC.
Regarding Parking at the Central A Parking Lot – 206 S. Central Avenue

RECITALS

The Medford Urban Renewal Agency (MURA) and Sky Park Medford LLC. (Developer) are parties to an agreement to build residential units above a portion of the Central A parking lot located at 206 S. Central Avenue and a lease of certain portions of the Central A parking lot.

This document is the agreement for leasing parking spaces for the residential development.

CITY OBLIGATIONS

1. MURA will add monthly permit spaces to the Central A parking lot.
2. One space per unit of the residential development will be leased to Developer at the going monthly rate.
3. Rates may increase at the discretion of MURA, consistent with rate increases for other permit parking spaces within the Central Business District.
4. MURA may oversell parking spaces for empty units or units not using parking spaces during parking enforcement hours. This is to assure the spaces are not left open during parking enforcement hours.

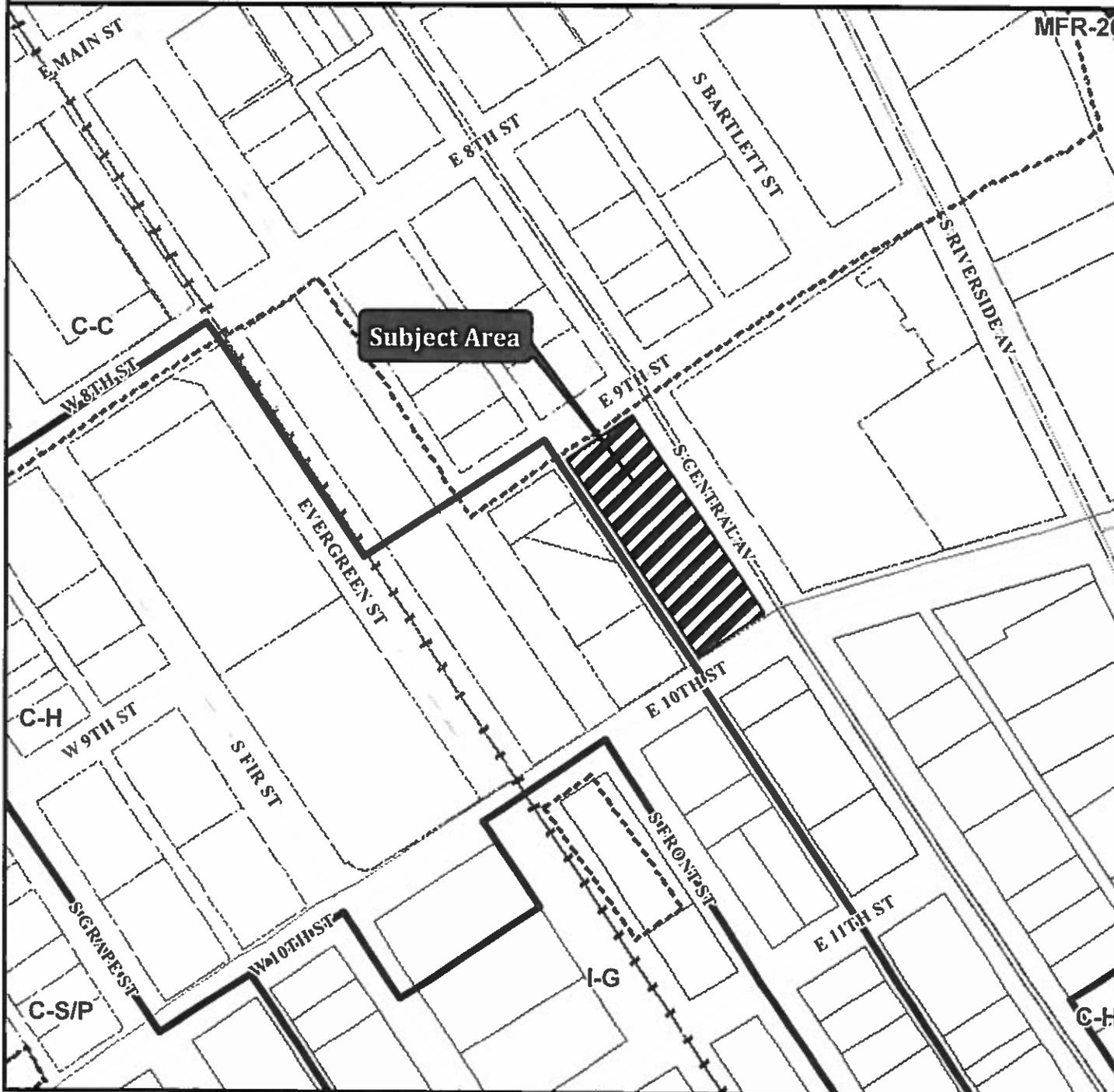
SKYPARK LLC. OBLIGATIONS

1. Developer will lease one parking space per unit at the going rate to be determined by MURA.
2. Developer will notify MURA when residential spaces are empty and parking spaces are available within 10 days of vacancy.
3. Developer will notify MURA when parking spaces are available due to non-use of unit.
4. Developer will provide a 30 day notice to MURA when unit is sold, leased, or rented and the date when parking space is required by the unit.

Q
11 of 11



Vicinity Map



Application Name/Description:

Skypark Medford

Proposal:

26 dwelling units and relief from minimum parking standards

File Numbers:

AC-15-007 & E-15-009

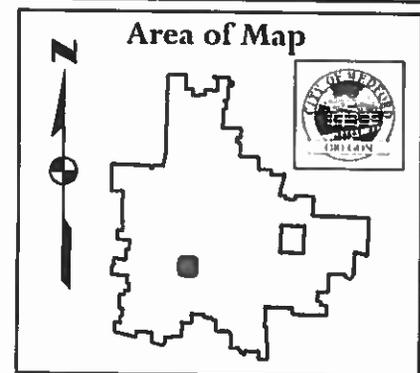
Applicant:

Skypark Medford LLC

Map/Taxlot:

371W30BC TL 11600

-  **Subject Area**
-  **Medford Zoning**
-  **UGB**
-  **Tax Lots**
-  **Central Business**
-  **Historic**



03/03/2015



CITY OF MEDFORD

PLANNING DEPARTMENT

STAFF REPORT

Date: March 27, 2015 for meeting of April 3, 2015
To: Site Plan and Architectural Commission
From: Jennifer Jones, Planner II
Reviewed By: Kelly Akin, Principal Planner 
Subject: Starbucks and Medical Office (AC-15-013)
Oregon Architecture, Inc., Applicant (Mark McKechnie, Agent)

BACKGROUND

Proposal

Consideration of plans for the construction of a 1,850 square foot coffee shop and a 3,285 square foot medical office building on a 1.01 acre parcel located on the south side of Barnett Road between Black Oak Road and Murphy Road, within a C-C (Community Commercial) zoning district.

