

PLANNING COMMISSION

AGENDA

MARCH 22, 2018



Commission Members

David Culbertson

Joe Foley

Bill Mansfield

David McFadden

Mark McKechnie

E. J. McManus

Patrick Miranda

Alex Poythress

Jared Pulver

Regular Planning Commission meetings

are held on the second and fourth

Thursdays of every month

Meetings begin at 5:30 PM

City of Medford

City Council Chambers

411 W. Eighth Street, Third Floor

Medford, OR 97501

541-774-2380



Planning Commission

Agenda

Public Hearing

March 22, 2018

5:30 PM

Council Chambers, City Hall, Room 300
411 West Eighth Street, Medford, Oregon

10. Roll Call

20. Consent Calendar/Written Communications (voice vote)

20.1 **LDS-17-170** Final Order of a request for tentative plat approval for PDK Village Subdivision, a 15-lot residential subdivision on approximately 1.61 acres located southeast of the intersection of Lozier Lane and Lozier Court within an SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district. (PDK Properties; Scott Sinner Consulting, Inc., Agent; Steffen Roennfeldt, Planner).

20.2 **LDS-15-141 / E-15-142** Consideration of request for a one-year time extension of the approval of Panther Landing Subdivision, a seven lot residential subdivision with an Exception to reduce the street dedication requirement for Columbus Avenue and an Exception to the number of units allowed to take access off of a minimum access easement for an 0.86 acre parcel located on the east side of S Columbus Avenue, approximately 120 feet north of Garfield Street, within the SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district (1579 S. Columbus Avenue – 372W36CA2200). (Tom Malot Construction Company, Inc., Applicant; Farber Surveying, Agent; Sarah Sousa, Planner).

30. Minutes

30.1 Consideration for approval of minutes from the March 8, 2018, hearing.

40. Oral and Written Requests and Communications

Comments will be limited to 3 minutes per individual or 5 minutes if representing an organization. **PLEASE SIGN IN.**

50. Public Hearings

Comments are limited to a total of 10 minutes for applicants and/or their representatives. You may request a 5-minute rebuttal time. All others will be limited to 3 minutes per individual or 5 minutes if representing a group or organization. **PLEASE SIGN IN.**

Continuance Request

50.1 **CUP-17-116** Consideration of a request for a Conditional Use Permit (CUP) for a proposed Bed & Breakfast to be located at 15 Geneva Street in the SFR-6 (Single-Family Residential – 6 dwelling units per gross acre) zoning district, and within the Historic Preservation Overlay District (371W30AB TL 16400).

Meeting locations are generally accessible to persons with disabilities. To request interpreters for hearing impaired or other accommodations for persons with disabilities, please contact the ADA Coordinator at (541) 774-2074 or ada@cityofmedford.org at least three business days prior to the meeting to ensure availability. For TTY, dial 711 or (800) 735-1232.

(Gloria Thomas & Cecil de Hass, Applicants; Julie Krason, Agent; Dustin Severs, Planner). **The applicant has requested to continue this item to the Thursday, June 14, 2018 Planning Commission meeting.**

New Business

- 50.2 **ZC-17-168** Consideration of a zone change from SFR-00 (Single Family Residential – one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot located northeast of the intersection of North Phoenix Road and East Barnett Road in southeast Medford (371W27 1605) The application also includes a request to modify a condition of approval in the matter of File No. ZC-15-041 limiting traffic generation for 955 North Phoenix Road (371W34 501). (North Phoenix Enterprises LLC, Applicant; CSA Planning, Agent; Steffen Roennfeldt, Planner).

- 50.3 **ZC-18-008** Consideration of a zone change on 1.06 acre parcel located south of East Barnett Road, approximately 530 feet east of Ellendale Drive from MFR-20 (Multi Family Residential – 15 to 20 dwelling units per gross acre) to MFR-30 (Multi-Family Residential – 20 to 30 dwelling units per gross acre) (371W32AB500). (Stylus Development LLC, Applicant; ORW Architecture, Agent; Steffen Roennfeldt, Planner).

60. Reports

60.1 Site Plan and Architectural Commission

60.2 Joint Transportation Subcommittee

60.3 Planning Department

70. Messages and Papers from the Chair

80. Remarks from the City Attorney

90. Propositions and Remarks from the Commission

100. Adjournment

**BEFORE THE MEDFORD PLANNING COMMISSION
STATE OF OREGON, CITY OF MEDFORD**

IN THE MATTER OF TENTATIVE PLAT APPROVAL OF _____)
PDK VILLAGE _____ [LDS-17-170]) **ORDER**

ORDER granting approval of a request for tentative plat for *PDK Village*, described as follows:

A 15-lot residential subdivision on approximately 1.61 acres located southeast of the intersection of Lozier Lane and Lozier Court within an SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district.

WHEREAS:

1. The Planning Commission has duly accepted the application filed in accordance with the Medford Land Development Code, Sections 10.265 through 10.267; and
2. The Medford Planning Commission has duly held a public hearing on the request for tentative plat approval for *PDK Village*, as described above, with the public hearing a matter of record of the Planning Commission on March 8, 2018.
3. At the public hearing on said tentative plat, evidence and recommendations were received and presented by the developer and Planning Department Staff; and
4. At the conclusion of said hearing, after consideration and discussion, the Medford Planning Commission, upon a motion duly seconded granted tentative plat for *PDK Village*, as described above and directed staff to prepare a final order with all conditions and findings set forth for the granting of the tentative plat approval.

THEREFORE LET IT BE HEREBY ORDERED that the tentative plat for *PDK Village*, stands approved per the Planning Commission Report dated March 8, 2018, and subject to compliance with all conditions contained therein.

AND LET IT FURTHER BE OF RECORD, that the action of the Planning Commission in approving this request for tentative plat approval is hereafter supported by the findings referenced in the Planning Commission Report dated March 8, 2018.

BASED UPON THE ABOVE, the Planning Commission determined that the tentative plat is in conformity with the provisions of law and Section 10.270 Land Division Criteria of the Land Development Code of the City of Medford.

Accepted and approved this 22nd day of March, 2018.

CITY OF MEDFORD PLANNING COMMISSION

Planning Commission Chair

ATTEST:

Planning Department Representative



PLANNING COMMISSION REPORT

for a Type-C quasi-judicial decision: Land Division

Project PDK Village
 Applicant: PDK Properties; Agent: Scott Sinner

File no. LDS-17-170

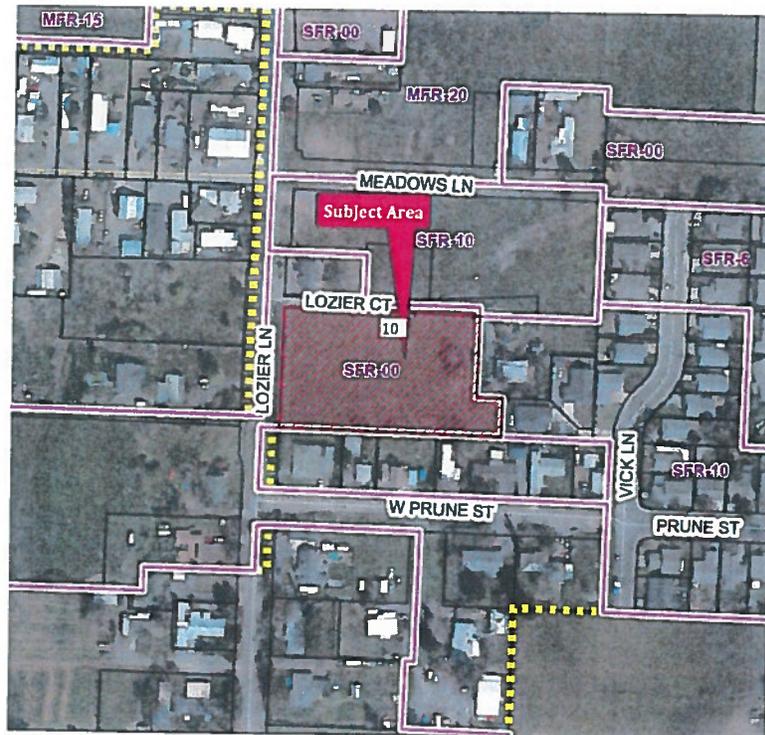
Date March 8, 2018

BACKGROUND

Proposal

Consideration of a request for tentative plat approval for PDK Village Subdivision, a 15-lot residential subdivision on approximately 1.61 acres located southeast of the intersection of Lozier Lane and Lozier Court within an SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district.

Vicinity Map



Subject Site Characteristics

Zoning	SFR-10	Single-family residential (6 to 10 dwelling units per gross acre)
GLUP	UR	Urban Residential
Use	Vacant	

Surrounding Site Characteristics

<i>North</i>	Zone:	SFR-00 and SFR-10 (Single-family residential – 6 to 10 dwelling units per acre)
	Use:	Single-family residential & vacant land
<i>South</i>	Zone:	RR-2.5 (County Zoning)
	Use:	Single-family residential
<i>East</i>	Zone:	SFR-10
	Use:	Single-family residential
<i>West</i>	Zone:	RR-2.5
	Use:	Single-family residential

Related Projects

ANNX-00-122	Blackford Annexation (Ordinance No. 2001-223)
ZC-01-185	Zone Change (Withdrawn)
LDS-02-005	Subdivision (Withdrawn)
ZC-04-073	Zone Change (Withdrawn)
LDS-04-074	Tentative plat approval for Lozier Court Subdivision Phase 1, a 4-lot subdivision (Expired)
ZC-17-128	Zone Change from SFR-00 to SFR-10

Applicable Criteria

Medford Land Development Code §10.270, Land Division Criteria

The approving authority (Planning Commission) shall not approve any tentative plat unless it first finds that, the proposed land division together with the provisions for its design and improvement:

- (1) Is consistent with the Comprehensive Plan, any other applicable specific plans thereto, including Neighborhood Circulation Plans, and all applicable design standards set forth in Article IV and V;
- (2) Will not prevent development of the remainder of the property under the same ownership, if any, or of adjoining land or of access thereto, in accordance with this chapter;

- (3) Bears a name that has been approved by the approving authority and does not use a word which is the same as, similar to, or pronounced the same as a word in the name of any other subdivision in the City of Medford; except for the words "town", "city", "place", "court", "addition", or similar words; unless the land platted is contiguous to and platted by the same applicant that platted the land division bearing that name; or unless the applicant files and records the consent of the party who platted the land division bearing that name and the block numbers continue those of the plat of the same name last filed;
- (4) If it includes the creation of streets or alleys, that such streets or alleys are laid out to be consistent with existing and planned streets and alleys and with the plats of land divisions already approved for adjoining property unless the approving authority determines it is in the public interest to modify the street pattern;
- (5) If it has streets or alleys that are proposed to be held for private use, that they are distinguished from the public streets or alleys on the tentative plat, and reservations or restrictions relating to the private streets or alleys are set forth;
- (6) Will not cause an unmitigated land use conflict between the land division and adjoining agricultural lands within the EFU (Exclusive Farm Use) zoning district.

ISSUES AND ANALYSIS

Project Summary

The subject site is composed of one lot totaling 1.61 acres located within a recently rezoned SFR-10 zoning district. The applicant is proposing a tentative plat consisting of 15 lots. One lot (Lot 13) will be a duplex lot and is proposed to be 7,003 square feet in size. All other lots are proposed for duplex style buildings with a lot line in the middle. The proposed lot sizes range from 3,000 to 3,726 square feet.

The subject site is located in the southwest of Medford with the westerly property line coinciding with the city limits.

Code Compliance

Density (Exhibit M)

The density range for the SFR-10 zone is between six and ten dwelling units per gross acre. The net parcel size is 1.61 acres; the gross parcel size, which includes the fronting half-streets of Lozier Lane and Lozier Court, is 1.83 acres. Based on the gross acreage, the density range is between 11 and 18 dwelling units. The proposal to create 16 dwelling units meets density standards.

Street Circulation

Lozier Lane is classified as a Major Collector street and was recently improved as part of the City's capital improvement project along this development's frontage. As no driveway access will be allowed to Lozier Lane, Lots 1 through 6 will take driveway access from the private alley.

Lozier Court is classified as a Minor Residential Street and is currently unimproved. The applicant shall improve Lozier Court to City standards as part of this project. Lozier Court is supposed to connect to Meadows Lane in the future as approved per West Meadows Village Subdivision (File No. LDS-15-118) in 2015 (Exhibit N).

Lastly, Beechwood Way is proposed as a Minor Residential Street which will provide access for all remaining lots (Lots 7 through 15).

Water Facilities

The subject property is within the Medford Water Commission service area. A condition of approval has been included requiring the applicant to comply with the Medford Water Commission Report (Exhibit G).

Stormwater

The applicant stated that PDK Properties also owns a portion of the West Meadows Village PUD which fronts on Lozier Court to the north and that the applicant has submitted construction documents to the City for stormwater detention and treatment plan that will provide the detention and treatment facilities for the proposed PDK Village subdivision.

Irrigation

The property has Medford Irrigation District (MID) facilities running along the southern boundary. MID is requesting to be part of the preparation of the comprehensive civil improvement plans and to approve these plans prior to the city engineer's approval. A condition of approval has been included.

Rogue Valley Sewer Services

The subject property is within the Rogue Valley Sewer Services (RVSS) service area. Sewer service to the subdivision will require a main line extension from along the proposed alley and Beechwood Way. A condition of approval has been included requiring the applicant to comply with the Rogue Valley Sewer Services Report (Exhibit J).

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit C) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of LDS-17-170 per the staff report dated February 27, 2018, including Exhibits A through O.

EXHIBITS

- A Conditions of Approval, dated February 27, 2018
- B Tentative Plat PDK Village Subdivision, received December 26, 2017
- C Applicants findings and conclusions, received December 26, 2017
- D e-mail from Scott Sinner re: Drainage Plan, received January 22, 2018
- E Conceptual Grading & Drainage Plan, received December 26, 2017
- F Public Works Department Staff Report, received February 7, 2018
- G Medford Water Commission Memo, received February 7, 2018
- H Medford Fire Department Memo, received February 7, 2018
- I Medford Building Department Memo, received February 7, 2018
- J Rogue Valley Sewer Services Memo, received February 1, 2018
- K Jackson County Roads Memo, received January 30, 2018
- L Medford Irrigation District Memo, received February 2, 2018
- M Density Calculation, created February 27, 2018
- N Approved Tentative Plat for West Meadows Village Subdivision, received January 26, 2016.
- O **West View Village Plan, received March 8, 2018**
Vicinity map

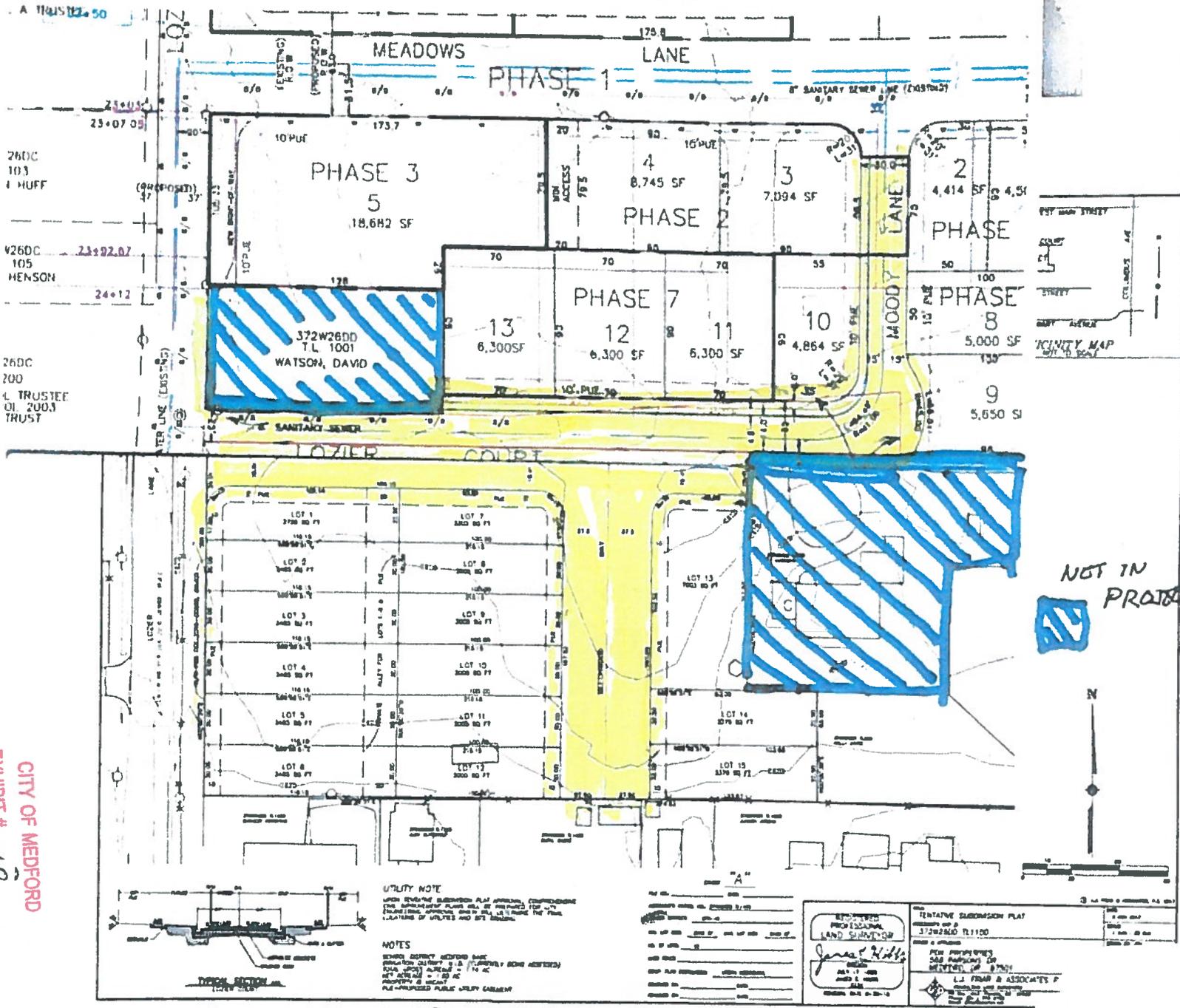
MEDFORD PLANNING COMMISSION

Patrick Miranda, Chair

PLANNING COMMISSION AGENDA:

MARCH 8, 2018

West View Village



PDK Village

RECEIVED

MAR 08 2018

PLANNING DEPT.

RECEIVED

MAR 08 2018

PLANNING DEPT.

CITY OF MEDFORD
EXHIBIT # 10
File # 108-17-170



City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT – EXTENSION OF TIME

Project Panther Landing Subdivision
Applicant: Tom Malot Construction Co. Inc.; Agent: Farber Surveying

File no. LDS-15-141/E-15-142

To Planning Commission *for meeting of March 22, 2018*

From Sarah Sousa, Planner IV

Date March 15, 2018

Request

Consideration of request for a one-year time extension of the approval of Panther Landing Subdivision, a seven lot residential subdivision with an Exception to reduce the street dedication requirement for Columbus Avenue and an Exception to the number of units allowed to take access off of a minimum access easement for an 0.86 acre parcel located on the east side of S Columbus Avenue, approximately 120 feet north of Garfield Street, within the SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district (1579 S. Columbus Avenue – 372W36CA2200).

Background

The Planning Commission adopted the Final Order granting approval of the project on March 10, 2016. The applicant is requesting an extension of time as allowed under Medford Land Development Code (MLDC) Section 10.269.

Project Review

Per MLDC Section 10.269, extensions shall be based on findings that the facts upon which the application was first approved have not changed to an extent sufficient to warrant refiling of the application. It can be found that neither the circumstances of approval nor applicable site development standards have changed to a degree that warrants refiling of the application. This is the only extension allowed under the Medford Land Development Code.

Recommended Action

Approve the one-year time extension to March 10, 2019, for LDS-15-141/E-15-142 per the Staff Report dated March 15, 2018.

Exhibits

- A Letter requesting extension received March 9, 2018
- B Tentative Plat for Panther Landing Subdivision
Vicinity Map

RECEIVED

MAR 09 2018

PLANNING DEPT.

Findings of Fact

for
PANTHER LANDING a subdivision

AN EXTENSION OF TIME FILE No LDS-15-141
CITY OF MEDFORD, OREGON
ADDRESSING THE CRITERIA OF MLDC §10.269
February 28, 2018

- *Text bulleted and italicized represents the findings per § 10.269*

10.269 Expiration of Tentative Plat approval

(1) Approval of a tentative plat application shall take effect on the date the final order for approval is signed, unless appealed, and shall expire two (2) years from the effective date unless the final plat has been approved by the Planning Director pursuant to Sections 10.276 - 10.280. If a request for an extension of a tentative plat application approval is filed with the Planning Department within two (2) years from the date of the final order, the Planning Commission shall grant an extension not to exceed one (1) additional year. Extensions shall be based on findings that the facts upon which the tentative plat application was first approved have not changed to an extent sufficient to warrant refile of the application. All approvals made prior to the adoption of this ordinance shall expire one (1) year from the date of adoption of this ordinance, notwithstanding permitted extensions and previous phasing authorizations.

- *A review of Chapter 10 of the LDO found no changes to the criteria since the approval of the tentative plat under consideration. The subdivision is very near completion. Construction delays caused the gap in the review of the final plat. Most of the construction has been completed with the review for final plat expected to continue later this week or the first of next week.*

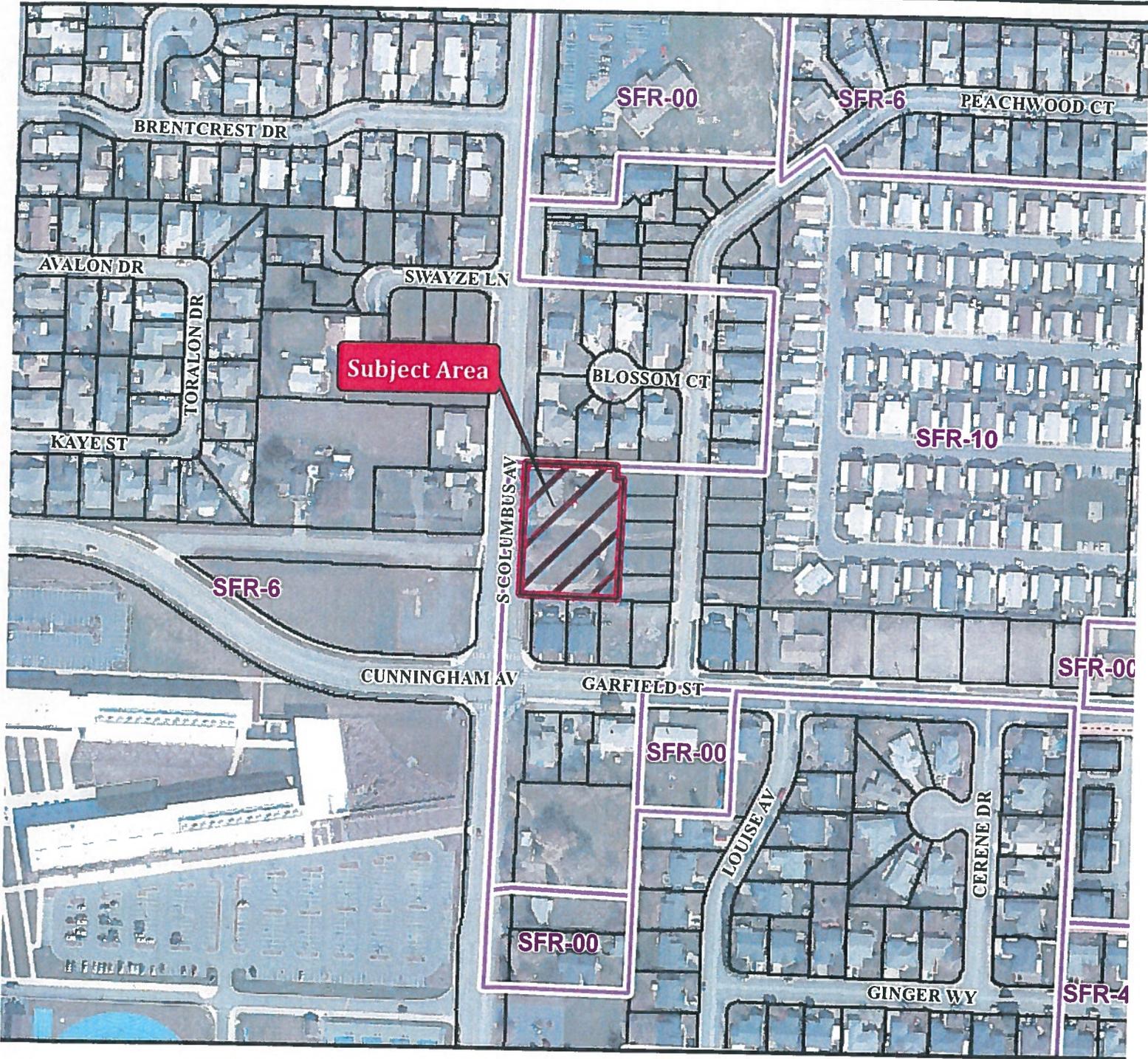
Respectfully submitted.



Herbert A Farber

Farber & Sons Inc
Dbas. Farber Surveying
431 Oak Street
Central Point Oregon 97502

Phone: 541 664-5599
Email: herb@farbersurveying.com

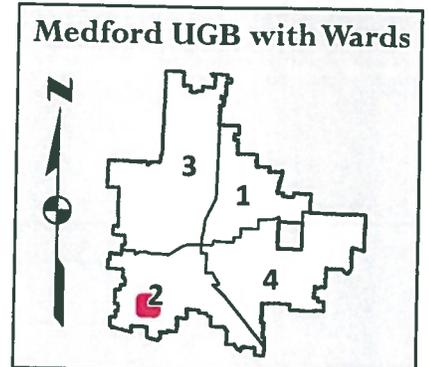


Project Name:
**Panther Landing Subdivision
& Exception**

Map/Taxlot:
372W36CA TL 2200



-  Subject Area
-  Medford Zoning
-  Tax Lots
- 





Planning Commission

Minutes

From Public Hearing on **March 8, 2018**

The regular meeting of the Planning Commission was called to order at 5:30 PM in the City Hall Council Chambers on the above date with the following members and staff in attendance:

Commissioners Present

Patrick Miranda, Chair
David McFadden, Vice Chair
David Culbertson
Joe Foley
Bill Mansfield
Mark McKechnie
E.J. McManus

Staff Present

Kelly Akin, Assistant Planning Director
Carla Paladino, Principal Planner
Eric Mitton, Deputy City Attorney
Alex Georgevitch, City Engineer
Greg Kleinberg, Fire Marshal
Terri Rozzana, Recording Secretary
Steffen Roennfeldt, Planner III

Commissioners Absent

Alex Poythress, Excused Absence
Jared Pulver, Excused Absence

Guest

Elaine Howard

10. Roll Call

20. Consent Calendar/Written Communications.

20.1 LDS-17-155 Final Order of a request for tentative plat approval of a proposed two-lot partition on a 1.5-acre parcel located at 914 Ross Lane within the SFR-10 (Single-Family Residential, ten dwelling units per gross acre) zoning district (372W23DD 4400). (Billy Hogue, Applicant; Scott Sinner Consulting, Inc., Agent; Dustin Severs, Planner).

Motion: The Planning Commission adopted the consent calendar as submitted.

Moved by: Vice Chair McFadden

Seconded by: Commissioner Foley

Voice Vote: Motion passed, 7-0.

30. Minutes

30.1. The minutes for February 22, 2018, were approved as submitted.

40. Oral and Written Requests and Communications. None.

Eric Mitton, Deputy City Attorney, read the Quasi-Judicial Statement.

50. Public Hearings – New Business

50.1 GF-18-022 Medford Urban Renewal Agency Substantial Amendment No. 6. (City of Medford, Applicant).

Kelly Madding, Deputy City Manager, reported that he Medford Urban Renewal Agency has moved forward with a substantial amendment to the City Center Revitalization Plan. Our consultant is present tonight and will review the Planning Commission's role which is to determine whether the substantial amendment conforms to City of Medford's Comprehensive Plan.

Ms. Madding spoke to the Planning Commission at their Monday, February 26, 2018, study session regarding urban renewal 101. She discussed where City staff has been with the substantial amendment and where they are going. The Planning Commission public hearing was the next stop. She asked that the Planning Commission make a recommendation to the City Council who will hear this for action on Thursday, April 19, 2018.

Ms. Madding introduced Elaine Howard who is the consultant that has been working on this substantial amendment.

Elaine Howard, 4763 SW Admiral Street, Portland, Oregon, 97221. Ms. Howard has a consulting firm that focuses solely on urban renewal. Ms. Howard has been in business for the last thirteen years.

Urban Renewal is governed by Oregon Revised Statute (ORS) 457. The statute prescribes a lot of things about urban renewal so that one can look at the statute and know exactly what to do at different times. The requirement for a substantial amendment is to take it to Planning Commission. When City Council adopts the amendment they are required to make a finding that it conforms to the Comprehensive Plan.

The role of the Planning Commission to determine whether the 6th Substantial Amendment to the City Center Revitalization Plan is in conformance with the Medford Comprehensive Plan and to forward a recommendation to the City Council.

The Amendment increases the maximum indebtedness of the plan (the total amount of money that can be spent of projects, programs, and administration). The existing maximum indebtedness is approximately \$67 million. The amount it can be increased is approximately \$20 million. The new maximum indebtedness is approximately \$87 million. The amendment also adds projects, eliminates the duration clause, changes the projected termination date to Fiscal Year 2023/2024 and does general updating including Comprehensive Plan conformance findings.

The Medford Urban Renewal Agency (MURA) did an initial evaluation of an amendment to the City Center Revitalization Plan to increase the maximum indebtedness and duration, with allocation of funds as 90% to be spent on improvements in Liberty Park area and 10% to be spent on seismic retrofitting in downtown Medford area.

Formation of an Advisory Committee was tasked with creating an initial Liberty Park improvement project list and gathering public input on said list. The Advisory Committee had two meetings prior to community input. The first meeting was urban renewal 101, City Center Revitalization Plan 101 and initial project discussion. The second meeting was further project discussion and finalization of recommended projects for input at community input meeting.

The community input meeting was attended by 35-40 residents. Attendees were engaged and participation level was high. Common themes from comment cards included a want for more public input opportunities, a need for better neighborhood safety and a desire for neighborhood beautification.

The formal recommendation was presented to MURA on February 1, 2018 in the Advisory Committee Summary memo. The Advisory Committee unanimously decided to issue a general, rather than specific, recommendation regarding project and funding allocations. The Committee felt a general recommendation allowed for more flexibility for MURA and for more public input opportunities for the neighborhood. The Advisory Committee recommended that the project categories be added to the Plan are street improvements on Manzanita and Edwards; Sewer Lateral Replacement Program; Housing Assistance – assistance for existing single family residential and both existing and new multifamily/mixed use; Liberty Park Neighborhood Master Plan projects placeholder, and downtown seismic retrofitting.

As Planning staff was reviewing the findings that were put together by the consultant they found minor changes that need to be made. Ms. Madding reviewed those with the Planning Commission.

The Public Hearing was opened.

a. Robert Shand, 406 Beatty Street, Medford, Oregon, 97501. Mr. Shand is hoping the infrastructure, thought process for current, past needs and future tie in with the rest of Medford and the community around it happens. The Liberty Park Neighborhood Association is slowly getting reconstituted.

The Public Hearing was closed.

Motion: The Planning Commission finds that based on the information provided in the staff report and attached amendment the 6th Substantial Amendment to the City Center

Revitalization Plan conforms to the Medford Comprehensive Plan and further recommends that the City Council adopt the proposed amendment to the City Center Revitalization Plan as per the staff report dated March 1, 2018, including Exhibits A and B, with the corrections presented by staff.

Moved by: Vice Chair McFadden

Seconded by: Commissioner McKechnie

Roll Call Vote: Motion passed, 7-0.

50.2 LDS-17-170 Consideration of a request for tentative plat approval for PDK Village Subdivision, a 15-lot residential subdivision on approximately 1.61 acres located southeast of the intersection of Lozier Lane and Lozier Court within an SFR-10 (Single Family Residential – 10 dwelling units per gross acre) zoning district. (PDK Properties; Scott Sinner Consulting, Inc., Agent; Steffen Roennfeldt, Planner).

Chair Miranda inquired whether any Commissioners have a conflict of interest or ex-parte communication they would like to disclose. Commissioner McKechnie reported that Scott Sinner is his neighbor but that would not affect his decision.

Chair Miranda inquired whether anyone in attendance wishes to question the Commission as to conflicts of interest or ex-parte contacts. None were disclosed.

Steffen Roennfeldt, Planner III, stated that the land division criteria can be found in the Medford Land Development Code Section 10.270. The applicable criteria were included in the staff report, property owner notices and hard copies are available at the entrance of Council Chambers for those in attendance. Mr. Roennfeldt gave a staff report. Mr. Roennfeldt stated that he received an exhibit right before tonight's meeting and the applicant's agent will review that with the Planning Commission during his presentation.

Vice Chair McFadden stated that the extension of Lozier Court exceeds the developer's requirement. Mr. Roennfeldt agreed.

Commissioner McKechnie asked, what was the exception with the development on the north side of Lozier Court? Mr. Roennfeldt stated there was an existing residence at the intersection of Lozier Lane and Lozier Court and there was not enough space for a full right-of-way width. No planter strip will be required. Mr. Roennfeldt deferred the question to the applicant's agent.

Commissioner McKechnie asked, if the developer builds single story units will the setback from the side lot line be 6 feet and if it is two stories it would be 14 feet? Mr. Roennfeldt stated it would be 4 feet and 6 feet.

Commissioner McManus stated that in the Fire Department's memorandum there is a comment regarding parking is prohibited on one side of Lozier Court. Is that on the north or south side? Mr. Roennfeldt reported that is up to the applicant.

The Public Hearing was opened.

a. Scott Sinner, Scott Sinner Consulting, Inc., 4401 San Juan Drive, Suite G, Medford, Oregon, 97504. Mr. Sinner reported that during the zone change there were two comments. One written comment from an adjoining property owner and public testimony from another property owner. PDK Village is the southerly portions of Lozier Court that is currently unimproved. West View Village Planned Unit Development was approved in 2016. It set the street location and the exception was for a three and a half or four foot planter strip instead of the seven or eight foot planter strip. The applicant has purchased lots from West View Village.

Commissioner McFadden asked about which side of the street parking will be on. Mr. Sinner that is a condition that the applicant will work out with the Medford Fire Department. The applicant is not sure which side but they will comply.

Mr. Sinner reserved rebuttal time.

The Public Hearing was closed.

Motion: The Planning Commission adopts the findings as recommended by staff and directs staff to prepare a Final Order for approval of LDS-17-170, per the staff report dated February 27, 2018, including Exhibits A through O.

Moved by: Vice Chair McFadden

Seconded by: Commissioner Foley

Roll Call Vote: Motion passed, 7-0.

60. Reports

60.1 Site Plan and Architectural Commission.

Commissioner Culbertson reported that the Site Plan and Architectural Commission did not have a meeting Friday, March 2, 2018.

60.2 Report of the Joint Transportation Subcommittee.

Chair Miranda reported that the Joint Transportation Subcommittee met Wednesday, February 28, 2018. Chair Miranda did not have his notes but will give a report at the next Planning Commission meeting. The meetings are getting more interesting and informative. The information is becoming more finite. They are now dealing with specific questions and issues, primarily what projects are being considered.

60.3 Planning Department

Kelly Akin, Assistant Planning Director, reported that the next Planning Commission study session is scheduled for Monday, March 12, 2018. Carla Paladino, Principal Planner will give a Citizen Involvement report and discuss the Transportation System Plan policies.

Thursday, March 29, 2018, there will be a joint study session with the City Council and Planning Commission to be held in the Prescott Room at the Police Department at 6:00 p.m. Dinner will be served at 5:30 p.m. Discussion will be on the Transportation System Plan. Commissioner Foley will not be able to attend.

The Planning Commission has business scheduled for Thursday, March 22, 2018, Thursday April 12, 2018 and Thursday, April 26, 2018.

At the last City Council meeting they continued the Evergreen street vacation to the Thursday, March 15, 2018, public hearing. They will also hear the Springbrook roundabout application.

70. Messages and Papers from the Chair. None.

80. Remarks from the City Attorney. None.

90. Propositions and Remarks from the Commission. None.

100. Adjournment

The meeting was adjourned at 6:15 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:

Terri L. Rozzana
Recording Secretary

Patrick Miranda
Planning Commission Chair

Approved: March 22, 2018



Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT – CONTINUANCE REQUEST

for a Type-C quasi-judicial decision: **Conditional Use Permit**

PROJECT Lady Geneva Bed & Breakfast
 Applicant: Gloria Thomas & Cecil Thomas de Haas
 Agent: Julie Krason

FILE NO. CUP-17-116

TO Planning Commission *for March 22, 2018 hearing*

FROM Dustin Severs, Planner III

REVIEWER Kelly Akin, Assistant Director

DATE March 15, 2018

BACKGROUND

Proposal

Consideration of a request for a Conditional Use Permit (CUP) for a proposed Bed & Breakfast to be located at 15 Geneva Street in the SFR-6 (Single-Family Residential – 6 dwelling units per gross acre) zoning district, and within the Historic Preservation Overlay District (371W30AB TL 16400).

Request

The applicant has requested that the item be continued to June 14, 2018, so that they may be present at the hearing.

EXHIBITS

- A Continuance request, received March 9, 2018.
Vicinity Map

PLANNING COMMISSION AGENDA:

DECEMBER 14, 2017
JANUARY 11, 2018
FEBRUARY 8, 2018
March 22, 2018

Dustin J. Severs

From: Gloria Thomas <ladygenevabb@gmail.com>
Sent: Friday, March 09, 2018 3:56 PM
To: Dustin J. Severs
Cc: David Herman; Cecil de Haas
Subject: Public Hearing request for postponement

Good Afternoon Dustin,
We would like to request a postponement for the public hearing to June 14th. By that date we will both be living permanently in Medford and will be able to attend.

Please advise if there is any issue with this date.

Regards and Thank you,
Gloria and Cecil

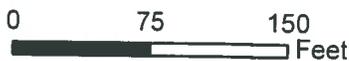


Project Name:

Lady Geneva
Bed & Breakfast

Map/Taxlot:

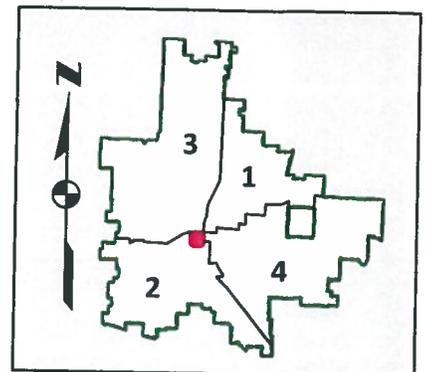
371W30AB TL 16400



09/26/2017

Legend

-  Subject Area
-  Medford Zoning
-  Tax Lots





STAFF REPORT

for a Type-C quasi-judicial decision: **Zone Change**

Project North Phoenix Enterprises
 Applicant: North Phoenix Enterprises LLC; Agent: CSA Planning Ltd.

File no. ZC-17-168

To Planning Commission *for 03/22/2018 hearing*

From Steffen Roennfeldt, Planner III

Reviewer Kelly Akin, Assistant Planning Director *ka.*

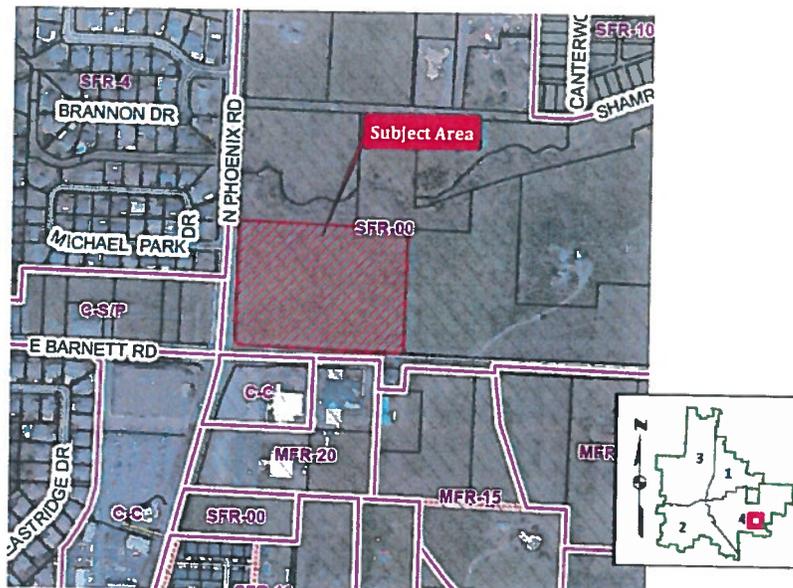
Date March 13, 2018

BACKGROUND

Proposal

Consideration of a zone change from SFR-00 (Single Family Residential – one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot located northeast of the intersection of North Phoenix Road and East Barnett Road in southeast Medford (371W27 1605). The application also includes a request to modify a condition of approval in the matter of File No. ZC-15-041 limiting traffic generation for 955 North Phoenix Road (371W34 501).

Vicinity Map



Subject Site Characteristics

Zoning	SFR-00	Single-Family Residential – 1 dwelling unit per existing lot
Overlay	SE	Southeast, Subarea 7a
GLUP	CM	Commercial
Use	Vacant	

Surrounding Site Characteristics

<i>North</i>	Zone:	SFR-00
	Use:	Vacant
<i>South</i>	Zone:	MFR-20 (Multi-Family Residential – 15 to 20 dwelling units per gross acre) C-C (Community Commercial)
	Use:	Bank & Fire Station
<i>East</i>	Zone:	SFR-00
	Use:	Vacant
<i>West</i>	Zone:	SFR-4 (Single-Family Residential – 2.5 to 4 dwelling units per gross acre) & C-S/P (Service Commercial and Professional Office)
	Use:	Low Density Residential & Vacant

Related Projects

PLA-07-283	Property Line Adjustment
PLA-08-149	Property Line Adjustment
DCA-14-083	Development Code Amendment for Commercial Center Core Area Master Plan for the Southeast Overlay District
ZC-15-41	Zone Change from MFR-20 to C-C (Rogue Credit Union)
AC-15-42	SPAC Review for Rogue Credit Union

Applicable Criteria

Medford Municipal Code §10.227, Zone Change Criteria

The approving authority (Planning Commission) shall approve a quasi-judicial zone change if it finds that the zone change complies with subsections (1) and (2) below:

- (1) The proposed zone is consistent with the Transportation System Plan (TSP) and the General Land Use Plan Map designation. A demonstration of consistency with the acknowledged TSP will assure compliance with the Oregon Transportation Planning Rule. Where applicable, the proposed zone shall also be consistent with the additional locational standards of the below sections (1)(a), (1)(b), (1)(c), or (1)(d). Where a special area plan requires a specific zone, any conflicting or additional requirements of the plan shall take precedence over the locational criteria below.

- (c) For zone changes to any commercial zoning district, the following criteria shall be met for the applicable zoning sought:

- (ii) The overall area of the C-C zoning district shall be over three (3) acres in size and shall front upon a collector or arterial street or state highway. In determining the overall area, all abutting property(s) zoned C-C shall be included in the size of the district.