Subject Site Zoning, GLUP Designation and Existing Uses

Subject Site Zoning: C-C (Community Commercial)
GLUP Designation: CM (Commercial)
Existing Uses: Vacant

Surrounding Property Zoning and Uses

North	C-S/P (Service Commercial and Professional Office) Hospital
South	C-C Medical Offices
East	C-C Medical Office
West	C-C Bank

10.290 Site Plan and Architectural Commission Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land; and
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

Corporate Names

The application lists Barnett Stage LLC as the owner of the subject property. As per the State of Oregon Business Registry, Dan Thomas is listed as the registered agent.

ISSUES/ANALYSIS

Background

The subject site is partially developed with a Bank of the Cascades building on the western portion of the site, approved in 2007. More than half of the 1.01 acre parcel remains undeveloped and is the subject of this application.

Scope of Project

The applicant is seeking to build a 1,850 square foot Starbucks with a drive-thru, and a 3,285 square foot building to be used as medical office, on the subject site. Subject to approval of the Commission, the Starbucks will feature an outdoor seating area to accommodate at least 16 people. As part of the proposal, the applicant is seeking approval for a reduction in required parking based on the specific nature of the businesses.

Building Elevations

Starbucks

The Starbucks building elevations show a one-story building featuring Hardie Board lap siding with Hardie Board accent trim (Exhibit G). The exterior color palette is very neutral with "Desert Sand" proposed as the primary color, and "Timber Bark" as the trim accent (Exhibit H). The building features steel canopies over both the entrance and the drive-thru window. Fabric and metal awnings are featured on all four sides of the building. Large, aluminum store front windows are prominent on the north, east, and west elevations. The south (rear) elevation has the fewest architectural features, but includes an awning over a delivery/service door, a window, and roof access. The service door

and much of the south elevation will be obstructed from view by the menu board and vehicles in the drive-thru lane.

Medical Office

The elevations for the medical office building feature a brick façade with aluminum store front windows comprising the majority of the wall space on all sides (Exhibit G). Architectural interest is created with a combination of tan stacked brick, dark reddish soldier course brick, stucco panels, and a stucco cornice. Steel canopies over the entrance doors create a consistent design theme with the adjacent Starbucks building.

Signage

The building elevations of the Starbucks show wall signage, as is typical of this business. The signage is for illustrative purposes only and was not reviewed to determine consistency with the maximum allowance per the Medford Land Development Code. All proposed signs are subject to an administrative sign permit through the Planning Department.

Vehicular Parking

The applicant is proposing 29 parking spaces plus stacking for eight vehicles in the drive-thru lane.

According to Medford Land Development Code 10.743-1, the minimum required parking for the Starbucks building would be 12.0 spaces per 1,000 square feet of gross floor area plus 5.0 spaces for drive-up window queue. This calculates to 22.2 spaces plus the 5.0 spaces for the drive-thru queue. In addition, for the proposed medical office building, the Code requires 4.5 spaces per 1,000 square feet of gross floor area plus 1.0 space per doctor. As a specific tenant has not been identified for the medical office building, an exact number of doctors is not yet determined. However, parking works out to be 14.7 spaces plus 1.0 per doctor, or a minimum of 16 spaces for an office with one doctor. Combined, these two proposed uses would require a minimum of 38 shared spaces plus 5.0 spaces for drive-thru queue.

The proposed number of parking spaces, 29 vehicular spaces, does not meet the required amount. However, 10.743(3) provides authority to the Commission to approve the proposal with less than the typically required parking, *if they find the applicant's detailed description of the proposed use demonstrates that the number of needed parking spaces is less than the minimum required.* The applicant's questionnaire explains that the two uses proposed typically experience different peak hours, with Starbucks maximum parking need between 6am and 9am, and a medical office building typically not opening until 8:30am. The applicant suggests that this offset of peak parking demand, together with additional spaces in the drive-thru queue of Starbucks (eight rather than the five required), and the presence of a large parking area just to the south of the site, is sufficient demonstration of a parking need less than that required by the Land Development Code.

As many of the customers for the Starbucks will likely be walk-up customers from the surrounding medical buildings and vehicular customers utilizing the drive-thru window only, it is reasonable to assume that the two uses would not experience a parking demand as great as contemplated in the Code requirement. Ultimately, it is up to the Commission to determine whether or not the applicant has provided adequate parking for the two uses proposed in this development.

Bicycle Parking

Medford Land Development Code Section 10.748 requires bicycle parking to be calculated at ten percent of the number of spaces provided for automobiles. The applicant is proposing to provide 29 parking spaces on site which would translate to three required bicycle spaces. The applicant has provided a detail of bicycle parking, which features four bicycle parking spaces for each building, utilizing an inverted "U" style bike rack with a fabric awning cover (Exhibit D). Four bicycle spaces at each building, for a total of eight bicycle spaces meets the requirements of the Code.

Pedestrian Walkways

Pedestrian walkways are proposed for the project to connect the sidewalk on Barnett Road to each of the proposed buildings and also to connect between the Starbucks and the medical office building. While these connections meet the requirements of Medford Land Development Code Section 10.773, considering the heavy pedestrian traffic that the Starbucks is likely to generate, additional connections with the businesses and parking areas to the south are recommended.