- (e) For purposes of (1)(c) and (1)(d) above, a zone change may be found to be "suitable" where compliance is demonstrated with one (1) or more of the following criteria:

- (ii) At least fifty percent (50%) of the subject property's boundaries abut zones that are expressly allowed under the criteria in (1)(c) or (1)(d) above;
- (iii) At least fifty percent (50%) of the subject property's boundaries abut properties that contain one(1) or more existing uses which are permitted or conditional uses in the zone sought by the applicant, regardless of whether the abutting properties are actually zoned for such existing uses; or

- (2) It shall be demonstrated that Category A urban services and facilities are available or can and will be provided, as described below, to adequately serve the subject property with the permitted uses allowed under the proposed zoning, except as provided in subsection (c) below. The minimum standards for Category A services and facilities are contained in Section 10.462 and Goal 2 of the Comprehensive Plan "Public Facilities Element" and Transportation System Plan.
- (a) Storm drainage, sanitary sewer, and water facilities must already be adequate in condition, capacity, and location to serve the property or be extended or otherwise improved to adequately serve the property at the time of issuance of a building permit for vertical construction.
- (b) Adequate streets and street capacity must be provided in one (1) of the following ways:
- (i) Streets which serve the subject property, as defined in Section 10.461(2), presently exist and have adequate capacity; or ***
- (ii) Existing and new streets that will serve the subject property will be improved and/or constructed, sufficient to meet the required condition

- and capacity, at the time building permits for vertical construction are issued; or
- (iii) If it is determined that a street must be constructed or improved in order to provide adequate capacity for more than one (1) proposed or anticipated development, the Planning Commission may find the street to be adequate when the improvements needed to make the street adequate are fully funded. A street project is deemed to be fully funded when one (1) of the following occurs:
 - (a) the project is in the City's adopted capital improvement plan budget, or is a programmed project in the first two (2) years of the State's current STIP (State Transportation Improvement Plan), or any other public agencies adopted capital improvement plan budget; or
 - (b) when an applicant funds the improvement through a reimbursement district pursuant to the MLDC. The cost of the improvements will be either the actual cost of construction, if constructed by the applicant, or the estimated cost. The "estimated cost" shall be 125% of a professional engineer's estimated cost that has been approved by the City, including the cost of any right-of-way acquisition. The method described in this paragraph shall not be used if the Public Works Department determines, for reasons of public safety, that the improvement must be constructed prior to issuance of building permits.
 - (iv) When a street must be improved under (b)(ii) or (b)(iii) above, the specific street improvement(s) needed to make the street adequate must be identified, and it must be demonstrated by the applicant that the improvement(s) will make the street adequate in condition and capacity.
- (c) In determining the adequacy of Category A facilities, the approving authority (Planning Commission) may evaluate potential impacts based upon the imposition of special development conditions attached to the zone change request. Special development conditions shall be established by deed restriction or covenant, which must be recorded with proof of recordation returned to the Planning Department, and may include, but are not limited to the following:
- (i) Restriction of uses by type or intensity; however, in cases where such a restriction is proposed, the Planning Commission must find that the resulting development pattern will not preclude future development, or intensification of development, on the subject property or adjacent parcels. In no case shall residential densities be approved which do not meet minimum density standards,
 - (ii) Mixed-use, pedestrian-friendly design which qualifies for the trip reduction percentage allowed by the Transportation Planning Rule,

- (iii) Transportation Demand Management (TDM) measures which can be reasonably quantified, monitored, and enforced, such as mandatory car/van pools.

ISSUES AND ANALYSIS

Background

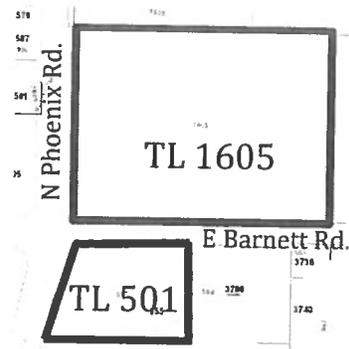
The property to be rezoned (Tax Lot 1605) is located within the Southeast Commercial Center Core Area, Area 7A. At the time of annexation, the property was given a holding zone of SFR-00, which can serve as a holding zone for both commercial and residential parcels. Land use and development within the Southeast (S-E) Overlay District shall conform to the Southeast Overlay District regulations, in addition to all other applicable City regulations. Within the S-E Overlay District, the GLUP Map is further refined by the Southeast Plan Map adopted as part of the Comprehensive Plan. The Southeast Plan Map shall determine GLUP Map consistency for purposes of zoning and zone changes. The zoning district(s) which each Southeast Plan land use category is consistent are set forth in Section 10.373. Table 10.373 shows the only permitted zoning for the subject area, Area 7A, as C-C (Community Commercial).

The second property that is part of this application (Tax Lot 501) was re-zoned as part of File Number ZC-15-041. As part of the zone change approval, a condition limiting traffic generation for Tax Lot 501 was placed on the parcel (Exhibit J). The condition read that development would be limited to that which would generate a total of 628 total daily trips. Said stipulation was to remain in effect until a traffic impact analysis showed that additional trips will have no significant impact to the transportation system.

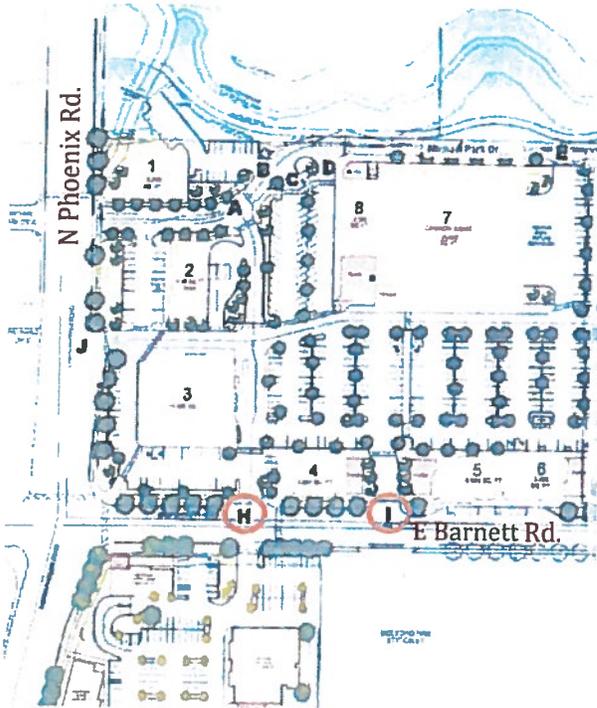
Agency Comments

Public Works

The Public Works Staff Report (Exhibit C) states that the sanitary sewer stipulation listed in the applicant's Findings of Fact (Exhibit B) is acceptable and that the site will be able to connect to exiting storm drain facilities in the area at the time of development. In addition, Public Works received a Traffic Impact Analysis (TIA) for the areas located northeast and southeast of N. Phoenix Road and Barnett Road intersection.



Tax Lot 501 is currently zoned C-C and has an existing trip cap of 628 ADT (Average Daily Trips) as stipulated per Zone Change Application ZC-15-041. A TIA that included lot 501 was submitted to the City on June 21, 2017. (The Executive Summary is included in this application as part of the Applicant's Exhibit 10a. The full document can be reviewed at the Planning Department.) Based on said TIA for the SE Commercial Center Core Area



from June 2017 (Applicant’s Exhibit 10a) and Comments & Revisions from August 1 & 8 2017 (Applicant’s Exhibit 10b), the applicant is proposing to increase the ADT by approximately 400 trips to 145 PM peak hour trips (adjusted from gross trips for pass-by and transit oriented development reduction). According to the TIA, the transportation system cannot accept the potential trip generation from the proposed zone change without mitigating the impact of the development traffic. The applicant proposes five stipulations tied to the traffic impacts (Numbers 1 to 5 on pages 13 & 14 of Exhibit B and Exhibit A, Discretionary Conditions).

Public Works does not concur with the conclusions drawn regarding driveway “H” on Barnett Road. Public Works recommends denial of the north side access at driveway “H”, and approval of driveway “I” as the only north side site access allowed on East Barnett Road. Public Works does recommend approval of the south side access at driveway “H” and to put in place four conditions of approval, adding to and clarifying the suggested stipulations by the applicant.

Medford Water Commission

Water facilities have adequate capacity to serve the subject property at the proposed density, according to the Medford Water Commission Report (Exhibit D).

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant’s findings and conclusions (Exhibit B).

Finding – Oregon Transportation Planning Rule

The City of Medford has an approved Transportation System Plan (TSP) consistent with the requirements of the State. The TSP requires all modes of transportation be

considered, including rapid transit, air, water, rail, highway, bicycle and pedestrian. A review of the property determines that water and rail are not available. The parcel has frontage on North Phoenix Road, classified as a Major Arterial, and East Barnett Road, classified as a Minor Arterial in the TSP.

RVTD does not provide direct access to the subject site. There is currently service on East Barnett Road and Murphy Road, approximately 0.75 miles west of the subject site.

Access to I-5 via the Phoenix Exit is available 2.5 miles to the south. The airport is located approximately 4.75 miles northwest of the subject site.

Conclusion – Oregon Transportation Planning Rule

The Planning Commission can find the property is currently served with adequate transportation facilities as required by Oregon Transportation Rule (OAR 660 Division 12).

Finding – Zone change to Commercial zoning district

The proposed zone change is over three acres in size and fronts on upon two arterial streets. The subject property is required to be zoned as C-C by the Southeast Overlay District Master Plan and MLDC Table 10.373.

Conclusion – Zone change to Commercial zoning district

The Planning Commission can find the requested zone to C-C is consistent with the requirements of MLDC 10.227(1)(c)(ii) and (1)(e).

Finding – Availability of Category A Urban Service Facilities

The site lies within the Larson Creek Drainage Basin. At the time of future development, the subject property will be able to connect to these storm drainage facilities. Also at the time of future development, the subject property will be required to provide stormwater quality and detention.

The subject property lies within the City of Medford Sewer Services area. As pointed out by the applicant in stipulation No. 6, a 190-foot segment of pipe will have to be upgraded prior to issuance of building permits for vertical construction. Once this stipulation is taken care of, there will be adequate capacity to serve this property at the proposed density.

The subject property can be served by the Medford Water Commission and there is adequate capacity to serve this property at the proposed density.

The entire site is designated as a Transit Oriented Development and a Transportation Impact Analysis has been prepared. The TIA demonstrates that development of the property as per the adopted master plan and with mitigation as recommended by the traffic engineer and Public Works will meet all adopted transportation performance standards.

Conclusion – Availability of Category A Urban Services and Facilities

The Planning Commission can find that Category A urban services and facilities are currently available or can and will be available at the time of development to adequately serve the subject property with the permitted uses under the proposed C-C zoning designation.

The conclusion can be made that all of the zone change criteria have been met.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of ZC-17-168 and amending Discretionary Condition 1 of Exhibit A of ZC-15-041 per the staff report dated March 13, 2018, including Exhibits A through J.

EXHIBITS

- A Conditions of Approval, dated March 13, 2018
- B Applicant's Findings of Fact & Conclusions of Law, received December 18, 2017
- C Public Works Department Staff Report, received January 24, 2018
- D Medford Water Commission Memo, received January 24, 2018
- E Medford Fire Department Memo, received January 24, 2018
- F Medford Building Department Memo, received January 24, 2018
- G City Surveyor Memo, received January 17, 2018
- H Jackson County Roads Memo, received January 17, 2018
- I Oregon Department of Fish and Wildlife E-Mail, received January 18, 2018
- J Conditions of Approval for ZC-15-041, dated June 4, 2015
Vicinity map

PLANNING COMMISSION AGENDA:

MARCH 22, 2018

EXHIBIT A

North Phoenix Enterprises
ZC-17-168
Conditions of Approval
March 13, 2018

DISCRETIONARY CONDITIONS

1. The Planning Commission accepts the stipulations for the following:
 - a. Vehicular trip generation for the North Side (TL 1605) shall be limited to 431 PM peak hour trips (adjusted from gross trips for pass-by and transit oriented development reduction).
 - b. Vehicular trip generation for the South Side (TL 501) shall be limited to 145 PM peak hour trips (adjusted from gross trips for pass-by and transit-oriented development reduction). The South Side trip cap shall replace the prior cap of 628 Average Daily Trips imposed through Planning File No. ZC-15-041.
 - c. Prior to issuance of permits for vertical construction of new buildings, traffic impact mitigation for the intersection of North Phoenix Road and East Barnett Road shall require adding protected-permissive traffic signal phasing to the north and southbound left-turn phases.
 - d. Prior to issuance of permits for vertical construction on Tax Lot 1605, owners of that property agree to construct a south-bound left turn lane for North Phoenix Road at Michael Park Drive, including modification of the existing median to accommodate the same. Final design shall be submitted for review and approval with Site Plan and Architectural Review application at the time development of the North Side (Tax Lot 1605) is proposed.
 - e. Driveway "J" as identified in the TIA (being the right-in only access from north-bound North Phoenix Road to Tax Lot 1605) shall not be included in future development plans for the property.
 - f. Prior to issuance of permits for vertical construction on Tax Lot 1605, the 190-foot segment of 12-inch diameter pipe section (ID No. S371W28DS0139, City of Medford Sanitary Sewer Master Plan (2205)) shall be upgraded to an 18-inch diameter pipe.

EXHIBIT A

North Phoenix Enterprises
ZC-17-168
Conditions of Approval
March 13, 2018

CODE CONDITIONS

1. Prior to issuance of building permits for vertical construction on Tax Lot 1605, the applicant shall:
 - a. Comply with the report from the Public Works Department, received January 24, 2018 (Exhibit C).

RECEIVED

MAR 13 2018

PLANNING DEPT.

BEFORE THE PLANNING COMMISSION

FOR THE CITY OF MEDFORD

JACKSON COUNTY, OREGON

IN THE MATTER OF A REQUEST FOR)
 CHANGE OF ZONE FROM SINGLE)
 FAMILY RESIDENTIAL (SFR-00) TO)
 COMMUNITY COMMERCIAL (C-C) FOR)
 AN 8.00 ACRE PARCEL ADJACENT)
 RIGHT-OF-WAY LOCATED AT)
 NORTHEAST OF THE INTERSECTION)
 OF NORTH PHOENIX ROAD WITH)
 EAST BARNETT ROAD WITHIN THE)
 SOUTHEAST VILLAGE COMMERCIAL)
 CENTER CORE AREA. THE PARCEL IS)
 ALSO IDENTIFIED AS TAX LOT 1605 IN)
 TOWNSHIP 37 SOUTH, RANGE 01)
 WEST (W.M.), SECTION 27 ("NORTH)
 PARCEL"). THE APPLICATION ALSO)
 INCLUDES A REQUEST TO MODIFY A)
 CONDITION OF APPROVAL IN THE)
 MATTER OF PLANNING COMMISSION)
 FILE ZC-15-041 LIMITING TRAFFIC)
 GENERATION FOR TAX LOT 501 IN)
 TOWNSHIP 37 SOUTH, RANGE 01)
 WEST (W.M.), SECTION 34 AT 955)
 NORTH PHOENIX ROAD ("SOUTH)
 PARCEL").)

FINDINGS OF FACT AND
CONCLUSIONS OF LAW

Applicants' Exhibit 1

Owners and Applicants: North Phoenix)
 Enterprises LLC, PCH1 LLC, and Louis)
 and Kathryn Mahar for North Parcel;)
 Rogue Credit Union for South Parcel)

Agent: CSA Planning, Ltd.)

NATURE AND SCOPE OF APPLICATION; BACKGROUND

The Application involves two parcels located in the Southeast Village Commercial Center Core Area (7A). Applicants hereby request:



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

- A Zone Change of North Parcel (8.00 acres¹ net) from SFR-00 (Single-Family Residential, 00 units per acre) to C-C (Community Commercial) consistent with the Southeast Plan for the Commercial Center Core Area (7A)
- Establishment of Trip Caps for North and South Side Parcels, thereby modifying Trip Cap previously established for South Parcel (Rogue Credit Union) through File ZC-15-041, based on Traffic Impact Analysis for both parcels by JRH Transportation Engineering included with the current application to accommodate buildout of the properties as per the adopted SE Commercial Center Core Area Master Plan
- Establishment of Trip Cap for North Parcel of 431 PM Peak Hour Net Trips based on Traffic Impact Analysis included with the current application

Medford's Southeast Plan requires that the properties within Subarea 7A (Commercial Center Core Area) be zoned Community Commercial (C-C). Pursuant to MLDC Section 10.227(2)(c), Applicant requests that the zone change be approved based on imposition of a special development conditions based on the Traffic Impact Analysis prepared by JRH Engineering with regard to transportation facilities and based on the Sanitary Sewer Study prepared by Construction Engineering Consultants, Inc. with regard to upgrading an off-site sanitary sewer main.

The TIA includes related analysis to support a revision of the condition limiting traffic generation for Rogue Credit Union's property (371W34-501, also referenced as the "South Side Parcel" in these findings. The application to re-zone that property, which was approved through Planning Commission File ZC-15-041, stated that a subsequent application would be filed jointly with owners of the remaining private property in the Commercial Center Core Area which would include a traffic impact analysis for the entirety of the Southeast Village Commercial Center Core Area. The prior re-zone of Rogue Credit Union's parcel was based on a stipulation that average daily traffic generation be limited to no more than 250 trips over what the MFR-20 zoning of that property would have generated. It made sense to defer a full TIA for a combined review with the remainder of the privately owned property in the Commercial Center Core Area.

II

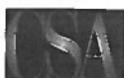
EVIDENCE SUBMITTED WITH APPLICATION

Applicant herewith submits the following evidence with its zone change application:

Exhibit 1. Applicants' Proposed Findings of Fact and Conclusions of Law

Exhibit 2. Vicinity/GLUP Map with Proposed Zone Change Area

¹ 9.03 gross acres as measured to adjacent street center-lines



- Exhibit 3.** Jackson County Assessor's Plat Maps 37-1W-27 & 37-1W-34
- Exhibit 4.** Legal Description of Zone Change Area (by Neathamer Surveying, Inc.)
- Exhibit 5.** Map of Proposed Zone Change Area (by Neathamer Surveying, Inc.)
- Exhibit 6.** Zoning Map (Current) on Aerial
- Exhibit 7.** Southeast Plan Map
- Exhibit 8.** Commercial Center Core Area Master Plan
- Exhibit 9.** Public Utilities Map for the subject area
- Exhibit 10.** Traffic Impact Analysis by JRH Transportation Engineering:
 - a. Initial TIA dated August 1, 2017
 - b. Reply dated September 13, 2017 to Medford City Traffic Engineer Comments of August 1 and 8, 2017
- Exhibit 11.** Sanitary Sewer Study by Construction Engineering Consultants, Inc.
- Exhibit 12.** Medford Water Commission Comments and Water Facility Map
- Exhibit 13.** Completed Zone Change Application Form with Duly Executed Limited Power of Attorney for CSA Planning, Ltd to represent Applicants/Property Owners

III

RELEVANT SUBSTANTIVE APPROVAL CRITERIA

The criteria under which a zone change application must be considered are in Section 10.227 of the Medford Land Development Code ("MLDC"). The relevant approval criteria are recited verbatim below:

MLDC 10.227 ZONE CHANGE CRITERIA (Inapplicable provisions omitted)

The approving authority (Planning Commission) shall approve a quasi-judicial zone change if it finds that the zone change complies with subsections (1) and (2) below:

- (1) The proposed zone is consistent with the Transportation System Plan (TSP) and the General Land Use Plan Map designation. A demonstration of consistency with the acknowledged TSP will assure compliance with the Oregon Transportation Planning Rule. Where applicable, the proposed zone shall also be consistent with the additional locational standards of the below sections (1)(a), (1)(b), (1)(c), or (1)(d). Where a special area plan requires a specific zone, any conflicting or additional requirements of the plan shall take precedence over the locational criteria below.

...

- (c) For zone changes to any commercial zoning district, the following criteria shall be met for the applicable zoning sought:

...

- (ii) The overall area of the C-C zoning district shall be over three (3) acres in size and shall front upon a collector or arterial street or state highway. In determining the overall area, all abutting property(s) zoned C-C shall be included in the size of the district.



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

...

(e) For purposes of (1)(c) and (1)(d) above, a zone change may be found to be "suitable" where compliance is demonstrated with one or more of the following criteria:

(i) The subject property has been sited on the General Land Use Plan Map with a GLUP Map designation that allows for only one zone;

(ii) At least 50% of the subject property's boundaries abut zones that are expressly allowed under the criteria in (1)(c) or (1)(d) above;

(iii) At least 50% of the subject property's boundaries abut properties that contain one (1) or more existing uses which are permitted or conditional uses in the zone sought by the applicant, regardless of whether the abutting properties are actually zoned for such existing uses; or

(iv) Notwithstanding the definition of "abutting" in MLDC 10.012 and for purposes of determining suitability under Section (1) (e), the subject property is separated from the "unsuitable" zone by a public right-of-way of at least 60 feet in width.

...

(2) It shall be demonstrated that Category A urban services and facilities are available or can and will be provided, as described below, to adequately serve the subject property with the permitted uses allowed under the proposed zoning, except as provided in subsection (c) below. The minimum standards for Category A services and facilities are contained in Section 10.462 and Goal 2 of the Comprehensive Plan "Public Facilities Element" and Transportation System Plan.

(a) Storm drainage, sanitary sewer, and water facilities must already be adequate in condition, capacity, and location to serve the property or be extended or otherwise improved to adequately serve the property at the time of issuance of a building permit for vertical construction.

(b) Adequate streets and street capacity must be provided in one of the following ways:

(i) Streets which serve the subject property, as defined in Section 10.461(2), presently exist and have adequate capacity; or

(ii) Existing and new streets that will serve the subject property will be improved and/or constructed, sufficient to meet the required condition and capacity, at the time building permits for vertical construction are issued; or

(iii) If it is determined that a street must be constructed or improved in order to provide adequate capacity for more than one (1) proposed or anticipated development, the Planning Commission may find the street to be adequate when the improvements needed to make the street adequate are fully funded. A street project is deemed to be fully funded when one (1) of the following occurs:

(a) the project is in the City's adopted capital improvement plan budget, or is a programmed project in the first two years of the State's current STIP (State Transportation Improvement Plan), or any other public agencies adopted capital improvement plan budget; or

(b) when an applicant funds the improvement through a reimbursement district pursuant to the MLDC. The cost of the improvements will be either the actual cost of construction, if constructed by the applicant, or the estimated cost. The "estimated cost" shall be 125% of a professional engineer's estimated cost that has been approved by the City, including the cost of any right-of-way acquisition. The method described in this paragraph shall not be used if the



Public Works Department determines, for reasons of public safety, that the improvement must be constructed prior to issuance of building permits.

- (iv) When a street must be improved under (b)(ii) or (b)(iii) above, the specific street improvement(s) needed to make the street adequate must be identified, and it must be demonstrated by the applicant that the improvement(s) will make the street adequate in condition and capacity.
- (c) In determining the adequacy of Category A facilities, the approving authority (Planning Commission) may evaluate potential impacts based upon the imposition of special development conditions attached to the zone change request. Special development conditions shall be established by deed restriction or covenant, which must be recorded with proof of recordation returned to the Planning Department, and may include, but are not limited to the following:
- (i) Restriction of uses by type or intensity; however, in cases where such a restriction is proposed, the Planning Commission must find that the resulting development pattern will not preclude future development, or intensification of development, on the subject property or adjacent parcels. In no case shall residential densities be approved which do not meet minimum density standards,
 - (ii) Mixed-use, pedestrian-friendly design which qualifies for the trip reduction percentage allowed by the Transportation Planning Rule,
 - (iii) Transportation Demand Management (TDM) measures which can be reasonably quantified, monitored, and enforced, such as mandatory car/van pools.

IV

FINDINGS OF FACT

The following findings of fact are reached and found to be true with respect to this matter:

1. **Property Location:** The property to be re-zoned (Tax Lot 1605) has no address at this time (being vacant). It is located at northeast corner of the intersection of East Barnett Road and North Phoenix Road. Tax Lot 501, for which modification of a prior zone change condition is requested, is located at 955 North Phoenix Road at the southeast corner of the same intersection. Both lots are within the corporate limits of the City of Medford and its adopted and acknowledged urban growth boundary.
2. **Property Description and Acreage:** The property to be re-zoned is identified in the records of the Jackson County Assessor as Tax Lot 1605 in Township 37 South Range 1 West in Section 27. The property consists of a single parcel having 8.0 net acres (9.03 acres gross). The remaining parcel already zoned C-C is identified as Tax Lot 501 in Township 37 South Range 1 West in Section 34. Tax Lot 501 has 2.139 net acres (2.85 acres gross). *See, Applicants' Exhibit 3 (Assessment Maps).*
3. **Subject Property Ownership:** Tax Lot 1605 is owned by North Phoenix Properties LLC, PHC1 LLC and Louis & Kathryn Mahar. Tax Lot 501 is owned by Rogue Credit Union.



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

4. **Comprehensive Plan Map Designation:** Tax Lots 501 and 1605 are both designated CM (Commercial) on the Medford Comprehensive Plan's GLUP Map.² See, Applicants' Exhibit 2.
5. **Zoning Map Designation:** Tax Lot 1605 is currently zoned SFR-00 (Single Family Residential, 1 dwelling per existing lot) and Tax Lot 501 is currently zoned C-C (Community Commercial). A legal description prepared by Oregon registered land surveyor Robert Neathamer for the proposed zone change area is provided at Applicants' Exhibit 4, and a corresponding map of the described area is provided at Applicants' Exhibit 5.
6. **Existing Land Use:** The property to be re-zoned (Tax Lot 1605) is vacant. Tax Lot 501 is improved with a credit union branch office and wireless transmission tower (monopole) located behind (east) the existing building.
8. **Topography:** The property is relatively flat (0-5% slope).
9. **Wetlands; Floodplain:** There are no wetland identified on local or national wetland inventories; the subject property is not within any FEMA mapped flood hazard area.
10. **Surrounding Land Uses:**
 - A. **North:** A 7.24 acre vacant tract of land consisting of Tax Lots 1601, 1608 and 1609) is owned by the City of Medford. The tract is was acquired to provide for greenway reach G-2 (Major Greenway – Not Riparian Corridor) as identified in the SE Plan along Herbert Creek. The area to the north of the creek and south of Shamrock Drive that is not city-owned includes Tax Lot 1604 (Musser) and Tax Lot 1602 (Levitt). Tax Lot 1604 is vacant and Tax Lot 1602 is improved with a single-family dwelling. This area north of the creek and south of Shamrock Drive is currently zoned SFR-00 but is designated as Service Commercial (SC) on the GLUP Map and the SE Plan Map (Area 7B) to be zoned C-S/P (Commercial, Service Professional and Office) in the future.
 - B. **South:** Adjacent and south of Rogue Credit Union's property is the 2.72 acre Tax Lot 502 (371W34) which is developed with the Medford Masonic Temple on land zoned MFR-20 (SE Plan Map Subarea 12 for UHDR). The building is single story of approximately 10,000 square feet. Further south is a vacant 2 acre parcel (371W34-600) zoned SFR-00 (SE Plan Map Subarea 15 for UR and future SFR-10 zoning). Beyond that Subarea 12 includes built residential planned unit developments including Harbrooke Court, Whitney Place, and Stonegate Estates.
 - C. **West:** North Phoenix Road, an arterial street, is located adjacent and west of the subject parcels. The Larson Creek Shopping Center is located to the west of the Rogue Credit Union parcel on a 12 acre tract across the street from the subject property. It includes a 50,000 grocery store sited at the intersection of North Phoenix

² "Generalized Land Use Plan" map.



and Barnett Roads. A fueling station is located on the south side of the grocery parking lot. South of that an additional 48,000 square feet of retail and office space in five buildings comprise the remainder of the shopping center. The shopping center is zoned C-C (Community Commercial) and is outside the Southeast Neighborhood Plan Area. West of the subject Tax Lot 1605 is the Joseph Office Park (zoned C-S/P) which is partially developed and to be built out for commercial offices. North of the office park is the SFR-4 zoned Michael Park Subdivision (37 lots).

D. East: East of the subject Tax Lot 1605 is a vacant 11.84 acre parcel (L. Mahar et al) currently zoned SFR-00. The westerly two acres of that parcel is designated on the GLUP Map and the SE Plan Map as Commercial land to be zoned C-C in the future as part of the Commercial Center Core Area (SE Plan Map Area 7A). The remainder to the east is designated as Service Commercial land to be zoned C-S/P in the future as part of SE Plan Map Area 7B within the SE Village Center. The City of Medford Fire Department's Station No. 6 is sited adjacent and east of Rogue Credit Union (Tax Lot 501) on a 1.64 acre parcel (371W34-504).

11. Essential (Category "A") Public Facilities: The comprehensive plan defines Category "A" public facilities as follows: (1) Storm Drainage; (2) Sanitary Sewer; (3) Water Facilities; (4) Streets and Street Capacity. The following facts are found with respect to each of the Category "A" public facilities:

A. Storm Water: The site lies within the Larson Creek Drainage Basin. Municipal stormwater collection lines are located within the adjacent street rights of way (See, Applicants' Exhibit 9 for catch basin, storm drain pipes/culverts, and pollution control manhole locations). This site would be able to connect to these facilities at the time of development and will be required to provide stormwater quality and detention at the time of development in accordance with Medford Land Development Code standards..

B. Sanitary Sewer Service: A sewer model analysis was performed by Construction Engineering Consultants to identify the potential capacity impacts to the existing sewer system downstream of the SE Plan 7A property as a result of the proposed zone change. See, Applicants' Exhibit 11. The analysis utilized flow specification from the adopted City of Medford Sanitary Sewer Master Plan (2005). Based on those parameters it is expected that the zone change will generate approximately 15 gallons per minute of additional flow. The model analysis showed that a 190-foot long section of 12-inch pipe section in Barnett Road west of Golf View Drive will need to be replaced as it is already undersized even without the zone change. No other capacity deficiencies were identified. The analysis noted that the deficient pipe section (identified in the plan as S371W28DS0139) has been identified in the current sewer master plan as a needed capital improvement project (East Barnett 6) in Table 1-9 of the plan, and that replacement of that 12-inch diameter pipe section with an 18-inch diameter pipe would increase capacity of that section to 2,341.5 gallons per minute. The needed capacity to accommodate projected flows from the zone change is 2,236 gallons per minute. Accordingly, completion of the project would more than accommodate the project flows from the zone change. Applicants hereby



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

stipulate to accept a condition of approval to upgrade the 190-foot long 12-inch diameter pipe section no. S371W28DS0139 with an 18-inch diameter pipe prior to issuance of a building permit for vertical construction.

C. Water Facilities: The Medford Water Commission has a 16-inch water line located in both E. Barnett Road and in N. Phoenix Road adjacent to the subject property that will adequately serve the proposed commercial zoning of the subject property. See, Applicants' Exhibit 12 - Medford Water Commission communication and Water Facility Map.

D. Streets and Traffic: A Transportation Impact Analysis (TIA) has been prepared by Applicants' registered professional traffic engineer, James R. Hanks PE, JRH Transportation Engineering. See, Applicants' Exhibit 10. The entire site is designated as a Transit Oriented Development (TOD) subject to buildout in accordance with the Southeast Commercial Center Core Area Master Plan Map and Standards. The TIA demonstrates that development of the property as per the adopted master plan and with mitigation as recommended by JRH, will meet all adopted transportation performance standards.

12. Police and Fire Protection: The property is served by the Medford Fire Department from its Fire Station 6, located adjacent and to the east of the Rogue Credit Union's property. Police protection is from the City of Medford Police Department.

V

CONCLUSIONS OF LAW

The following conclusions of law are reached for each of the relevant substantive criteria with respect to this matter:

***City of Medford Approval Criteria
Medford Land Development Code (MLDC) 10.227***

MLDC 10.227 ZONE CHANGE CRITERIA

The approving authority (Planning Commission) shall approve a quasi-judicial zone change if it finds that the zone change complies with subsections (1) and (2) below:

Criterion 1

- (1) The proposed zone is consistent with the Transportation System Plan (TSP) and the General Land Use Plan Map designation. A demonstration of consistency with the acknowledged TSP will assure compliance with the Oregon Transportation Planning Rule. Where applicable, the proposed zone shall also be consistent with the additional locational standards of the below sections (1)(a), (1)(b), (1)(c), or (1)(d). Where a special area plan requires a specific zone, any conflicting or additional requirements of the plan shall take precedence over the locational criteria below.

Findings: The subject property is within Sub-Area 7A of the Southeast Neighborhood Plan, which is an adopted refinement plan of the Medford Comprehensive Plan and a designated Transit Oriented Development area in the Medford TSP. Sub-Area 7A is the Commercial Center Core Area of the Southeast Village Center. Table 10.373 in the Medford Land



Development Code specifies only one zoning district to be allowed in Sub-Area 7A: Community Commercial (C-C). Therefore, it is concluded that the proposed zone is consistent with acknowledged TSP and the General Land Use Plan Map designation in accordance with this approval criterion.

The relevant locational standard in MLDC 10.227 (1) (a) through (d) for the C-C zoning district is at subsection 10.227(1)(c)(ii), which requires that the overall area of the C-C zoning district shall be over three (3) acres in size and shall front upon a collector or arterial street of state highway. The subject property exceeds three acres in size and is adjacent to an existing C-C zoning district that already exceed three acres in area, in compliance with the locational requirements for the district. Additionally, the subject property is within the Southeast Area Plan which requires specifically that the subject property be zoned C-C (Community Commercial) which takes precedence in the case of a conflict with the locational criteria.

The Southeast Plan includes additional requirements for zone changes within Sub-Area 7A as implemented by the Southeast Overlay District provisions in the Medford Land Development Ordinance and the adopted Southeast Village Commercial Center Core Area (7A) Master Plan. MLDC Section 10.374(B)(1) requires that all zone changes, PUDs, other land use actions, and permits within Area 7A shall conform to the Master Plan. MLDC Section 10.374(B)(2) incorporates the Master Plan by reference as part of the regulations for the S-E Overlay District. The first chapter of the Master Plan explains the intent and use of the Master Plan and includes a Master Plan Map which lays out the future development of the Core Area by sector.

Below the map, the plan states that for this map and all maps in the master plan, any depicted access onto North Phoenix Road and Barnett Road (which are arterial streets) shall not be approved until justified by a traffic impact analysis. The map shows two access points along North Phoenix Road and two access points along Barnett Road within the project area for subject Tax Lot 1605, and all of those are located within the "North Phoenix & Barnett-North" sector of the master plan.

Chapter 4 of the Master Plan establishes the relevant requirements and standards for that sector, and describes the connections planned through the commercial center. The plan states there that the transportation impact analysis (TIA) to be required at the time of zone change shall determine whether the Michael Park Drive connection to North Phoenix Road can be a full-movement intersection and whether a drop lane will be required. The commercial right-in-only access must also be supported by the TIA or otherwise to be removed from the final development plans.

Although specific access drives are not normally evaluated in a TIA at the time of a zone change, the TIA provided with this application includes analysis of the specified access points in accordance with the additional requirements of the special area plan. Applicants stipulate to accept conditions of approval as recommended in the TIA to remove the right-in-only commercial access along North Phoenix Road which was not supported TIA and to provide the south bound left turn lane for the intersection of North Phoenix Road with



Michael Park Drive (as discussed above). The TIA otherwise concludes that the safety and operations of the project's proposed access spacing and location will at the time of development and extended to the planning horizon, be equal or better than the application of the standards in Medford Code 10.550(3)(a) for the studied locations.

Conclusion – Compliance with Criterion 1: Based upon the foregoing findings of fact and conclusions of law, and subject to conditions of approval as recommended by the TIA, it is concluded that the application is consistent with the requirements of Criterion 1.

* * * * *

Criterion 2

10.227 Zone Change Criteria

(1)(c) For zone changes to any commercial zoning district, the following criteria shall be met for the applicable zoning sought:

...

(ii) The overall area of the C-C zoning district shall be over three (3) acres in size and shall front upon a collector or arterial street or state highway. In determining the overall area, all abutting property(s) zoned C-C shall be included in the size of the district.

Conclusions of Law (Criterion 2): The proposed zone change area is 9.03 gross acres and fronts upon North Phoenix Road and East Barnett Road, which are both arterial streets. The zone change will also expand a larger existing C-C zoning district of approximately 15 acres in area which includes the Rogue Credit Union Property to the south and the Larson Creek Shopping Center to the southwest. Accordingly, it is concluded that the proposed zone change complies with MLDC 10.2274(1)(c) under Criterion 2 herein. In any case, the subject property is required to be zoned as C-C by the Southeast Area Plan which supersedes this location criterion in the event of any conflict pursuant to MLDC Section 10.227(1) above and 10.227(1)(e) below.

* * * * *

Criterion 3

10.227 Zone Change Criteria (cont'd)

(1)(e) For purposes of (1)(c) and (1)(d) above, a zone change may be found to be "suitable" where compliance is demonstrated with one or more of the following criteria: (i) The subject property has been sited on the General Land Use Plan Map with a GLUP Map designation that allows for only one zone; (ii) At least 50% of the subject property's boundaries abut zones that are expressly allowed under the criteria in (1)(c) or (1)(d) above; (iii) At least 50% of the subject property's boundaries abut properties that contain one or more existing uses which are permitted or conditional uses in the zone sought by the applicant, regardless of whether the abutting properties are actually zoned for such existing uses; or (iv) Notwithstanding the definition of "abutting" in MLDC 10.012 and for purposes of determining suitability under Section (1) (e), the subject property is separated from the "unsuitable" zone by a public right-of-way of at least 60 feet in width.



Conclusions of Law: The subject property, as established herein above in the Findings of Fact, is sited in the Commercial Center Core Area 7A of the Southeast Plan Area Map, a refinement plan for GLUP Map, which allows for only C-C zoning district to be applied. Accordingly, it is concluded that the proposed zone change is “suitable” under Criterion 3.

* * * * *

Criterion 4

10.227 Zone Change Criteria

- (2) It shall be demonstrated that Category A urban services and facilities are available or can and will be provided, as described below, to adequately serve the subject property with the permitted uses allowed under the proposed zoning, except as provided in subsection (c) below. The minimum standards for Category A services and facilities are contained in Section 10.462 and Goal 2 of the Comprehensive Plan “Public Facilities Element” and Transportation System Plan.
 - (a) Storm drainage, sanitary sewer, and water facilities must already be adequate in condition, capacity, and location to serve the property or be extended or otherwise improved to adequately serve the property at the time of issuance of a building permit for vertical construction.

Discussion; Conclusions of Law: Goal 2 of the Public Facilities Element is to assure that General Land Use Plan (GLUP) designations and development approval process remain consistent with the City of Medford’s ability to provide adequate levels of essential public facilities and services. The following conclusions of are reached with respect to each of the Category “A” infrastructure components:

Sanitary Sewer: A Sanitary Sewer Study completed by Construction Engineering Consultants, Inc. at Applicants’ Exhibit 11 discussed in the findings of fact herein at Section IV. The analysis utilized flow specification from the adopted City of Medford Sanitary Sewer Master Plan (2005). Based on those parameters it is expected that the zone change will generate approximately 15 gallons per minute of additional flow. The model analysis showed that a 190-foot long section of 12-inch pipe section in Barnett Road west of Golf View Drive will need to be replaced as it is already undersized even without the zone change. No other capacity deficiencies were identified. The analysis noted that the deficient pipe section (identified in the plan as S371W28DS0139) has been identified in the current sewer master plan as a needed capital improvement project (East Barnett 6) in Table 1-9 of the plan, and that replacement of that 12-inch diameter pipe section with an 18-inch diameter pipe would increase capacity of that section to 2,341.5 gallons per minute. The needed capacity to accommodate projected flows from the zone change is 2,236 gallons per minute. Accordingly, completion of the project would more than accommodate the project flows from the zone change. Applicants hereby stipulate to accept a condition of approval to upgrade the 190-foot long 12-inch diameter pipe section number S371W28DS0139 with an 18-inch diameter pipe prior to issuance of a building permit for vertical construction.

Storm Drainage System: Based upon the findings of fact in Section IV, it is concluded that storm drainage facilities are available for connection to the subject property and that storm drainage is in any case required to be detained and treated in a separate, private stormwater



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

quality and detention facility at the time of building permit in accordance with MLDC Section 10.486.

Water System: Based upon the findings of fact in Section IV and the Medford Water Commission's response at Applicants' Exhibit 12, it is concluded that the water system is sufficient to provide the subject property with a permanent water supply having adequate water pressure and volume for projected commercial fire control needs consistent with the GLUP designation, and that these facilities are available to adequately serve the property under the proposed Community Commercial zone.

10.227 Zone Change Criteria (cont'd)

(2)(b) Adequate streets and street capacity must be provided in one of the following ways:

(i) Streets which serve the subject property, as defined in Section 10.461(2), presently exist and have adequate capacity; or

(ii) Existing and new streets that will serve the subject property will be improved and/or constructed, sufficient to meet the required condition and capacity, at the time building permits for vertical construction are issued; or

(iii) If it is determined that a street must be constructed or improved in order to provide adequate capacity for more than one (1) proposed or anticipated development, the Planning Commission may find the street to be adequate when the improvements needed to make the street adequate are fully funded. A street project is deemed to be fully funded when one (1) of the following occurs:

(a) the project is in the City's adopted capital improvement plan budget, or is a programmed project in the first two years of the State's current STIP (State Transportation Improvement Plan), or any other public agencies adopted capital improvement plan budget; or

(b) when an applicant funds the improvement through a reimbursement district pursuant to the MLDC. The cost of the improvements will be either the actual cost of construction, if constructed by the applicant, or the estimated cost. The "estimated cost" shall be 125% of a professional engineer's estimated cost that has been approved by the City, including the cost of any right-of-way acquisition. The method described in this paragraph shall not be used if the Public Works Department determines, for reasons of public safety, that the improvement must be constructed prior to issuance of building permits.

(iv) When a street must be improved under (b)(ii) or (b)(iii) above, the specific street improvement(s) needed to make the street adequate must be identified, and it must be demonstrated by the applicant that the improvement(s) will make the street adequate in condition and capacity.

(c) In determining the adequacy of Category A facilities, the approving authority (Planning Commission) may evaluate potential impacts based upon the imposition of special development conditions attached to the zone change request. Special development conditions shall be established by deed restriction or covenant, which must be recorded with proof of recordation returned to the Planning Department, and may include, but are not limited to the following:

(i) Restriction of uses by type or intensity; however, in cases where such a restriction is proposed, the Planning Commission must find that the resulting development pattern will not preclude future development, or intensification of development, on the subject property or



adjacent parcels. In no case shall residential densities be approved which do not meet minimum density standards,

(ii) Mixed-use, pedestrian-friendly design which qualifies for the trip reduction percentage allowed by the Transportation Planning Rule,

(iii) Transportation Demand Management (TDM) measures which can be reasonably quantified, monitored, and enforced, such as mandatory car/van pools.

Discussion; Conclusions of Law (Continued): A Transportation Impact Analysis (TIA) has been prepared by Applicants' registered professional traffic engineer, James R. Hanks PE, JRH Transportation Engineering. *See*, Applicants' Exhibit 10. The entire site is designated as a Transit Oriented Development (TOD) subject to buildout in accordance with the Southeast Commercial Center Core Area Master Plan Map and Standards. The TIA demonstrates that development of the property as per the adopted master plan and with mitigation as recommended by JRH, will meet all adopted transportation performance standards.

The only street facility that would fail to meet the adopted "Level of Service D" performance standard at build-out, absent mitigation, is the intersection of North Phoenix Road and East Barnett Road. The recommended traffic impact mitigation by JRH for that intersection is to add protected-permissive traffic signal phasing to the north and southbound left-turn phases. Applicants agree to add the recommended signal phasing.

The TIA also recommends that a center turn-lane be provided on North Phoenix Road at its intersection with Michael Park Drive, although Michael Park Drive is not an arterial or collector street. Medford Public Works responded that a south-bound left turn lane be provided rather than a two-way center median. Applicants agree to provide a left turn lane with final design to be coordinate with the City at the time Site Plan and Architectural Review or other application is filed for future development of that property.

Conclusions of Law Continued: Based upon the foregoing findings of fact and conclusions of law, it is concluded that the application is consistent with the requirements of Zone Change Criterion 4.

* * * * *

VI

AGREED TO STIPULATIONS

Applicants herewith agree to stipulate to the following if the same is made a condition attached to the approval of this land use application:

1. Vehicular trip generation for the North Side (TL 1605) shall be limited to 431 PM peak hour trips (adjusted from gross trips for pass-by and transit-oriented development reduction).



Findings of Fact and Conclusions of Law

Zone Change Application

Applicants: North Phoenix Properties LLC, et al

2. Vehicular trip generation for the South Side (TL 501) shall be limited to 145 PM peak hour trips (adjusted from gross trips for pass-by and transit-oriented development reduction). The South Side trip cap shall replace the prior cap of 628 Average Daily Trips imposed through Planning File No. ZC-15-041.
3. Prior to issuance of permits for vertical construction of new buildings, traffic impact mitigation for the intersection of North Phoenix Road and East Barnett Road shall require adding protected-permissive traffic signal phasing to the north and southbound left-turn phases.
4. Prior to issuance of permits for vertical construction on Tax Lot 1605, owners of that property agree to construct a south-bound left turn lane for North Phoenix Road at Michael Park Drive, including modification of the existing median to accommodate the same. Final design shall be submitted for review and approval with Site Plan and Architectural Review application at the time development of the North Side (Tax Lot 1605) is proposed.
5. Driveway "J" as identified in the TIA (being the right-in only access from north-bound North Phoenix Road to TL 1605) shall not be included in future development plans for the property.
6. Prior to issuance of permits for vertical construction on Tax Lot 1605, the 190-foot segment of 12-inch diameter pipe section (ID No. S371W28DS0139, City of Medford Sanitary Sewer Master Plan (2005)) shall be upgraded to an 18-inch diameter pipe.

VII

ULTIMATE CONCLUSIONS

Based upon the preceding findings of fact and conclusions of law, it is ultimately concluded that the case for a zone change to Community Commercial (C-C) is consistent with all of the relevant substantive approval criteria.

Dated: December 18, 2017 – as revised March 13, 2018

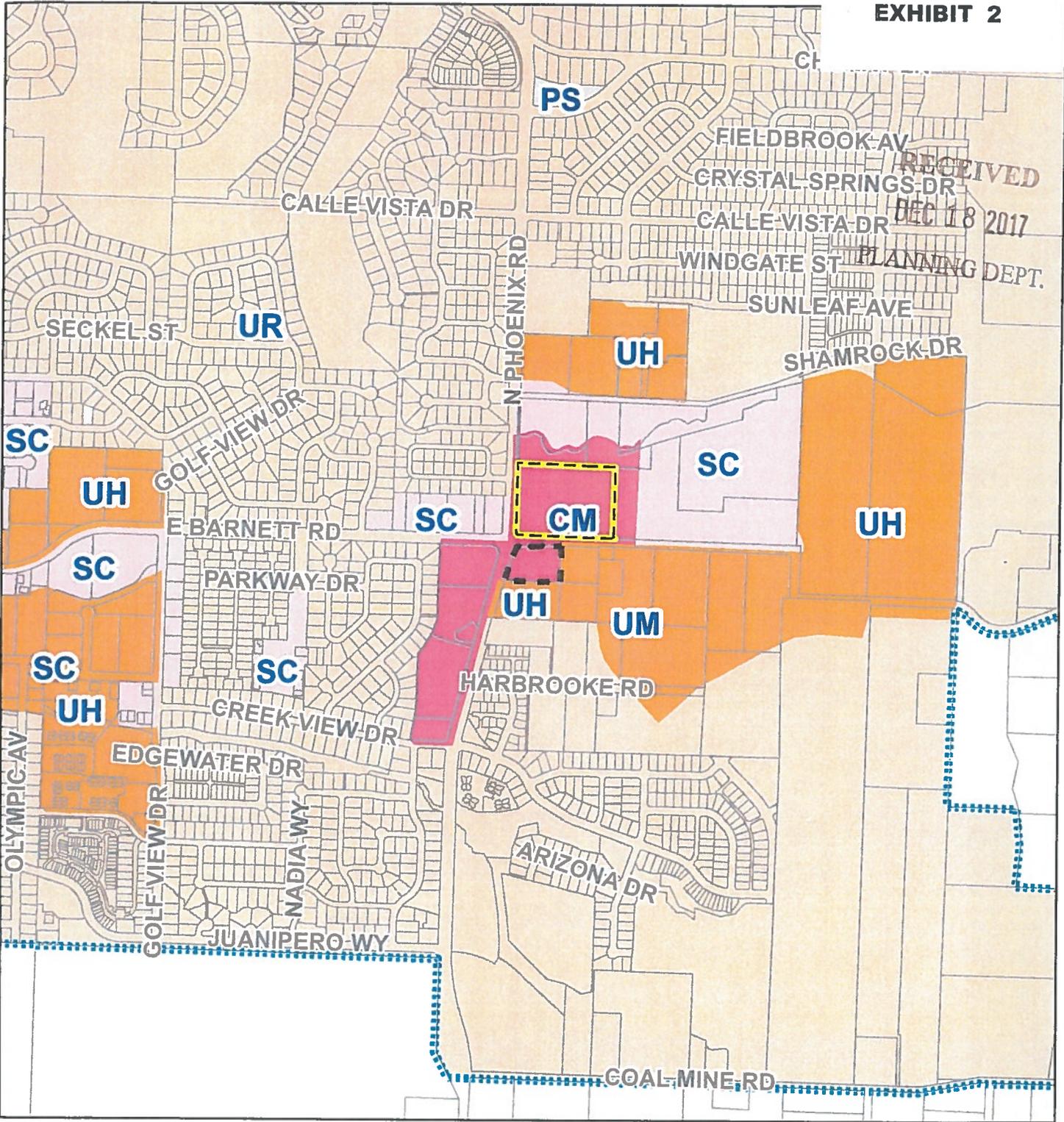
Respectfully submitted on behalf of applicant:

CSA PLANNING, LTD.



Raul Woerner
Consulting Planner





 Subject Lot 1605
 Subject Lot 501
 Tax Lots
 Urban Growth Boundary

9-13-2016 Source: CSA Planning, Ltd. Jackson County GIS

Vicinity - GLUP Map

North Phoenix Enterprises LLC
 Zone Change
 37-1W-27-1605 & 37-1W-34-501

0 500 1,000 2,000 Feet

1 inch = 1,000 feet

CITY OF MEDFORD
 EXHIBIT # B
 File # ZC-17-168

CSA Planning, Ltd.

EXHIBIT " "

EXHIBIT RECEIVED

DEC 18 2017

OUTLINE DESCRIPTION OF PROPOSED ZONING AREA

All that real property located in the Southwest One-quarter of Section 27, Township 37 South, Range 1, West of the Willamette Meridian, in the City of Medford, Jackson County, Oregon, being more particularly described as follows:

PLANNING DEPT.

Beginning at the northeast corner of Tract 1 as described in Instrument Number 2009-001973, of the Official Records of Jackson County, Oregon, and as depicted on Survey Number 20296, as filed in the office of the Jackson County Surveyor; thence along the easterly boundary of said Tract 1, South 00°03'51" East, 503.52 feet to the northerly right-of-way of Barnett Road; thence leaving the easterly boundary of said Tract 1 and along the southerly extension thereof, South 00°03'51" East, 30.00 feet to the centerline of said Barnett Road; thence along the centerline thereof, North 89°38'05" West, 732.91 feet to intersect the centerline of North Phoenix Road; thence leaving the centerline of Barnett Road and along the centerline of said North Phoenix Road, North 00°03'51" West, 540.33 feet to intersect the westerly extension of the northerly boundary of said Tract 1; thence leaving the centerline of said North Phoenix Road, South 89°06'09" East, 45.01 feet to the easterly right-of-way of said North Phoenix Road, and the northwest corner of said Tract 1; thence leaving said right-of-way and along the northerly boundary of said Tract 1, South 89°06'09" East, 687.98 feet to the Point of Beginning.

Said outlined area contains 9.03 acres, more or less, including the area within the right-of-ways for North Phoenix Road and Barnett Road.

This description is prepared for the distinct purposes of outlining an area to be re-zoned in the City of Medford, Oregon, and is not sufficient for the conveyance of real property, determining or creating property boundaries.

Prepared By: Neathamer Surveying, Inc.
3126 State Street, Suite 203
PO Box 1584
Medford, Oregon 97501
Phone: (541) 732-2869
Project Number: 16062

Date: November 28, 2016

REGISTERED
PROFESSIONAL
LAND SURVEYOR

Robert V. Neathamer

OREGON
JULY 19, 1994
ROBERT V. NEATHAMER
2675

RENEWAL: DEC. 31, 2016

CITY OF MEDFORD
EXHIBIT # B
File # ZC-17-168

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DEC 18 2017
PLANNING DEPT.

45'

37 1W 27
TAX LOT 1609

37 1W 27
TAX LOT 1601

589°06'09"E

687.98'

45.01'

TRACT 1

POINT OF BEGINNING

MICHAEL
PARK DRIVE

AREA TO BE
RE-ZONED
(9.03 AC)

37 1W 27
TAX LOT 1605

503.52'

500°03'51"E

37 1W 27
TAX LOT 1606

NORTH PHOENIX ROAD

N00°03'51"W 540.33'

SURVEY NUMBER 20296

N89°38'05"W 732.91'

30.00'

30'

BARNETT ROAD

37 1W 34
TAX LOT 501

37 1W 34
TAX LOT 504

TAX
LOT
503

37 1W 34
TAX LOT 505

37 1W 34
TAX LOT 507

REGISTERED
PROFESSIONAL
LAND SURVEYOR

Robert V. Neatham

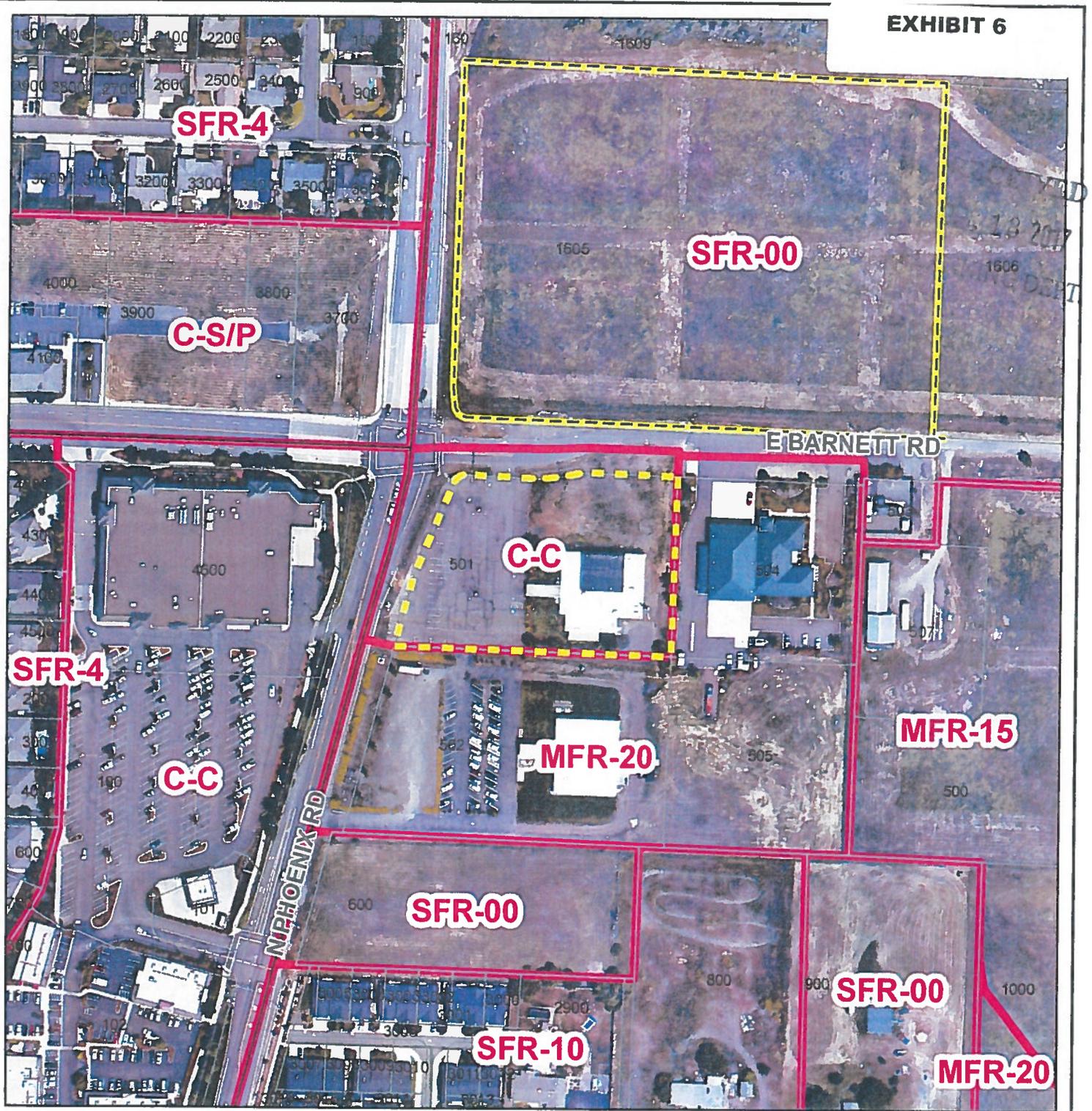
OREGON
JULY 19, 1994
ROBERT V. NEATHAM
CITY OF MEDFORD

Renewed ~~EXHIBIT #~~ 12/31/16 B

File # ZC-17-168



SCALE 1" = 150'



-  Subject Lot 1605
-  Subject Lot 501
-  Tax Lots
-  Medford Zoning

2012 Aerial

Medford Zoning

North Phoenix Enterprises LLC
 Zone Change
 37-1W-27-1605 & 37-1W-34-501

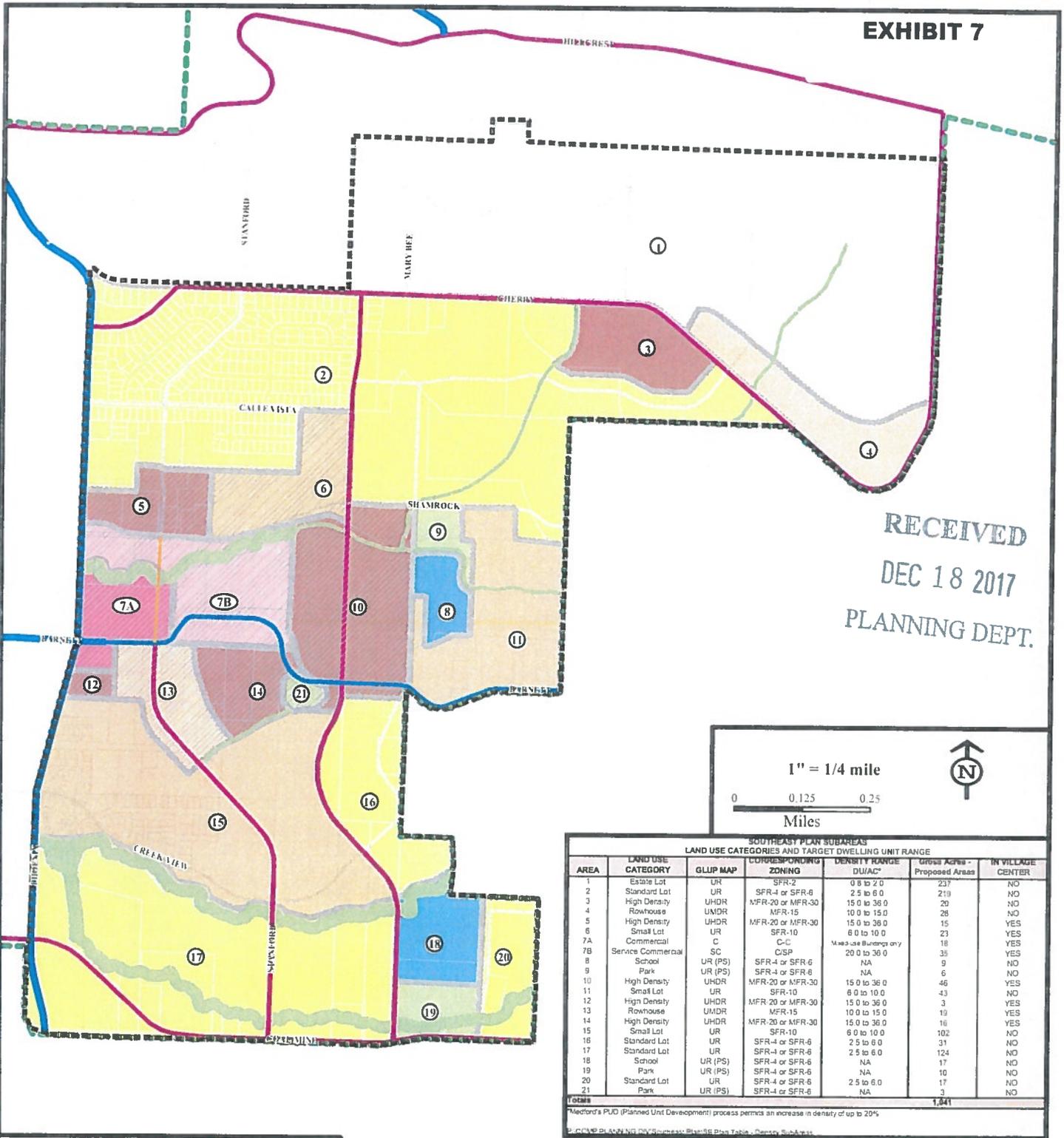


CITY OF MEDFORD
 EXHIBIT # B
 File # ZC-17-166 h = 200 feet

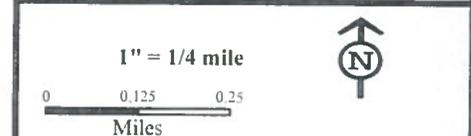


CSA Planning, Ltd.

9-13-2016 Source: CSA Planning, Ltd. Jackson County GIS



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DEC 18 2017
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SOUTHEAST PLAN SUBAREAS LAND USE CATEGORIES AND TARGET DWELLING UNIT RANGE						
AREA	LAND USE CATEGORY	GLUP MAP	CORRESPONDING ZONING	DENSITY RANGE DU/AC*	Gross Acres - Proposed Areas	IN VILLAGE CENTER
1	Estate Lot	UR	SFR-2	0.8 to 2.0	237	NO
2	Standard Lot	UR	SFR-4 or SFR-6	2.5 to 6.0	219	NO
3	High Density	UHDR	MFR-20 or MFR-30	15.0 to 36.0	20	NO
4	Rowhouse	UMDR	MFR-15	10.0 to 15.0	26	NO
5	High Density	UHDR	MFR-20 or MFR-30	15.0 to 36.0	15	YES
6	Small Lot	UR	SFR-10	6.0 to 10.0	23	YES
7A	Commercial	C	C-C	Make Use Buildings only	18	YES
7B	Service Commercial	SC	C-SF	20.0 to 36.0	35	YES
8	School	UR (PS)	SFR-4 or SFR-6	NA	9	NO
9	Park	UR (PS)	SFR-4 or SFR-6	NA	6	NO
10	High Density	UHDR	MFR-20 or MFR-30	15.0 to 36.0	46	YES
11	Small Lot	UR	SFR-10	6.0 to 10.0	43	NO
12	High Density	UHDR	MFR-20 or MFR-30	15.0 to 36.0	3	YES
13	Rowhouse	UMDR	MFR-15	10.0 to 15.0	19	YES
14	High Density	UHDR	MFR-20 or MFR-30	15.0 to 36.0	16	YES
15	Small Lot	UR	SFR-10	6.0 to 10.0	102	NO
16	Standard Lot	UR	SFR-4 or SFR-6	2.5 to 6.0	31	NO
17	Standard Lot	UR	SFR-4 or SFR-6	2.5 to 6.0	124	NO
18	School	UR (PS)	SFR-4 or SFR-6	NA	17	NO
19	Park	UR (PS)	SFR-4 or SFR-6	NA	10	NO
20	Standard Lot	UR	SFR-4 or SFR-6	2.5 to 6.0	17	NO
21	Park	UR (PS)	SFR-4 or SFR-6	NA	3	NO
Totals					1,341	

*Medford's PUD (Planned Unit Development) process permits an increase in density of up to 20%
S:\COMP\PLAN\NO\DCV\Source\Map\SE Plan Table - Density Subarea

ADOPTED March 7, 2013
ORDINANCE #2013-42

No guarantee or warranty is expressed or implied in terms of data accuracy or legitimacy. This product is intended for use as public information and precise interpretations of the official record should be solicited from the Medford Planning Department.

**SOUTHEAST
PLAN MAP**

- UGB
- SE Plan Boundary
- Village Center TOD
- Existing Taxlots
- Greenway
- Major Arterial
- Minor Arterial
- Major Collector
- Minor Collector
- Commercial Street
- Standard Residential

- Estate Lot
- Standard Lot
- Small Lot
- Row House
- High Density
- Commercial
- Service Commercial
- Schools
- Parks

CITY OF MEDEORD
EXHIBIT # B
File # ZC-17-168

Design and Development Standards

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PLANNING DEPT.

City of Medford

Southeast Village Commercial Center Core Area Master Plan



Adopted by Ordinance no. 2014-160 on 18 December 2014



CITY OF MEDFORD
EXHIBIT # B
File # ZC-17-168

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Chapter 1. Intent and Use of the Master Plan

Commercial Center Planning in the Southeast Village Area

These design and development standards are incorporated by reference as part of Chapter 10 of Medford Municipal Code and shall apply to development within the Commercial Center Core Area (7A), as depicted on the City of Medford's Southeast Plan Map. The Area 7A, approximately 19 acres in size, is the retail core of a 53-acre Commercial Center, which itself is nestled within the 178-acre Southeast Village Center Transit-Oriented District.

The Master Plan functions as a component of the Southeast (S-E) Overlay District. Pursuant to Section 10.374(4), the Southeast Overlay District regulations establish that a Master Plan adopted by the City Council shall govern design and development within the Commercial Center Core Area (7A). All zone changes, PUDs, other land use actions, and permits within the Commercial Center Core Area shall conform to the Master Plan. Section 10.377, 'Special Design Standards for Southeast Village Center,' further provides that design standards for the Commercial Center Core Area shall be established through the adopted Master Plan rather than under Section 10.377. The Master Plan provisions are additive to 10.378, 'Special Standards for Commercial Center, S-E,' and 10.379, 'Streetscape, Planter Strip, and Tree Design Standards, S-E.' Pursuant to 10.381, 'Special Street Design and Vehicle Access Standards, S-E,' Commercial Center Core Master Plan controls over the City of Medford Engineering Standards and Specifications where modifications have been adopted as part of the Master Plan. According to Section 10.383, 'Standards for Development Abutting Arterial or Collector Streets, S-E,' access shall be consistent with Commercial Center Core Area Master Plan where applicable.

Land use and development within the Master Plan area will accordingly conform to the Master Plan in addition to all other applicable land use and development regulations. In the situation of a conflict with other regulations of the Medford Land Development Code, the Master Plan Standards shall supersede.

The Master Plan is designed to create a transition from the existing suburban pattern west of North Phoenix Road into the planned neo-traditional form of the Southeast Plan Area. Six distinct sectors effect the transition between the higher-order major arterial North Phoenix Road to a traditional "Main Street" of retail commercial shops and eateries along Stanford Avenue. Setbacks, for instance, shrink as development progresses from the west to the east. Common design standards applicable to all sectors assure the use of high-quality materials and pedestrian-oriented scale throughout the Commercial Center Core Area while also accommodating flexibility of final design to foster an overall vibrancy of individual prefer-

Southeast Village Commercial Center Core Area Master Plan

ences. Sector-specific standards are also established to address particular attributes, opportunities, and objectives of each subarea. Pictures within the Master Plan illustrate design forms that are to be achieved or avoided in accordance with the accompanying standards.



Commercial Center Core Area Master Plan Map

For this map and all maps in this master plan, any depicted access onto North Phoenix Road and Barnett Road shall not be approved until justified by a traffic impact analysis.

In adopting this master plan, the Council included in its motion an acknowledgement of testimony regarding the location of the roundabout, specifying that it should be adjusted by "eight feet" (northward, according to testimony. Rather than attempt to depict such a small, specific change on a conceptual plan of this scale, it is noted here for reference.

Chapter 2. Common Design Standards

The following standards apply to all development within the Commercial Center Core Area. All commercial and public right-of-way landscaping, street furnishings, irrigation, and maintenance shall be provided by the abutting property owners or association of property owners unless accepted by the City for public dedication and maintenance.

1. Storefront scale and character

- a. Break overall building masses into segments or modules through use of pilasters and columns at intervals of no more than 20 feet.
- b. Provide varied building and parapet heights of at least 20 feet.
- c. Provide awnings, canopies, arcades, or other shelter for pedestrians along adjacent sidewalk areas subject to the following standards:

- i. Awnings/canopies:

Depth: Five feet minimum from façade, eight feet maximum.

Height: 10 feet minimum clearance from sidewalk

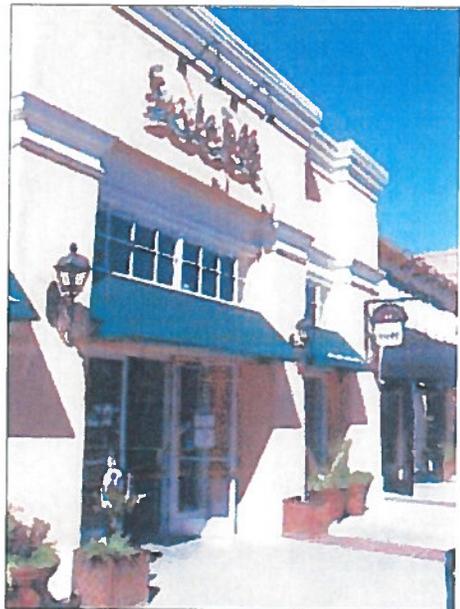
Location: Over individual display windows between prominent vertical elements such as pilasters or columns. Awnings and canopies may extend over a street right-of-way up to eight feet subject to obtaining a revocable permit from the City.

Materials: Metal or fabric, not shiny.

- ii. Colonnades/arcades:

Depth: Eight feet minimum from façade to inside column face.

Height: 14 feet minimum ceiling clearance from sidewalk



Storefront modules compatible with the Commercial Center Core Area design standards.



Awnings at a pedestrian scale provide shelter without dominating building frontages.

Location: Colonnades and arcades shall not extend into the street right-of-way area; the building façade shall be set back to accommodate these where proposed adjacent to street rights-of-way.

Openings: Six-foot minimum width and 10-foot minimum height for openings between arches or columns

iii. Balconies:

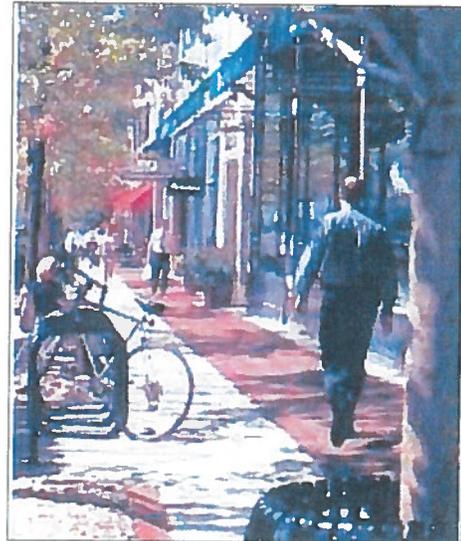
Depth: Six feet minimum over sidewalk area, 10 feet maximum

Height: 10 feet minimum clearance

Location: Balconies may extend over the street right-of-way up to eight feet, subject to obtaining a revocable permit from the City.

2. Provide a richness of architectural façade depth and detail

- a. Express columns and beams on the building's exterior.
- b. Provide a minimum of three façade layers (e.g., front of columns or pilasters, wall plane, window frame, and window glass) to building facades that face streets, plazas, parking, and public areas.
- c. Walls in excess of 100 feet horizontally shall employ architectural details to add visual interest, such as varying the height of the parapet, vertical change in materials, or variation in the façade in the horizontal plane.
- d. Finish wall tops with overhangs, projecting cornices, and column caps that provide a strong visual terminus to the structure.
- e. All roofs, whether flat or sloped, shall have eave, gable end, or parapet treatment, which should include a cornice, a cap, outriggers, dentil molding, or other architectural devices to add visual interest to building elevations.
- f. Use applied and integrated design elements such as but not limited to exposed rafter tails (for sloped roofs), cornice moldings, and applied medallions.



Variety of high-quality materials and details at pedestrian scale.

3. Provide a unified design around all sides of buildings

- a. Maintain a continuity of design, materials, color, form, and architectural detail for all elevations of a building that are visible from adjacent streets, plazas, parking, and public areas.
- b. Service and loading areas are to be screened through use of landscaping and/or structural elements that repeat the architectural form of the building.

4. Avoid blank walls and service areas which are visible from adjacent streets and public areas

- a. Orient buildings to avoid blank walls and service areas which are visible.
- b. For walkways between buildings and other facades visible to the general public where there are no entries, windows, or other openings, add pilasters, trellises, lattices, and/or artwork of permanent materials (metal, glass, stone, and fired ceramics) along with landscaping to make the facades more attractive.

5. Integrate or screen all trash and service areas

- a. Match or complement wall materials to that of the building.
- b. Where screen walls are prominently visible, repeat architectural details of the main structure (e.g., wall caps similar to those on the primary structure).
- c. Dense vegetative screening may be used with a plain wall or fence (e.g., concrete block) if full vegetative screening of the wall or fence will be achieved within three years of installation.



Example of integrated equipment housing on roof.

6. Screen all roof equipment

- a. All roof equipment must be screened from view from adjacent streets (opposite sidewalk) and properties.
- b. Roof screens shall be constructed from materials as similar to the



Roof-mounted mechanical equipment is screened by parapet walls on all sides.

building walls as possible, and should be designed to appear as an architecturally integrated part of the building rather than an added-on element.

- c. A roof plan shall be included at the time of submittal for architectural re-review and approval. The plan shall show the location, type and size, including height, of all roof-mounted equipment and appurtenances.
- d. Special conditions for roof-mounted solar energy systems:



Solar panels parallel to roof plane are permitted outright by ORS 227.505.

- i. Hide or otherwise integrate system components into the architectural forms and character of the building and/or the surrounding landscape.
- ii. Solar voltaic and solar thermal energy systems that comply with ORS 227.505 are permitted outright.



Low planter edge buffers service drive.

7. Provide visual buffering of on-site utility elements

- a. Locate on-site ground utility elements in areas inconspicuous from public rights-of-way.
- b. Where inconspicuous location is not possible, screen utility elements from view with landscaping, street furniture, or other means as allowable by the utility provider.



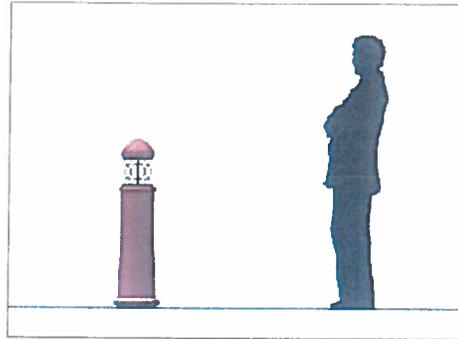
Low hedge as parking lot screen.

8. Screen off-street parking from public rights-of-way

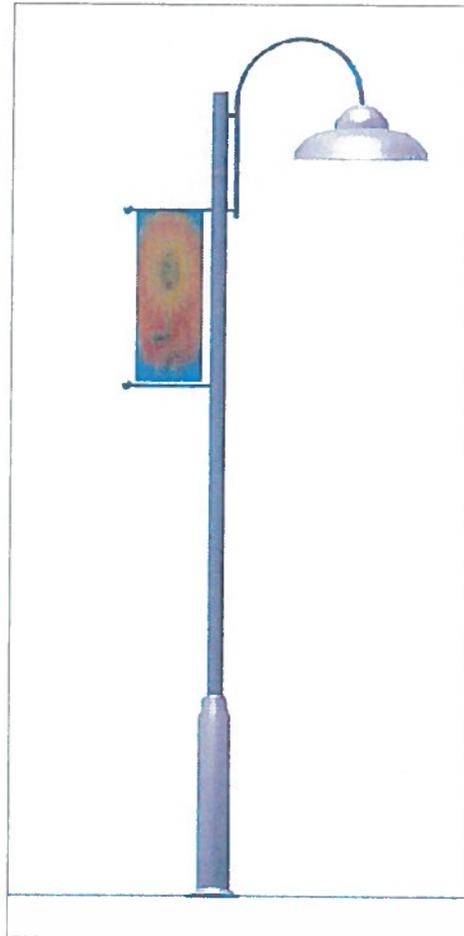
- a. Provide low walls, landscaping, and other streetscape treatments at parking lot edges where adjacent to public streets.
- b. Flowering plant materials shall be included with edge treatments.

9. Provide safe and attractive pedestrian access through off-street parking areas

- a. Provide a curbed and raised sidewalk at least five feet in paved unobstructed width within off-street parking area where interior parking area sidewalk access is specified on the Master Plan. Where parking is located adjacent to the sidewalk, wheel stops, planters, bollards, or other similar features will be provided to prevent parked vehicles from obstructing the sidewalk.
- b. Sidewalks adjacent to building entries facing parking areas shall be at least eight feet in width and include pedestrian-oriented lighting and street trees in planters or tree wells.
- c. Include pedestrian access ways and crosswalks.
 - i. The Pedestrian Walkway Standards at Section 10.772 through 10.776 shall be met. Pedestrian walkways crossing driving surfaces shall be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks or stamped concrete. Stamped asphalt with or without stain is not permitted.
 - ii. Pedestrian access between buildings along street frontages shall be provided at spacing not to exceed 300 feet in order to connect off-street parking areas to the retail streets. Integrate pass-through walkways with other outdoor activity areas such as plazas, patios, or entry courts rather than as secluded narrow corridors.



Use bollard lighting along smaller paths and landscape accents.



Fixture not to exceed 14 feet in height within off-street parking areas. Banner brackets are optional.

- d. Include pedestrian-scale lighting along public streets and along off-street walkways and sidewalks.
 - i. Use bollard lighting along minor paths as indicated in the sector-specific standards.
 - ii. Provide pedestrian-scale street lighting (see figure) within off-street parking areas along major walkways and sidewalks.
 - iii. Where banner or planter brackets are provided, property owners are to maintain banners or plantings in good condition at all times. Drip irrigation systems shall be provided for planter brackets.
 - iv. Use the 'Street Light Standards, S-E,' for public street lighting.

10. Utilize high-quality building materials and details

- a. For walls, at least two of the following materials shall be employed: Stucco, brick, split-faced or polished-face concrete block, stone (real or cultured), wood, and glazed or unglazed ceramic tile rated for exterior applications.
- b. For sloped roofs, the following materials may be used: non-reflective metal, architectural/dimensional shingles, clay or concrete tile, or slate (natural or synthetic).
- c. For windows and doors: wood or aluminum storefront, either natural in color or in a painted color. Anodized bronze or black (light, medium, or dark) metal is not permitted.
- d. For street furnishings, use black powder or durable enamel-coated metal, natural wood, stone, or concrete. Plastic or resin furnishings are not permitted for outdoor areas.

11. Maintain a high degree of transparency at ground-floor window areas

- a. Ground level windows shall be provided in accordance with 10.377(4).

12. All projects shall be landscaped in accordance with 10.377(6), 10.735, and 10.780, with the following additional standards:

- a. Intersection corners and commercial entry drives shall be presented as area focal points for landscaping.
- b. Planter pots, window boxes, and/or other smaller-scale elements shall be provided along sidewalks near storefronts to provide visual interest to the streetscapes and interior walkways (*i.e.*, facing grocery and parking courts).
- c. Provide large shade trees on the interior and perimeter of parking lots.

13. Utilize colors that are appropriate to the use and surrounding areas

- a. Use muted tones with stronger accent colors limited to smaller areas of trim. Intense, bright, or fluorescent colors shall not be used as a primary building color.
- b. Distinguish individual buildings by varying tones and hues.
- c. A range of analogous or complementary colors shall be provided throughout the Commercial Center Core rather than a single dominant paint color and shade.

14. Drive-Through Facilities

- a. Drive-through facilities shall be restricted to the three locations depicted on the Master Plan Layout. Drive-through facilities shall be subordinate and ancillary to the primary structure in conjunction with a pharmacy, bank, or credit union. See Master Plan Map and Chapter 3 for interim use of existing building for credit union drive-through window in the North Phoenix & Barnett South sector. No other drive-through uses are allowed within the Commercial Center Core Area.
- b. Drive-through facilities shall be oriented and designed so as not to substantially disrupt pedestrian activity or surrounding uses. Utilize low landscape materials to maintain safe visibility at sidewalk crossings and to separate the lane from adjacent parking and circulation areas.
- c. The service windows shall be architecturally integrated with the building by extension of the roof structure or provision of a canopy over the adjacent service lane.
- d. Automated teller machines (ATMs) shall be integrated as part of the architecture of the building or canopy support pylons for the bank and the credit union buildings.



Example of small bank drive-through facility with appropriate landscape treatment.

15. Plazas

- a. Plazas shall be located as indicated on the Master Plan and shall be designed as “public plazas” within the meaning of 10.012.
- b. Public plazas are areas adjoining a sidewalk or walkway that provide places for pedestrians to sit, stand, or rest. Plazas are located at transit stops, building entrances, or intersections, and connect directly to adjacent sidewalks, walkways, transit stops, building entrances, and intersec-

tions. They are usually paved with concrete, pavers, bricks, or similar materials, and include seating, pedestrian-scale lighting, and similar pedestrian improvements. Low walls or planters and landscaping are provided to create a semi-enclosed space and to buffer and separate the plaza from any adjoining parking lots and vehicle-maneuvering areas.

- c. Plazas must be provided at the time the adjacent structures are built, and must be maintained by the property owner or association of property owners unless otherwise accepted by the City for public ownership and maintenance.

16. Bicycle Parking

- a. Bicycle parking shall be provided in accordance with 10.747 through 10.751, except that the amount of bicycle parking provided shall be two-times the amount required by 10.748, 'Bicycle Parking Standards.'
- b. Bicycle parking facilities shall be located as shown on the pedestrian and bicycle circulation plan at the end of this chapter. Additional locations may be sited in accordance with 10.749.

17. Include a Transportation Demand Management Program

A transportation demand management plan shall be included with applications for site plan and architectural review. The plans must score a minimum of 10 points each under the following schedule:

- a. Physical: 10 points needed per designated Sector

Options	Score/Points
Provide additional sheltered bicycle parking over adopted minimums	1 point per each 50 percent increase over minimum
Provide bicycle lockers or secured bicycle parking area, or fixed bicycle maintenance station	1 point per secured bicycle space, 5 points per locker, or 5 per maintenance station
Provide on-site shower and lockers for employees	5 points for shower and locker facility
Provide transit shelter with landscaping and trash receptacles	Full point allotment for transit plaza sector
Provide two-wheeled vehicle parking	1 point per space
Provide off-site public pedestrian/bicycle trails or equivalent monetary contribution for greenway, park areas, or substandard rights-of-way within one-quarter mile	1 point for 10 linear feet improved or funded

b. Programmatic: 10 Points needed per annum per designated Sector.

Options	Score/Points
Host and event such as a bicycle rally, walk-athon, health fair, contest, etc., to promote benefits of walking, cycling, using transit, or ride-sharing	2 points per event
Cost share or provision of group transit passes for employees	8 points per group transit program
Cost share or provision of bicycles for employees	8 points per bicycle
Car-share program	5 points per fleet/shared vehicle
Carpool/vanpool	4 points per car for carpool, 8 points per van for vanpool
Unbundle parking from residential rents (charge for parking space: residents who do not have a car do not pay for parking)	1 point per “unbundled” residential unit
Establish a bicycle commuter benefit program [maintenance funds provided as fringe benefit using pre-payroll tax code allowances]	4 points
Become a marketing partner with RVTD or other alternative transportation mode provider [e.g., reciprocal advertising and sponsorships]	8 points for full year campaign; 2 points for single quarter campaign

An administrator/ETC (employee transportation coordinator) shall be designated by Sector (or consolidated area) to monitor and maintain records on ongoing program participation and events.

Chapter 3. North Phoenix and Barnett South

Southwest Entry to the Commercial Center

The sector is a redevelopment site containing a single 2.2-acre (net) parcel located at the intersection of two major arterial streets, North Phoenix Road and East Barnett Road. It will function as a cornerstone gateway into the Commercial Center Core Area from the west.

As of 2011, this sector is improved with a single-story building of approximately 11,000 square feet, formerly used as a fraternal lodge, sited on the southeast corner to the rear of the parcel with appurtenant parking located to the front. A wireless transmission tower (monopole) is located behind the building.

A fire station is located on the adjacent property to the east. The property to the south is improved with a fraternal lodge of similar style. The parking lots are connected for cross-access needs. A grocery store and community shopping center are located to the west across North Phoenix Road, and a professional office park is located to the northwest across the intersection of North Phoenix Road and East Barnett Road.

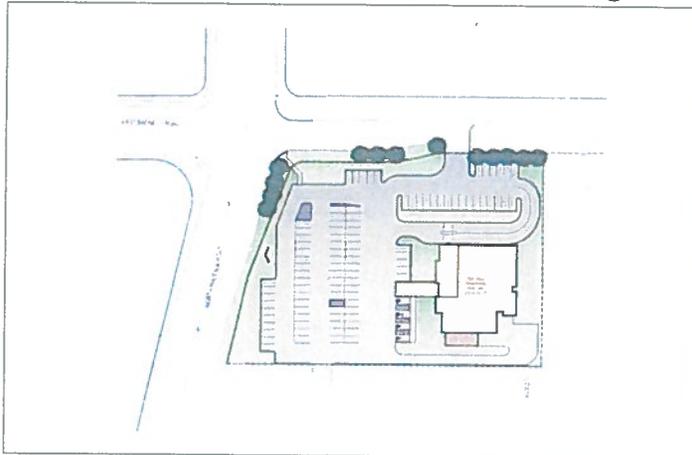


North Phoenix & Barnett – South Sector



Master Plan Layout

A credit union acquired the subject land for the purpose of establishing a neighborhood branch. Initially, it plans to re-purpose a 1,800-square-foot portion of the existing 11,200-square-foot building and make the remaining space available for community meetings and events. A drive-through window would be integrated into the north side of the building for the interim credit union use. Ultimately, the credit union plans to construct a new building of approximately 3,500 square feet as the permanent branch which would be sited on the northwest corner of the property. The new building would include a drive-through service window to the rear, to be



Interim Plan Layout for Credit Union Use of Existing Building

architecturally integrated and designed in accordance with the Common Design Standards (Chapter 2). The original building will be re-purposed or, alternatively, replaced with a new structure for commercial or institutional uses. The interim drive-through service is to be removed from that building upon completion of the new credit union building.

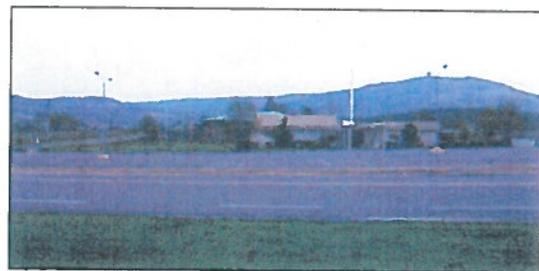
A third building of approximately 4,200 square feet for retail uses will complete the redevelopment project. That building site would be located in the southwest corner of the property.

Sector-Specific Standards

The following design and development standards apply in this sector:

1. Site Design

- a. North Phoenix Road Frontage (approximately 260 feet)
 - i. Utilize the standard major arterial cross-section (10.428) to buffer pedestrians from higher-velocity traffic and to promote a consistent treatment with existing improvements on the west side of the street. This will provide a six-foot-wide bike lane, a 10-foot-wide planter strip, and five-foot-wide sidewalk.



Existing building and parking from N. Phoenix.

- ii. Provide street furniture along the sidewalk area adjacent to building sites.
- iii. A minimum landscaped setback of 10 feet shall be maintained from the fronting right-of-way line. Cluster additional trees and shrubs to screen the drive-through service lane from the sidewalk and street right-of-way.
- iv. The ten-foot setback line shall be treated as a positive edge with one or more of the following:

- Low wall
- Hedge
- Trellis structure
- Building



Example of a gabled roof treatment added to a re-purposed building of similar style to existing lodge building.

- v. Front-setback edge treatments shall be at least three feet in height to screen the view of automobiles in the parking lot and interior access lanes except where the same would conflict with standards for clear view of intersecting streets pursuant to 10.735.

b. East Barnett Road Frontage (approximately 310 feet)

- i. The west 250 feet of East Barnett Road shall utilize the Major Arterial cross-section pursuant to 10.428(1) except that minimum 12-foot-wide sidewalk shall be provided and tree-wells may be used in lieu of planter strips.
- ii. East of the major arterial segment, the Minor Arterial cross-section at 10.428(2) shall be utilized except that a minimum 12-foot-wide setback and tree wells shall be used in lieu of planter strips.



A building corner treatment with inviting street presence.

- iii. A minimum setback of 10 feet shall be maintained from the fronting right-of-way line.
 - iv. Pedestrian lighting shall be provided in accordance with 10.380, 'Street Lighting Standards, S-E.'
 - v. Define the edge between the sidewalk and parking area along this frontage with a three- to four-foot stucco wall, wrought iron fence, or box hedges.
- c. Driveway entries to sector shall include the following elements:
- i. Pedestrian crossings shall be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks or stamped concrete. Stamped asphalt with or without stain is not permitted.
 - ii. Flowering plants shall be provided in landscape planters at entries.
- d. Clear and direct walkways shall be provided between fronting streets and the primary building entries.
- e. Improve parking areas that pre-exist the Master Plan adoption and new parking areas with interior and frontage landscaping.
- i. Provide landscape islands in conformance with normal standards to visually break up long parking aisles and to provide separation for the drive through service lane.
 - ii. Provide edge treatments to define the south property line and cross-access location.