Details for the pedestrian walkways have not yet been provided by the applicant and are not specifically explained on the site plan. However, all pedestrian walkways are required to meet the standards set forth in Section 10.775 of the Land Development Code, including width, materials, and lighting. Pedestrian walkway design compliance is recommended as a condition of approval (Exhibit A).

Concealments

A CMU block trash enclosure is proposed on the south side of the site. An elevation for the trash enclosure has been submitted which shows anchor pins and side hinged doors consistent with the standards of Rogue Disposal & Recycling (Exhibit D). The materials also show compliance with the Medford Land Development Code.

The mechanical equipment will be roof-mounted and screened behind the parapets of each building (Exhibits E & F).

Landscaping

The Landscape Plan shows a variety of plantings around the proposed buildings, along Barnett Road, within the parking areas, and adjacent to the drive-thru aisle (Exhibit C). The landscaping for the project is critical to the design and serves to separate patrons from the drive-thru lane, to shield the parking area from the public and to provide shade

for parking. Structural soils have been utilized in the Landscape Plan to ensure the health of the trees in the parking areas. The subject site has approximately 168-feet of frontage along Barnett Road and meets or exceeds all frontage landscaping requirements per MLDC 10.797. However, while the square footage and counts of plantings are adequate and are filled out on the application, they are not presented on the Landscape Plan as required. A revised Landscape Plan should be submitted with the necessary information and is included as a Condition of Approval (Exhibit A).

Right-Of-Way Dedication

As explained in the Public Works Report, section 10.428 of the Medford Land Development Code requires a total right-of-way width of 100-feet (50-feet from centerline) (Exhibit J). As the current right-of-way is 36-feet from centerline, the required street dedication along Barnett Road, a major arterial street, is 14-feet. The applicant has indicated a desire to apply for an exception to this street dedication requirement. The exception application has not yet been received. Approval of an exception and any associated site design modifications is included in the Conditions of Approval (Exhibit A).

No other issues have been identified by staff.

FINDINGS OF FACT

MLDC Section 10.290 – Site Plan & Architectural Commission Approval Criteria

- (1) The proposed development is compatible with uses and development that exist on adjacent land;

The Commission can find that there is sufficient evidence contained in the Applicant's narrative and questionnaire to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

RECOMMENDED ACTION

Direct staff to prepare a Final Order for approval of AC-15-013 per the Staff Report dated March 27, 2015, including Exhibits A through M.

EXHIBITS

- A Conditions of Approval dated March 27, 2015
- B Site Plan received March 25, 2015

**Starbucks and Medical Office (AC-15-013)
Revised Staff Report**

March 27, 2015

- C Landscape Plan received February 6, 2015
- D Site Details received March 26, 2015
- E Starbucks Roof Plan, Floor Plan, and Elevations received January 30, 2015
- F Medical Office Roof Plan, Floor Plan, and Elevations received January 30, 2015
- G Color Renderings of Building Elevations received February 24, 2015
- H Proposed Materials received February 24, 2015
- I Applicant's Questionnaire received January 30, 2015
- J Public Works Staff Report received March 18, 2015
- K Fire Department Report received March 17, 2015
- L Building Department Memo received March 18, 2015
- M Medford Water Commission Memo received March 26, 2015
Vicinity Map

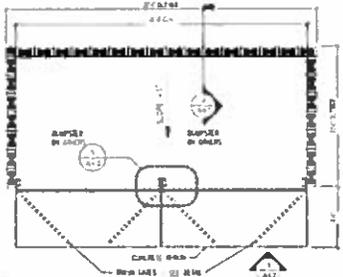
SITE PLAN & ARCHITECTURAL COMMISSION AGENDA: APRIL 3, 2015

EXHIBIT A

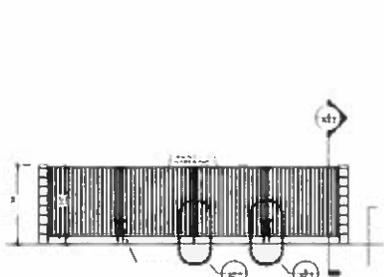
Starbucks & Medical Office
AC-15-013
Conditions of Approval
April 3, 2015

CODE REQUIREMENTS

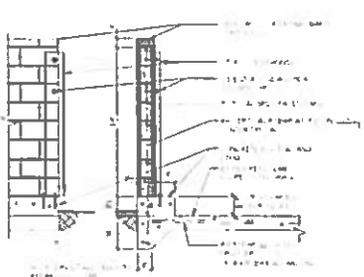
1. Prior to issuance of the first building permit, the applicant shall:
 - a. Comply with the Public Works Department Memo received March 18, 2015 (Exhibit J).
 - b. Comply with the memo from the Medford Water Commission Memo dated March 26, 2015 (Exhibit M).
 - c. Submit a detail of the pedestrian walkway to be in compliance with the requirements listed in Medford Land Development Code section 10.775.
 - d. Submit a revised Landscape Plan which includes the total square footage of all landscaping areas and high water use landscape elements, as required by Medford Land Development Code section 10.780F(4).
2. Prior to installation of any signage, administrative sign permits must be submitted for review and approval to the Medford Planning Department.



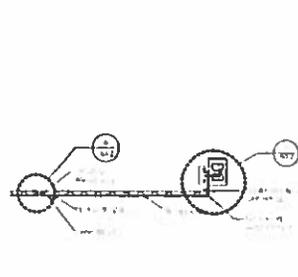
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SCALE: 1/2" = 1'-0"



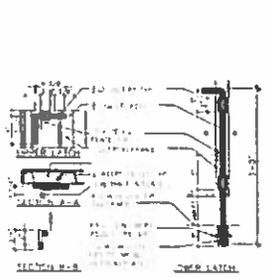
3 TRASH ENCLOSURE ELEVATION
SCALE: 1/2" = 1'-0"



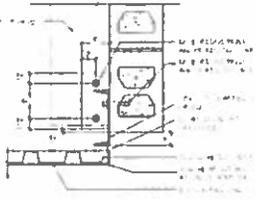
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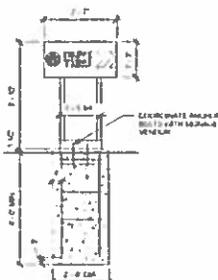
5 GATE
SCALE: 1/2" = 1'-0"