2. Building Design

- a. Exterior treatments to the existing building shall be provided at the time of redevelopment for commercial use to screen roof-mounted equipment and to add interest to side facades in a manner consistent with the Common Design Standards.
- b. Limit height of structures within 150 feet of North Phoenix Road to 35 feet.
- c. New buildings shall be located as shown on the Master Plan Map and designed in accordance with 10.377, 'Special Design Standards for the Southeast Village Center.'

Chapter 4. North Phoenix and Barnett North

Northwest Entry to the Commercial Center

The sector includes approximately 3.3 net acres of vacant land fronting on North Phoenix Road to the west and East Barnett Road to the south. At the time of Master Plan adoption, this sector along with the East Barnett Transit Station and the Market–Grocery Center sectors were part of a single eight-acre parcel.

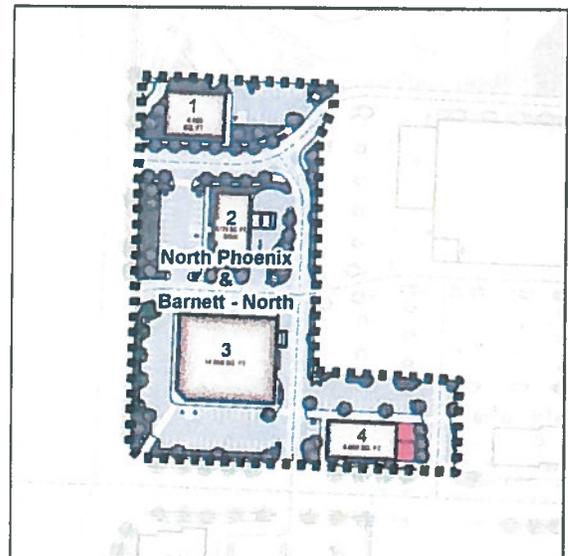
Natural grade is gently sloped rising to the east and draining northerly to North Larson Creek. The creek, which flows east to west, is the major feature to the north in the adjacent Michael Park/Greenway Sector of the Master Plan. The Medford Canal, owned by the Medford Irrigation District, crosses the creek from the north and flows through a culvert under North Phoenix Road at the northwest corner of this plan sector.

A residential subdivision of 37 single-family homes is located to the west of North Phoenix Road, opposite the northern half of this sector. A closed-loop street system—Michael Park Drive—provides the access to the neighborhood. The homes are oriented to the internal street system. Four lots are located along North Phoenix Road with fencing and vegetative buffering as side or rear yard boundaries. South of the subdivision is a professional office park.

Development of this sector will establish the needed infrastructure connections that will serve the remaining Commercial Center Core Area to the east. Sanitary sewer tie-in to the regional system is located near the northwest corner of the sector, and will be extended along the new street and access ways. Water mains will similarly be



North Phoenix and Barnett North sector



Master Plan Layout

extended and looped from North Phoenix Road to East Barnett Road, where the sector will extend to an entry drive across from the existing fire station.

East-west connections through the Commercial Center Core area initiate in this sector. These include the major arterial intersection at the southwest corner of the sector and the extension of Michael Park Drive from its intersection at the northwest corner of this sector. Michael Park Drive will be collinear to the North Larson Creek Greenway. A private commercial access with right-in-only movement is shown on the Master Plan Layout which will lead through the Grocer's Market Sector and rise to the east plaza in the Stanford Avenue Sector. The transportation impact analysis (TIA) to be required at the time of zone change shall determine whether the Michael Park Drive connection to North Phoenix Road can be a full-movement intersection or a restricted-movement intersection, and whether a drop-lane will be required. The commercial right-in-only access must also be supported by the TIA or otherwise to be removed from the final development plans.

Sector-Specific Standards

The following design and development standards will apply in this sector:

1. Site Design

- a. North Phoenix Road Frontage (approximately 500 feet)
 - i. Utilize the standard major arterial cross section (10.428) to buffer pedestrians from higher-velocity traffic and to provide a consistent street corridor treatment as the sector located south the Barnett intersection. This will provide a six-foot-wide bike lane, a 10-foot-wide planter strip, and five-foot-wide sidewalk.
 - ii. Maintain landscaped setbacks of at least 10 feet from the fronting right-of-way line. Cluster additional trees and shrubs near the north sector boundary to achieve a park-like transition to the Greenway area.
 - iii. Provide well-defined project and building entries.
 - Provide monument signage, street furniture, and accent lighting with generous landscaping at commercial entries and the public street intersections.
 - The Michael Park Drive entry shall include wayfinding elements to the Greenway trailhead area. Greenway trail markers and/or iconic elements shall extend along Michael Park Drive leading to key greenway interface areas.

- iv. The interior edge of the front setback (i.e., away from the street edge) will be treated as a positive edge with one or more of the following:
 - Low wall
 - Hedge
 - Trellis structure
 - Buildings
 - v. Front-setback edge treatments shall be at least three feet in height to screen the view of automobiles in the parking lot and interior access lanes.
 - vi. Final site designs shall be coordinated with and approved by the Medford Irrigation District with regard to need for access restrictions, setbacks, safety railings/fencing, modified crossing needs, and drainage/erosion/pollutant controls within and adjacent to the irrigation easement area.
- b. East Barnett Road Frontage (approximately 400 feet)
- i. The west 250 feet of East Barnett Road shall utilize the Major Arterial cross-section pursuant to 10.428(1) except that minimum 12-foot-wide sidewalk shall be provided and tree-wells may be used in lieu of planter strips.
 - ii. East of the major arterial segment, the Minor Arterial cross-section at 10.428(2) shall be utilized except that a minimum 12-foot-wide sidewalk and tree wells shall be used in lieu of planter strips.
 - iii. There is no minimum setback from the right-of-way line subject to provision of a public utility easement through the interior parking and circulation area and the clear-vision triangle standards or 10.735 are met. The maximum setback shall be 15 feet.
 - iv. Pedestrian lighting shall be provided in accordance with 10.380, 'Street Lighting Standards, S-E.'
 - v. Define the edge between the sidewalk and parking area along this frontage with a three- to four-foot stucco wall, wrought iron fence, or box hedges.
 - vi. Street furniture shall be provided along the minor arterial segments. At a minimum, benches and a bicycle rack shall be placed at intervals no greater than 200 feet. Street furniture is to be located so as to maintain a clear pedestrian path and shall be placed within six feet of the curb or, alternatively, adjacent to the building or property line. Benches and other street furniture shall be made of metal painted or powder coated with a black or dark green matte finish and/or wood with a natural finish. Durable materials such as stone or concrete may be used for bench planters, urns, and other furnishings.

- vii. A pedestrian crossing to the south sector across East Barnett Road is indicated on the Master Plan in this sector at the minor arterial transition. Final design and location shall be considered at the time of zone change in coordination with the Public Works Department. The crossing is to be incorporated with the final development plan for this sector.
 - viii. Define the outdoor plaza on the west side of Building 4 with edge elements such as bollards, low walls, hedges, or trellises.
- c. Driveway entries to the sector shall include the following elements:
 - i. Pedestrian crossings shall be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks or stamped concrete inlays. Stamped asphalt with or without stain is not permitted.
 - ii. Flowering plants shall be provided in landscape planters at entries.
 - d. Clear and direct walkways shall be provided between fronting streets and the primary building entries.

2. Building Design

- a. Limit height of structures within 150 feet of North Phoenix Road to 35 feet and shall maintain the standard front setbacks applicable to the C-C zoning district.
- b. A cross-access easement shall be provided over the Building 1 parking area to provide for access to the greenway trailhead parking area. The developer of Building 1 will coordinate with the City to construct both parking areas concurrently if the City agrees to provide funding for its facility. If the City elects not to have the greenway parking constructed at the time of Building 1 site development, the Building 1 parking area shall be designed and constructed to accommodate and reserve a future connection point.
- c. Final design for drive-thru facilities shall comply with the Common Design Standards.
- d. Service areas shall be screened or architecturally integrated through use of similar materials and forms utilized for the adjacent building.
- e. Landscaping shall be integrated into building and site design including edge and parking area treatments.

Chapter 5. East Barnett Transit Station

Transit Plaza and Retail Shops

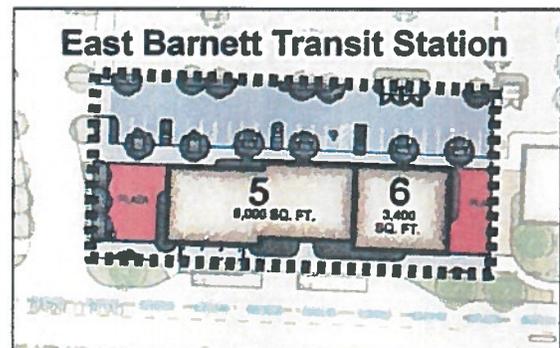
The sector is 0.70 net acres fronted by East Barnett Road to the south. It will function as a public transit center with retail shops. The East Barnett Transit Station will provide a central transit hub to serve the residents, employees, and the public when service becomes available to the area.

Regional transit service is provided by the Rogue Valley Transportation District. As of 2011, the nearest service line terminates approximately one mile to the west at Black Oak Drive adjacent to the Rogue Valley Medical Center. Extension of service to North Phoenix Road is a priority listed in the district's adopted long-range plan as funding becomes available. Conditions needed to sustain an extended route include increases in population, residential densities, and the tax base. Those conditions will be advanced by installation of infrastructure in tandem with the Commercial Center Core Area development.

Bus bays and transit platforms will be provided along with frontage improvements for this sector at time of construction even if transit service has not yet been extended to the area. The bays may be used for off-street parking and loading until such service is available. Two plazas located adjacent to the transit stop will provide space for public art, planters, kiosks, and dining alongside two attached retail buildings. These will also function in tandem with the building design standards to invite pedestrians and the transit public to explore, shop and dine.



East Barnett Transit Station Sector



Master Plan Layout

Sector-Specific Standards

The following design and development standards will apply in this sector:

1. Site Design

a. East Barnett Road Frontage (approximately 260 feet)

- i. The transit station shall include bus pull-outs and shelters. Design of shelters and other details is to be coordinated with the transit service provider.
- ii. A transit area plaza shall be provided between the buildings in this sector and the Stanford Avenue sector. Buildings abutting the plaza shall incorporate entries oriented to the plaza. Plaza furnishings shall include planters, trash receptacles, accent pavers, lighting, and an information kiosk for transit service, special events, and the Commercial Center Core Area. The final design will be expected to promote a high level of pedestrian interest and activity. Outdoor dining and food vendor stands are to be strongly encouraged and accommodated by design. Plaza design shall also function as a passageway between buildings with wayfinding elements to lead people to other areas of interest nearby.
- iii. Frontage treatment shall utilize the Minor Arterial cross-section at 10.428(2) with minimum 12-foot-wide sidewalk and tree wells in lieu of planter strips. Bus bays shall be provided consistent with the Sector plan. These may be used as on-street parking or service delivery/loading until needed for transit provider use.
- iv. Structures shall be built to the right-of-way line except to accommodate public utility easements. If an alternative easement location is not available, the structures shall be built to the interior boundary of the easement.



RVTD bus at Front Street Transit Station.



Graphic design example of a shelter that would not obstruct view of storefronts.

- v. Tree grates with tree lighting fixtures shall be provided every 48 feet on center.
 - vi. Pedestrian lighting shall be provided in accordance with 10.380, 'Street Lighting Standards, S-E.'
 - vii. Street furniture shall be provided along the public street. At a minimum, benches and a bicycle rack shall be placed at intervals no greater than 200 feet. Street furniture is to be located so as to maintain a clear pedestrian path and shall be placed within six feet of the curb or, alternatively, adjacent to the building or property line. Benches and other street furniture shall be made of metal painted or powder coated with a black or dark green matte finish and/or wood with a natural finish. Durable materials such as stone or concrete may be used for bench planters, urns, and other furnishings.
- b. The north side of the buildings shall include sidewalks with tree planters at the curb, pedestrian lighting (may be building mounted), and outdoor furnishings similar to the public street frontage.

2. Building Design

- a. To promote a strong storefront presence, the north and south facades of the buildings in this sector shall include minimum 20-foot-high front facades with zero setback from adjacent sidewalks, except at paved vestibule entries or facade off-sets of up to six feet in depth. Public utility easements shall be provided to the rear of the buildings through the common areas, except where a utility provider indicates that an easement along the street frontage is required by the City for a utility provider. In that situation, the building front shall be set back no further than the width of the easement and the intervening area shall include non-structural landscaping or additional sidewalk area.
- b. Ground-floor windows shall be provided in accordance with 10.377(4).
- c. Display window lighting to enhance night-time vibrancy is encouraged. Methods may include shielded or recessed spotlights to highlight display merchandise or pin lights to define window outlines.
- d. Use wall materials primarily composed of stone, brick or stucco for facades. Avoid non-traditional materials such as wood, shingles, cultured stone, small tiles, rough finished materials, and metal.
- e. Primary entrances shall be provided consistent with Section 10.377.

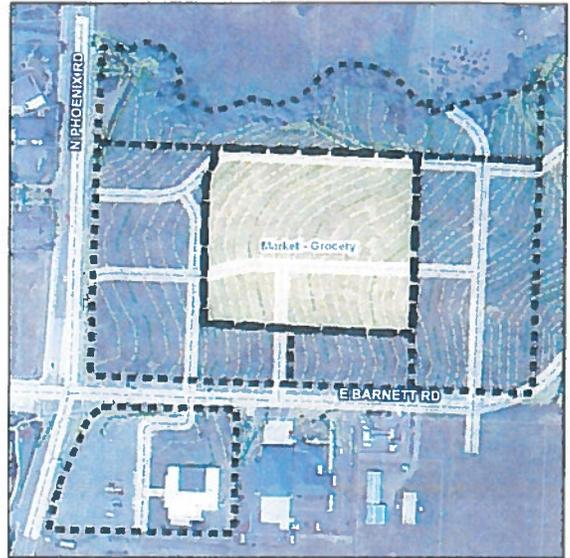
Chapter 6. Market Grocery Center

Anchor Store

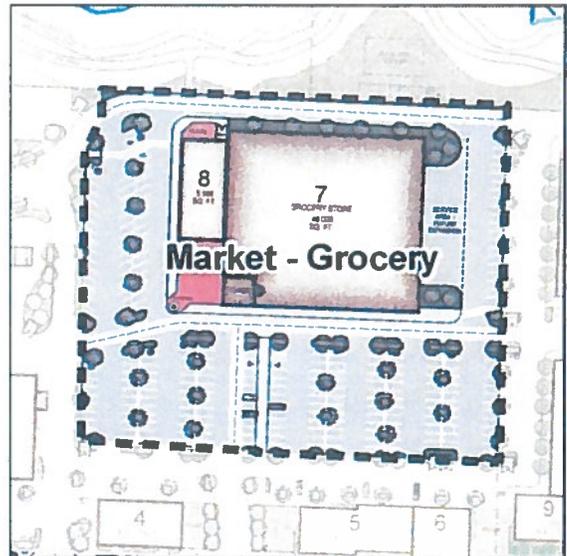
The sector includes approximately 3.9 net acres of vacant land. This sector is designed to accommodate a grocery store of up to 50,000 square feet. An additional 5,500 square feet will be provided for retail, service, or restaurant uses in an end-cap building on the west side of the grocery store.

The central off-street parking area for the Commercial Center Core Area is located between the grocery store and the retail shops fronting the surrounding public streets. The parking area is less than 200 feet deep from the front of the grocery store to the retail shops that will line East Barnett Road. The design facilitates convenient pedestrian access to all retail street frontages within the Commercial Center Core Area in a manner that also functions well for grocery store use.

The layout presented in the Master Plan reflects site requirements commonly sought by community retail grocers. Retailers grade sites for store locations by considering elements such as visibility, access, population and household income within the sales area (i.e., the “marketshed”), and both automobile and pedestrian traffic. The Master Plan provides for an anchor store site that will be visible from the surrounding streets without hiding the smaller retail shops along the street frontages. The site is well designed to function both in the near term to serve the prevailing existing households as well as the planned transit-oriented community of the future when high-density residential districts are built out.



Market Grocery Center



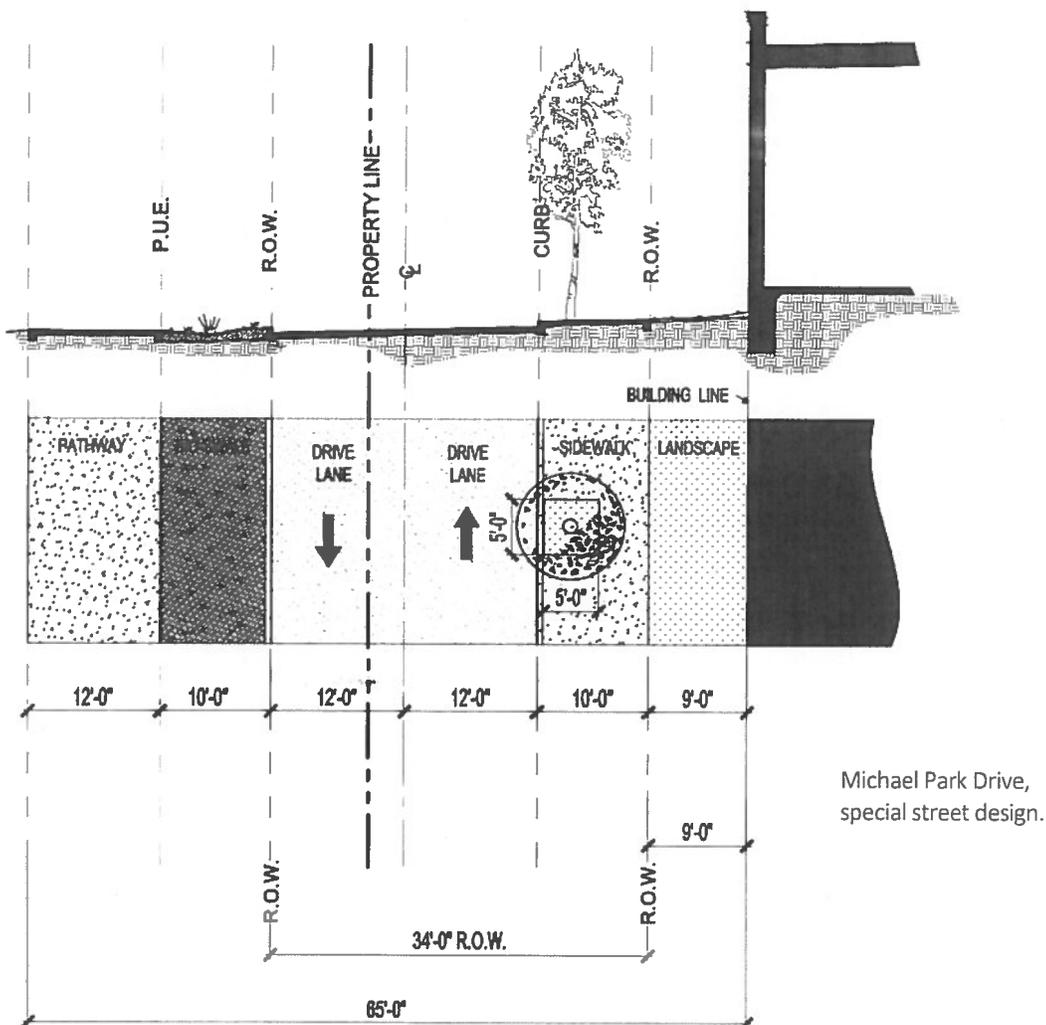
Master Plan Layout

Sector-Specific Standards

The following design and development standards will apply in this sector:

1. Site Design

- a. Michael Park Drive Frontage (approximately 430 feet)
 - i. The proposed alignment of Michael Park Drive coincides with the property line to south the North Larson Creek Greenway. A special street design for Michael Park Drive will provide greenway frontage improvements along the north side to include a bio-swale (variable width as needed for stormwater treatment) and a 12-foot-wide shared-use path on the north side of the street which will function



- as the greenway trail. Frontage improvements along the south side of the street will include a ten-foot-wide sidewalk with street tree wells and a nine-foot-wide landscape planter as a transitional treatment from the greenway to the commercial shopping area. Bump-out parking refuges may be provided as an option along the north side of the street as an option for the final street design.
- ii. Pedestrian lighting shall be provided along the sidewalks and greenway trail.
 - iii. An outdoor seating area oriented to the greenway area shall be provided along the north side of Building 8.
- b. Pedestrian walkways shall conform to the standards of Sections 10.773, 'Pedestrian Walkway Connections and Routing,' and 10.775, 'Pedestrian Walkway Design Standards.'
 - c. Sidewalks, street furnishings, pedestrian light fixtures and tree wells shall be included along the interior access drives fronting the buildings.
 - d. Pedestrian walkways crossing driving surfaces shall be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks or stamped concrete.
 - e. Bicycle parking shall be provided adjacent to the plaza in front of Buildings 7 and 8. An additional bicycle rack will be provided adjacent to the southeast corner of the building along the front walkway.
 - f. The service and loading area to the east of Building 7 shall be designed in conformance with Section 10.742 with screening elements to minimize visual impacts from general view.

2. Building Design

- a. Final design of the building complex in this sector will be consistent with the 'Special Development Standards for Large Retail Structures' at Section 10.725.
- b. The principal entry for the anchor store shall be located adjacent to the smaller retail building and shall include a plaza area and prominent entry feature such as a tower.
- c. Shopping cart storage shall be incorporated into the building design to screen stored carts and in a manner that avoids the plaza area.



Conceptual renderings of Buildings 7 and 8 provided by Oregon Architecture, Inc. depicts facade roof treatments with a variety of animating features, windows, repeating elements, materials, and colors with prominent entries. The renderings also illustrate the 45-foot-wide Michael Park Drive design alternative where the street is located entirely within the boundaries of this sector.

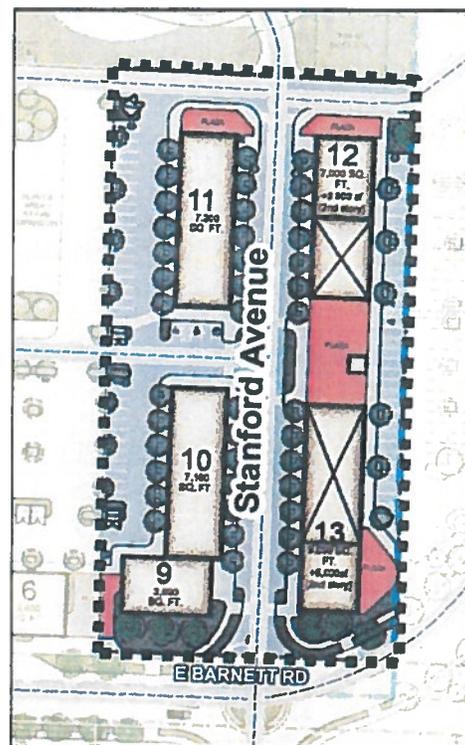
Chapter 7. Stanford Avenue

A Main Street Style Retail District

The Stanford Avenue Sector is a retail block of shops of approximately 2.9 acres in the style of a traditional “Main Street” shopping district. This sector will provide highly attractive and pedestrian-friendly community space for the surrounding residential areas in the Southeast Plan Area. Entry from the north is across and along the North Larson Creek Greenway. Entry from the south will be through a rotary intersection (roundabout) at East Barnett Road.



Stanford Avenue Sector



Master Plan Layout

A plaza located mid-block on the east side of Stanford Avenue sits at the high point of the central east-west axis through the Commercial Center Core Area. The plaza will have commanding views over the Commercial Center Core Area and the valley beyond. Multi-story buildings will flank the north and south side of the plaza, and a clock tower sited within the plaza will visually link the East Plaza with the western sectors. The plaza will also function as a link to the larger Commercial Center Area 7B to the east.

Shared off-street parking areas will be located to the rear of the retail shop buildings in the adjacent Grocery-Market Sector to the west and Commercial Center Subarea 7B to the east. All façades will be designed with storefront treatments but with

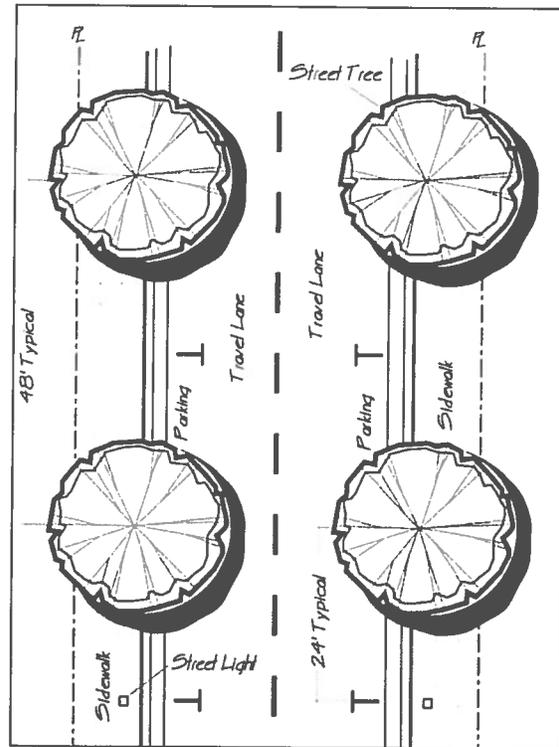
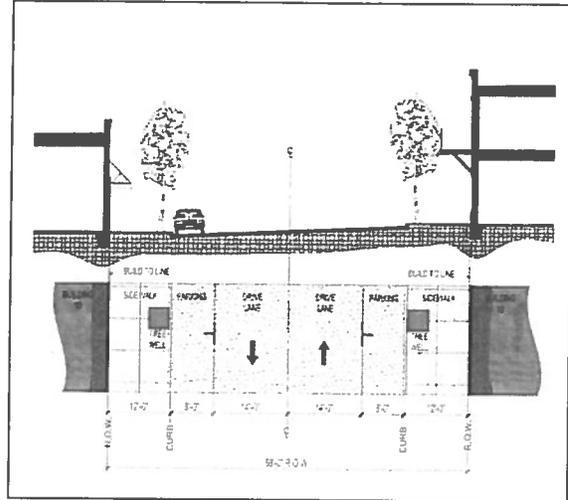
special focus to enlivening the Stanford Avenue streetscape with a high degree of pedestrian activity.

Sector-Specific Standards

The following design and development standards will apply in this sector:

1. Site Design

- a. Stanford Avenue frontage (approximately 475 feet):
 - i. The streetscape for this sector is based on buildings “built-to” the right-of-way line along minimum 12-foot-wide sidewalks.
 - ii. At least one storefront per building shall be provided a minimum 14-foot-wide sidewalk to accommodate an eight-foot-wide area for outdoor dining plus a six-foot-wide pedestrian corridor.
 - iii. Minimum 12-foot-wide sidewalk shall otherwise be provided between street curb and retail buildings.
 - iv. Structures shall be built to the right-of-way line except to accommodate recessed vestibule entryways and architectural offsets (see, Building Design standards below) or to accommodate public utility easements if alternative easement location is not available, in which case the structures shall be built to the interior boundary of the easement.
 - v. Pedestrian streetlight fixtures shall be located within three feet of the curb and at intervals of no greater than 80 feet.



The Stanford Avenue cross-section with zero-setback streetscape.

- vi. At a minimum, street furniture shall include benches and bicycle racks placed at intervals no greater than 200 feet. Street furniture shall be located so as to maintain a clear pedestrian path and shall be placed within 6 feet of the curb. Benches and other street furniture shall be made of metal painted or powder coated with a black or dark green matte finish and/or wood with a natural finish. Durable materials such as stone or concrete may be used for bench planters, urns, and other furnishings.
 - vii. Sidewalk cafés may be operated in the public right-of-way area consistent with Section 10.358(c), 'Sidewalk Cafés in Commercial and Industrial Zones.'
- b. The rear of the buildings shall include sidewalk with tree wells at the curb, pedestrian lighting (may be building mounted), and storefront façade treatments.
 - c. The East Plaza
 - i. The plaza shall include a prominent tower feature.
 - ii. The plaza shall connect directly to the adjacent sidewalks, walkways, and building entrances.
 - iii. It will be paved with concrete, pavers, bricks, or similar materials, and include seating, pedestrian-scale lighting, and similar pedestrian improvements.
 - iv. Amenities shall include a covered community information board or kiosk, art works, public restrooms, and space for small or temporary vendors.
 - v. Low walls or planters and landscaping shall be provided to create a semi-enclosed space and to buffer and separate the plaza from any adjoining parking lots and vehicle maneuvering areas.
 - vi. At least 20 percent of the plaza area shall be landscaped with live plantings subject to Section 10.780, 'Landscape and Irrigation Requirements,' and 50 percent of the seating area is to be shaded with trees, canopies, or structural elements.
 - d. East Barnett Intersection and corners
 - i. The Master Plan depicts a roundabout intersection with East Barnett Road as the preferred intersection type to facilitate the extension of mass transit service to the area which would not otherwise occur until the public street grid for the neighborhood is further developed. The design will also function as an important civic feature to foster a clear sense of place. A roundabout design will be considered warranted as a matter of policy in the Southeast Neighborhood Plan by the City of Medford in order to safely circulate mass transit buses at this location. The roundabout may be designed for later conver-

sion to a standard intersection when the local street grid is connected well enough to circulate a bus route.

- ii. The final design and location shall be coordinated with the adjacent property owners. Minor adjustments to the location and alignment may be required to accommodate full circle construction at the time the Stanford Avenue intersection is constructed.
- iii. Final design for the center element shall avoid placement of any permanent structures over the water main or other in-ground utilities.
- iv. Both corner buildings sites will include plaza areas as extended public space from the intersection streetscape.
 - The East Barnett Transit Station Plaza shall be extended through the Building 9 site on the westerly corner.
 - A small plaza shall be provided at the easterly corner at Building 13.

e. Michael Park Drive

- i. A small plaza shall be provided along Michael Park Drive to the north of Building 11 to provide for outdoor seating/dining oriented to the greenway corridor.

2. Building Design

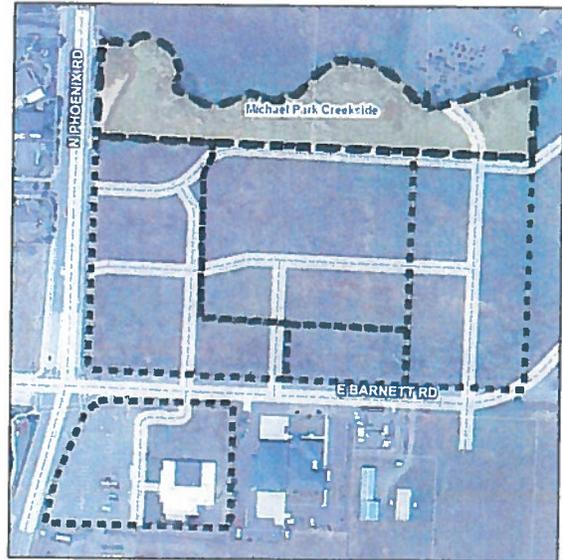
- a. To promote a strong storefront presence, the buildings in this sector shall include minimum 20-foot-high front façades with zero setback from adjacent sidewalks, except at paved vestibule entries or façade off-sets of up to six feet in depth. Public utility easements shall be provided to the rear of the buildings through the common areas except where a utility provider indicates that an easement along the street frontage is required by the City for a utility provider. In that situation, the building front shall be set back no further than the width of the easement and the intervening area shall include non-structural landscaping or additional sidewalk area.
- b. Ground-floor windows shall be provided in accordance with 10.377(4). Display window lighting to enhance night-time vibrancy is encouraged. Methods may include shielded or recessed spotlights to highlight display merchandise or pin lights to define window outlines.
- c. Use wall materials primarily composed of stone, brick or stucco for façades. Avoid non-traditional materials such as wood, shingles, cultured stone, small tiles, rough finished materials, and metal.
- d. Primary entrances shall be provided consistent with Section 10.377.
- e. Buildings fronting the east side of Stanford Avenue shall have at least two stories or otherwise have the appearance of having two or more stories.

- i. Second-level floors may be used for commercial or residential use. Additional levels may be added for residential units subject to provision of dedicated residential parking facilities consistent with the standards of Section 10.376.
 - ii. Upper levels shall include design elements such as bay windows, projecting balconies with French doors, or awnings over the windows.
 - iii. Window proportions on the upper levels shall generally be smaller than ground floor windows, vertical in proportion, and related to ground floor windows.
- f. Weather protection shall be provided along sidewalk areas through use of awnings or arcades.

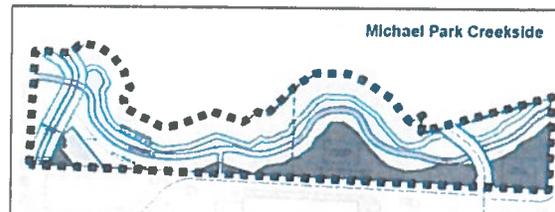
Chapter 8. Michael Park Creekside

Village Center Greenway

This sector includes approximately 3.7 acres of vacant land oriented along the North Fork of Larson Creek. A major greenway overlay through this area has been designated over the creek corridor as shown on the Southeast Circulation Plan Map. As described therein, the greenway area includes the streambed and area upland within 50 feet of either stream bank. Its north boundary also corresponds to the northern extent of the Commercial Center Core Area (7A). The creek and greenway meander over the south portion of two parcels of land that are otherwise designated in Area 7B (Commercial Center) as they extend north of the greenway boundary to Shamrock Drive. There are small areas on these parcels located to the south of the greenway boundary within Area 7A.



Michael Park Creekside Sector



Master Plan Layout

The Southeast Overlay District standards for greenway development were originally crafted in anticipation that greenways would be developed by property owners at the time “adjacent” land is developed. However, the majority of developable commercial land in Area 7A was in different ownership than the land on which the greenway is located. The Master Plan layout was therefore prepared in anticipation that the City would acquire the designated greenway corridor and additional land to accommodate public access and parking at the trailhead. Remnant land south and outside of the greenway boundary can accommodate two building envelopes of approximately 2,500 square feet each. “Creekside” commercial use and development standards are established for the sector to assure that greenway and commercial uses complement one another. Michael Park is predominantly located within the adjacent Market–Grocery Sector to the south to assure its timely construction in the event that public acquisition of the greenway area is forestalled and/or the privately held land in the MP-Creekside Sector is similarly delayed. The cross-section proposed for Michael Park Drive at this

location is intended to function with the adjacent public and commercial spaces to accommodate special community events such as street fairs, celebrations, contests, and the like. The layout also will accommodate public use of enclaved areas rather than commercial building sites as discussed in the alternative below.

The Master Plan encourages the City to acquire adequate upland area to accommodate development of a regional stormwater detention and water quality facility as described in Chapter 9, "Capital Improvement Program" of the adopted *City of Medford Stormwater Management Plan*. As described in the plan, the water quality facility "would simply be a buffered low-flow channel and the detention would be dedicated upland area where flows from the one-year storm would back up. The slope allows the backing, and the upland area could serve as open space. This facility would work hand in hand with site-specific WQ control measures and low-impact development designs." The described facility could be intergrated with the trail, recreational, and open space components of the greenway in an attractive manner. If the City acquires enough upland area, actual construction could be funded by nearby developers in lieu of constructing on-site facilities or in exchange for storm drainage SDC credits. Such projects constructed on public land would include a required recreational or trail amenity to be constructed or funded. This would leave more private land available for taxable improvements to the benefit of the general fund.

Other funding opportunities could be made available to individuals and organizations in the form of sponsorships, memorials, or licensing, which could fund tree plantings, artwork, and gateway gardens throughout the greenway while fostering grassroots community involvement and sense of ownership. Although this greenway segment is just under 1,000 feet in length, the model could be exported beyond the Commercial Center Core Area for the entire course of the greenway, ultimately to Chrissy Park two miles to the east.

Sector-Specific Standards

The following design and development standards will apply in this sector:

1. Site Design

a. Trailhead Parking

- i. The trailhead parking area is to be a major access point to the greenway.
- ii. The City shall coordinate with the developer of Building 1 in the adjacent sector to the south to construct the trailhead parking concurrently with the parking area for the Building 1 site, in accordance with the related provisions for the adjacent sector. A development

- agreement or similar instrument will be negotiated to establish the terms for cost sharing.
- iii. Special consideration shall be given to the interface with the Medford Canal in coordination with the irrigation district with regard to need for access restrictions, setbacks, safety railings/fencing, modified crossing needs, and drainage/erosion/pollutant controls. Crossings or other improvements within the irrigation district easement will require review and approval by the irrigation district.
 - iv. There shall be considerable flexibility provided in the final parking area design plan to allow adjustment based on site conditions that will minimize intrusion impacts to nearby and adjacent natural features and to provide the most value for the public investment. For example, the parking area may be reduced in size or eliminated in favor of more on-street parking. Parallel, angled, or perpendicular parking bays alongside Michael Park Drive where area is available outside the greenway boundary. Parking bays along the street may also be used for event booth or public seating areas for special events such as street fairs, parades, or races.
- b. Michael Park Drive frontage
- i. An open edge treatment in lieu of a standard street tree and sidewalk design shall be promoted for this frontage along this sector to promote the open space connectivity with the street space. A multi-use path and water quality swale or rain garden plantings shall be provided along the north street frontage. Parking bays, curbing, and sidewalk shall be incorporated in the final design where adequate space exists outside the greenway boundary.
 - ii. Tree and other plantings in this sector shall be targeted to improvement and enhancement of existing groves along the creek, water quality improvements, and recreational open space considerations in accordance with a greenway landscape plan as adopted by the City for the public areas. Areas acquired for public use will be designed by the City for park and greenway use, stormwater detention, and water quality enhancement projects. The design plan shall be consistent with the standards established at 10.384(D) and based on public input in accordance with neighborhood park and open space planning objectives.
 - iii. Building sites A and B outside the greenway boundary as shown on the Master Plan if developed are subject to the creekside development standards of 10.384(D). The sites may instead be utilized for park, open space, and storm-water management facilities to include buildings accessory to the same.
 - iv. Final site design shall accommodate the stream-crossing alignment for Stanford Avenue in a manner consistent with the Southeast Circulation Plan Map.



The figure above depicts a street with bioswale along a downgrade edge. The photo shows a built example of a street with a bioswale edge.

Chapter 9. Signage Standards

The following signage standards are applicable within the Master Plan Area:

1. Prohibited Signage Types

- a. Lighted signs that flash on and off, fluctuate or appear to move. Moving signs that rotate or move in any fashion, except barber poles
- b. Attraction Boards and Movable Letter Signs (except for theaters, performing arts facilities, or similar uses that have frequently changing events or showings)
- c. A-frame Signs
- d. Off-premises Signs
- e. Electronic Signs
- f. Projected Light Signs which are flashed or projected onto walls or other structures by means of a projector or other device
- g. Roof-mounted Signs
- h. Billboards
- i. Cloth, paper, or fabric signs hung from the building or placed in windows except for Temporary Signs otherwise allowed under the sign regulations
- j. Any signs specifically prohibited under the sign ordinances where not otherwise expressly provided for below.

2. Building Signs

- a. Building signs are panels or individual letters mounted flat against and parallel to a building wall or roof fascia.
- b. Place building signs within a sign band area that is relatively flat and does not contain doors, windows, or projecting molding or trim.
- c. Limit one building sign per store front.
- d. Building signage shall not exceed 15 percent of the building façade.
- e. Use either individually applied letters to the face of the wall, or apply sign letters to a board or panel mounted on the wall face.
- f. Sign copy and graphics applied to a board or panel may consist of any of the following:
 - i. Individual letters and graphics of wood, metal, or similar materials
 - ii. Individual letters and graphics carved into the surface of a wood panel and engraved or cut into the surface of a metal panel
 - iii. Letters and graphics painted directly onto the surface of the panel
- g. Do not paint signs directly onto wall surfaces.
- h. Conceal all sign and sign lighting raceway and other connections
- i. Provide sign illumination appropriate to the streetscape

- j. Interior illuminated “can” signs which include multiple letters within a single sign enclosure shall not be allowed for any wall sign.
- k. Interior illuminated individual letters may be used except along Stanford Avenue where shielded exterior illumination shall be used for building signs.

3. Awning Signs

- a. Awning signs consist of letters and graphics applied directly to the face or valance of awnings. Awning signs are often used effectively in combination with window signs.
- b. Place awning signs for easy visibility from the street level.
- c. Apply signs to awning front valances (i.e., the flat vertical surface of awnings) or to sloped awning faces with a slope of at least 2 to 1.
- d. Limit awning signs to the business name, business logo, services or type of business, and/or the business address number.
- e. Limit the size of logos or text placed on awnings to a maximum of 15 percent of the valance surface areas.
- f. Limit sign width on awning valances to a maximum of 85 percent of the awning width.
- g. Limit the letter height to a maximum of 80 percent of the valance height.
- h. Backlit awnings are not allowed.
- i. Signage on an awning’s sloped face may be illuminated by shielded and attractive directional lights.

4. Window Signs

- a. Window signs are primarily oriented to passing pedestrians, and are generally applied to the inside of display windows.
- b. Limit the amount of signage area (including graphic logos and images) to a maximum of 25 percent within any individual window
- c. Entry doors or adjacent window may also indicate tenancy in non-illuminated lettering a maximum of six inches high placed on the glass between four and seven feet above the exterior sidewalk
- d. Limit the maximum height of lettering to 10 inches, with an exception for leading capital letters of text which may be up to 14 inches in height.
- e. Use paint or vinyl film applied directly to the inside of the window. Wood or metal panels with applied lettering may be used within a product-display window.
- f. Paper signs placed in windows are not allowed.

5. Projecting and Hanging Signs

- a. Projecting signs are relatively flat, two-sided solid panels attached to brackets which are mounted on and perpendicular to the face of buildings and storefronts. They may include graphic images in addition to text, and

express the unique personality of an individual business. Hanging signs are similar, but are smaller and suspended below awnings, bay windows, balconies, and similar projections.

- b. Use wood or metal material.
- c. Limit number to either one projecting or hanging sign per business frontage along a sidewalk or plaza area. A minimum interval of 15 feet is required between signs.
- d. Limit the size of any projecting sign panel to five square feet, and the size of any hanging sign panel to three square feet.
- e. Project and hanging signs will be located no more than 36 inches from the building face, and provide at least six inches between the inside edge of the sign and the building.
- f. Provide at least nine feet clearance from the bottom of signs to the ground, and locate the top of the projecting sign no more than 14 feet above the sidewalk.
- g. Provide sign lighting only with shielded spotlights. Utilize cylinder spots or decorative fixtures. Do not use exposed standard spot or flood light bulbs.

6. Plaque Signs

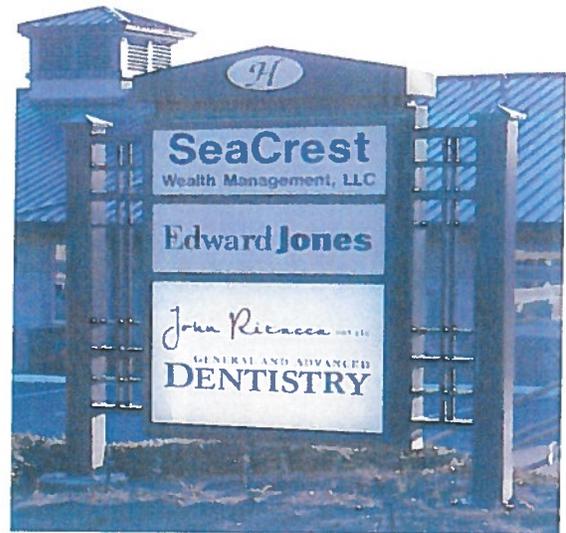
- a. Plaque signs are pedestrian-oriented flat panels mounted to wall surfaces near the entry to a business. They include signs that identify a specific business, directory signs for multiple businesses, and menu boxes for restaurants.
- b. Locate plaque signs only on wall surfaces adjacent to tenant entries or entry passageways to off-street courtyards.
- c. A plaque sign identifying a single business shall be limited to an area of four square feet.
- d. Directory plaque signs for the identification of multiple second floor or courtyard tenants may be larger, but no more than eight square feet in area.

7. Menu Signs or Boxes

- a. Use menu signs or boxes for the display of restaurant menus to promote the village as a restaurant district where customers are able to walk from one to the next to compare menus and prices.
- b. Menu signs or boxes shall not exceed six square feet in area. For establishments featuring live entertainment, a second sign or box of similar size may be provided for promotional information.
- c. Limit one menu sign or box per establishment.
- d. Menu signs or boxes shall have internal indirect lighting or direct lighting using decorative fixtures.