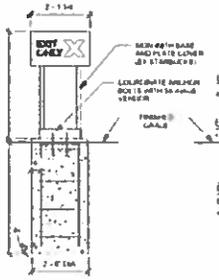
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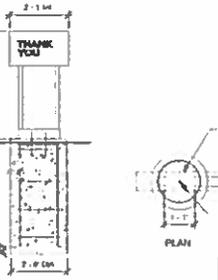
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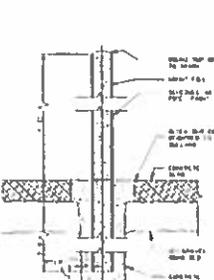
8 SIGN FOOTINGS
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9 SIGN FOOTINGS
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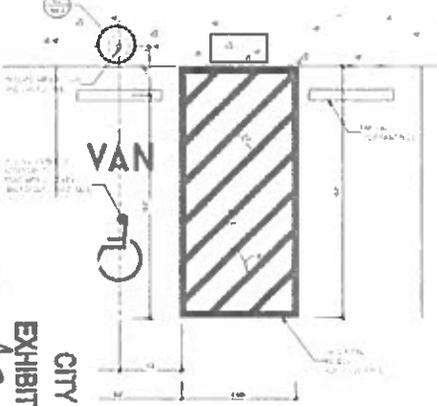
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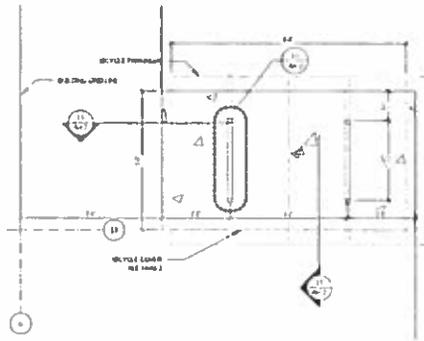
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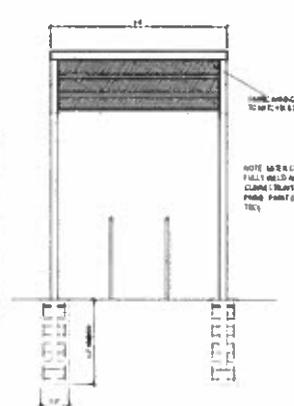
DISABLED PARKING SIGN



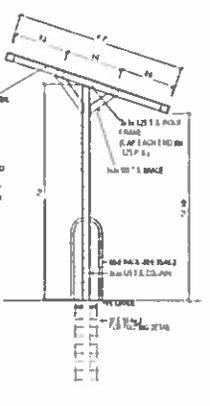
VANDICAP PARKING
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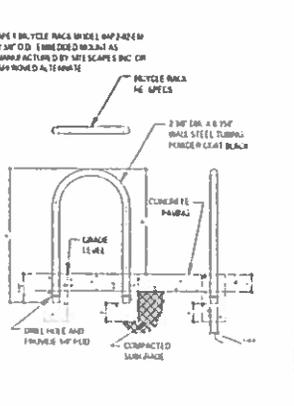
14 BICYCLE PARKING
SCALE: 1/2" = 1'-0"



15 BICYCLE COVER
SCALE: 1/2" = 1'-0"



14 BICYCLE RACK
SCALE: 1/2" = 1'-0"



Proposed New Commercial Building
INTENDED TENANT

MEDICAL OFFICES



OREGON ARCHITECTURE
 221 West Tenth Street
 Astoria, Oregon 97101
 Phone 541-722-4372 Fax 541-409-6020

STARBUCKS COFFEE
 27th EAST BARNETT RD, ASTORIA, OR
 97101-4141

Revised	By	Date

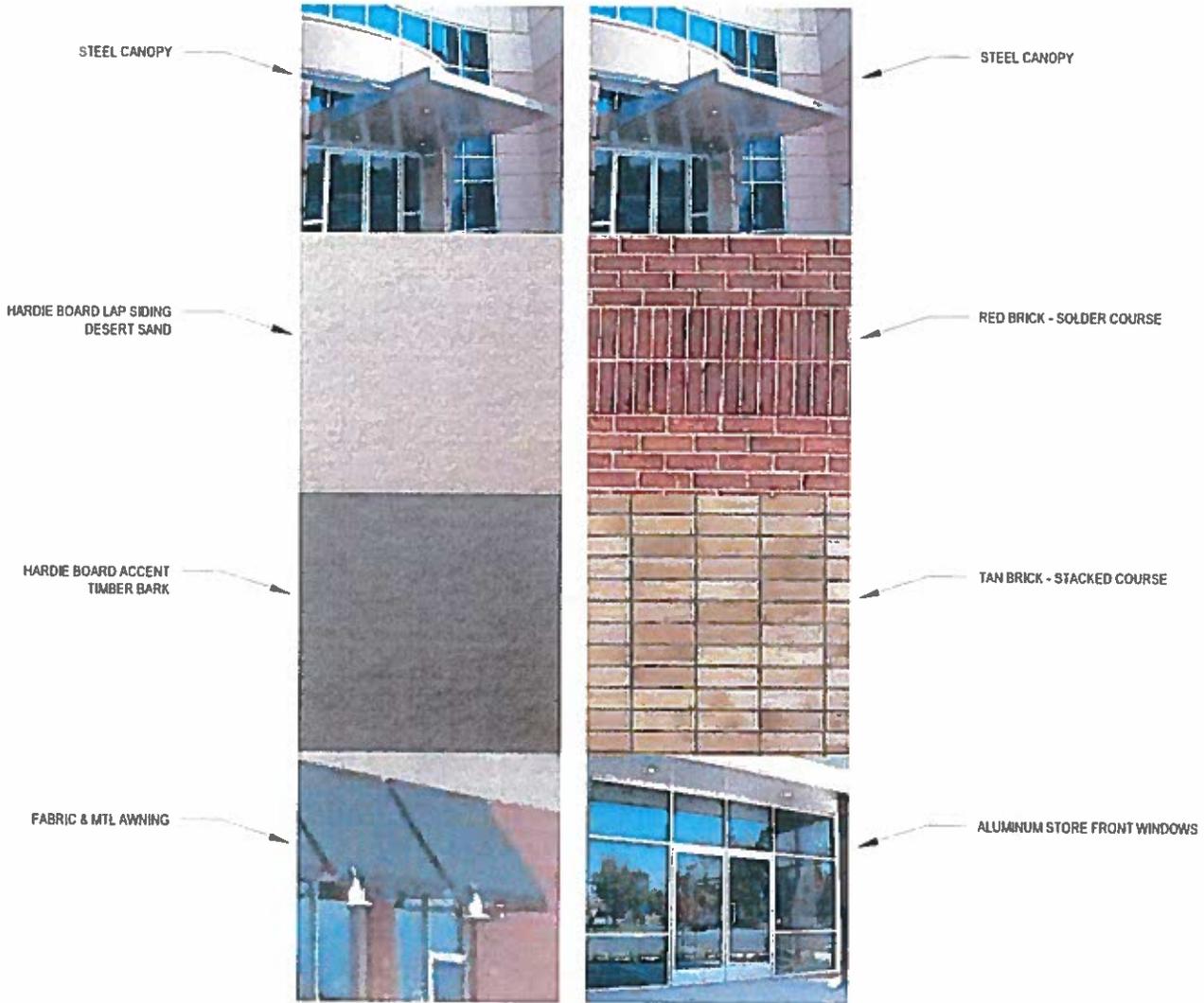
A2.5

Proposed New Commercial Building

INTENDED TENANT

STARBUCKS COFFEE

MEDICAL OFFICES

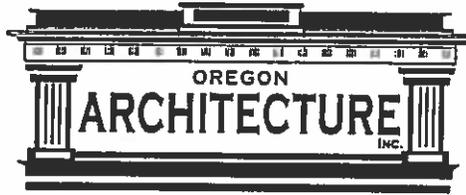


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FEB 24 2015
PLANNING DEPT.



STANDARD SPECIFICATIONS
 NO. 1001
 CITY OF MEDFORD
 1001
 1001
 1001

PROJECT NO.	1001
SHEET NO.	A2.2
DATE	2/15/15
BY	...
CHECKED BY	...
APPROVED BY	...



RECEIVED
JAN 30 2015
PLANNING DEPT.

(541) 772-4372

221 WEST 10TH ST • MEDFORD, OR 97501 • FAX: (541) 499-6329 • WWW.OREGONARCHITECTURE.BIZ

January 21, 2015

NARRATIVE FOR TWO PROPOSED BUILDINGS ON THIS SITE, A STARBUCKS AND A MEDICAL OFFICE BUILDING

PROJECT DESCRIPTION:

The Developer is proposing to construct a 1,850 square foot single-story freestanding Starbucks Coffee Shop with a drive-thru on the east end of this site, which is currently occupied by a branch bank on the west side of the one acre plus property. The proposed Starbucks building will be clad in horizontal lap siding of varying depths of exposure and will have aluminum storefront windows.

In addition, the Owner proposes to construct a 3,285 square foot single story medical office building between the proposed Starbucks and the Bank of the Cascades structure. The balance of the site will be taken up with parking spaces and landscaping. The proposed medical office building will be sheathed in brick.

The site is only accessed from an internal driveway along the southern property line. It has no direct access on to Barnett Road/

PROJECT COMPATIBILITY WITH THE NEIGHBORHOOD

- A. List the existing uses and development adjacent to your project site. Describe the architecture, age, and condition of the adjacent buildings. Along with this list describe the architecture, age and condition of the adjacent buildings.**

The one acre parcel fronts on East Barnett Road and is immediately adjacent on the east to the driveway entrance for the Southern Oregon Orthopedics Building. That building is located to the south of this site and abuts Larsen Creek. The driveway, which is signal controlled, is located on the east side of the subject parcel.

Southern Oregon Orthopedics is currently constructing a new annex building on the east side of their existing driveway. The Rogue Valley Hospital campus is to the north of the subject site on the north side of East Barnett Road. West of the project site is a Bank of the Cascades branch, which was constructed within the last few years. To the south of the site is a large parking lot reserved for the Southern Oregon Orthopedics building, again located on the south side of the abutting parking lot.

- B. Describe building architecture and exterior treatments in this proposal and how they fit with and complement adjacent buildings and development.**

The exterior facades of the two buildings will be a mix of materials in a scale sympathetic to surrounding development. The area in which these buildings are located is characterized by one-story medical office buildings primarily with brick or stucco exteriors. These buildings will fit within this context and provide additional visual variety for drivers along this stretch of East Barnett.

- C. Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass.**

The Starbucks Building is only 24 feet wide on the face parallel with East Barnett and the adjacent office building will have approximately 50 feet of frontage along East Barnett, so neither building is overly large. In addition to that the structures will employ cornices, projections and decorative trim to add visual interest. In the case of

the Starbucks Building, the lap exposure of the siding will vary from 10-inches at the base to 4-inches at the top of the parapet, as well as trim boards in a contrasting color to provide additional visual interest.

D. Describe how the placement and orientation of the proposed building relates to the street facilities and how this orientation promotes a more pedestrian friendly site design.

The buildings have been placed on a north/south grid, with building faces parallel with East Barnett to maintain the symmetry of the block. There is a small parking area between the buildings, but the bulk of the parking will be behind the office building and away from the street.

E. If the site lies within 600-feet of an existing transit stop describe compliance with the standards of Section 10.808.

East Barnett Road is a transit route with established bus stops, but they do not appear to be within 600 feet of the site. However, the Developer expects a significant amount of foot traffic on the site from employers around the area, including the hospital across the street. Consequently, no parking has been placed between the public sidewalk and either the coffee shop's outdoor space or the entry to the medical building. The front door of the Starbucks will be approximately 30 feet from the public side walk, which is pretty close to the street considering there is an existing 15 foot PUE along Barnett Road, which prevents that building from being closer to the street.

The medical office building will have its main entrance on a structure corner, convenient to both street pedestrian traffic and those coming from the parking area.