8. Monument Signs

- a. Monument Signs are low ground signs that identify community gateways (non-commercial) and assist motorists in finding commercial businesses along streets where businesses are separated from the street front by landscaped setbacks.
- b. Maximum size of Monument Signs along North Phoenix Road:
 - i. Area: 30 square feet per sign
 - ii. Height: 12 feet
- c. Maximum size of Monument Signs along East Barnett Road:
 - i. Area: 20 square feet per sign
 - ii. Height: 7 feet
- d. Maximum size of Monument Signs in other sectors:
 - i. Area: 16 square feet per sign
 - ii. Height: 5 feet
- e. Locate Monument Signs within landscaped yards and outside utility easements and the public rights-of-way. Maintain required turning movement sight triangles to avoid blocking vehicular or pedestrian sight lines.
- f. Multi-tenant Monument Signs are limited to three tenants. A multi-tenant sign will use a common background color throughout and provide distinct panels for each tenant.



A multi-tenant Monument Sign will be limited to three tenants with distinct panels with a uniform background color. Example shown is located at the Hillcrest Office Park, one mile north of the Master Plan area.

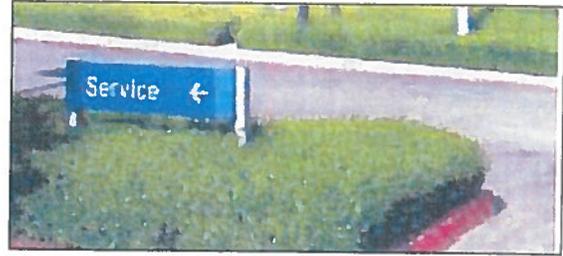


Example shows a community entry Monument sign in east Medford integrated as part of a decorative wall with durable materials and direct spotlight illumination.

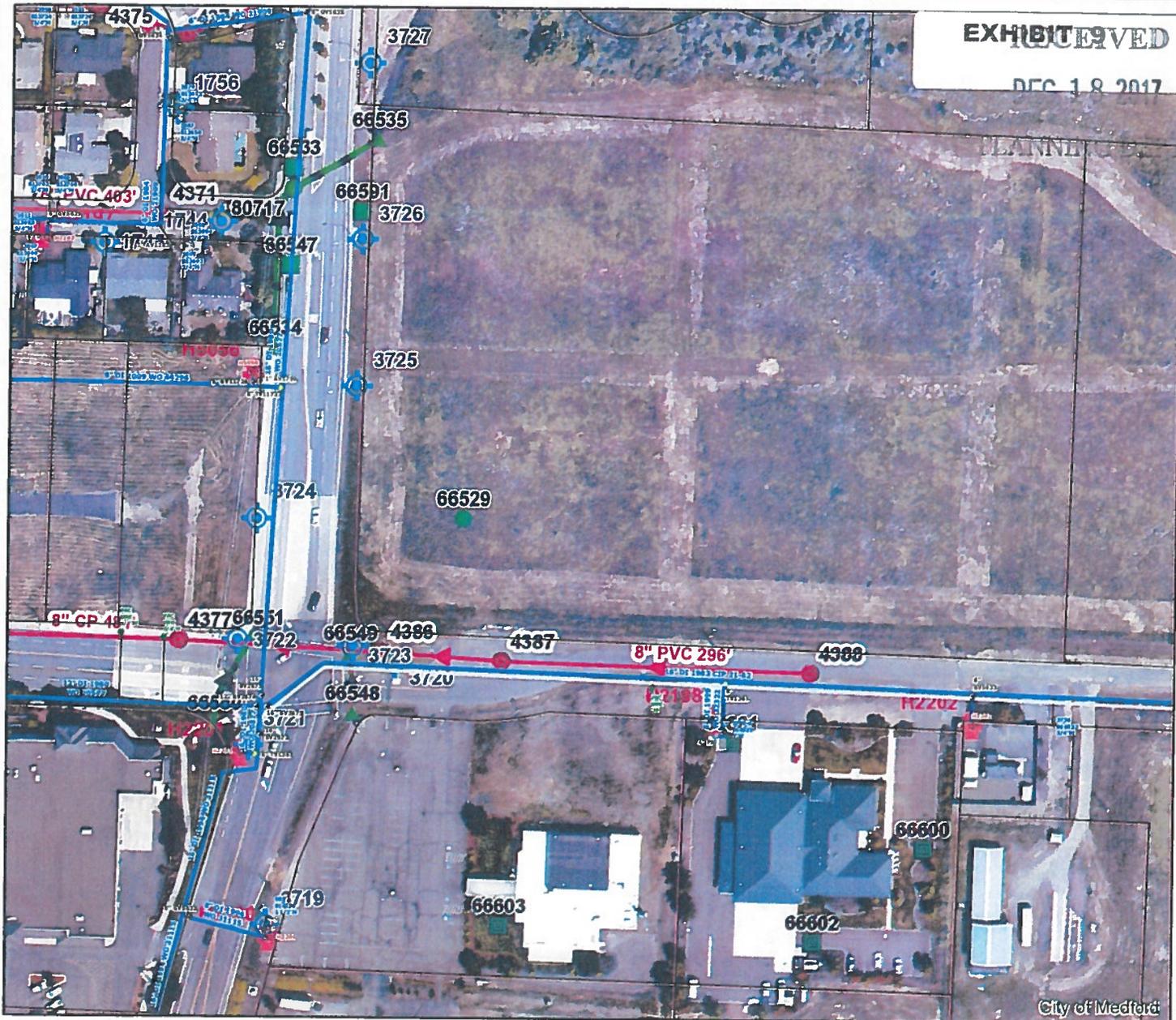
9. Free-standing signs

- a. Freestanding Signs are tall ground signs appropriate only within 150 feet of the signalized major arterial intersection of East Barnett Road and North Phoenix Road. One freestanding sign will be allowed for the sector to the south of East Barnett Road, and one for the sector to the north.

- b. Locate Freestanding Signs in landscaped setback areas so as not to project into the public right-of-way or encroach into public utility easements.
- c. Maximum size of Freestanding Signs:
 - i. Area: 150 square feet per sign
 - ii. Height: 20 feet, and not to project higher than the roof peak of the nearest building.
- d. Multi-tenant Freestanding Signs will be limited to three tenants. A multi-tenant sign will use a common background color throughout and provide distinct panels for each tenant.
- e. Metal posts and housings where used for ground signs shall be of a non-reflective black matte finish.
- f. Community entry Monument Signs will be provided with prominent entry landscape treatments at the Michael Park Drive intersection corners. The signs will be integrated into decorative wall elements. Lighting for community entry signs will be by direct spotlight illumination from fixtures mounted either at the top of the sign or on the ground below the sign. Fixtures must be shielded to avoid direct view of the bulbs.
- g. Backlit sign faces may otherwise be used where ground signage is allowed.
- h. Direction signs limited to a maximum area of three square feet each may be provided within off-street parking areas to guide internal circulation.



Small directional signs are appropriate for internal circulation in off-street parking areas.



City of Medford

Utilities

Zone Change Application North Phoenix Properties, LLC, et al



1 inch = 150 feet



ACTIVE WATER	Abandoned Manhole	Catch Basin	Detention Manhole	Unknown
LATERAL (7)	Clean Out	Curb Inlet	Drop Manhole	Abandoned
Reservoir/Drain	End Cap	Clean Out	End Cap	D25
ServiceLine	Junction	Type D Inlet	Inlet/Outlet	Irrigation Pipe
Street Light Supports	Pump Station	Irrigation Box	Abandoned	Private Pipe
Fire Hydrants	Public Gravity Pipe	Junction	Box Culvert	Storm Drain Pipe
Unknown	Siphon	Manhole	Culvert	Unknown
Public Manhole	Interceptor	Poll Cntrl Manhl	Private Culvert	RVSS Sewer Manholes
Drop Manhole	Private Pipe	WW	RVSS Sewer Lines	Tax Lots (33)
Private Manhole	Abandoned Pipe			
	Sewer Pressurized Main			

CITY OF MEDFORD

EXHIBIT # 5
File # ZC-17-168



SE COMMERCIAL CENTER CORE AREA

Traffic Impact Analysis

WATERBURY REGION

Full document is on file with the Planning Department

CITY OF MEDFORD
EXHIBIT # 6
File # ZC-17-168



Traffic Impact Analysis

SE COMMERCIAL CENTER CORE AREA

Medford, Oregon



RENEWS 6/30/17

June 21, 2017



SE COMMERCIAL CENTER CORE AREA

TRAFFIC IMPACT ANALYSIS

EXECUTIVE SUMMARY

This traffic impact analysis (TIA) demonstrates that, with recommended mitigation, development of The SE Commercial Center Core Area (“The SE Center”) will meet all adopted transportation performance standards for approval of the proposed zone change.

The development is planned for the east side of N. Phoenix Road, both north and south of E. Barnett Road. The north side property is a vacant eight-acre parcel planned as a 93,825 square-foot shopping center that includes a 50,000-square foot super market. Development will require rezoning from SFR-00 to CC, Community Commercial. CC is compatible with the comprehensive plan designation of CM, Commercial and is the only zoning permitted by the Southeast Plan Map for the subject sub-area 7A.

South of E. Barnett Road, The SE Center includes a 2.14-acre parcel occupied by a 10,348-square foot building. The building houses a Rogue Credit Union branch with a drive-through window, and general office space. The site master plan relocates the branch office to the northwest corner of on the site. The master plan also depicts a third building on the southwest corner. It may include office, retail or mixed-uses. The credit union property is currently zoned CC and does not need to be re-zoned. It is, however, subject to a 628 average-daily trip limitation (trip cap) that must be increased for the property to completely develop. This analysis of the Rogue Credit Union site demonstrates that the 628-vehicle trip cap can increase by 436 daily and 48 PM peak-hour trips to 1064 daily and 117 PM peak-hour trips. This fully accommodates the planned development of the site.

The entire site is designated as a Transit Oriented Development (TOD). As part of the project, a major transit stop is to be sited along the north side of E. Barnett Road. Project building entrances will be positioned facing adjacent streets to facilitate pedestrian, bicyclist, and transit rider access to the businesses.



Trip Generation - PM Peak Hour

Lane Use	ITE Code	Sq. Ft	Daily Rate	Daily Trips	PM Rate	PM Trips	In%	PM In	Out %	PM Out
North Side										
Shopping Center	820	93825	$\ln(T) = (0.65\ln(X) + 5.83)$	6515	$\ln(T) = 0.67 \ln(X) + 3.31$	574	48%	276	52%	299
Pass-by Trips			n/a	n/a	0.25	144		69		75
Northside Trips						431		207		224
South Side										
Specialty Retail	826	4,200	44.32	186	2.71	11	44%	5	56%	6
Pass-by Trips			n/a	n/a	0.25	3		1		1
Specialty Retail Trips						9		4		4
General Office	710	2,007	$\ln(T) = 0.76 * \ln(X) + 3.68$	28	Incremental Equation See Above	2	17%	1	83	2
Drive-in Bank	912	1,493	148.15	222	24.30	36	50%	18	50%	18
Pass-by Trips			n/a	n/a	0.25	9		5		5
New Trips from Credit Union Site						27		13		13
Southside Subtotal				436		37		19		20
Total Development Trips						468		225		243
Transit Reduction 10%						47		23		24
Analysis Vehicle Trips						421		202		219

In Equations: T = Trips X = Square feet/1000 Ln = Natural Logarithm

Table 5 from the TIA, shows that approximately 272 additional PM peak-hour trips will be generated by the development. This includes a 25-percent pass-by reduction in commercial trips and a 10-percent overall reduction to reflect the projects status as a mixed-use TOD.



The number of trips generated by the drive-in bank and general office are calculated using the difference between proposed uses compared with the current use. The existing 10,348 sq. ft. building contains 8,341 sq. ft. of office and 2,007 sq. ft. of drive-in bank. Under the proposed plan, the 2007 sq. ft. will be converted to office and a new 3,500 sq.-ft. drive-in bank will be added to the property. The net result of these changes is to increase the amount of office space by 2,007 sq. ft. and drive-in bank by 1,493 sq. ft.

For this analysis, project generated trips are distributed to the road system based in by proportional analysis based on travel distance and relative population within the market area of The SE Center. The market area was determined by a gravity model provided by CSA Planning.

Future background growth is calculated at one-percent per year for the study years of 2016, 2020 and 2023. 2015 turning-movement counts form the base for the study-year volumes. Pipeline trips (trips from approved developments, not yet developed) are added to the projected future growth to complete the 2020 and 2023 analyses.

In accordance with the scope of work provided by the City of Medford, this TIA looks at the performance of the site driveways and all intersections of arterials and /or collector streets having more than 25 PM Peak-hour vehicle-trips from the SE Center. Any intersection projected to have a Level-of-Service (LOS) greater than LOS D must be mitigated to at least LOS D before development can occur. Table 8 from the main body of the TIA is shown below. It lists the study intersections and shows their performance in the periods designated by the City.

Site driveways must also meet the Level-of-Service D standard. There is no adopted performance standard for local streets such as Michael Park Drive.

The analysis demonstrates that all intersections and driveway accesses will operate at level-of-service D or better through the 2023 planning horizon. The intersection of N. Phoenix Road with East Barnett Road will require mitigation adding permissive-protected signal operation for the east and westbound directions. The unsignalized intersection of N. Phoenix Road and Michael Park Road requires restriping to provide a two-way left turn lane both north and south of the intersection. This striping allows two-phase, left-turn exits from Michael Park Road.

Part 4 of Section 1 of this TIA shows that crash rates at all study intersections fall below the one crash per million vehicle-mile standard requiring additional attention and consideration.

The detailed studies for each intersection and scenario in Section 2 of the TIA show that all intersection approaches provide sufficient space to accommodate queuing.



Intersection Performance Analysis Summary

Intersection	Performance Standard	Existing	2020 No-Build	2023 No-Build	2020 Build	2023 Build
Foothill Rd at Hillcrest Rd	LOS D	LOS B	LOS B	LOS C	LOS B	LOS C
N. Phoenix Rd at Cherry Ln	LOS D	LOS B	LOS B	LOS B	LOS B	LOS B
N. Phoenix Rd at Michael Park	LOS D	LOS C	LOS E	LOS E	LOS D	LOS D
N. Phoenix Rd at E. Barnett Rd	LOS D	LOS B	LOS B	LOS B	LOS C	LOS C
N. Phoenix Rd at Juanipero Way	LOS D	LOS C	LOS D	LOS D	LOS D	LOS D
E. Barnett Rd at Golf View	LOS D	LOS A	LOS A	LOS A	LOS A	LOS A
E. Barnett Rd at Murphy Rd	LOS D	LOS B	LOS B	LOS B	LOS B	LOS B
E. Barnett Rd at Black Oak	LOS D	LOS B	LOS B	LOS B	LOS B	LOS B
Project Access A at Michael Park Drive	N/A	N/A	N/A	N/A	LOS A	LOS A
Project Access B at Michael Park Drive	N/A	N/A	N/A	N/A	LOS A	LOS A
Ex Project Access H/Credit Union at E. Barnett Rd	N/A	N/A	N/A	N/A	LOS C	LOS C
Project Access I/Ex. Fire Station at E. Barnett Rd	N/A	N/A	N/A	N/A	LOS C	LOS C

The analysis in this TIA includes nearly 800 pipeline trips (550 eastbound-248 westbound) on E Barnett Road adjacent to the site. These trips are projected from approved, but not constructed, projects. A majority of all land east of The SE Center, where these trips would be headed (eastbound) or coming from (westbound) is vacant and also not zoned for development. It would take adding over 400 homes and 75,000 square-feet of office space to reach the level of development evaluated in this TIA.

Detailed studies for each intersection and scenario in Section 2 of the TIA show that all intersection approaches provide sufficient space to accommodate queuing.

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MAR 13 2018

PLANNING DEPT.

**SE COMMERCIAL CENTER
CORE AREA**

**Reply to Medford Comments
on TIA, August 1 and 8,
2017**

SEPTEMBER 13, 2017

**CITY OF MEDFORD
EXHIBIT # B
File # ZC-17-168**



September 13, 2017

Karl MacNair
City Traffic Engineer
200 South Ivy Street
Medford, OR 97501

**RE: Response to City Comments on August 1 and 8 Regarding June 21, 2017
SE Commercial Center Core Area Traffic Impact Analysis (TIA)**

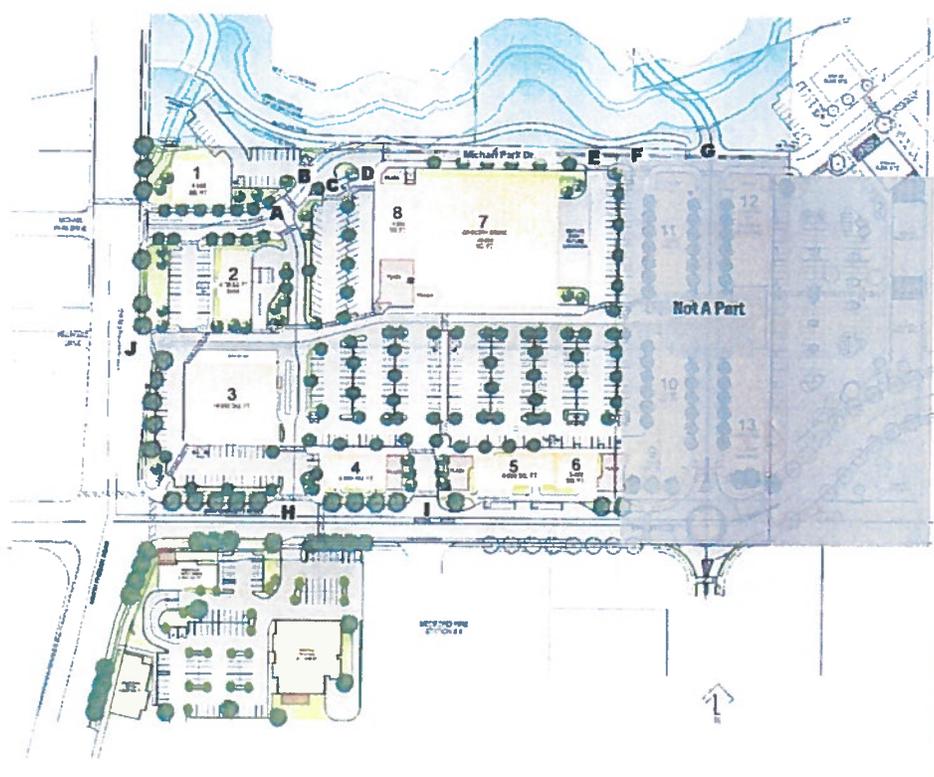
Dear Mr. MacNair

This letter responds to the comments made by the City regarding the subject TIA dated August 1 and August 8, 2017. For ease of understanding, I have copied the comment and then provided our response. Following this letter, we have attached a technical analysis for each response requiring it. As discussed with Peter Mackprang of your staff, this letter supplements the information in the TIA and republication of the TIA will not be necessary.

The August 8, 2017 letter provides additional information regarding comments one and two, it is quoted in the text of this letter. The remainder of the comments are from the August 1 letter.

Comment 1

“Comment 1; Provide operational analysis for driveway H. Drive way H is so close to the intersection of Barnett and N. Phoenix that eastbound and westbound traffic will be transitioning to make left turns from opposite directions in the same physical space. The analysis should include a safety component looking at the common space issue and a safety and efficiency component for traffic waiting in the through lanes when the space is occupied by another vehicle. We think the driveway should be moved east to alleviate this conflict.”



Site Plan Adopted in SE Commercial Center Plan

Response to Comment 1:

As outlined below, the TIA provides an extensive evaluation of Site Access H in the location adopted in the South East Commercial Center Core Area Master Plan (SE Center Plan). The SE Center is the gateway to a neo-traditional Transit Oriented Development (TOD) balancing the needs of bicyclists, pedestrians, and transit users with those of auto users. As a gateway, it announces to drivers that they have arrived in the SE Neighborhood. Emphasizing this, the SE Center Plan states: “The Master Plan is designed to transition from the existing suburban pattern west of North Phoenix Road into the planned neo-traditional form of the SE Plan Area.



As described in the TIA, the SE Commercial Center Core Area Plan, and the SE Overlay Plan, the gateway announces itself by a series of architectural and engineering features. Benches, landscaping, and pedestrian-level lighting fixtures will punctuate sidewalks constructed to twice the normal width of typical Medford walkways. Shops will front E Barnett Road adjacent to the sidewalk. Driveways are located to replicate the grid pattern of a downtown. Bike lanes will flow along each side of E. Barnett Road. Bike parking will be integrated into the overall design of the project. A major transit stop, placed directly along the sidewalk near the roundabout, helps to encourage pedestrian and bicycle use of the SE Center.

An important element of achieving the neighborhood neo-traditional form is assuring that auto speeds remain compatible with the multi-modal character of the neighborhood. The plan has many features that help with this assurance. Traffic approaching and leaving a traffic signal tend to drive slower than locations between signals. A roundabout marks the east end of the Gateway area. Properly designed, a roundabout limits traffic speed to approximately 25 miles per hour.

To further reduce traffic speeds, the TIA recommends reducing width of automobile travel lanes to 10-feet and widening bike lanes and sidewalks to a minimum of six feet. Automobile travel lane widths of 10-feet have the same capacity as the City standard 11 or 12-foot width. Traffic speed in ten-foot wide lanes is generally slower than in otherwise comparable wider lanes. Ten-foot lanes do not appreciably affect traffic capacity.

Site Access H is a component of a larger vision for the SE Center. It connects from the Rogue Credit Union site to Michael Park Road. This layout emphasizes the neo-Traditional grid pattern promoted in the plan.

By intent, site accesses shown in the adopted SE Neighborhood Center Core Area Plan are more closely spaced than driveways along a typical arterial. This is in recognition of the Plan's intent to create a central core for the neighborhood rather than development accessed primarily by automobiles only at concentrated points of entry as is typical of strip developments elsewhere. The driveways themselves are activity points that tend to reduce speeds. Similar to drivers in a downtown area, drivers will drive slower and be more attuned to potential conflicts than they would if they were on an arterial designed like E. Barnett and N. Phoenix Roads as they approach the project.



Current Medford arterial standards place driveways far enough apart for high-speed traffic to reach cruising speed before encountering conflicting traffic. This is not conducive to lowering traffic speeds.

The Public Works staff suggests that Site Access H move further east. That would require the recently constructed driveway for the Rogue Credit union to be relocated to remain aligned with Site Access H. Without both drives being relocated, the left-turn pockets into the each of the driveways would overlap head-on into the opposing left-turn lanes.

Relocation of the Credit Union driveway has topographical limitations. The fire station next door is on higher ground above the Credit Union. There is a substantial retaining wall between the two properties. In addition, there is a fiber-optic cable that has the potential to interfere with the relocation.

The more important issues involve the design of the SE Center itself. Two of the SE Center Plan design principles included in the Master Plan would be negatively affected. First, the internal street pattern replicating a neo-traditional grid would be impossible. The current plan, based on marketable building dimensions, places the internal roadway along the face of the buildings aligning with N. Phoenix Road. Any material movement to the east of the driveway would limit the viability of this design. The on-site roadway provides connectivity, unhindered by parking between the Masonic Temple and Michael Park Drive.

Second, moving Site Access H to the west would materially impact the size of Building 4. The Master Plan has buildings 5 and 6 on sites adjacent to the bus transfer facility. There is no way to keep the dimensions of Building 4 and accommodate a driveway relocated to the east.

As the SE Commercial Master Plan was developed the Planning Commission and City Council recognized that there would be tension between the adopted Plan and the City Code sections regarding arterial streets. The City Adopted Southeast Village Commercial Center Core Area Master Plan itself states: “Land use and development within the Master Plan area will accordingly conform to the Master Plan in addition to all other applicable land use and development regulations. **In the situation of a conflict with other regulations of the Medford Land Development Code, the Master Plan shall supersede.**” (Emphasis added)

At the time of the SE Center Plan adoption, the City had not, and still has not, adopted roadway standards that reflect the City’s vision of a transit-oriented development. In the more than a decade of deliberations involving Plan approval,



decision makers challenged the property owners to address the loss of neighborhood that is a visible result of current standards. Because the decision makers recognized that existing rules may not accommodate the vision they saw, they provided a procedure to make the important decisions involved in the SE Plan Area. They are included in the Section 10.550-3) c of the Medford City Code and quoted below:

*“(3) Area Plans: Access spacing and location may be evaluated as part of a special area plan. The approving authority may adopt specific standards through a special area plan such as a neighborhood plan and/or master plan. **Where such plans are adopted, any conflict between the special area plan access and location standards and the standards in Section 10.550 (3) (a) and/or (b) above shall be resolved in favor of the special area plan provisions.** [Emphasis added.]*

“(4) New Development: At an applicant's request, the approving authority will evaluate alternative access spacing and location on a project basis in conjunction with procedural Class C plan authorizations. Evaluation of alternative access location and spacing for projects shall be based upon a Transportation Impact Analysis (TIA) prepared by a professional engineer licensed in the State of Oregon with expertise in transportation. The Public Works Director (or designee) will provide a scope of work for the TIA and will issue a report to the approving authority stating his/her professional opinion as to the technical adequacy of the TIA and whether it demonstrates compliance with the criteria for access spacing and location for the project. The TIA will consider motorists, cyclists and pedestrians. The approving authority will evaluate the project's access spacing and location, in one of the following ways:

i. If the conclusions of the TIA and the professional opinion of the Public Works Director (or designee) concur that the safety and operations of the project's proposed access spacing and location will, at the time of



development, be equal to or better than application of the standards in 10.550 (3) (a) and/or (b) above for all studied facilities, the approving authority will accept the access spacing and location proposed at the time of project review.

If the Public Works Director's professional opinion is not consistent with the conclusions of the TIA, the approving authority will review the competing testimony by the professional engineers with expertise in transportation and will approve, modify, or deny the proposed alternative access design; or

'ii. If the conclusions of the TIA and the professional opinion of the Public Works Director (or designee), concur that the safety and operations of the project's proposed access spacing and location will, at the time of development, be equal to or better than application of the standards in 10.550 (3) (a) and/or (b) above for the transportation system as a whole, the approving authority may accept the access spacing and location proposed at the time of the major project review provided all facilities will still meet basic transportation engineering safety requirements; or

'iii. If the conclusions of the TIA find that the safety of the project's proposed access location and spacing will, at the time of development, meet basic transportation engineering safety requirements and the approving authority concludes that the proposed access spacing and location will significantly advance one or more of the Goals and/or Policies of the Comprehensive Plan, the approving authority may accept the access spacing and location proposed at the time of project review.'

An extensive evaluation of Access H is discussed on pages 30 – 34 of the June 21, 2017 TIA. TIA Section 2, Vistro Traffic Evaluation, provides detailed technical analysis of the E. Barnett Road intersection with Site Access H on pages 119 – 120 for the 2020 PM Build Scenario and on pages 229 – 30 for the 2023 PM Build Scenario. A summary of the highlights follows:



The technical analysis shows that the 95th percentile queue for the westbound left turn from E. Barnett Road to southbound N. Phoenix Road is 75 feet. The westbound queue for left-turns into Site Access H has a 95th percentile length of 25 feet. The sum of the two, should they both occur during the same time interval, leaves slightly less than half of the total storage-length between the two queues as a factor of safety.

The TIA on page 34 concludes: “Based on speed, level of service and queuing, the safety and operations of the projects proposed access spacing, and location will, at the time of development, and extended to the planning horizon, be equal or better than the application of the standards in Medford Code 10.550 (3)(a) for the studied locations.’ This is the finding required under paragraph 4) ii, above. If the Public Works Director does not agree, this will trigger the need for the “approving authority’ to decide.

We believe that if the roadway is built to the currently adopted standards, there is no reason to expect anything different than is currently in place on the arterials approaching the SE Center. As such, we believe the status quo does not meet the goals of the SE Plan Overlay and the SE Commercial Center Core Area Master Plan. We strongly recommend that the location and spacing for Site Access H and Site Access be approved as located in the Adopted Plan.

Comment 2

“Comment 2; For all build scenarios, the development should be conditioned to construct a southbound left turn lane at N. Phoenix @ Michael Park including a modification to the existing median, not a two way turn lane per page 41. This facility should be a full left turn pocket designed to ODOT standards to allow for deceleration and queuing owing to the high speed of N. Phoenix Rd. A southbound left turn lane built within the space provided between the street intersection and existing median islands to the north does not appear to provide space for deceleration...”



Response to Comment 2:

The applicant agrees to a left-turn lane, that would not require the widening of existing N. Phoenix Road. We would propose a southbound lane with 125 feet of storage with a “s” deceleration distance of 170 feet, which includes a taper length of 120 feet. This will require the removal of most of the trees planted in median north of Michael Park Road.

We believe that details of the striping plan for N. Phoenix Road should be part of the overall road plan approval and not as a condition of a zone change. Although the design of the turn lane may seem simple, until a detailed design is undertaken, there is no way to determine if there is an issue that might be dictated. These issues are usually accommodated as part of the roadway design process not in a land-use process.

Comment 3

“Page 3 — mitigation; Clarify mitigation at Barnett and N. Phoenix; east and westbound left turns are already protected permissive.”

Response to comment 2: The comment is correct. The mitigation should be adding protective – permissive phasing to the north and southbound left-turn phases. The Vistro analysis for the location correctly represented the east and westbound protected-permissive left turns as existing and added the north-south protected permissive, as mitigation for the build scenarios. The narrative text in the TIA however is incorrect.

Comment 4

“The revised trip cap for the south side is calculated assuming that the site is already generating the maximum trips allowed under the existing trip cap but there is no data provided to substantiate this. We recommend calculating the existing trip generation of the site and adding it to the new trips calculated in table 5 as a reasonable method of establishing the revised trip cap.”



Response to comment 4:

The table immediately following provides the gross trips using the methodology described in Comment 4. The table following it provides the calculations for the net trips used in the Trip Cap.

South Side (Credit Union) Trip Generation

Existing PM Peak Trip Generation

Land Use	ITE Code	Sq. Ft	Daily Rate	Daily Trips	PM Rate	PM Trips
Specialty Retail	826	0	44.32	0	2.71	0
General Office	710	12,702	$\ln(T)=0.76*Ln(X) +3.68$	113	$T=(1.12*(X)/1000) -78.45$	93
Drive-in Bank	912	2,007	148.15	297	24.30	49
Subtotal, Existing						141

Added PM Peak Trip Generation

Specialty Retail	826	4,200	44.32	186	2.71	11
General Office	710	2,007	$\ln(T)=0.76*Ln(X) +3.68$	28	**Incremental Equation See Below	2
Drive-in Bank	912	1,493	148.15	221	24.30	36
Subtotal, Added						50
Grand Total				846	191	

**Incremental Equation:
 $\text{Trips} = (1.12 * (12,701+2007)-78.45)-$
 $(1.12*12,701)-(78.45)$



As shown below, the net trips, after allowance for TOD and pass-by reductions establishes a Trip Cap of 145 Trips. This accommodates full development according to the SE Center Plan assuming required mitigation. The City’s practice at building permit application is to take the determine the gross trips using the ITE *Highway Capacity Manual* and then use applicable credits to reduce gross trips to the net trips used in Trip Cap determination.

Calculation of PM Peak Hour Net Trips

	Gross PM Pk- Hour Trips	10-Percent TOD	25-Percent Passby		Net Trips
Specialty Retail	11	-1.1382	-3		7
General Office	95	-9.49	-24		62
Drive-In Bank	85	-8.48	0		76
Trip Cap					145

Comment 5:

“The TIA analysis uses 431 PM peak hour trips (per table 5) instead of 624 PM peak hour trips (per the stipulation letter in Appendix 2) for the north side of the development. Provide a stipulation that caps the number of trips allowed on the north side of the development to 431PM peak hour trips.”

Response to Comment 5:

We stipulate that the North Side development is subject to a Trip Cap, after accounting for required mitigation, and allowed reductions from gross trips for pass-by and transit oriented development. is 431 PM peak-hour trips as shown in the TIA in Table 5 on page 21. The City’s practice at building permit application is to take the determine the gross trips using the ITE *Highway Capacity Manual*



and then use applicable credits to reduce gross trips to the net trips used in Trip Cap determination.

Comment 6

The report should include a statement as to which hour was determined to be the peak and used for calculations. Note that the peak hour used should be consistent with the requirements of Medford Municipal Code section 10.461 (6) (b).

Response to Comment 6:

The PM peak-hour, consistent with the quoted code section is 4:45 to 5:45 PM.

Comment 7

“Page 23, the last sentence of the first paragraph states that the “City requires...”. This is not accurate as the city does not have a LOS standard for driveways.”

Response to Comment 6: In revising the earlier TIA’s for this project, we inadvertently overlooked the sentence. Nevertheless, Table 8, PM Peak-Hour Intersection Performance in the current TIA shows “N/A” for the performance standard at each of the driveways.

Comment 8

Comment 8a)

“a) Int B @ Michael Park, check, this should be “Int E” according to the narrative on page 8 and the list on page 11. Check geometry; there should be no stop signs on Michael Park and there should be a stop sign on the driveway.”

Response to 8a):

Traffic from all accesses along Michael Park have been combined into Accesses A and B. Access A has most of the project traffic assigned to it



as Buildings 2, 5, and 7 are initially focused on the major access way. All traffic assumed to use accesses B, C, D, and E has been assigned to Access B.

In an earlier version of the TIA we used Access E as the combination of all the accesses other than Access A. Because Michael Park will not extend to the east as part of this project, using Access E for analysis produces a trivial result. Moving to Access B for analysis produces a more reasonable test of level of service.

The incorrect STOP sign location was partially a result of Michael Park Road being north-south at its intersection with Site Access A. To remove the confusion, we have completely redone the analysis for Site Accesses A and B in 2023, which, as explained below, also serves as Analysis for Site Accesses A and B in 2020.

Until Michael Park Road extends to the east, Site Accesses A and B are not affected by external traffic. North of Michael Park Road, the SE Center Plan shows a parking lot for the recreational area following Michael Park Road. Although the schedule for this area is unclear, the TIA assumes it will be constructed before 2020 and shows it approaching Site Access B from the north. Based on this, the 2020 traffic volumes and analysis results are identical to those of 2023.

As shown in Appendix 1, the calculated LOS for all movements at Site Accesses A and B remain LOS A.

Comment 8 b)

“b) Site Driveway H @ Barnett Rd, check geometry, Barnett should have two thru lanes and a center turn lane westbound according to the Masterplan.”

Response to 8 b):

As shown on figure 2, the outside eastbound lane of E. Barnett Road along the Rogue Credit Union Site will be constructed and hatched out. This will make the lane available when E. Barnett Road is extended to the east.

Appendices 2 and 3 show the technical analysis of two lane configurations for the additional lane. Appendix 2 assumes that the outside lane extends to the east and is conformed as a through-right lane and has a through-only lane to its left.



Appendix 3 assumes that the outside lane is a right-turn only lane ending at the Credit Union Site Access. A through lane is located between the right-turn lane and a left-turn lane

These two alternatives, and the one modelled in the original TIA operates at the same LOS and virtually identical queues for all movements.

Comment 8 c)

“c) Site Drive I/Fire © Barnett, check geometry, Place stops on driveways.”

Response to 8 c):

Appendix 4 provides the corrected analysis for East Barnett Rd at Site Access I / Fire Station for the 2020 PM Build Scenario. Appendix 5 shows the same information for the 2023 PM Build Scenario. Both show LOS C operation.

Comment 9

“a) Intersection A @ Michael Park, there is no southbound approach, the eastbound approach should not be stop controlled and should be shared right and thru, the westbound approach should be shared left and thru, the northbound approach including left thru and right is stop controlled.”

Response to 9 a):

See Response to comment 8 a)

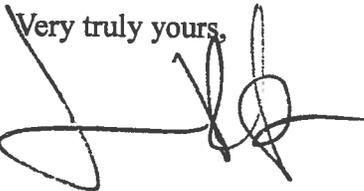
Comment 9 b)

N. Phoenix © Michael Park, we recommend that the westbound approach be configured with a shared left and thru and a separate right turn lane.”

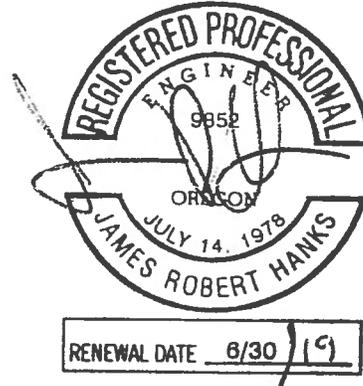
Response to 9 b):

We will follow this recommendation.

We appreciate your comments. Please let us know if you need any further information.

Very truly yours,


James R. Hanks, PE,
JRH Transportation Engineering



Attachments:

Appendix 1 – Site Accesses A and B: 2023 PM Peak- Hour Vistro Performance Analysis – (2020 Analysis is Identical)

Appendix 2 – 2023 PM Peak-Hour Build Analyses of Eastbound Through, Through-Right Lane Option at Site Access H / Credit Union connection to E. Barnett Road

Appendix 3 – 2023 PM Peak-Hour Build Analyses of Eastbound Through Plus Right-Turn Only Lane Option at Site Access H / Credit Union connection to E. Barnett Road

Appendix 4 – Corrections to Site Access I / Fire Station analysis for 2020 Build Scenario

Appendix 5 – Corrections to Site Access I / Fire Station analysis for 2020 Build Scenario

Appendix 6 – Copy of City of Medford August 1 and August 8, 2017 comment letters regarding the June 21, 2017 SE Commercial Center Core Area Transportation Impact Analysis

RECEIVED

MAR 12 2018

PLANNING DEPT.

REPLY TO MEDFORD COMMENTS- AUGUST 1 AND AUGUST 8, 2017

Appendices

Appendix 1 – Site Accesses A and B: 2023 PM Peak-Hour Vistro Performance Analysis – (2020 Analysis is Identical)

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CITY OF MEDFORD
EXHIBIT # 15
File # ZC-17-168

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SE Commercial Center Core Area

Vistro File: C:\...IAAA 2438 SE Commercial Core Medford
082917.vistro

Scenario 17 17 2023 Build

Report File: C:\...2438 Medford Vistro PM Build 2023
Michael Park at INT A & B 083017.pdf

8/30/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
16	Int A@ Michael Park	Two-way stop	HCM 2010	NB Left	0.029	8.8	A
23	Int B @Michael Park	Two-way stop	HCM 2010	NWB Thru	0.000	9.1	A

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.



Intersection Level Of Service Report
Intersection 16: Int A@ Michael Park

Control Type: Two-way stop
Analysis Method: HCM 2010
Analysis Period: 15 minutes

Delay (sec / veh): 8.8
Level Of Service: A
Volume to Capacity (v/c): 0.029

Intersection Setup

Name	Int A		Michael Park		Michael Park	
Approach	Northbound		Eastbound		Westbound	
Lane Configuration	↔		↗		↖	
Turning Movement	Left	Right	Thru	Right	Left	Thru
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0
Pocket Length [ft]	122.00	122.00	122.00	122.00	122.00	122.00
Speed [mph]	25.00		25.00		25.00	
Grade [%]	0.00		0.00		0.00	
Crosswalk	No		No		No	

Volumes

Name	Int A		Michael Park		Michael Park	
Base Volume Input [veh/h]	0	0	0	0	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.08	1.08	1.08	1.08	1.08	1.08
In-Process Volume [veh/h]	0	0	0	0	0	0
Site-Generated Trips [veh/h]	29	5	1	53	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0
Total Hourly Volume [veh/h]	29	5	1	53	0	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	7	1	0	13	0	0
Total Analysis Volume [veh/h]	29	5	1	53	0	0
Pedestrian Volume [ped/h]						



Intersection Settings

Priority Scheme	Stop	Free	Free
Flared Lane	No		
Storage Area [veh]	0	0	0
Two-Stage Gap Acceptance	No		
Number of Storage Spaces in Median	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.03	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	8.77	8.56	7.32	7.32	7.32	7.32
Movement LOS	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.11	0.11	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	2.65	2.65	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	8.74		0.00		3.66	
Approach LOS	A		A		A	
d_I, Intersection Delay [s/veh]	3.38					
Intersection LOS	A					



Intersection Level Of Service Report
Intersection 23: Int B @Michael Park

Control Type: Two-way stop
Analysis Method: HCM 2010
Analysis Period: 15 minutes

Delay (sec / veh): 9.1
Level Of Service: A
Volume to Capacity (v/c): 0.000

Intersection Setup

Name	Michael Park			Michael Park			Int B			Int B		
Approach	Northeastbound			Southwestbound			Northwestbound			Southeastbound		
Lane Configuration	+			+			+			+		
Turning Movement	Left	Thru	Right									
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0		0	0		0	0		0	0		0
Pocket Length [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Michael Park			Michael Park			Int B			Int B		
Base Volume Input [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08
In-Process Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Site-Generated Trips [veh/h]	1	5	0	0	0	0	0	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	1	5	0	0	0	0	0	0	0	0	0	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	0	1	0	0	0	0	0	0	0	0	0	0
Total Analysis Volume [veh/h]	1	5	0	0	0	0	0	0	0	0	0	0
Pedestrian Volume [ped/h]												



Intersection Settings

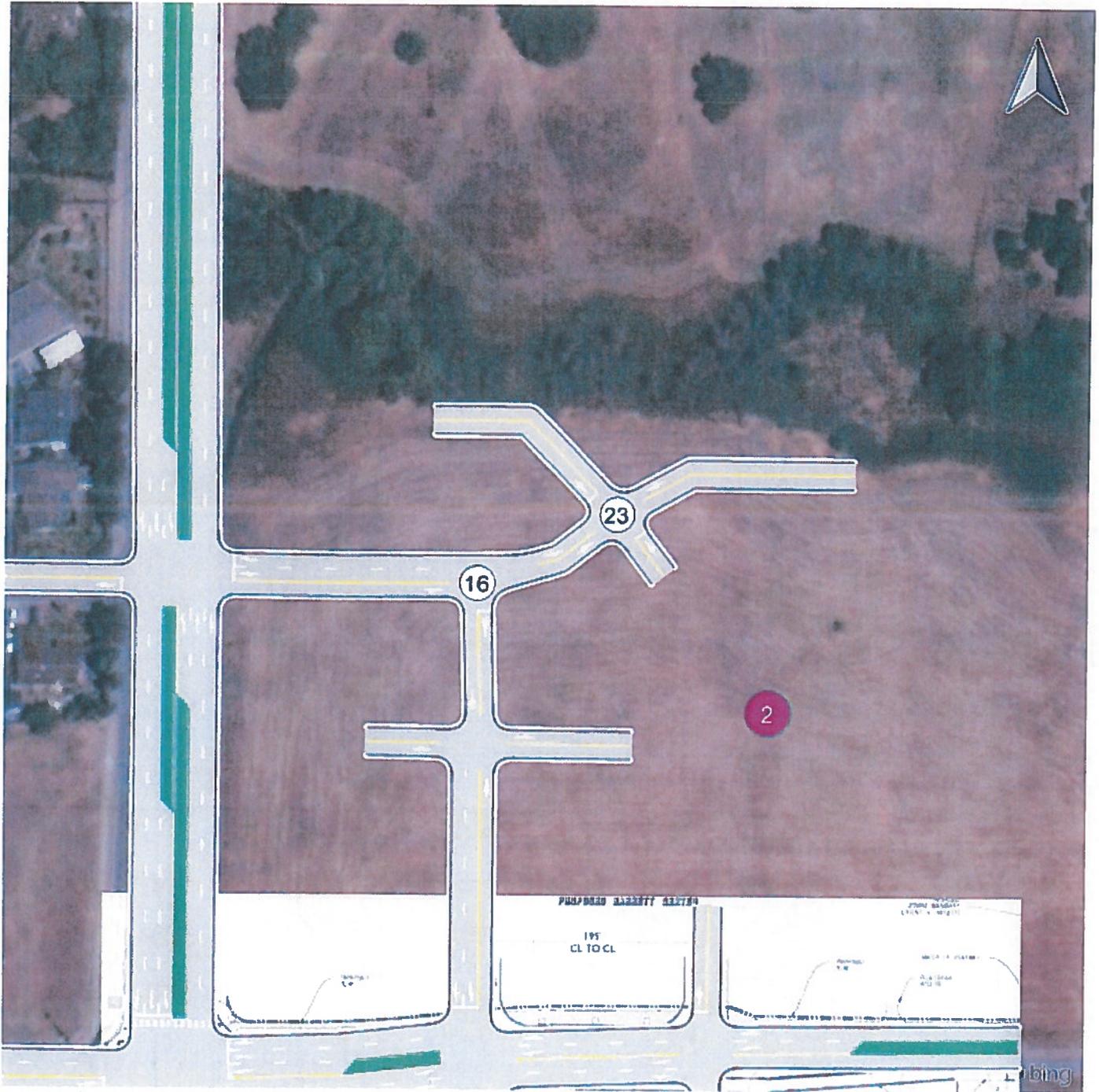
Priority Scheme	Free	Free	Stop	Stop
Flared Lane			No	No
Storage Area [veh]				
Two-Stage Gap Acceptance			No	No
Number of Storage Spaces in Median				

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	7.22	8.31	8.26	7.23	8.27	8.22	8.56	9.06	8.34	8.56	9.06	8.32
Movement LOS	A	A	A	A	A	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.01	0.01	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.28	0.28	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	1.20			2.41			8.65			8.64		
Approach LOS	A			A			A			A		
d_I, Intersection Delay [s/veh]	1.20											
Intersection LOS	A											



Report Figure 1: Study Intersections

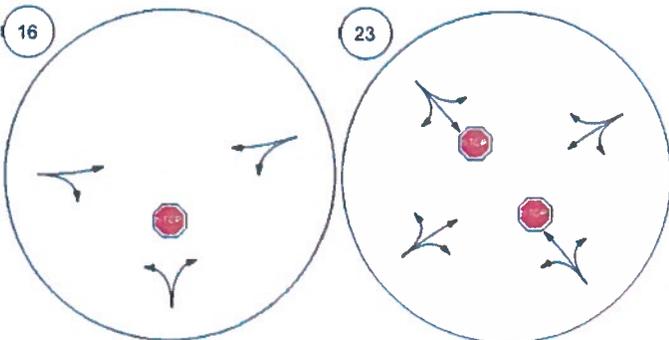


Report Figure 2: Lane Configuration and Traffic Control



Int A @ Michael Park

Int B @ Michael Park



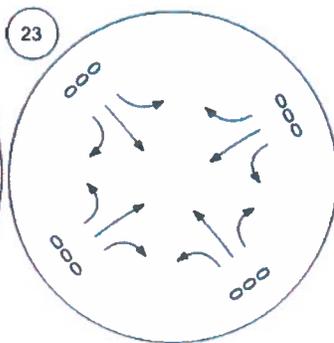
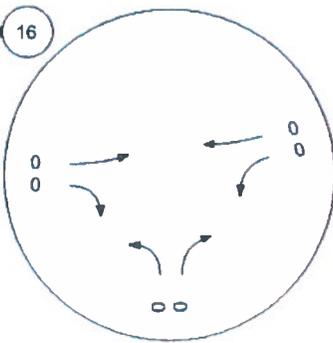


Report Figure 3a: Traffic Volume - Base Volume



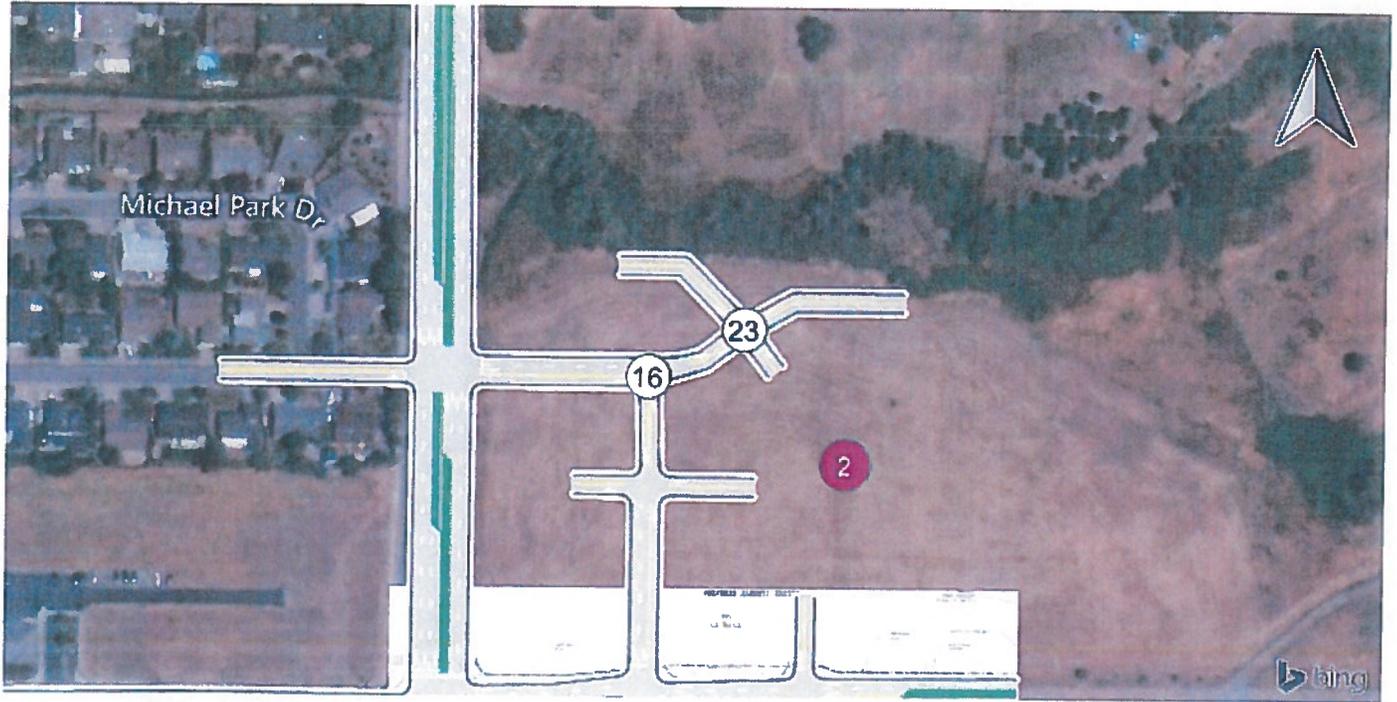
Int A @ Michael Park

Int B @ Michael Park

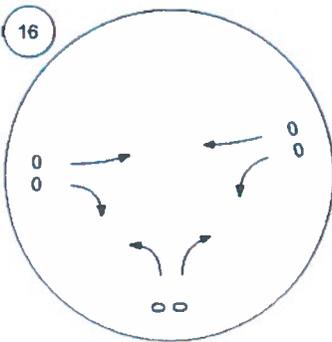




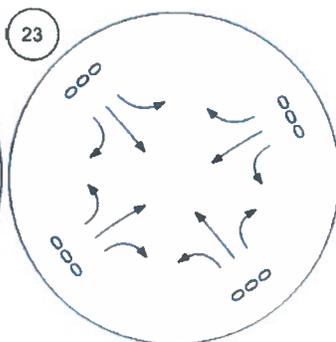
Report Figure 3b: Traffic Volume - In-Process Volume



Int A @ Michael Park



Int B @ Michael Park



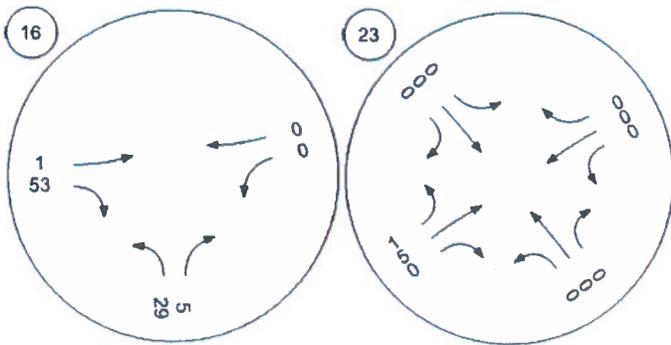


Report Figure 3c: Traffic Volume - Net New Site Trips



Int A @ Michael Park

Int B @ Michael Park



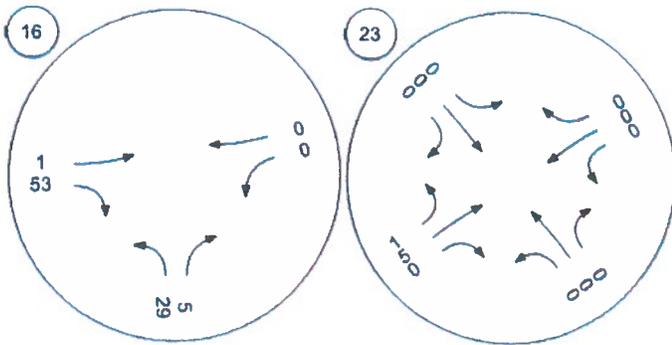


Report Figure 3e: Traffic Volume - Future Total Volume



Int A @ Michael Park

Int B @ Michael Park



Report Figure 4: Traffic Conditions



Int A @ Michael Park

Int B @ Michael Park

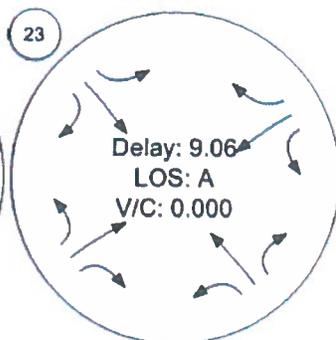
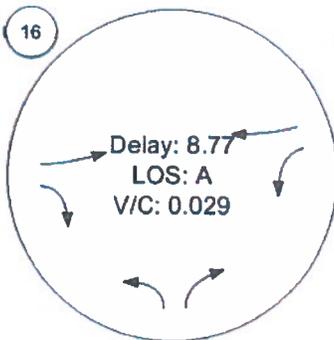


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SE Commercial Center Core Area

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Scenario 17 17 2023 Build

Report File: \...\2438 2023 Build PM Barnett Drive H EB L T
R Vistro.pdf

8/23/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
24	Site Drive H@Barnett Rd	Two-way stop	HCM 2010	SB Left	0.000	24.6	C

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.



Intersection Level Of Service Report
Intersection 24: Site Drive H@Barnett Rd

Control Type: Two-way stop
Analysis Method: HCM 2010
Analysis Period: 15 minutes

Delay (sec / veh): 24.6
Level Of Service: C
Volume to Capacity (v/c): 0.000

Intersection Setup

Name	Credit Union			Site Drive H			Barnett			Barnett		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration	↔			↔			↔			↔		
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Credit Union			Site Drive H			Barnett			Barnett		
Base Volume Input [veh/h]	5	0	0	0	0	0	0	18	10	0	10	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08
In-Process Volume [veh/h]	0	0	0	0	0	0	0	510	0	0	248	0
Site-Generated Trips [veh/h]	15	0	0	0	0	113	92	41	12	0	60	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	9	0	0	0	0	25	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	29	0	0	0	0	138	92	570	23	0	319	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	7	0	0	0	0	35	23	143	6	0	80	0
Total Analysis Volume [veh/h]	29	0	0	0	0	138	92	570	23	0	319	0
Pedestrian Volume [ped/h]	0			0			0			0		



Intersection Settings

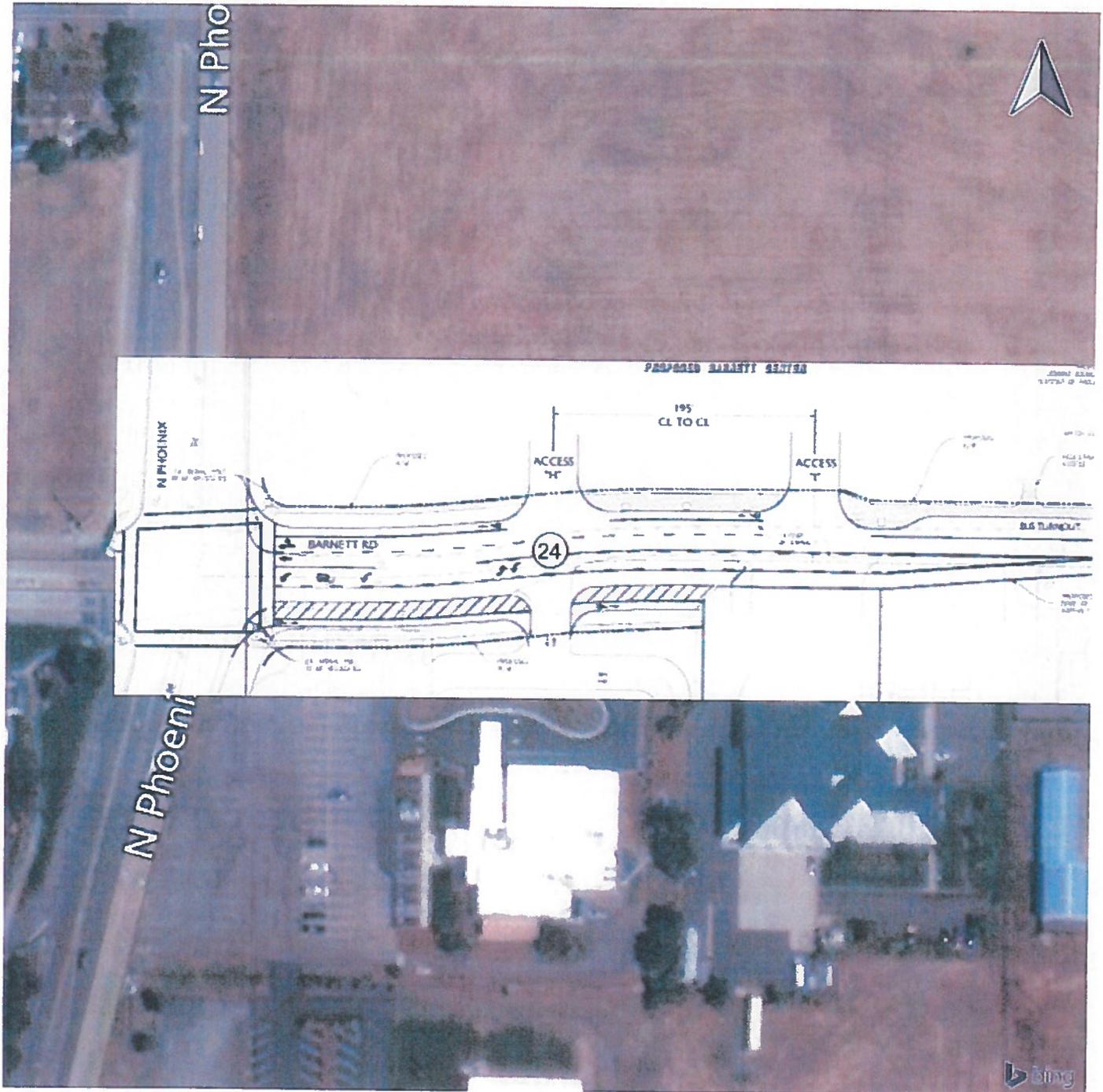
Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	Yes	No		
Number of Storage Spaces in Median	1	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.13	0.00	0.00	0.00	0.00	0.19	0.07	0.00	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	22.94	17.38	11.91	24.64	24.40	11.16	8.13	0.00	0.00	8.66	0.00	0.00
Movement LOS	C	C	B	C	C	B	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.43	0.00	0.00	0.00	0.70	0.70	0.24	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	10.66	0.00	0.00	0.00	17.56	17.56	6.00	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	22.94			11.16			1.09			0.00		
Approach LOS	C			B			A			A		
d_I, Intersection Delay [s/veh]	2.52											
Intersection LOS	C											

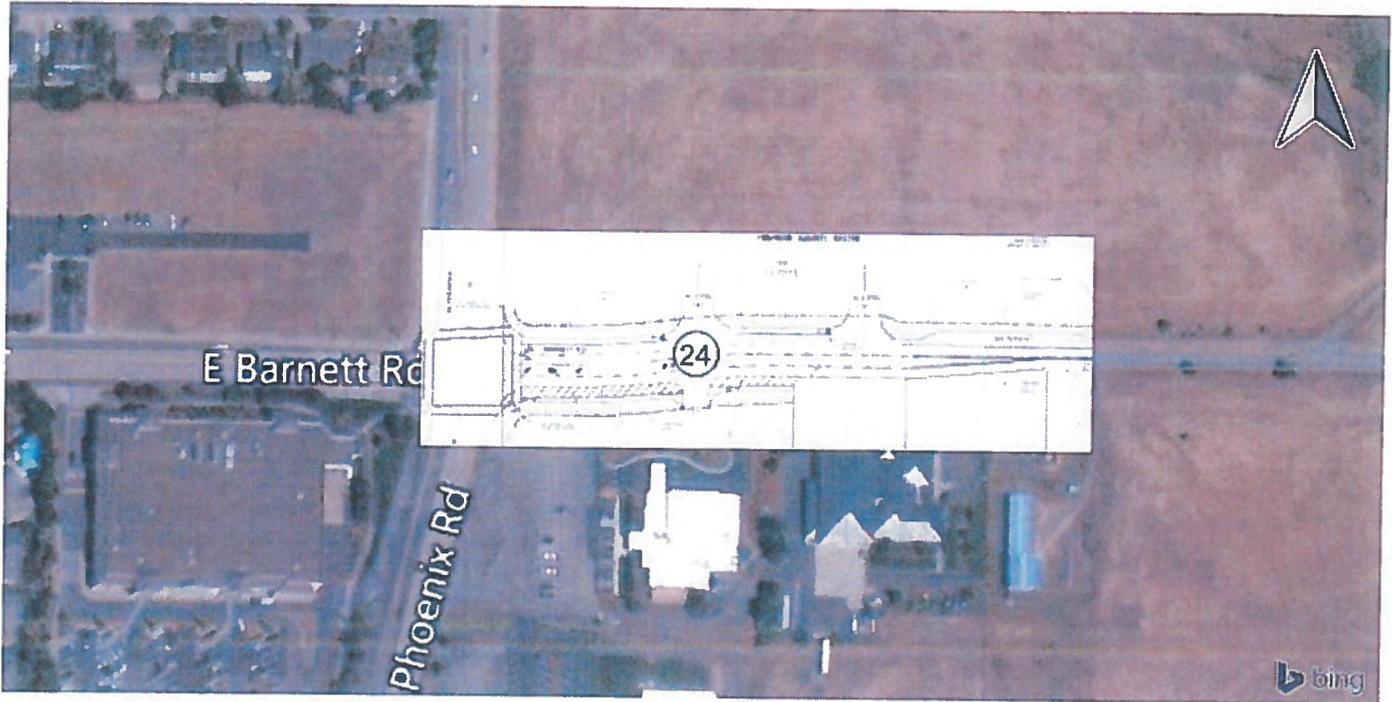


Report Figure 1: Study Intersections

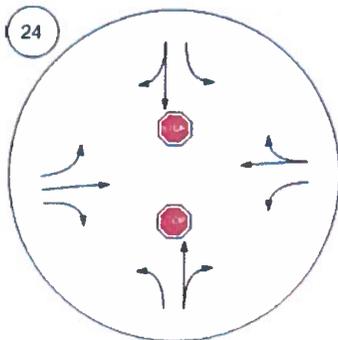




Report Figure 2: Lane Configuration and Traffic Control

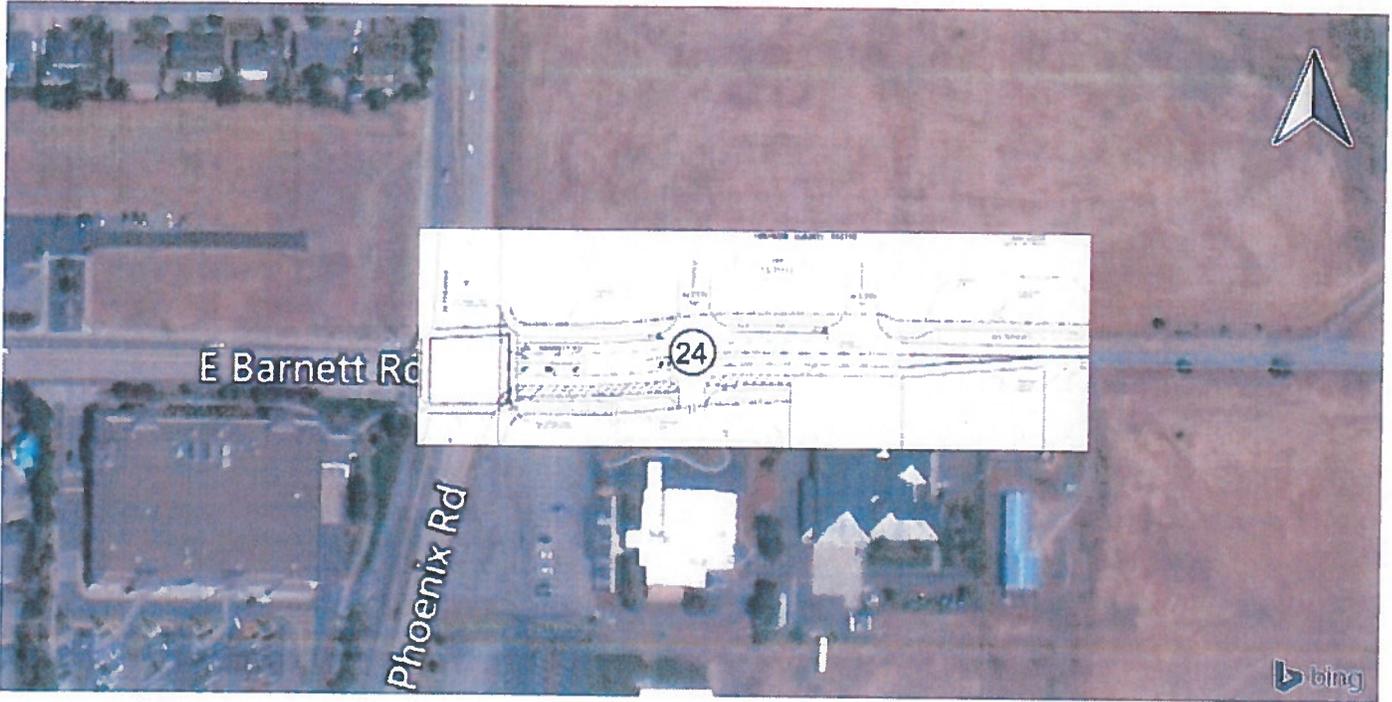


Site Drive H@Barnett Rd

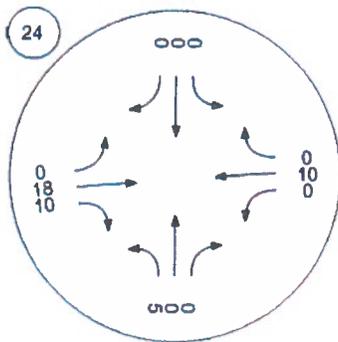




Report Figure 3a: Traffic Volume - Base Volume

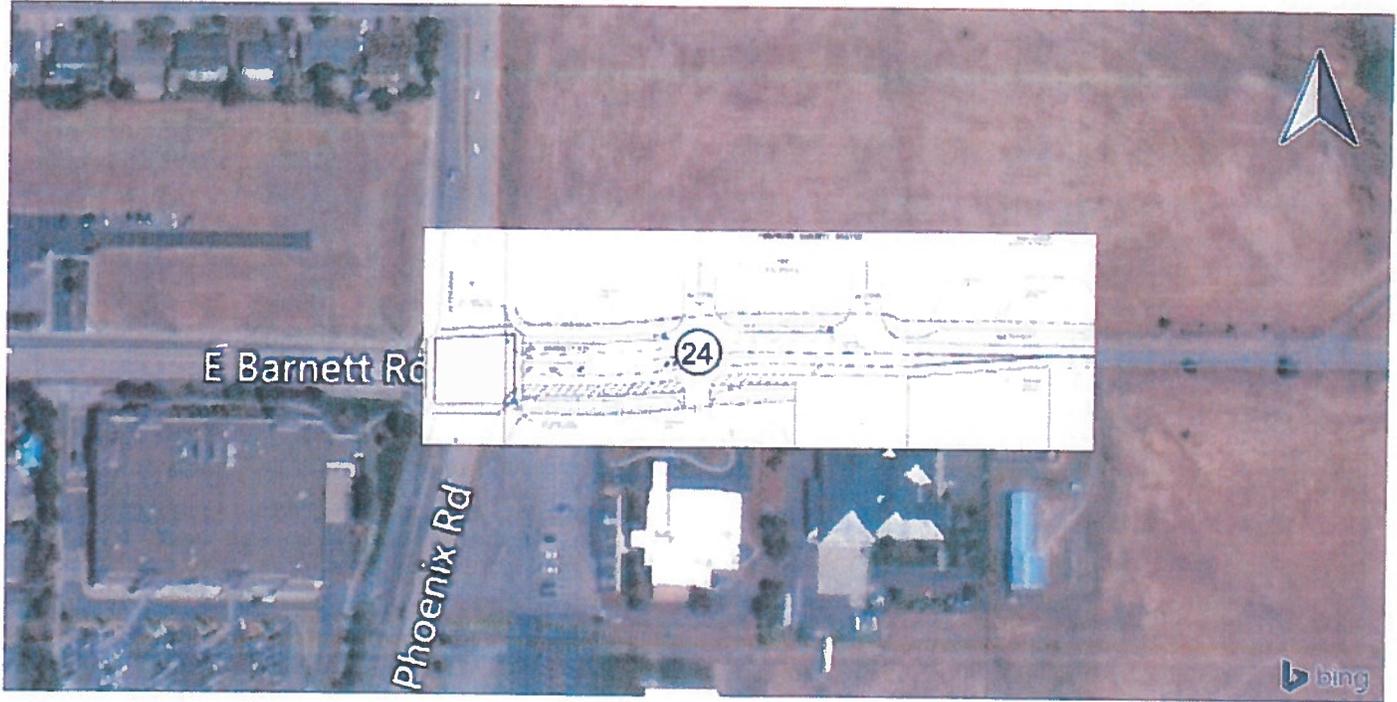


Site Drive H@Barnett Rd

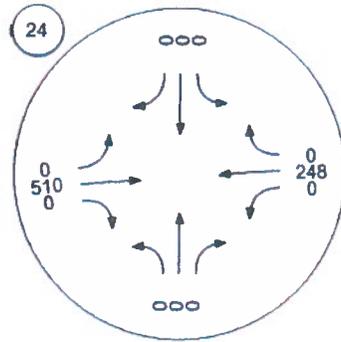




Report Figure 3b: Traffic Volume - In-Process Volume

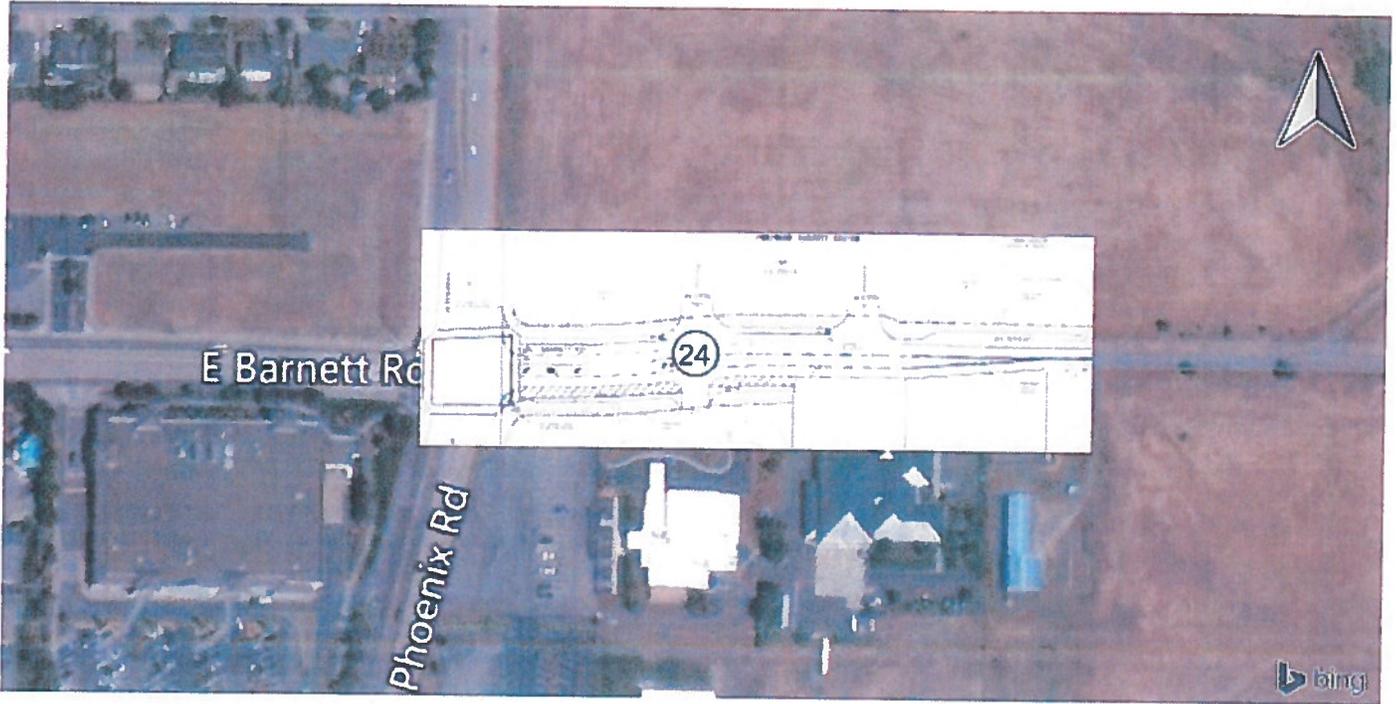


Site Drive H@Barnett Rd

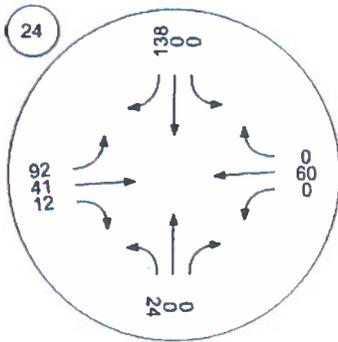




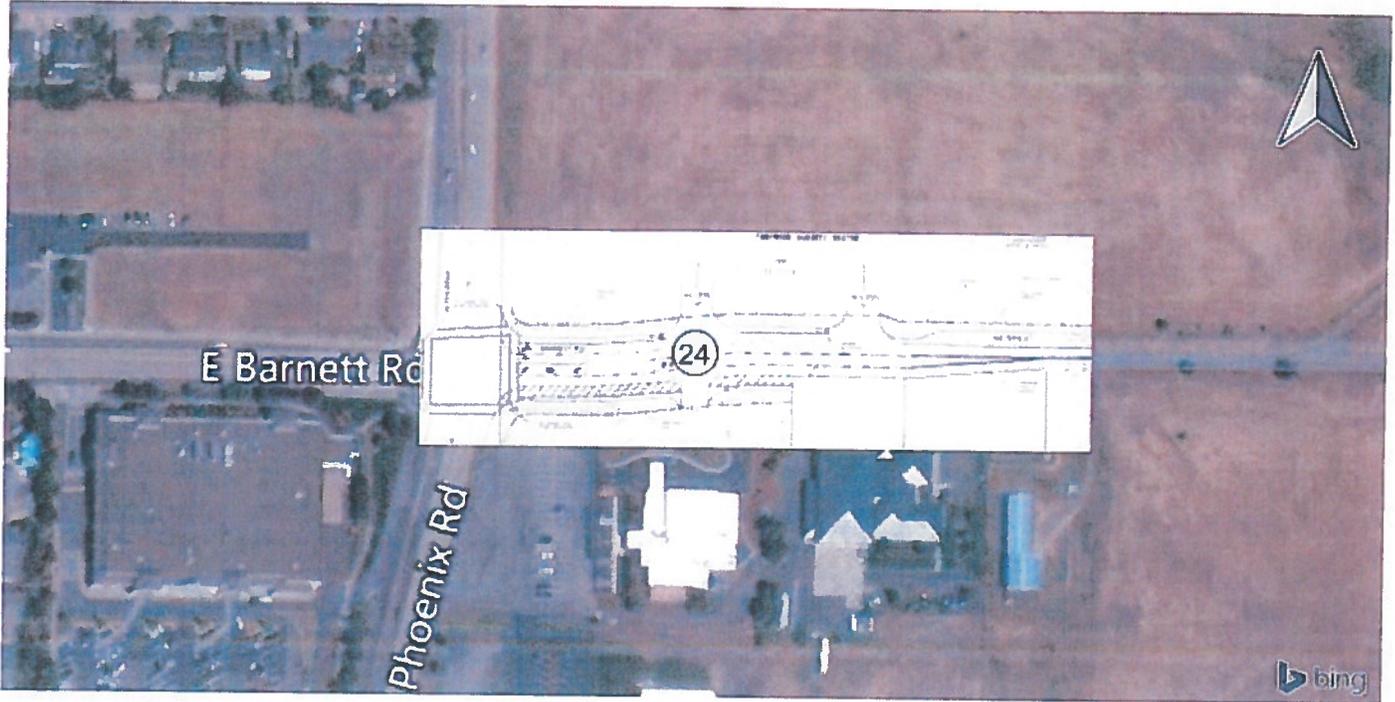
Report Figure 3c: Traffic Volume - Net New Site Trips



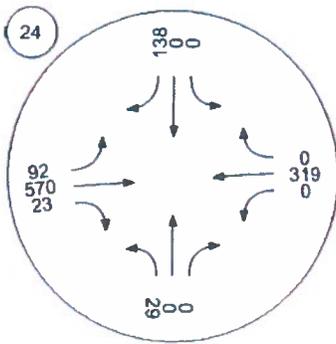
Site Drive H@Barnett Rd



Report Figure 3e: Traffic Volume - Future Total Volume

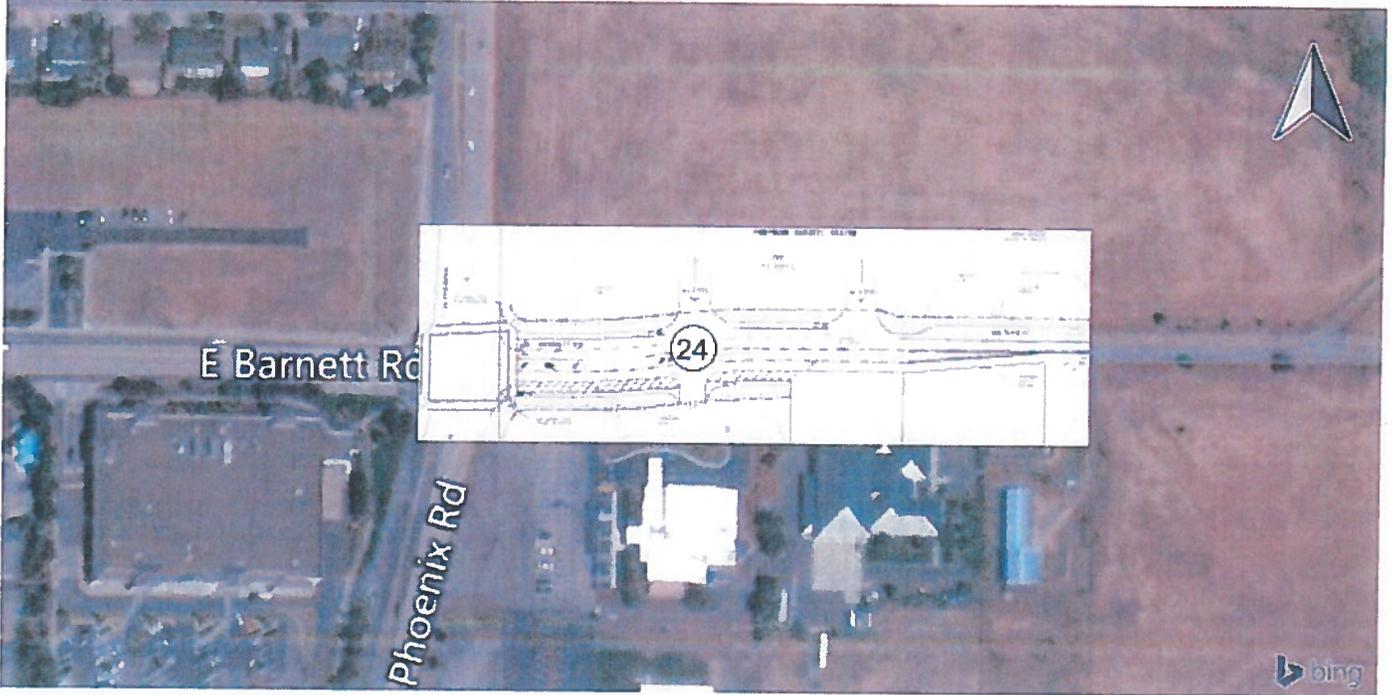


Site Drive H@Barnett Rd





Report Figure 4: Traffic Conditions



Site Drive H@Barnett Rd

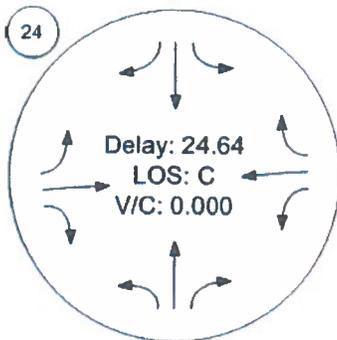


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SE Commercial Center Core Area

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H EB L T R.vistro

Scenario 17 17 2023 Build

Report File: \...\2438 2023 Build PM Barnet Drive H EB L T
R Vistro.pdf

8/23/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
24	Site Drive H@Barnett Rd	Two-way stop	HCM 2010	SB Left	0.000	24.6	C

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.



Intersection Level Of Service Report
Intersection 24: Site Drive H@Barnett Rd

Control Type:	Two-way stop	Delay (sec / veh):	24.6
Analysis Method:	HCM 2010	Level Of Service:	C
Analysis Period:	15 minutes	Volume to Capacity (v/c):	0.000

Intersection Setup

Name	Credit Union			Site Drive H			Barnett			Barnett		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration	↔			↔			↔↔			↔		
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	0	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Credit Union			Site Drive H			Barnett			Barnett		
Base Volume Input [veh/h]	5	0	0	0	0	0	0	18	10	0	10	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08
In-Process Volume [veh/h]	0	0	0	0	0	0	0	510	0	0	248	0
Site-Generated Trips [veh/h]	15	0	0	0	0	113	92	41	12	0	60	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	9	0	0	0	0	25	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	29	0	0	0	0	138	92	570	23	0	319	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	7	0	0	0	0	35	23	143	6	0	80	0
Total Analysis Volume [veh/h]	29	0	0	0	0	138	92	570	23	0	319	0
Pedestrian Volume [ped/h]	0			0			0			0		



Intersection Settings

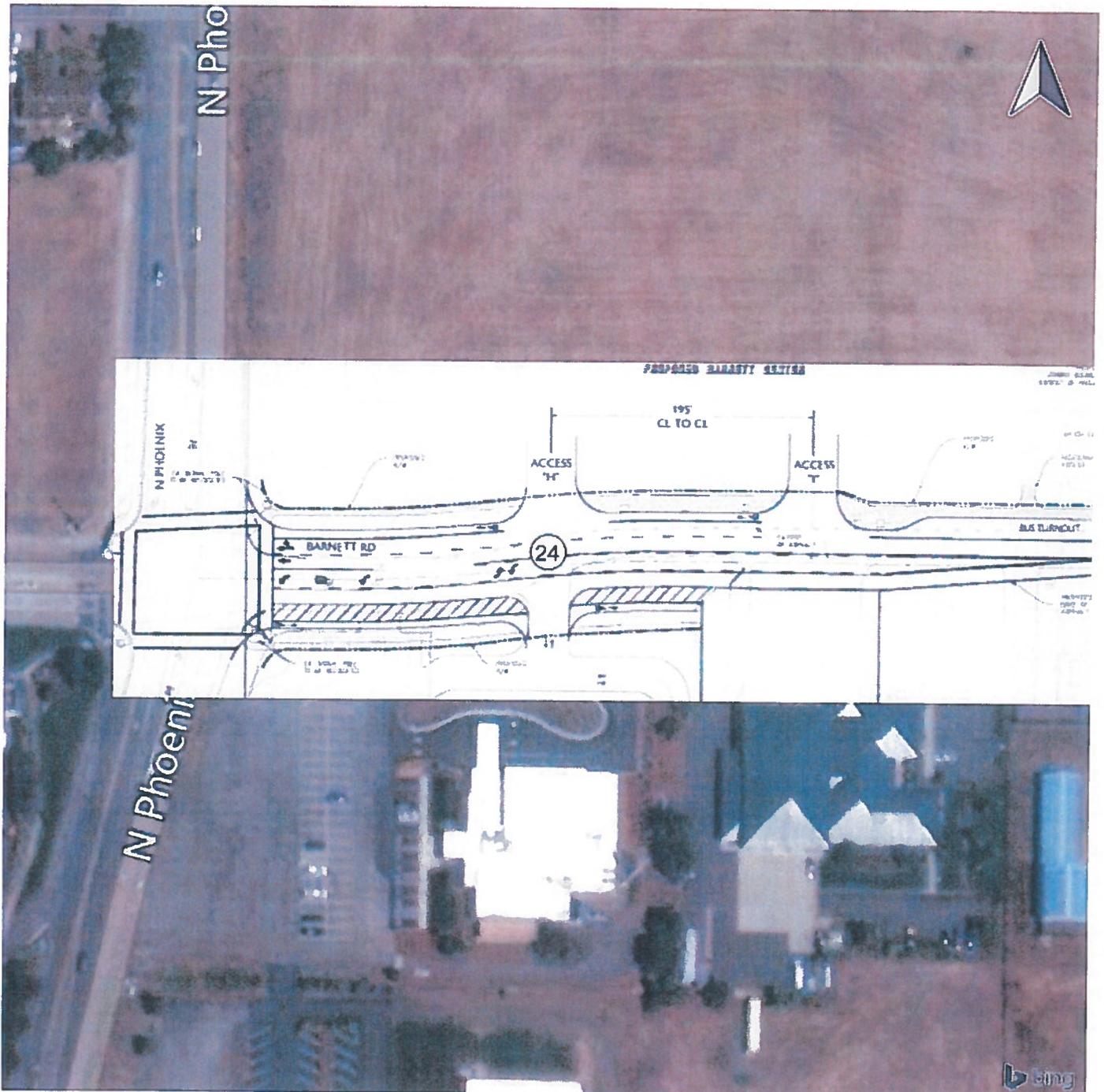
Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]				
Two-Stage Gap Acceptance	Yes	No		
Number of Storage Spaces in Median	1			

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.13	0.00	0.00	0.00	0.00	0.19	0.07			0.00		
d_M, Delay for Movement [s/veh]	22.94	17.38	11.91	24.64	24.40	11.16	8.13			8.66		
Movement LOS	C	C	B	C	C	B	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.43	0.00	0.00	0.00	0.70	0.70	0.24	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	10.66	0.00	0.00	0.00	17.56	17.56	6.00	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	22.94			11.16			1.09			0.00		
Approach LOS	C			B			A			A		
d_I, Intersection Delay [s/veh]	2.52											
Intersection LOS	C											