F. Describe pedestrian facilities and amenities on your site and how they will function for pedestrians.

Part of the site amenities for this project is a patio space in front of the Starbucks building, easily accessible from the public sidewalk. As per Starbucks corporate policy, the patio and its tables are available to the public and not just reserved for patrons. That is an amenity that is often used at many Starbucks locations.

G. Describe vehicle and pedestrian access to the site and how it relates internally on the site and to adjacent sites.

Pedestrian access to either building from the public sidewalk has already been addressed above. Pedestrian access to either proposed structure will be by means of sidewalks from the parking areas. Vehicle access is from an access road on the south side of the site that parallels East Barnett. No access is available directly from East Barnett except through the signalized intersection to the east of the site, or from the west on the other side of Bank of the Cascades.

H. Describe if and how the proposed plan is sensitive to retaining any existing trees or significant native vegetation on the site.

There are no trees or significant native vegetation existing on the site. The site will be landscaped to meet current code requirements.

I. Describe stormwater detention facilities on the site. If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.

Stormwater will be handled with detention piping under the parking lot. The land slopes away from East Barnett towards Larsen Creek, with the bulk of the landscaped area on the opposite side of the building and upslope from the parking area, so a landscaped swale for detention is not a practical solution on this project.

J. Describe how your proposed landscaping design will enhance the building and other functions on the site.

Landscaping can be a critical element in the design of a building. We will be using a planted hedge to separate patrons from the drive-through lane. We will use landscaping to shield the parking area from the public sidewalk. We will use landscape planters to provide shade trees for the parking area.

K. Describe how your exterior lighting illuminates the site, and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of section 10.764.

The parking areas will be lit with freestanding poles with fixtures designed to not cast light above the horizontal plane. Some of the parking may also be lit with building mounted fixtures. There will also be a small amount of decorative lighting.

All fixtures and their placement will be designed to not illuminate the night sky and to eliminate glare on adjacent properties.

L. Describe any proposed signage and how it will identify the location of the occupant and serve as an attractive complement to the site.

The Starbucks building will have a freestanding pylon sign facing perpendicular to East Barnett, in addition to its standard building signs. The Starbucks sign package is well recognized and no changes are proposed to their standard sign package, which is expected to meet City of Medford requirements.

The medical office building is expected to just have building signage.

M. Explain any proposed fencing, including its purpose, and how it has been incorporated as a functional, attractive component of the development.

No fencing, other than a guard rail along the drive-thru lane, is proposed.

N. Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of Section 10.752-10.761.

Neither location is expected to generate any noise above ordinary conversation levels.

O. Explain anything else about your project that adds to the compatibility of the project with adjacent development and uses.

These two uses fit nicely into this existing neighborhood and, with the current construction on the adjacent block, complete the development along East Barnett Road of facilities that serve the medical needs of the citizens of Medford.

P. List and explain any exceptions or modifications requested and provide reasons for such.

Section 10.743 lists the parking requirements for these two uses – 20 for the Starbucks and 13 for the medical office. It also requires 5 spaces in the drive-thru lane. We have provided room for eight cars in the drive-thru lane. Section 10.744 talks about shared parking facilities. In this project I believe a case can be made for allowing the overlap of the maximum parking requirements for each use. A Starbucks typically sees its maximum parking between 6 and 9 am. Typically a medical office doesn't open until 8:30 am, with its maximum parking in the afternoons. We believe there will be adequate parking on this site for these two uses and request that a 13% overlap be allowed. In addition, there is a very large, very empty existing lot immediately to the south of this site that can serve as overflow parking in the unlikely event things get crowded.

Q. List any petition for relief of landscaping standards. Provide rationale for requested deviation from standard.

No exceptions or modifications to the Land Development landscape standards are contemplated or proposed at this time.



Continuous Improvement Customer Service

CITY OF MEDFORD

RECEIVED
MAR 18 2015
PLANNING DEPT.

Date: 3/18/2015
File Number: AC-15-013

PUBLIC WORKS DEPARTMENT STAFF REPORT MEDICAL OFFICE / COFFEE SHOP

Project: Consideration of plans for construction of a 1,850 square foot coffee shop and a 3,285 square foot medical office building on a 1.01 acre parcel.

Location: South side of Barnett Road between Black Oak Road and Murphy Road; 371EW33b TL 434.

Zoning: C-C (Community Commercial) District.

Applicant: Thomas Fox Properties, LLC

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Barnett Road right-of-way and PUE dedications
- Submittal and approval of plans for site grading and drainage
- Submittal and approval of plans for site public improvements and work within the public right-of-way, if required.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.

P:\Staff Reports\AC\2015\AC-15-013 Oregon Architecture, Inc\AC-15-013 Medical Office Starbucks Staff Report - DB.docx Page 1

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552
CITY OF MEDFORD

EXHIBIT # J

File # AC-15-013

A. STREETS

1. Dedications

Barnett Road is classified as a major arterial street, and in accordance with Medford Land Development Code (MLDC) Section 10.428, it requires a total right-of-way width of 100 feet. The Developer shall dedicate sufficient area south of the centerline to provide 50 feet of right-of-way measured from centerline, which is half of the full 100 feet required by the Development Code for the entire width. The existing right-of-way south of the centerline appears to be 36 feet. Prior to issue of any permit for construction, the Developer shall dedicate an amount of right-of-way necessary to result in a half-width measure of 50 feet. **The amount of additional right-of-way needed appears to be 14 feet.**

The Developer may request an Exception to the City's right-of-standards on Barnett Road to reduce the amount of right-of-way to be dedicated from 14 feet to 8 feet. The 6 feet of reduction would mean the elimination of a bike lane on Barnett Road in the future. Public Works and Planning has been involved with creating a multi-use path along Larson Creek parallel to Barnett Road and approximately 500 feet south, to provide bike facilities in this area in-lieu of Barnett Road. Based on this work, Public Works does not anticipate it will try to provide bike lanes on Barnett in the future, and would have no objection to this request.