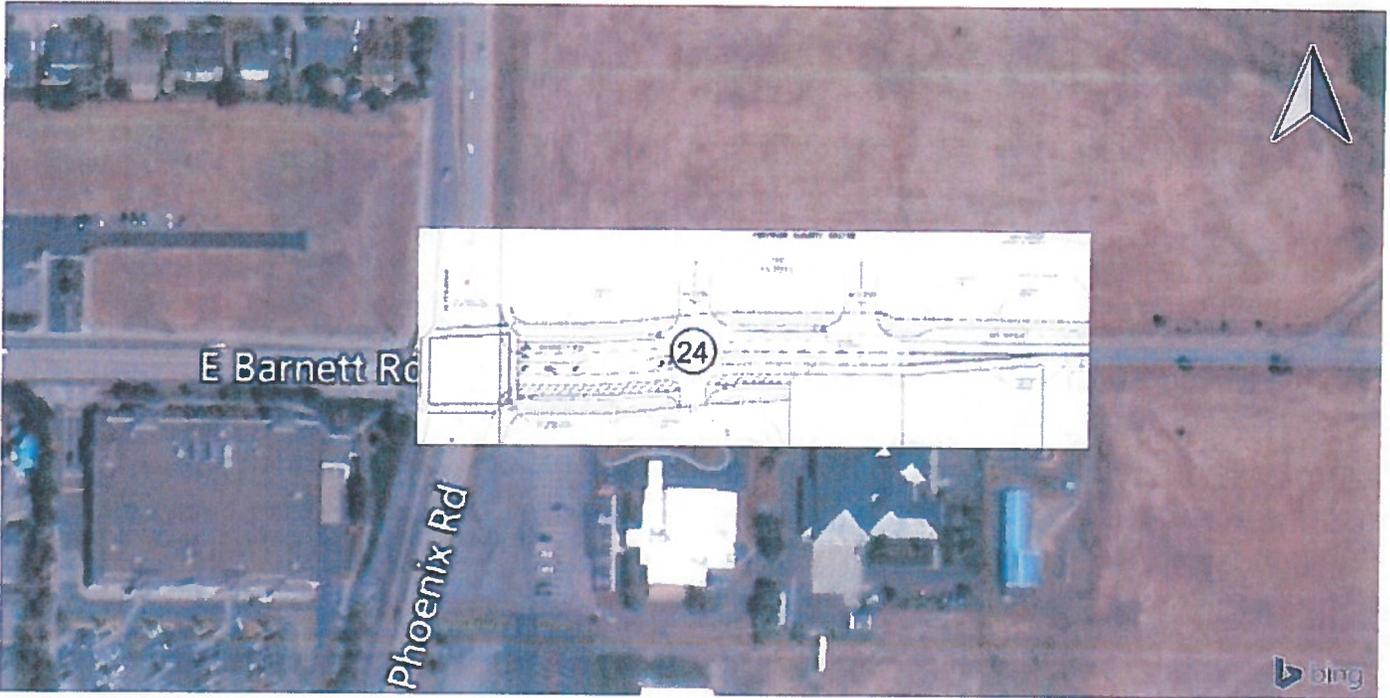


Report Figure 1: Study Intersections

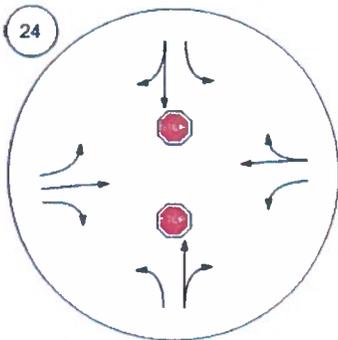




Report Figure 2: Lane Configuration and Traffic Control

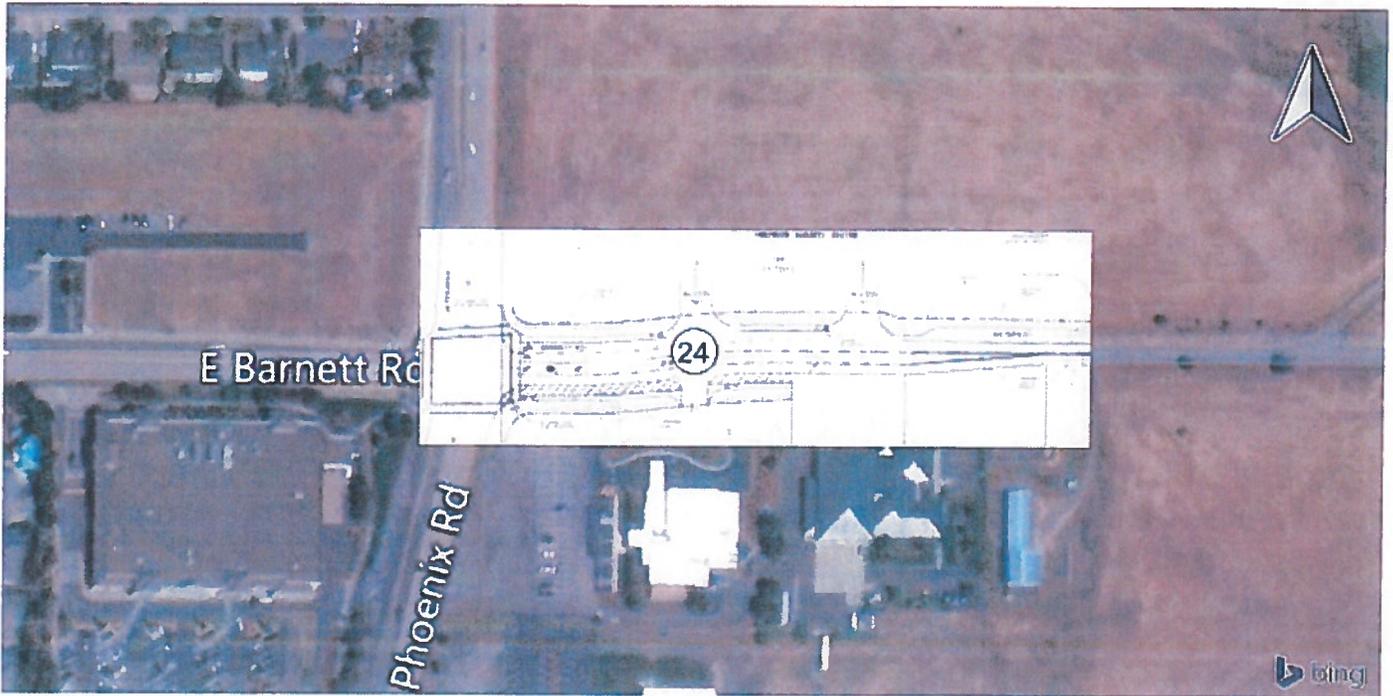


Site Drive H@Barnett Rd

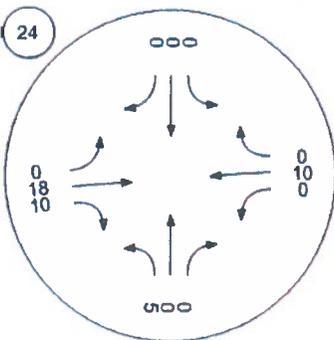




Report Figure 3a: Traffic Volume - Base Volume

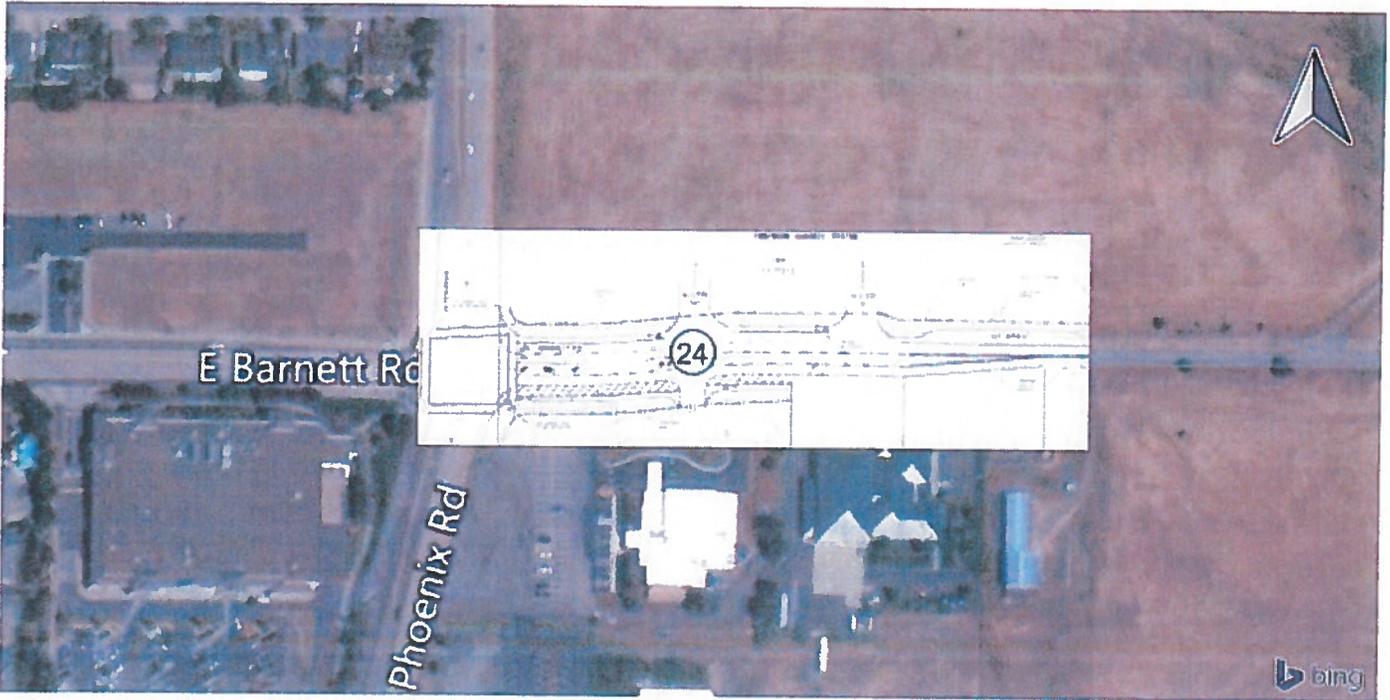


Site Drive H@Barnett Rd

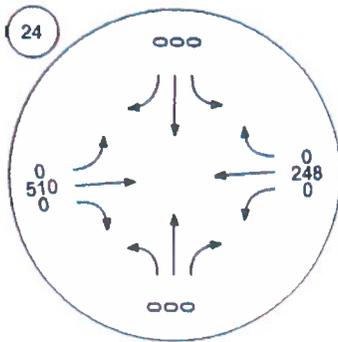




Report Figure 3b: Traffic Volume - In-Process Volume

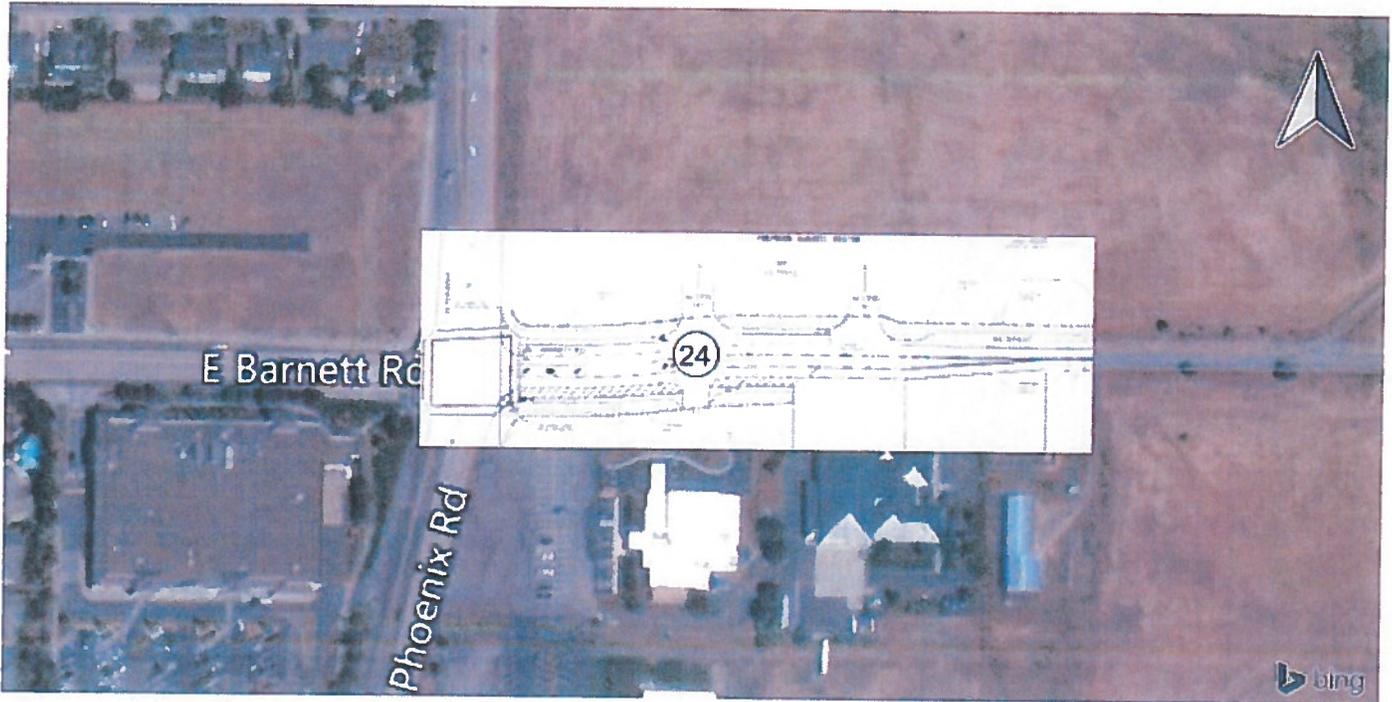


Site Drive H@Barnett Rd

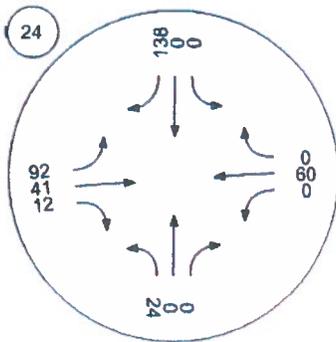




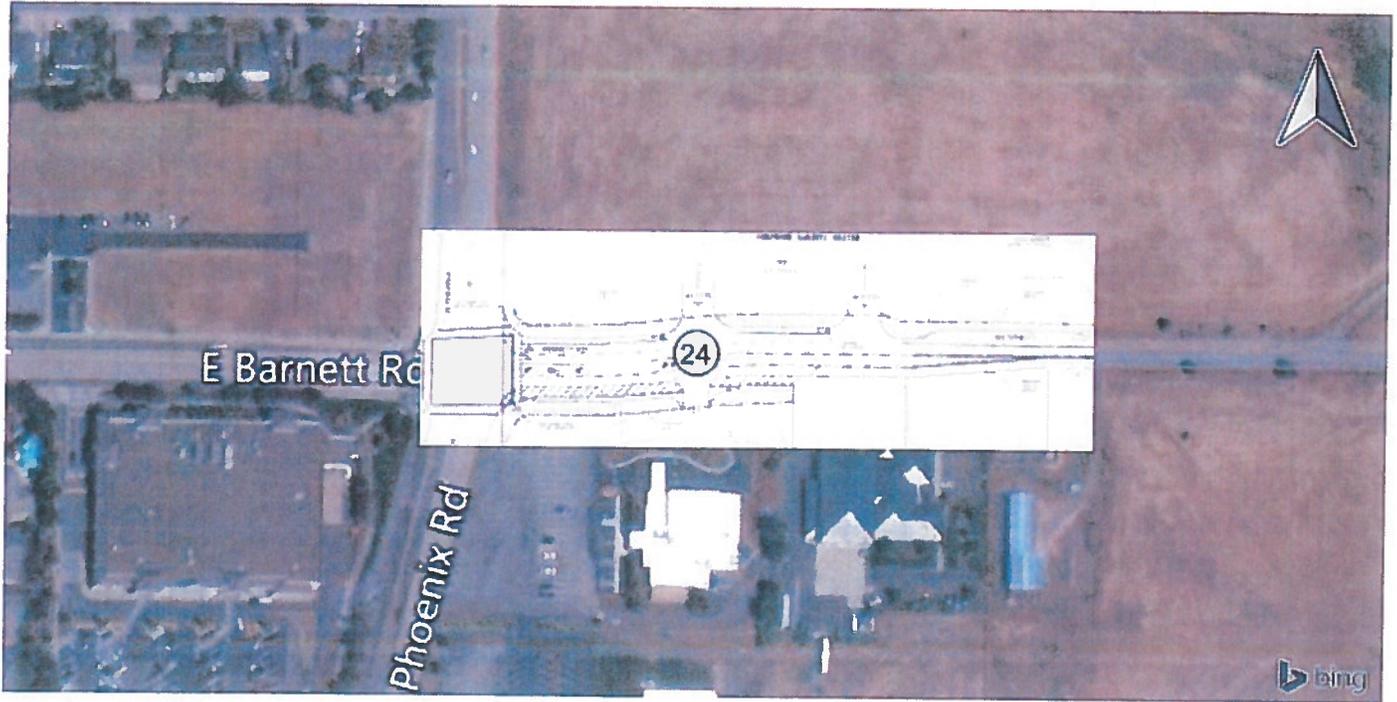
Report Figure 3c: Traffic Volume - Net New Site Trips



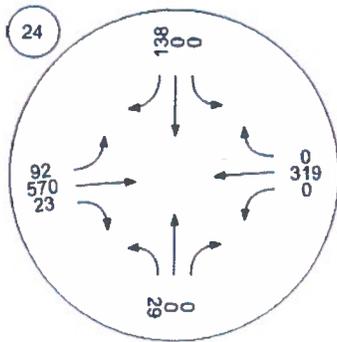
Site Drive H@Barnett Rd



Report Figure 3e: Traffic Volume - Future Total Volume

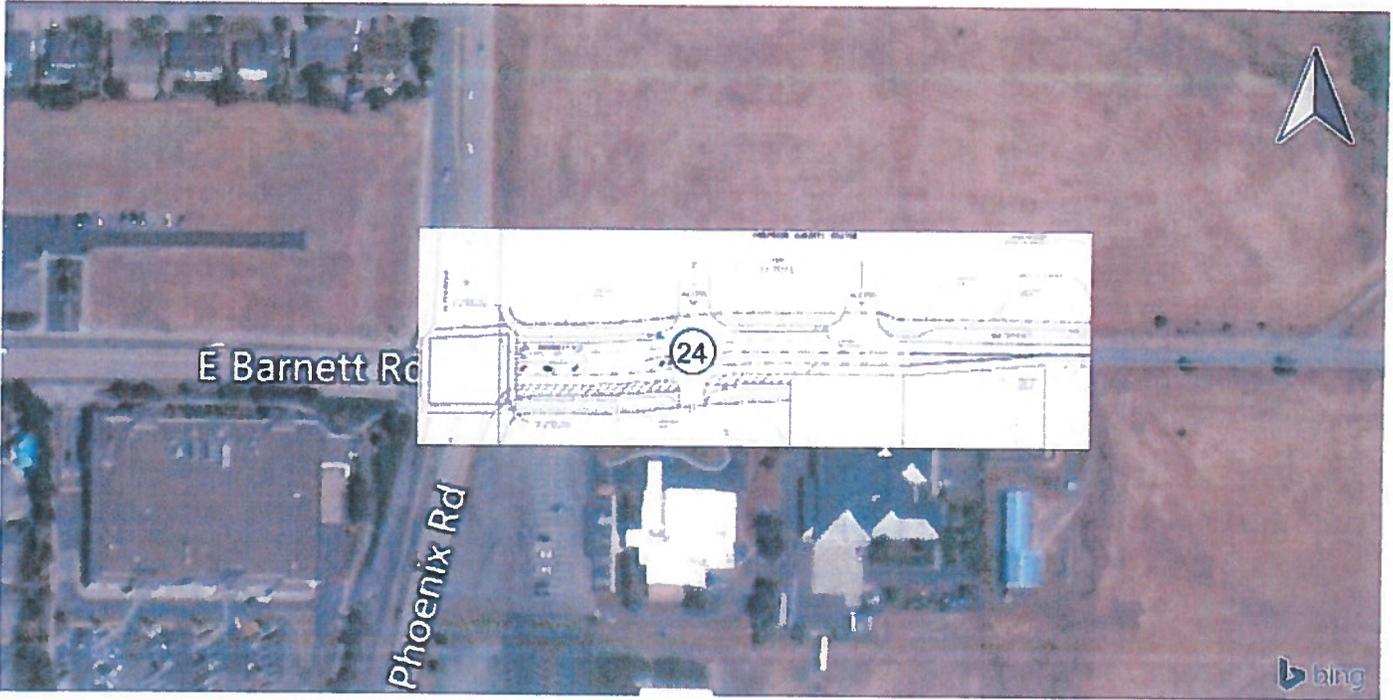


Site Drive H@Barnett Rd





Report Figure 4: Traffic Conditions



Site Drive H@Barnett Rd

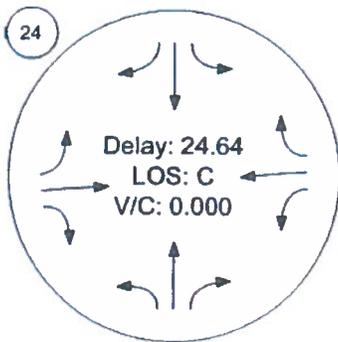


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Scenario 19 2020 Build

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Barnett 082417.pdf

8/24/2017

Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
29	Site Drive I/Fire@ Barnett	Two-way stop	HCM 2010	NB Left	0.012	19.5	C

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.



Intersection Level Of Service Report
Intersection 29: Site Drive I/Fire@ Barnett

Control Type: Two-way stop
Analysis Method: HCM 2010
Analysis Period: 15 minutes

Delay (sec / veh): 19.5
Level Of Service: C
Volume to Capacity (v/c): 0.012

Intersection Setup

Name	Fire Sta			Site Drive I			Barnett			Barnett		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration	+			+			T			T		
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Fire Sta			Site Drive I			Barnett			Barnett		
Base Volume Input [veh/h]	3	0	0	0	0	0	0	18	0	0	7	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.05	1.05	1.05	1.05	1.05	1.05	1.05	1.05	1.05	1.05	1.05	1.05
In-Process Volume [veh/h]	0	0	0	0	0	0	0	510	0	0	238	0
Site-Generated Trips [veh/h]	0	0	0	0	0	60	41	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	3	0	0	0	0	60	41	529	0	0	245	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	1	0	0	0	0	15	10	132	0	0	61	0
Total Analysis Volume [veh/h]	3	0	0	0	0	60	41	529	0	0	245	0
Pedestrian Volume [ped/h]	0			0			0			0		



Intersection Settings

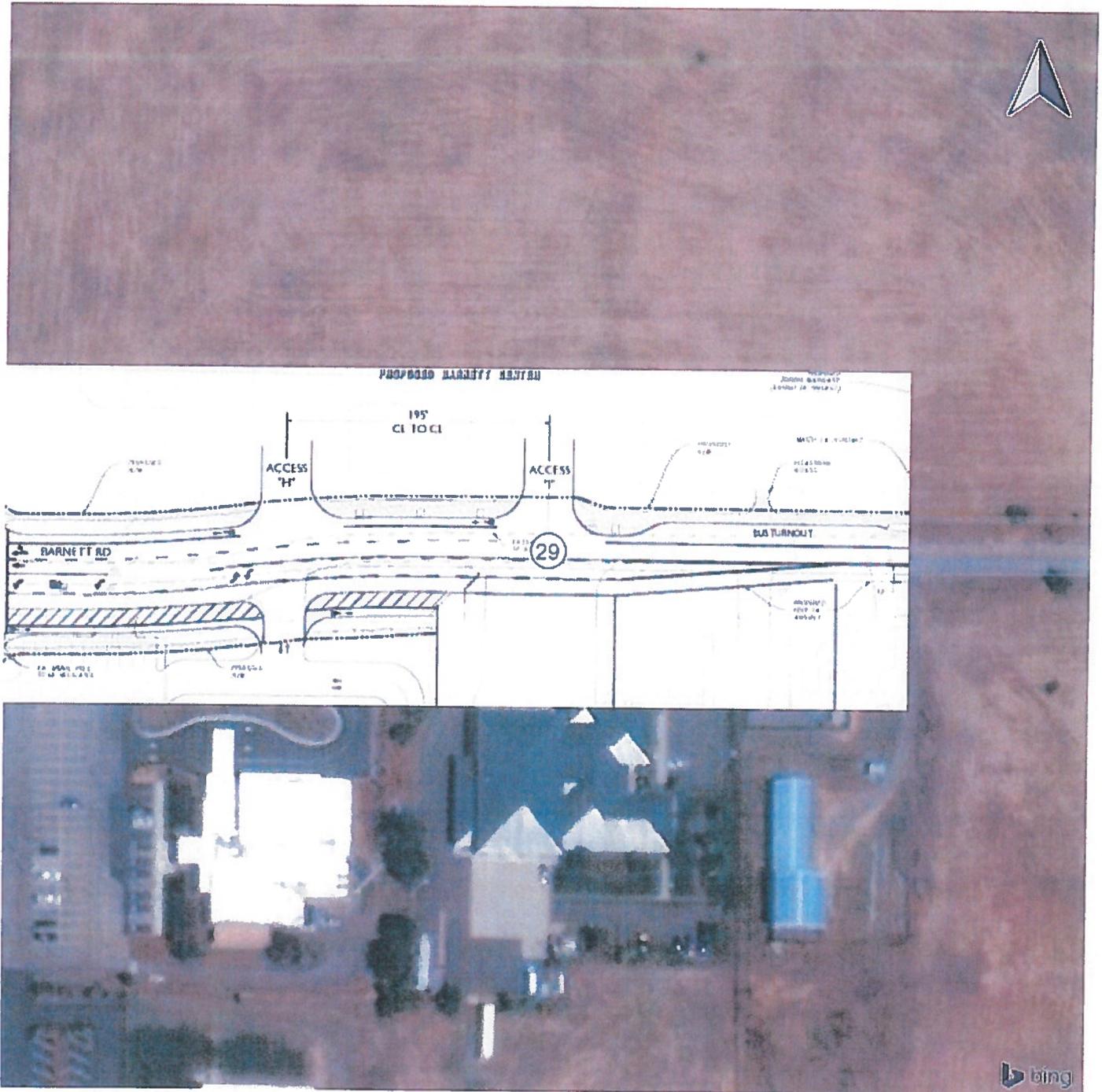
Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]	0	0	0	0
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median	0	0	0	0

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.01	0.00	0.00	0.00	0.00	0.08	0.03	0.01	0.02	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	19.53	17.76	11.72	18.64	17.96	9.91	7.81	4.35	6.00	8.47	8.00	6.00
Movement LOS	C	C	B	C	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.04	0.04	0.04	0.24	0.24	0.24	0.10	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.91	0.91	0.91	6.12	6.12	6.12	2.40	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	19.53			9.91			0.56			0.00		
Approach LOS	C			A			A			A		
d_I, Intersection Delay [s/veh]	1.11											
Intersection LOS	C											

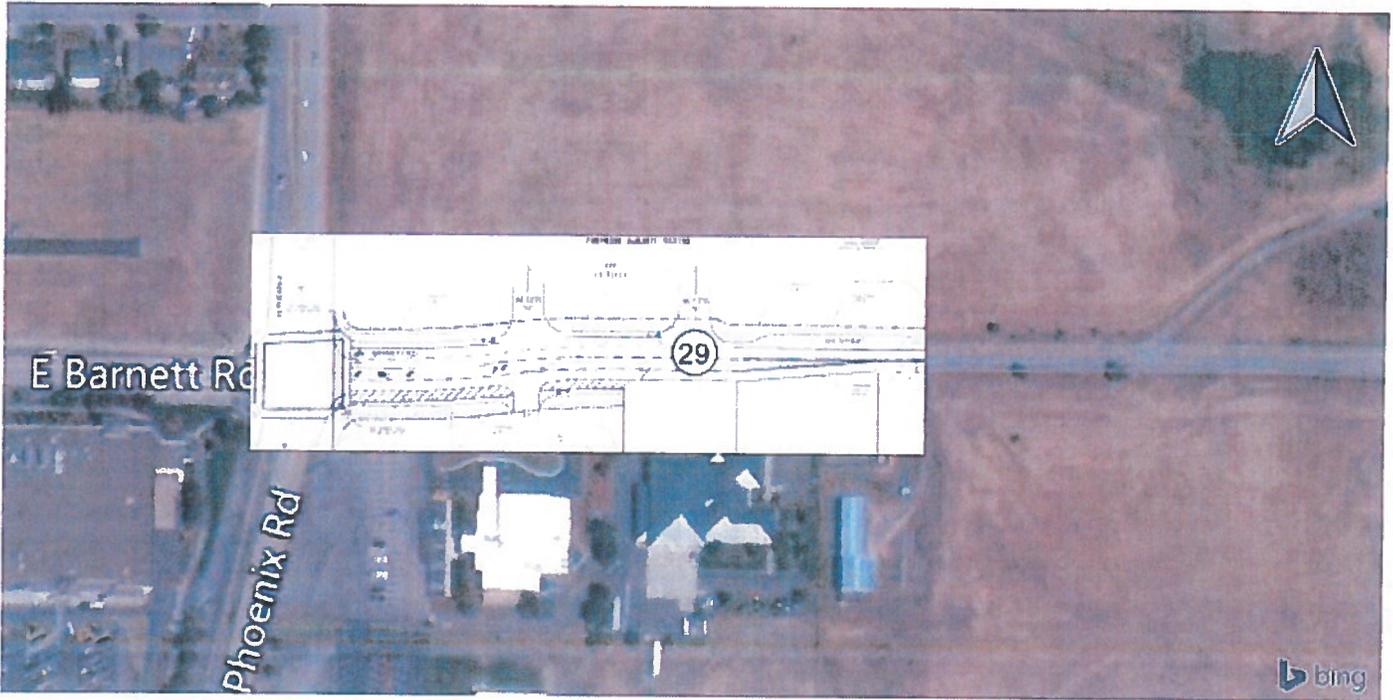


Report Figure 1: Study Intersections

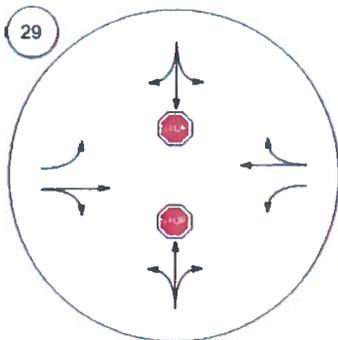




Report Figure 2: Lane Configuration and Traffic Control

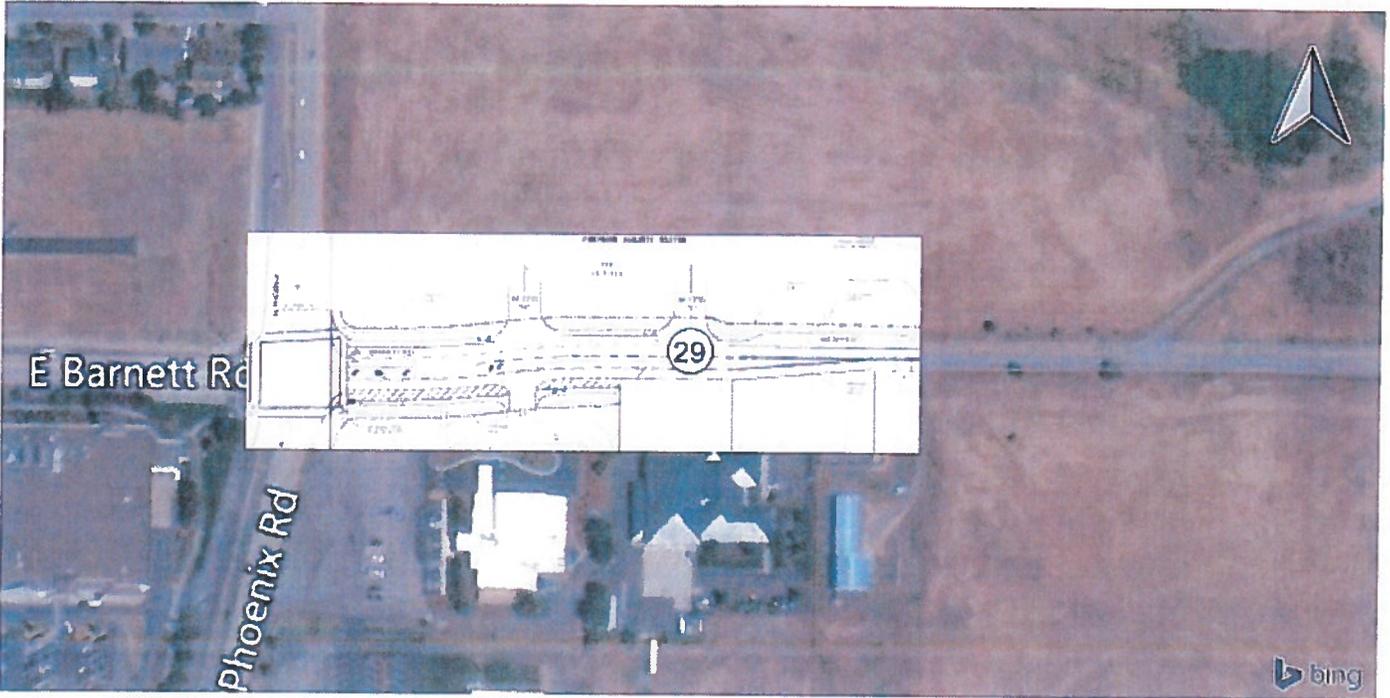


Site Drive I/Fire@ Barnett

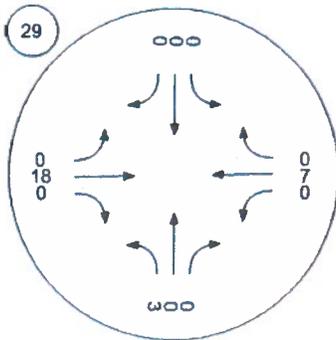




Report Figure 3a: Traffic Volume - Base Volume

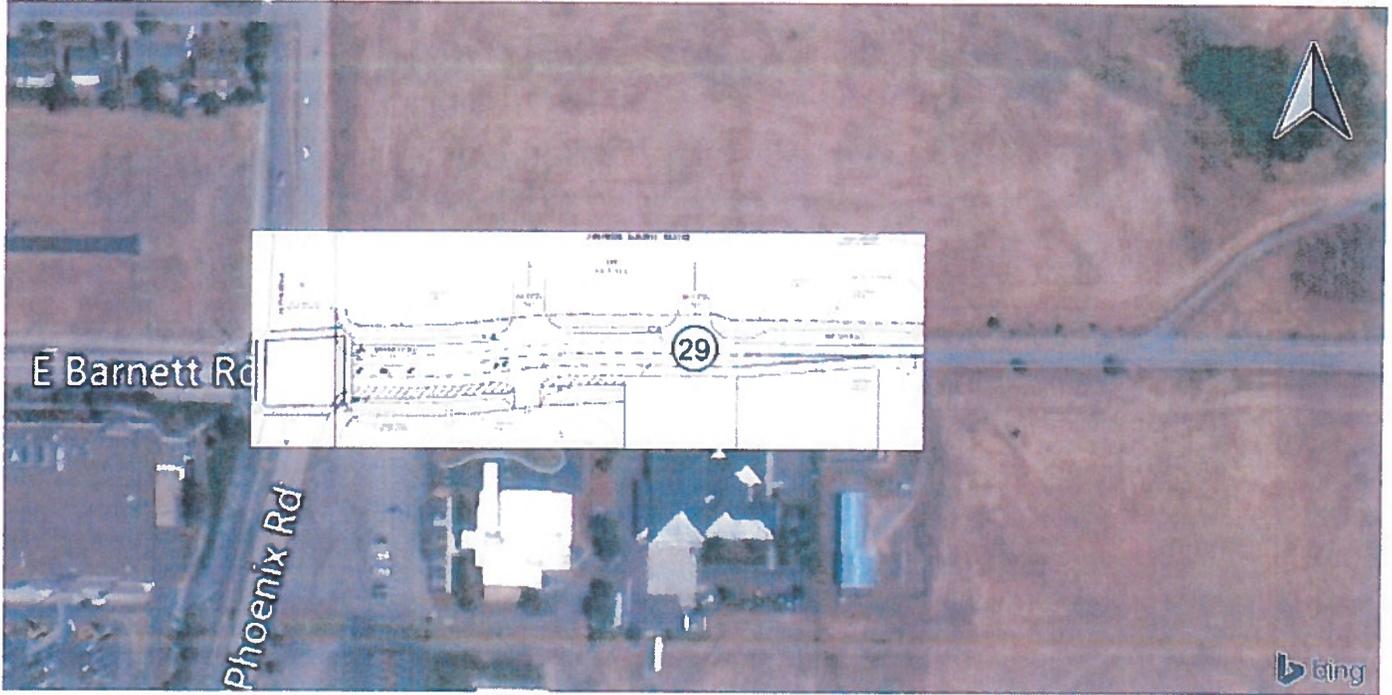


Site Drive I/Fire@ Barnett

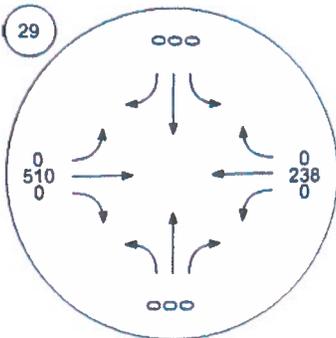




Report Figure 3b: Traffic Volume - In-Process Volume

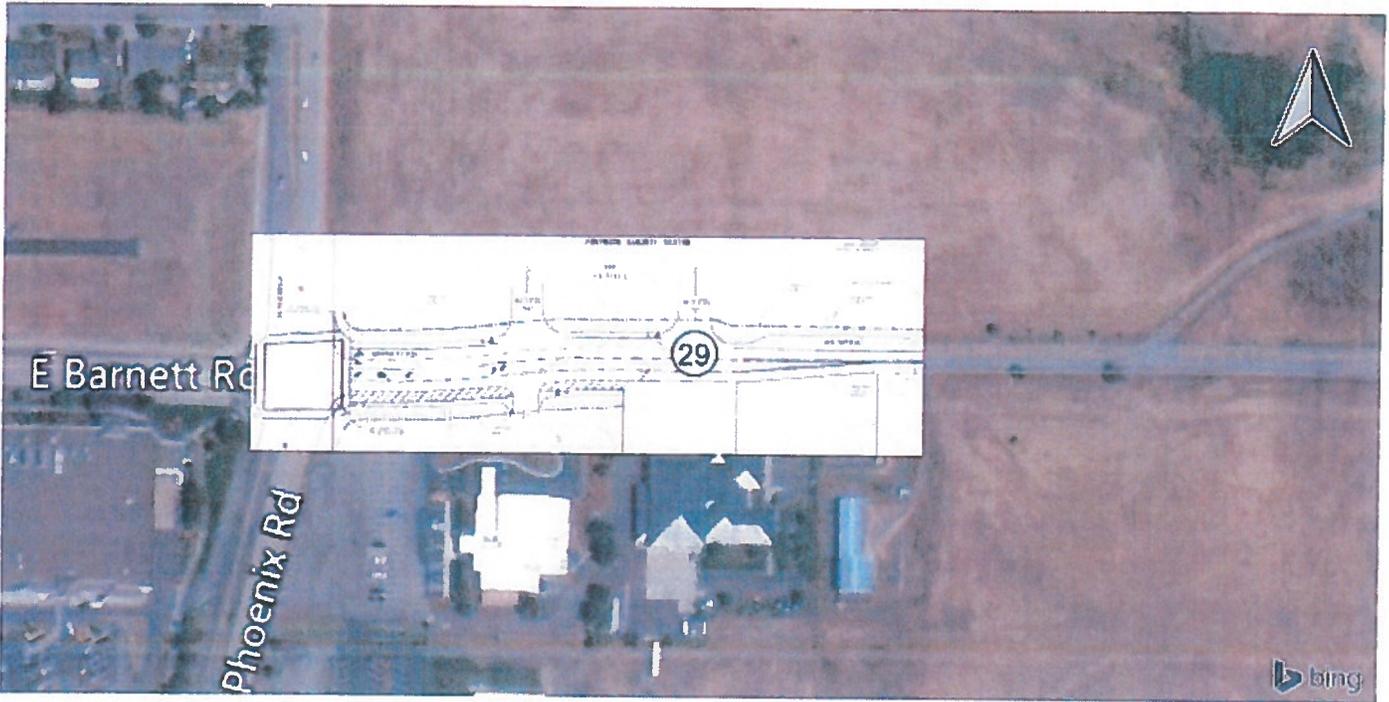


Site Drive I/Fire@ Barnett

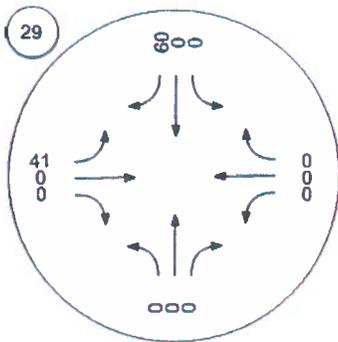




Report Figure 3c: Traffic Volume - Net New Site Trips

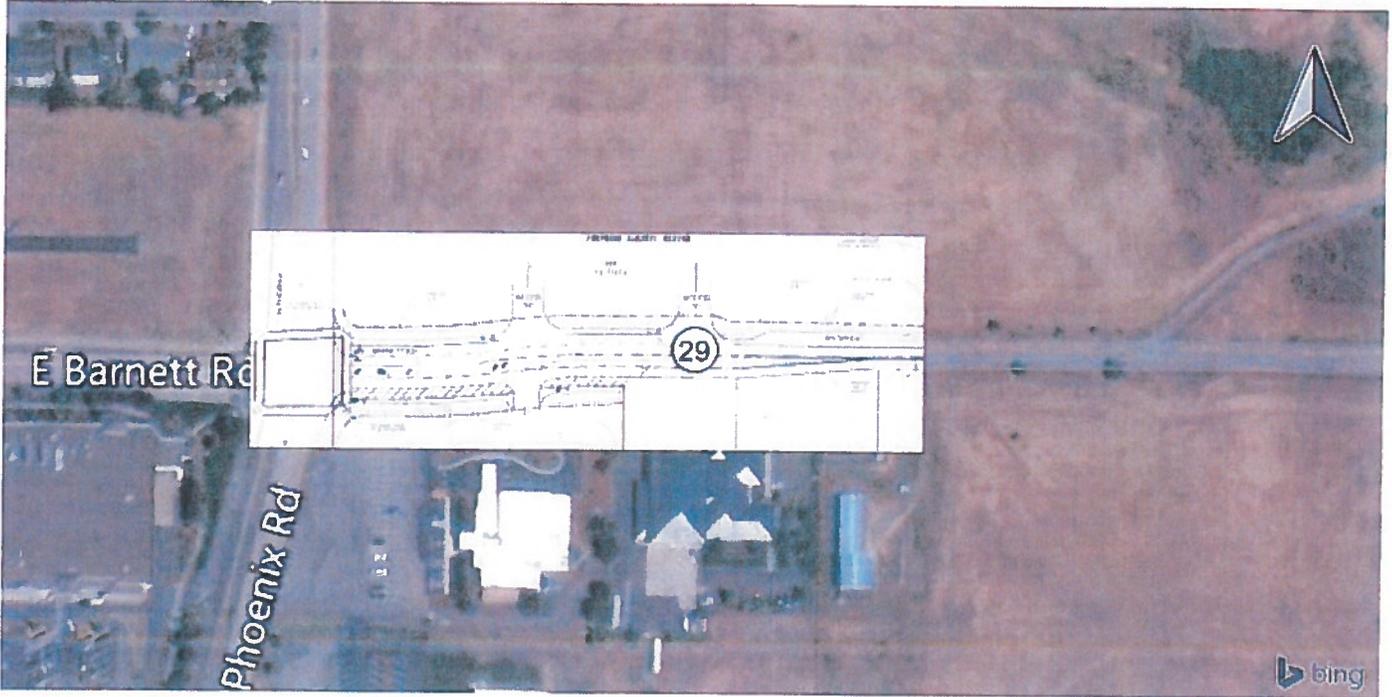


Site Drive I/Fire@ Barnett

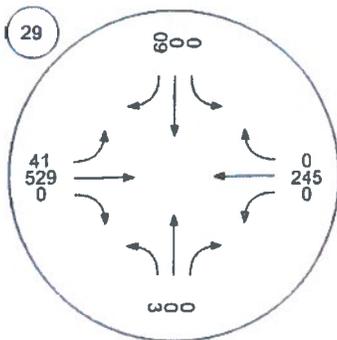




Report Figure 3e: Traffic Volume - Future Total Volume

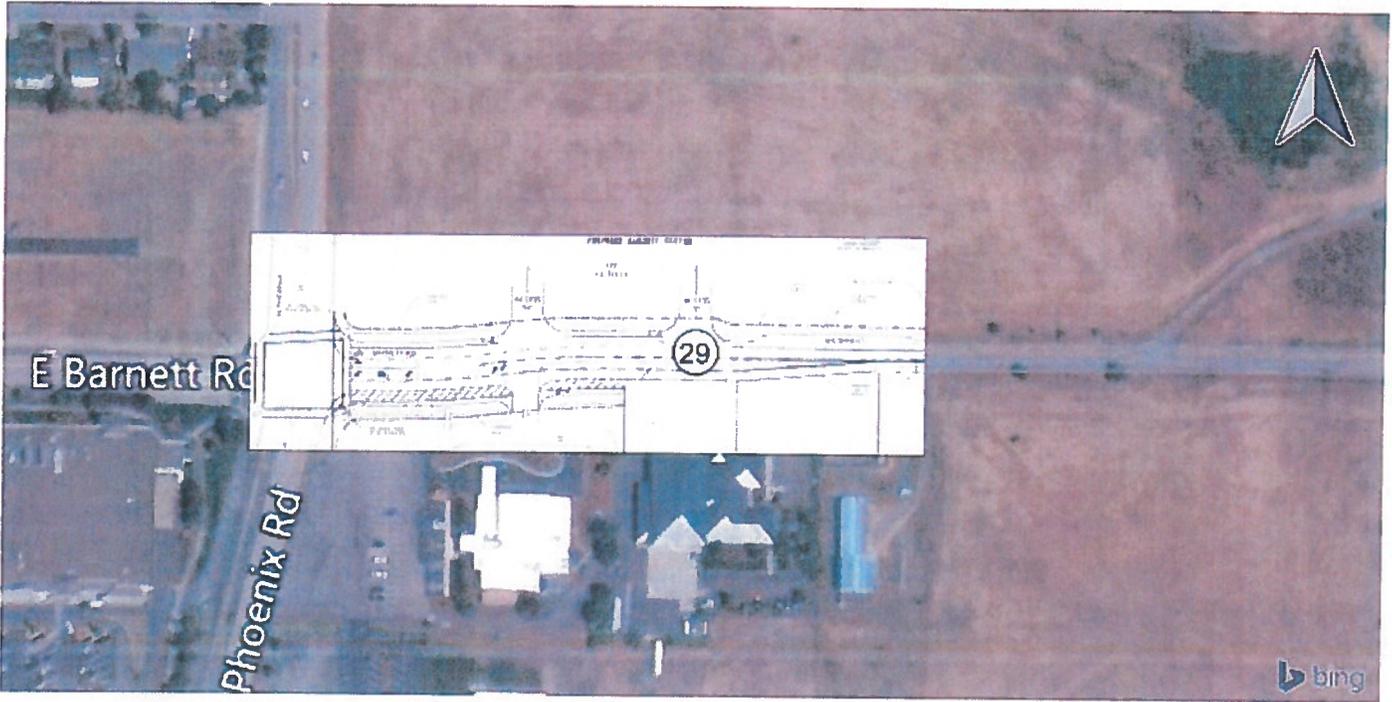


Site Drive I/Fire@ Barnett





Report Figure 4: Traffic Conditions



Site Drive I/Fire@ Barnett

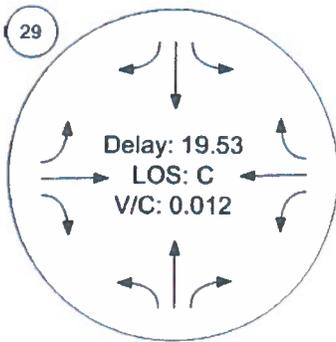


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Intersection Analysis Summary

ID	Intersection Name	Control Type	Method	Worst Mvmt	V/C	Delay (s/veh)	LOS
29	Site Drive I/Fire@ Barnett	Two-way stop	HCM 2010	NB Left	0.012	19.6	C

V/C, Delay, LOS: For two-way stop, these values are taken from the movement with the worst (highest) delay value. for all other control types, they are taken for the whole intersection.