The developer will receive S.S.D.C. (Street System Development Charge) credits for the public right-of-way dedication on Barnett Road, per the methodology established by the MLDC 3.815. **Should the developer elect to have the value of the land be determined by an appraisal, a letter to that effect must be submitted to the City Engineer within sixty (60) calendar days of the date of the Final Order of the Site Plan and Architectural Commission. The City will then select an appraiser, and a cash deposit will be required as stated in Section 3.815.**

In accordance with MLDC 10.471, **the property owner shall dedicate a 10 foot wide public utility easement (PUE) adjacent to the new right-of-way line on Barnett Road.**

The right-of-way and PUE dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: right-of-way and PUE dedications, a copy of a current lot book report, preliminary title report, or title policy; a mathematical closure report (if applicable), and the Planning Department file number, all for review and signature acceptance by the City Engineer prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the areas dedicated.

Medical Center Drive is a private drive and not public right-of-way. Applicant shall provide documentation of legal right to use Medical Center Drive for access to the proposed project.

2. Public Improvements

a. Public Streets

All standard street section improvements have been completed on Barnett Road, including pavement, curb and gutter and sidewalk. **No additional street are required.**

b. Street Lights

The Developer shall protect and preserve all existing street lighting, power supply, and appurtenances.

c. Pavement Moratoriums

There is a pavement cutting moratorium on Barnett Road at this frontage effective until 8/17/2019. No pavement cuts are allowed in Barnett Road except as provided in MMC 3.070.

3. Access and Circulation

Access to this site shall be restricted to Medical Center Drive.

4. MLDC Section 10.668 Analysis

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless: (1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose, and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or (2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and are supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, and pedestrians. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the required dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Also, the City is allowed to consider the benefits to the development from the dedication and

improvements when determining “rough proportionality.”

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Barnett Road:

The additional right-of-way will provide the needed width for a future planter strip ~~and bike lane~~ on Barnett Road. If an Exception is applied for and approved, then the additional 8 feet will just be for a future 10 foot wide planter strip. Barnett Road is a 35 mile per hour facility, which currently carries approximately 16,000 vehicles per day. The 10’ planter strip moves pedestrians a safe distance from the edge of the roadway. Barnett Road will be the primary route for pedestrians traveling to and from this development.

The City assesses System Development Charges (SDC’s) to help pay for acquisition of right-of-way and construction of additional Arterial & Collector street capacity required as a result of new development. Because a mechanism exists in the form of SDC credit for right-of-way dedication and street improvements in accordance with Medford Municipal Code (MMC) 3.815 and other applicable parts of the Code, to fairly compensate the applicant, the conditions of MLDC, Section 10.668 are satisfied.

B. SANITARY SEWERS

This site lies within the Medford sewer service area. The proposed site plan does not indicate the location of building connections to the existing sanitary sewer main. The Developer shall ensure that this connection is to an existing main as a separate individual lateral or make a new connection to an existing main with a separate lateral.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval. All area catch basins shall meet Department of Environmental Quality (DEQ) requirements, which include a down-turned elbow and sump.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development shall be submitted with the building permit application for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Storm water quality and detention facilities shall be required in accordance with Medford Land Development Code Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings

P:\Staff Reports\AC\2015\AC-15-013 Oregon Architecture, Inc\AC-15-013 Medical Office Starbucks Staff Report - DB.docx Page 5

(3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the Site Plan and Architectural Commission's Final Order, together with all pertinent details and calculations. The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges

Buildings in this development are subject to street, storm drain, sanitary sewer collection and treatment system development charges (SDC). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Kris Lillie, RH2 Engineering

P:\Staff Reports\AC\2015\AC-15-013 Oregon Architecture, Inc\AC-15-013 Medical Office Starbucks Staff Report - DB.docx Page 6

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

SUMMARY CONDITIONS OF APPROVAL

Medical Office / Coffee Shop

AC-15-013

A. Streets

1. Street Dedications to the Public:

Dedicate 14' of additional public right-of-way on south side of Barnett Road. Developer may apply for an exception to reduce public right-of-way dedication to 8'.

Dedicate 10 foot Public Utility Easement (PUE) along frontage.

2. Improvements:

No additional street improvements are required for this development.

Pavement moratoriums are currently in effect.

B. Sanitary Sewer:

Construct separate individual sanitary sewer connection.

C. Storm Drainage:

Provide a comprehensive grading and drainage plan.

Provide water quality and detention facilities, calculations and O&M Manual.

Provide engineers certification of stormwater facility construction.

Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
E-mail www.fire@ci.medford.or.us

RECEIVED

MAR 17 2015

PLANNING DEPT.

LAND DEVELOPMENT REPORT - PLANNING

To: Jennifer Jones

LD Meeting Date: 03/18/2015

From: Fire Marshal Kleinberg

Report Prepared: 03/10/2015

File #: AC - 15 - 13

Site Name/Description: Coffee shop and medical office building

Consideration of plans for the construction of a 1,850 square foot coffee shop and a 3,285 square foot medical office building on a 1.01 acre parcel located on the south side of Barnett Road between Black Oak Road and Murphy Road, within a C-C (Community Commercial) zoning district; Oregon Architecture, Inc., Applicant/Agent. Jennifer Jones, Planner.

DESCRIPTION OF CORRECTIONS	REFERENCE
<u>Approved as Submitted</u> Meets Requirement: No Additional Requirements	

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.



RECEIVED

MAR 18 2015

PLANNING DEPT.

Memo

To: Jennifer Jones, Planner, Planning Department
From: Tanner Fairington, Building Department
CC: Oregon Architecture, Applicant / Agent
Date: March 17, 2015
Re: March 18, 2015 LDC Meeting: AC-15-013 – Item #4

Please Note:

This is not a plan review. These are general notes based on general information provided. Plans need to be submitted and will be reviewed by a commercial plans examiner to determine if there are any other requirements for this occupancy type.

Fees are based on valuation. Please contact the front counter for estimated fees.

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Code and Design Information" on left side of screen; click on "Design Criteria"; and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" on left side of screen for information.

General Comments:

3. The comments below are based on the site plans and elevations submitted. When final plans are submitted, and building information is provided, a more thorough review can be provided.

Site Plan

4. Accessible parking spaces to be per section 1106.7 of the 2014 OSSC, including figures referenced (see Building Codes Division for 2014 OSSC Errata which included these figures, dated October 16, 2014). Locations appear to be acceptable.
5. Please ensure accessible routes are provided from accessible parking spaces to main entrances.

CITY OF MEDFORD

EXHIBIT # L

File # AC-15-013



TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-15-013

PARCEL ID: 371W33B TL 434

PROJECT: Consideration of plans for the construction of a 1,850 square foot coffee shop and a 3,285 square foot medical office building on a 1.01 acre parcel located on the south side of Barnett Road between Black Oak Road and Murphy Road, within a C-C (Community Commercial) zoning district; Oregon Architecture, Inc., Applicant/Agent. Jennifer Jones, Planner.

DATE: March 16, 2015

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Applicant or applicants' civil engineer shall coordinate with MWC engineering staff for location and size of proposed domestic water meters and fire sprinkler protection system.
4. Dedication of a 10 foot wide access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement to be submitted to MWC for review and recordation prior to construction.
5. Installation of an MWC approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35> .

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction may be required.
3. MWC-metered water service does not exist to this property.
4. Access to MWC water lines is available. There is a 10-inch water line in Barnett Road, and an 8-inch water line in the re-aligned driveway to Southern Oregon Surgery Center.

Vicinity Map

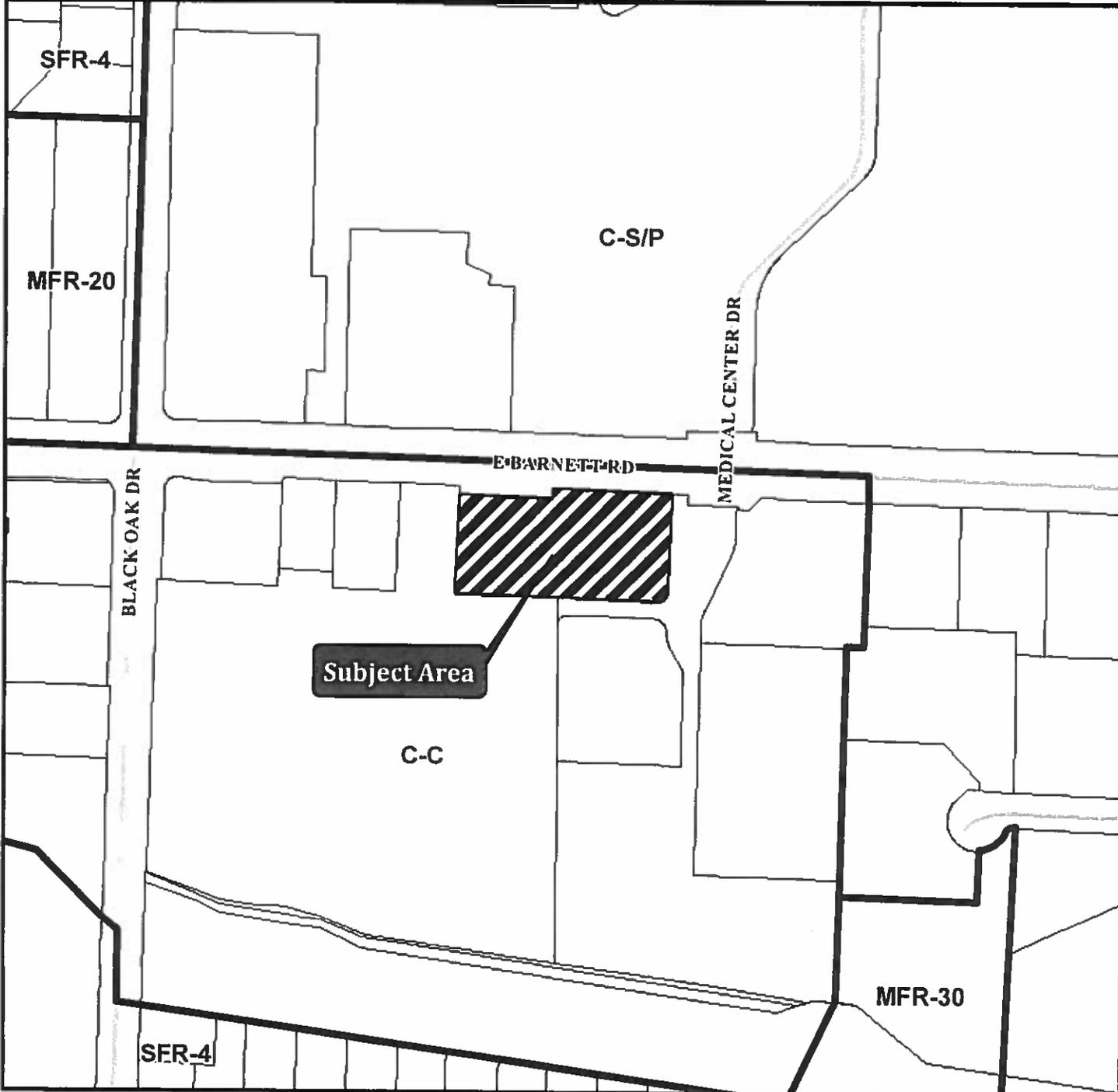
Application Name/Description:
Starbucks & Medical Office Building

Proposal:
Coffee shop with drive thru & medical office

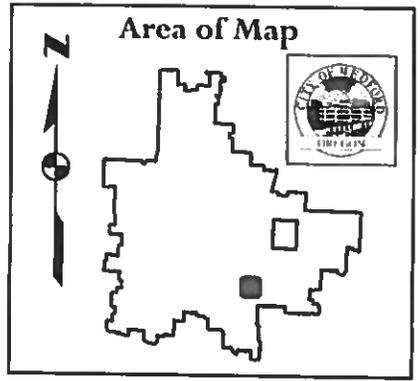
File Numbers:
AC-15-013

Applicant:
Oregon Architecture, Inc

Map/Taxlot:
371W33B TL 434



	Subject Area
	Medford Zoning
	UGB
	Tax Lots
	Central Business
	Historic



01/30/2015