Intersection Level Of Service Report
Intersection 29: Site Drive I/Fire@ Barnett

Control Type: Two-way stop
 Analysis Method: HCM 2010
 Analysis Period: 15 minutes

Delay (sec / veh): 19.6
 Level Of Service: C
 Volume to Capacity (v/c): 0.012

Intersection Setup

Name	Fire Sta			Site Drive I			Barnett			Barnett		
Approach	Northbound			Southbound			Eastbound			Westbound		
Lane Configuration	+			+			T			T		
Turning Movement	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Lane Width [ft]	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00	12.00
No. of Lanes in Pocket	0	0	0	0	0	0	0	0	0	1	0	0
Pocket Length [ft]	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00	100.00
Speed [mph]	25.00			25.00			25.00			25.00		
Grade [%]	0.00			0.00			0.00			0.00		
Crosswalk	No			No			No			No		

Volumes

Name	Fire Sta			Site Drive I			Barnett			Barnett		
Base Volume Input [veh/h]	3	0	0	0	0	0	0	18	0	0	7	0
Base Volume Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Heavy Vehicles Percentage [%]	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00	2.00
Growth Rate	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08	1.08
In-Process Volume [veh/h]	0	0	0	0	0	0	0	510	0	0	238	0
Site-Generated Trips [veh/h]	0	0	0	0	0	60	41	0	0	0	0	0
Diverted Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Pass-by Trips [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Existing Site Adjustment Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Other Volume [veh/h]	0	0	0	0	0	0	0	0	0	0	0	0
Total Hourly Volume [veh/h]	3	0	0	0	0	60	41	529	0	0	246	0
Peak Hour Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Other Adjustment Factor	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000	1.0000
Total 15-Minute Volume [veh/h]	1	0	0	0	0	15	10	132	0	0	62	0
Total Analysis Volume [veh/h]	3	0	0	0	0	60	41	529	0	0	246	0
Pedestrian Volume [ped/h]	0			0			0			0		



Intersection Settings

Priority Scheme	Stop	Stop	Free	Free
Flared Lane	No	No		
Storage Area [veh]				
Two-Stage Gap Acceptance	No	No		
Number of Storage Spaces in Median				

Movement, Approach, & Intersection Results

V/C, Movement V/C Ratio	0.01	0.00	0.00	0.00	0.00	0.08	0.03	0.04	0.00	0.00	0.00	0.00
d_M, Delay for Movement [s/veh]	19.55	17.78	11.72	18.66	17.98	9.91	7.81	8.47	8.47	8.47	8.47	8.47
Movement LOS	C	C	B	C	C	A	A	A	A	A	A	A
95th-Percentile Queue Length [veh]	0.04	0.04	0.04	0.24	0.24	0.24	0.10	0.00	0.00	0.00	0.00	0.00
95th-Percentile Queue Length [ft]	0.91	0.91	0.91	6.12	6.12	6.12	2.40	0.00	0.00	0.00	0.00	0.00
d_A, Approach Delay [s/veh]	19.55			9.91			0.56			0.00		
Approach LOS	C			A			A			A		
d_I, Intersection Delay [s/veh]	1.11											
Intersection LOS	C											



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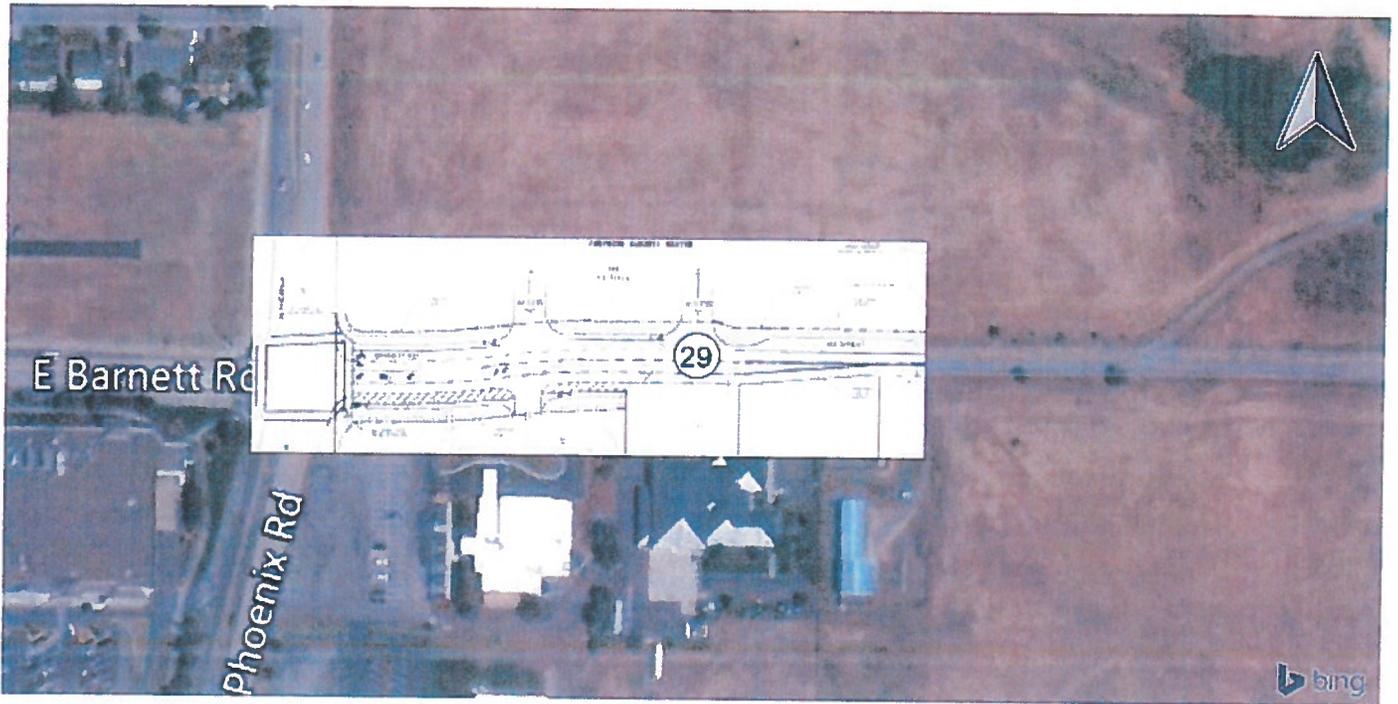
Trip Generation summary

Added Trips

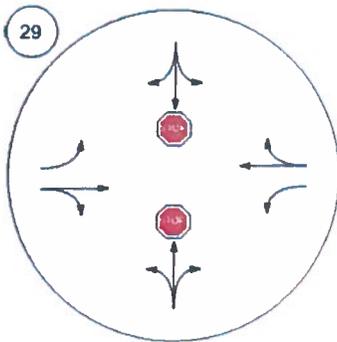
Zone ID: Name	Land Use variables	Code	Ind. Var.	Rate	Quantity	% In	% Out	Trips In	Trips Out	Total Trips	% of Total Trips
1: zone	Mixed			1.000	34.000	48.00	52.00	16	18	34	8.06
2: zone	Shopping	820		1.000	388.000	48.00	52.00	186	202	388	91.94
Added Trips Total								202	220	422	100.00



Report Figure 1: Lane Configuration and Traffic Control

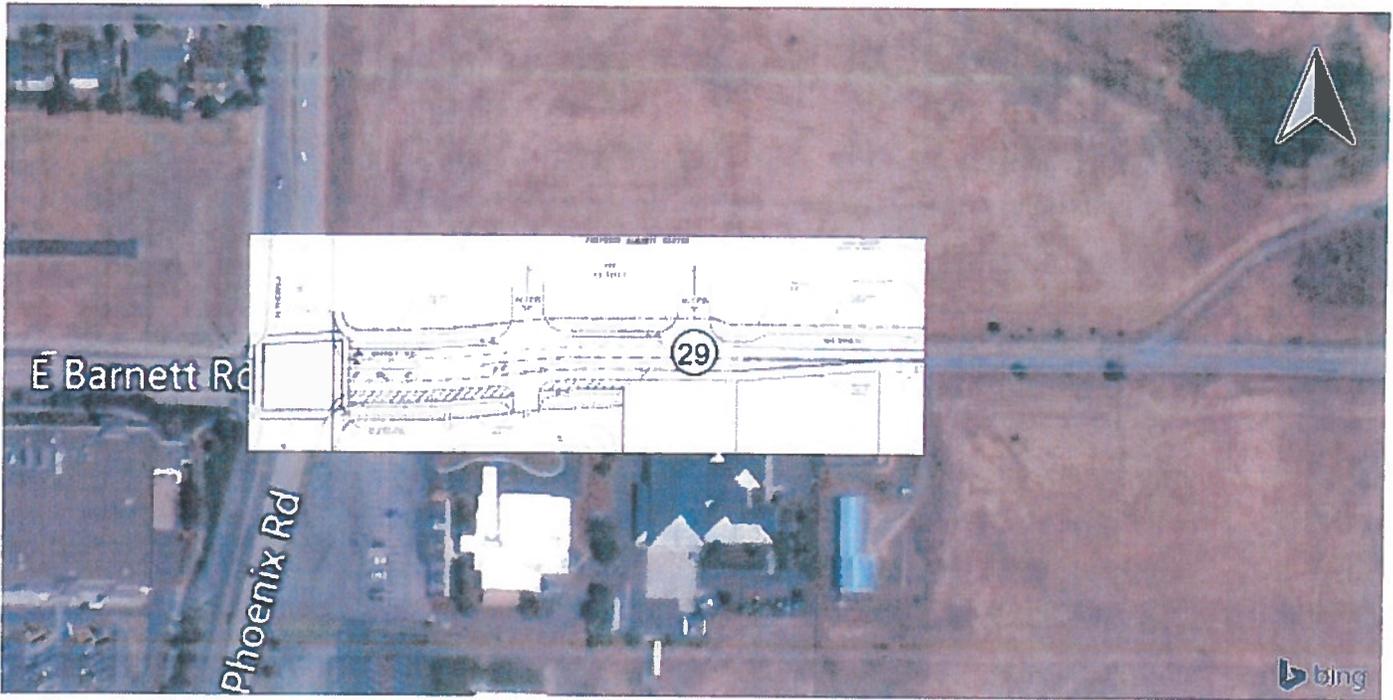


Site Drive I/Fire@ Barnett

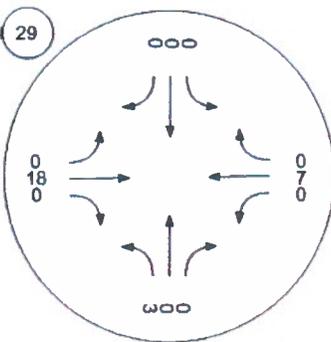




Report Figure 2a: Traffic Volume - Base Volume

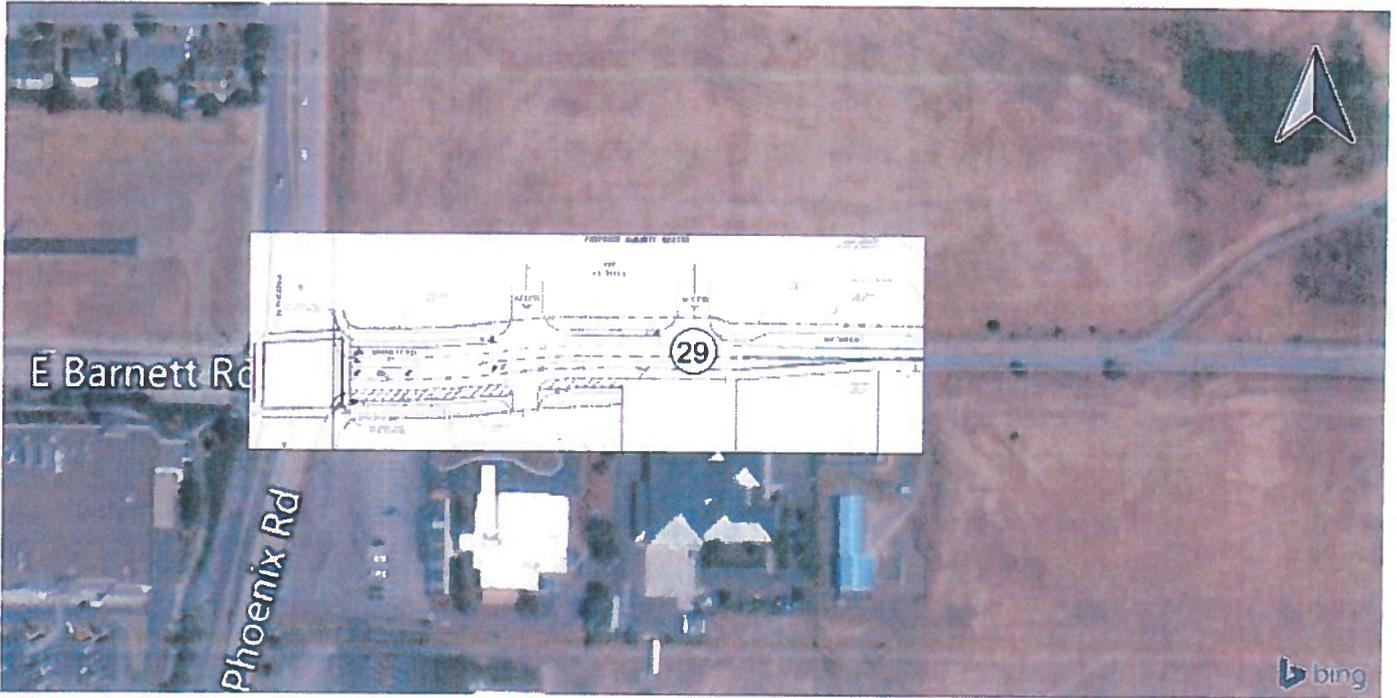


Site Drive I/Fire@ Barnett

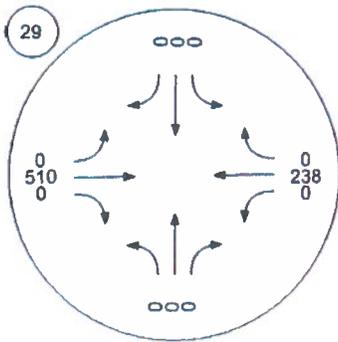




Report Figure 2b: Traffic Volume - In-Process Volume

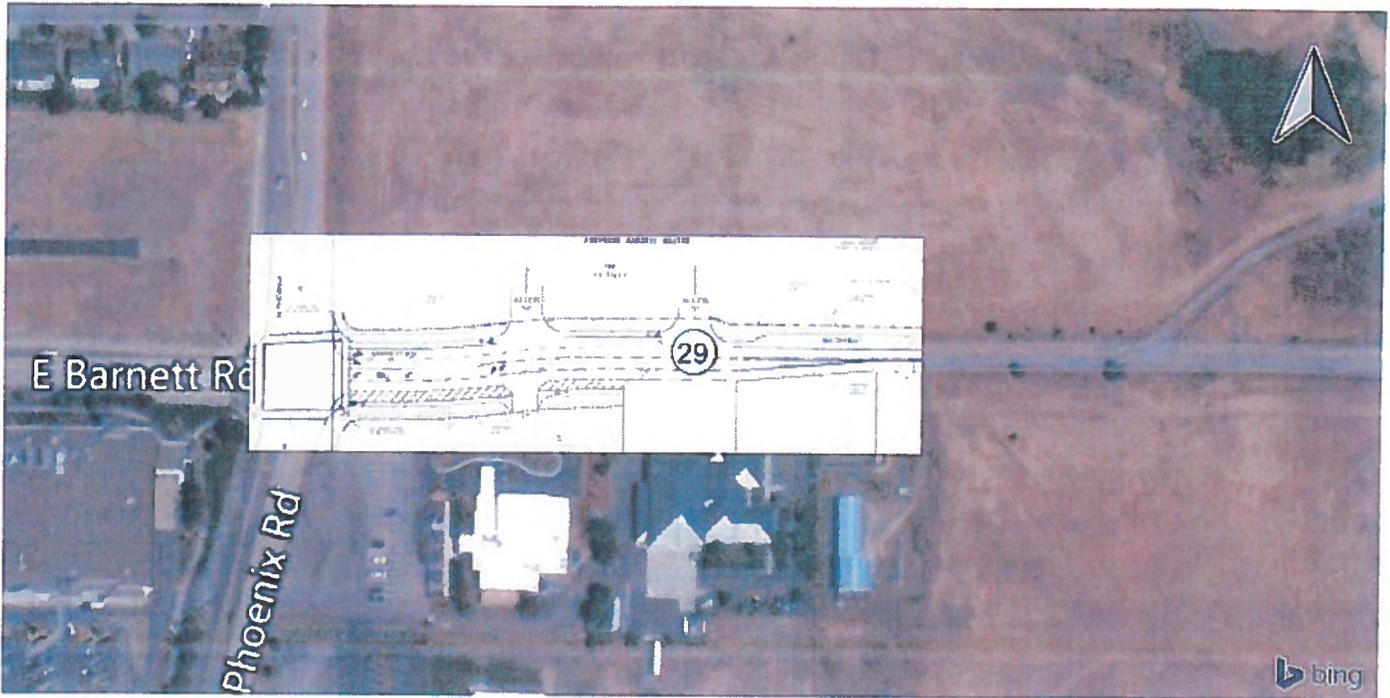


Site Drive I/Fire@ Barnett

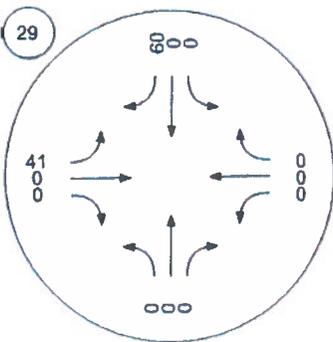




Report Figure 2c: Traffic Volume - Net New Site Trips

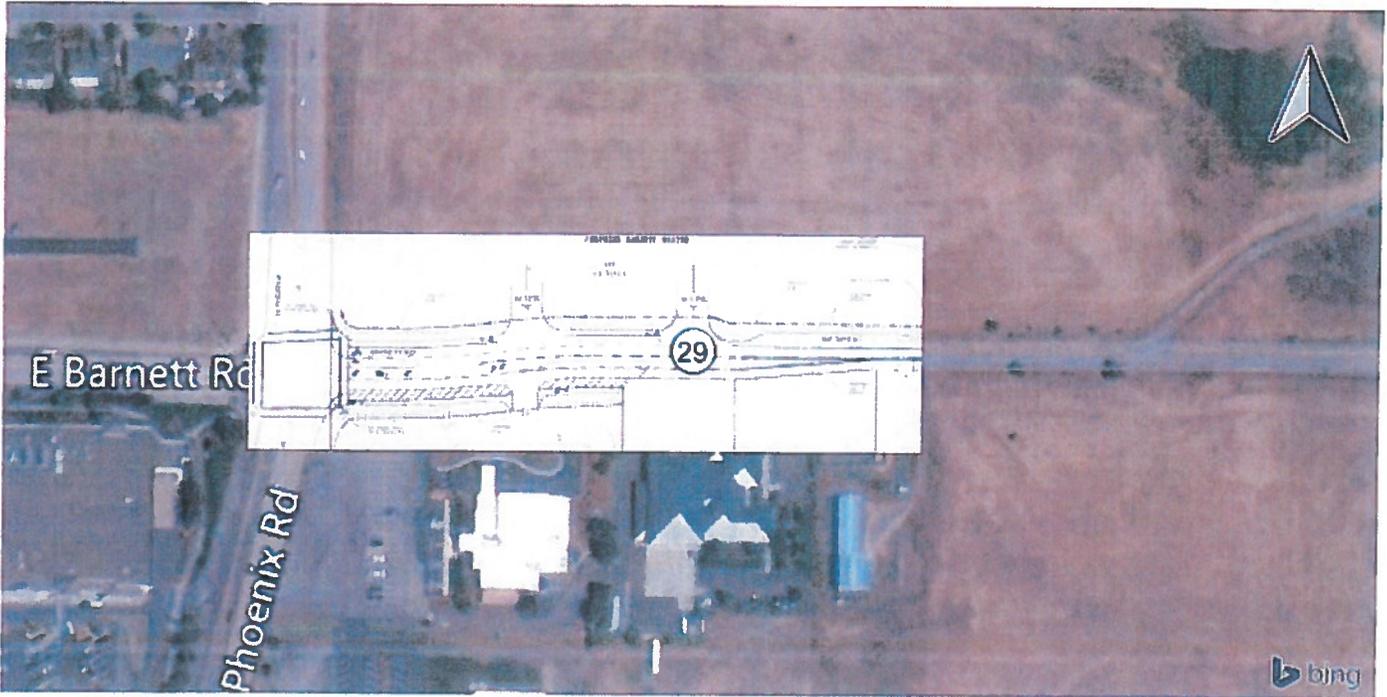


Site Drive I/Fire@ Barnett

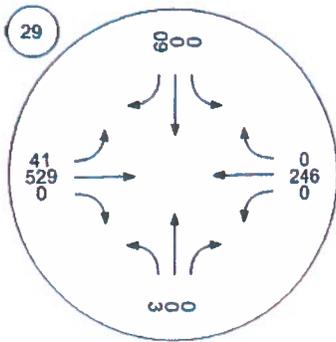




Report Figure 2e: Traffic Volume - Future Total Volume



Site Drive I/Fire@ Barnett



REPLY TO MEDFORD COMMENTS

Appendix 6 Comment Letters from City of Medford August 1 and August 8, 2017



Continuous Improvement Customer Service

**PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION**

**CITY OF MEDFORD
200 SOUTH IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us**

**TELEPHONE (541) 774-2100
FAX (541) 774-2552**

August 1, 2017

**JRH Transportation Engineering
3672 River Pointe Dr.
Eugene, OR. 97408**

We have received your revised Traffic Impact Analysis (TIA) report for the Southeast Medford Neighborhood Commercial Center, dated October 14, 2016, and have the following comments:

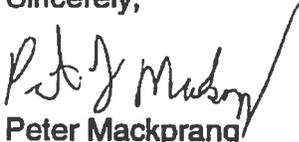
- 1. Provide operational analysis for driveway H. The driveway is located within the transition area for the westbound left turn lane at the intersection of Barnett and N. Phoenix. Traffic movements in preparation for the westbound left turn will make eastbound left turns very difficult.**
- 2. For all build scenarios, the development should be conditioned to construct a southbound left turn lane at N. Phoenix @ Michael Park including a modification to the existing median, not a two way turn lane per page 41.**
- 3. Page 3 – mitigation; Clarify mitigation at Barnett and N. Phoenix; east and westbound left turns are already protected permissive.**
- 4. The revised trip cap for the south side is calculated assuming that the site is already generating the maximum trips allowed under the existing trip cap but there is no data provided to substantiate this. We recommend calculating the existing trip generation of the site and adding it to the new trips calculated in table 5 as a reasonable method of establishing the revised trip cap.**
- 5. The TIA analysis uses 431 PM peak hour trips (per table 5) instead of 624 PM peak hour trips (per the stipulation letter in Appendix 2) for the north side of the development. Provide a stipulation that caps the number of trips allowed on the north side of the development to 431 PM peak hour trips.**
- 6. Page 13 – The report should include a statement as to which hour was determined to be the peak and used for calculations. Note that the peak hour used should be consistent with the requirements of Medford Municipal Code section 10.461 (6) (b).**
- 7. Page 23, the last sentence of the first paragraph states that the “City requires...”. This is not accurate as the city does not have a LOS standard for driveways.**
- 8. Page 195 and 259**
 - a. Int B @ Michael Park, check, this should be “Int E” according to the narrative on page 8 and the list on page 11. Check geometry; there should be no stop signs on Michael Park and there should be a stop sign on the driveway.**

- b. Site Driveway H @ Barnett Rd, check geometry, Barnett should have two thru lanes and a center turn lane westbound according to the Masterplan.
 - c. Site Drive I/Fire @ Barnett, check geometry, Place stops on driveways.
9. Page 194 and 258,
- a. Int A @ Michael Park, There is no southbound approach, the eastbound approach should not be stop controlled and should be shared right and thru, the westbound approach should be shared left and thru, the northbound approach including left thru and right is stop controlled.
 - b. N. Phoenix @ Michael Park, we recommend that the westbound approach be configured with a shared left and thru and a separate right turn lane.

Provide responses to the above comments so that we can continue to review the traffic impact analysis.

If you have questions, please contact me at (541) 774-2121.

Sincerely,



Peter Mackprang
Associate Traffic Engineer

CC: Jay Harland
4494 Brownridge, Suite 101
Medford, OR. 97504



Continuous Improvement Customer Service

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

CITY OF MEDFORD
200 SOUTH IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

August 8, 2017

JRH Transportation Engineering
3672 River Pointe Dr.
Eugene, OR 97408

During our telephone conversation on Friday Aug. 4, 2017 regarding our letter dated August 1, 2017, you raised two questions that I will try to answer.

Comment 1; Provide operational analysis for driveway H...Drive way H is so close to the intersection of Barnett and N. Phoenix that eastbound and westbound traffic will be transitioning to make left turns from opposite directions in the same physical space. The analysis should include a safety component looking at the common space issue and a safety and efficiency component for traffic waiting in the through lanes when the space is occupied by another vehicle. We think the driveway should be moved east to alleviate this conflict.

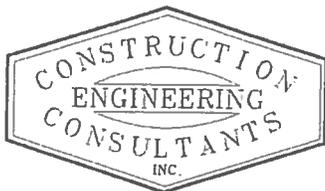
Comment 2; For all build scenarios, the development should be conditioned to construct a southbound left turn lane at N. Phoenix @ Michael Park including a modification to the existing median, not a two way turn lane per page 41. This facility should be a full left turn pocket designed to ODOT standards to allow for deceleration and queuing owing to the high speed of N. Phoenix Rd. A southbound left turn lane built within the space provided between the street intersection and existing median islands to the north does not appear to provide space for deceleration. Additionally if eastbound left turn traffic uses it as a refuge to turn north onto N. Phoenix, they would be in conflict with the southbound left turns, which poses a safety concern.

Please address these and the other items in the subject letter and resubmit for our consideration.

If you have questions, please contact me at (541) 774-2121.

Sincerely,

Peter Mackprang
Associate Traffic Engineer



August 1, 2016

CSA Planning, Ltd.
Attn: Raul Woerner
4497 Brownridge, Suite 101
Medford, Oregon 97504

RECEIVED
DEC 18 2017
PLANNING DEPT.

RE: South East Plan 7A Property Sanitary Sewer Study

Dear Mr. Woerner,

The purpose of this study is to analyze the South East Plan 7A property and the potential capacity impacts to the existing sewer system as a result of the proposed comprehensive plan amendment from Urban Residential to Service Commercial and zone change from SFR-00 to C-C, respectively. The study area is located at the northeast of the intersection of Barnett Road and North Phoenix Road. This 8 acre site is located in Map 371W27; Tax Lot 1605. The City Utility Engineer has identified existing sanitary sewer pipe section S371W28DS0139 in Barnett Road as a potential capacity issue, see enclosed map.

This sewer analysis was performed using the current City of Medford Sanitary Sewer Master Plan (2005) and City supplied data. The study was conducted on the existing sanitary sewer system downstream of the SE Plan 7A property. Flows generated are based on full build-out of the flow basin (Per City of Medford Full Build-Out Flow Calculations). As specified in the City's Sanitary Sewer Master Plan, commercial properties are to use a flow factor of 1700gal/acre/day to determine the anticipated sewer flows. Based on the described parameters it is expected that the zone change will generate approximately 15 gallons per minute of additional flow, see enclosed Flow Summary Estimate.

A sewer model analysis was run with the "Pre-Zone Change" flows and the "Post-Zone Change" flows. The design capacity of the pipe in question is 1625.0gpm. The "Pre-Zone Change" basin flow at buildout is anticipated to be 2221.6gpm and the "Post-Zone Change" basin flow at buildout is anticipated to be 2236.6gpm. This model analysis shows that the system is already undersized even without the zone change. Additionally, this pipe section has been identified in the current sewer master plans as capital improvement project East Barnett 6, see enclosed Table 1-9 of the 2005 Master Plan.

With this analysis it has been determined that the existing 12" pipe section S371W28DS0139 should be upgraded for future buildout flows of 2236.6gpm. The 2005 Master Plan identifies this pipe section as being replaced with an 18" pipe giving a capacity of 2341.5gpm. See enclosed Appendix B, page 45 of 86 of the 2005 Master Plan.

Options for increasing the system capacity are:

- 1) The Developer would upgrade the pipe in this section to increase the capacity for future buildout conditions. The Developer should receive SDC credits for upgrading the pipe.
- 2) The City can create a larger capital improvement project that will increase the capacity for the entire basin system as identified in the master plan as project East Barnett 6. The Developer would participate in the costs to increase the pipe section S371W28DS0139 and pay SDC fees.

Please see the enclosed calculations, map, and reference material from the City of Medford Sanitary Sewer Master Plan (2005).

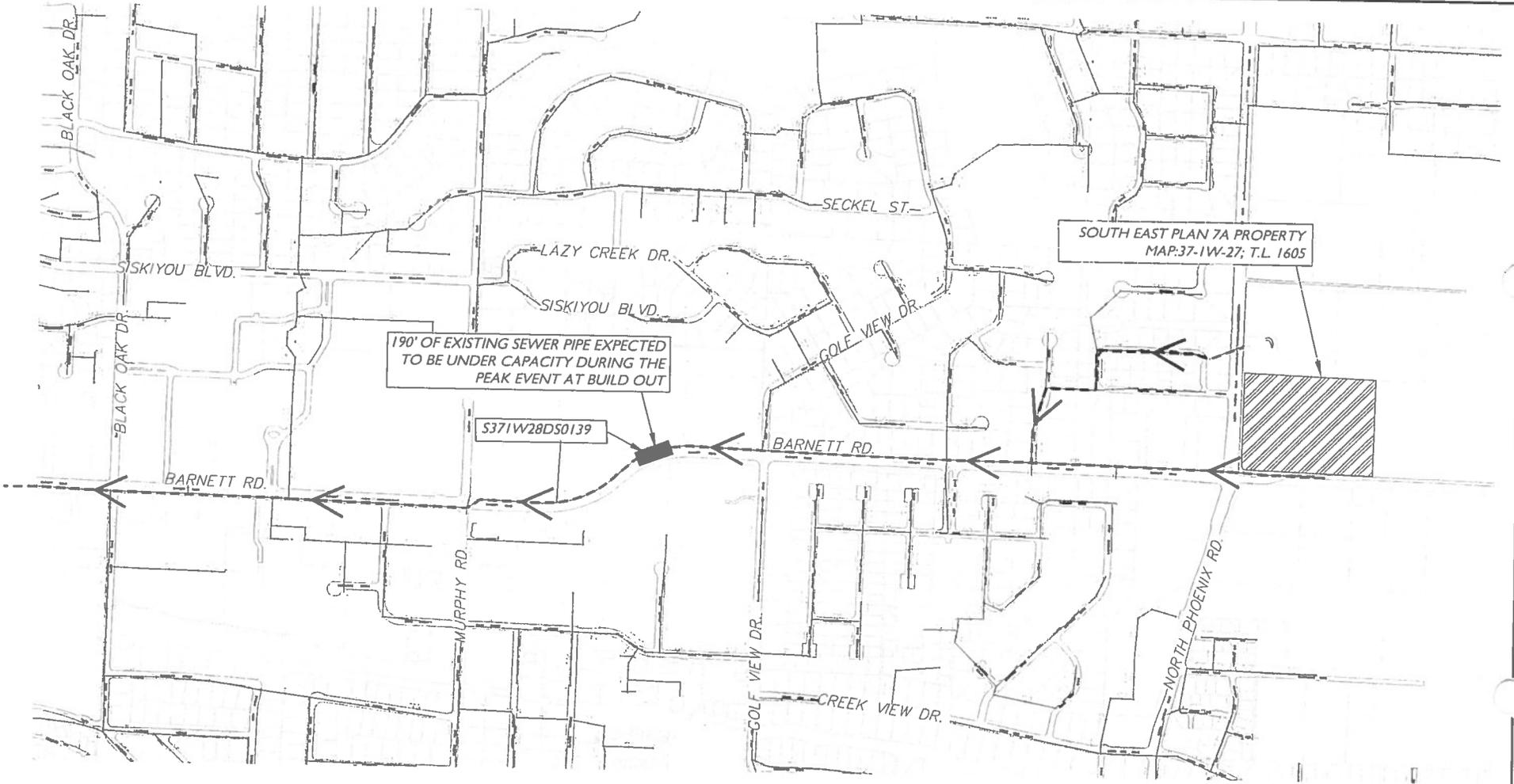
Sincerely,

Tony Bakke, P.E.
Construction Engineering Consultants, Inc.



EXPIRES: 12/31/17

CITY OF MEDFORD
EXHIBIT # B
File # ZC-17-168



S.S. FLOW ROUTE

NORTH
SCALE: 1"=500'



P.O. BOX 1274 S.W. 48TH AVE. MIAMI, FL 33155
TEL: (305) 555-5555 FAX: (305) 555-2000

DRAWN BY: BSH	DATE: 06/16
CHECKED BY: AMB	DATE: 06/16
APPROVED RVS:	DATE:
APPROVED:	DATE:
APPROVED:	DATE:
APPROVED:	DATE:

NO	REVISION	DATE	BY

CONSTRUCTION ENGINEERING CONSULTANTS	
SOUTH EAST PLAN 7A EXHIBIT	
PROJECT NO:	
DRAWING NO:	
REV. CITY ENGINEER:	DATE:
NAME: SOUTH EAST 7A SS MAP/DWG PROJ: 16-47 PLOT DATE: 07-28-16	

Flow Summary Estimate for Sewer

Land Use Unit Flow Factors

Land Use	Flow
Commercial	1,700 gpad
Industrial	1,000 gpad
*MFR	180 gpd
*SFR	200 gpd
*Inflow & Infiltration	1000 gpad

These factors are from the 2005 City of Medford Sanitary Sewer Master Plan, Ch.2 Table 2-5

* Note: Adjusted or provided by the City Utility Engineer

- Gallons per acre per day (gpad)
- Gallons per day per dwelling unit (gpd)
- SFR Population 2.5 people per Dwelling
- MFR Population 2.2 people per Dwelling

SE Plan 7A Property Proposed Conditions

Estimated Flows -Commercial

Basin	GPD	Acres	Base Flow	Flow (cfs)	Flow (GPM)
J	1700	8.00	0.02	0.033	15.00

Flows calculated from COM for Future Build Out Applied @ Maaike Drive	2.130	956.01
Flows calculated from COM for Future Build Out Applied @ Juanipero Way	0.670	300.72

**Table 1-9
Project Definition Growth Related Hydraulic Projects**

Facility ID	Diameter, inches	Depth, feet	Length, feet	Existing Reqmts. inches	Future Reqmts. inches	Future Hyd. Projects, Cost	Project Name	Project Priority	Future CIP, \$
S371W28DS0084	8	6.51	263.87	8	15	\$ 45,386	Crystal Mt 1	C3	\$ 132,803
S371W28DS0085	8	2.82	112.55	8	15	\$ 16,342	Crystal Mt 1		
S371W28DS0086	8	3.73	125.17	8	15	\$ 18,175	Crystal Mt 1		
S371W28DS0089	4	2.63	137.14	4	10	\$ 16,457	Crystal Mt 1		
S371W28DS0090	8	3.05	250.99	8	15	\$ 36,444	Crystal Mt 1		
S371W32BS0002	21	7.20	370.15	21	24	\$ 83,284	East Barnett 1	C5	\$ 310,082
S371W32BS0003	21	7.69	233.93	21	24	\$ 52,634	East Barnett 1		
S371W32BS0004	21	7.95	275.77	21	24	\$ 62,048	East Barnett 1		
S371W32BS0005	21	7.79	181.13	21	24	\$ 40,754	East Barnett 1		
S371W32BS0053	8	5.80	446.01	8	12	\$ 71,362	East Barnett 1		
S371W32AS0005	15	7.72	91.22	15	21	\$ 18,244	East Barnett 2	B3	\$ 307,006
S371W32AS0007	15	7.77	311.83	15	21	\$ 62,366	East Barnett 2		
S371W32AS0008	15	9.59	303.55	15	21	\$ 60,710	East Barnett 2		
S371W32AS0009	15	7.62	350.42	15	21	\$ 70,084	East Barnett 2		
S371W32AS0010	15	12.79	386.27	15	24	\$ 95,602	East Barnett 2		
S371W33BS0035	15	9.00	406.92	15	24	\$ 91,557	East Barnett 3	B3	\$ 226,919
S371W33BS0036	18	12.28	363.07	18	27	\$ 98,846	East Barnett 3		
S371W33BS0037	15	14.18	147.54	15	24	\$ 36,516	East Barnett 3		
S371W33BS0027	12	14.17	470.17	12	21	\$ 103,437	East Barnett 4	C3	\$ 213,171
S371W33BS0038	12	10.14	498.79	12	21	\$ 109,734	East Barnett 4		
S371W28CS0108	12	10.00	401.76	12	15	\$ 69,103	East Barnett 5	A3	\$ 173,143
S371W28CS0110	12	12.00	487.55	12	15	\$ 92,244	East Barnett 5		
S371W33BS0002	12	5.50	68.58	12	15	\$ 11,796	East Barnett 5		
S371W28DS0100	12	7.40	482.16	12	18	\$ 88,717	East Barnett 6	C3	\$ 124,355
S371W28DS0139	12	5.96	193.68	12	18	\$ 35,637	East Barnett 6		
S371W33BS0016	8	6.92	274.32	8	12	\$ 43,891	East La Marada	C3	\$ 50,210
S371W33BS0017	8	6.18	39.49	8	12	\$ 6,318	East La Marada		
S371W33BS0004	8	5.39	300.3	8	12	\$ 48,048	East Murphy	C3	\$ 48,048
S371W28DS0027	10	5.02	283.54	10	18	\$ 52,171	Golf View 2	C3	\$ 134,078
S371W28DS0028	8	6.12	143.65	8	15	\$ 24,708	Golf View 2		
S371W28DS0029	8	6.00	113.1	8	15	\$ 19,453	Golf View 2		
S371W28DS0030	8	6.23	219.45	8	15	\$ 37,745	Golf View 2		
S371W28DS0018	8	6.89	67.62	8	12	\$ 10,819	Golf View 3	C3	\$ 166,598
S371W28DS0020	10	7.50	83.52	10	18	\$ 15,368	Golf View 3		
S371W28DS0021	10	7.70	321.82	10	18	\$ 59,215	Golf View 3		
S371W28DS0022	10	6.90	274.2	10	18	\$ 50,453	Golf View 3		
S371W28DS0024	10	10.19	108.4	10	15	\$ 20,509	Golf View 3		
S371W28DS0025	10	9.82	59.5	10	15	\$ 10,234	Golf View 3		
S371W28DS0015	10	8.96	167.43	10	15	\$ 28,798	Golf View 4	C3	\$ 96,158
S371W28DS0016	10	4.59	115.25	10	18	\$ 18,408	Golf View 4		
S371W28DS0017	10	6.97	107.56	10	18	\$ 19,791	Golf View 4		
S371W28DS0161	8	7.89	182.26	8	12	\$ 29,162	Golf View 4		
S371W27BS0083	8	7.00	246.53	8	12	\$ 39,445	Greystone	C3	\$ 85,755
S371W27BS0084	8	8.20	229.34	8	12	\$ 36,694	Greystone		
S371W27BS0086	8	5.54	33.69	8	12	\$ 5,390	Greystone		
S371W27BS0103	8	5.50	26.41	8	12	\$ 4,226	Greystone		
S371W27BS0091	8	0.78	194.54	8	12	\$ 25,679	Greystone Extension	C3	\$ 42,930
S371W28AS0093	8	3.98	130.69	8	12	\$ 17,251	Greystone Extension		
S372W13BS0071	8	7.80	87.91	8	10	\$ 11,428	Howard 2	A3	\$ 68,180
S372W13BS0072	8	8.06	436.55	8	10	\$ 56,752	Howard 2		
S372W14AS0041	12	9.16	318.62	12	15	\$ 54,803	Mace	C3	\$ 118,506
S372W14AS0042	12	11.61	336.7	12	15	\$ 63,704	Mace		
S371W33BS0003	15	6.30	165.72	15	24	\$ 37,287	Murphy 1	B3	\$ 216,777
S371W33BS0007	15	8.75	140.31	15	21	\$ 28,062	Murphy 1		
S371W33BS0008	15	6.00	316.45	15	21	\$ 63,290	Murphy 1		
S371W33BS0015	15	6.17	440.69	15	21	\$ 88,138	Murphy 1		

Appendix B - Detailed Results of Hydraulic Analyses for Future Build-Out Conditions

Facility ID	Original diameter, inches	Future Required Diameter, inches	Length, feet	Max Q, gpm	Qmanning, gpm	Q/Qm	Existing Freeboard, feet	Freeboard with Improvement, feet
S371W28DS0073	4		168	0.9	179	0.01	5.86	5.86
S371W28DS0074	4		175	3.1	196	0.02	4.97	4.97
S371W28DS0076	8		293	8.1	1,345	0.01	4.54	4.54
S371W28DS0077	8		237	28.7	470	0.06	4.45	5.12
S371W28DS0078	8		153	23.3	486	0.05	4.09	4.10
S371W28DS0079	6		53	18.4	283	0.07	4.09	4.10
S371W28DS0081	8		126	26.0	542	0.05	4.42	4.42
S371W28DS0082	6		227	23.8	222	0.11	5.89	5.89
S371W28DS0083	8		225	36.8	448	0.08	2.46	5.30
S371W28DS0084	8	15	264	40.4	248	0.16	-1.53	1.30
S371W28DS0085	8	15	113	99.5	248	0.40	-1.54	1.30
S371W28DS0086	8	15	125	45.3	254	0.18	-1.54	1.30
S371W28DS0089	4	10	137	35.4	33	1.06	-0.84	2.09
S371W28DS0090	8	15	251	186.5	252	0.74	-1.53	1.30
S371W28DS0091	8	15	68	833.6	882	0.95	3.18	5.30
S371W28DS0093	8	15	177	827.3	423	1.96	-1.81	4.40
S371W28DS0094	6		177	15.7	506	0.03	0.75	5.61
S371W28DS0095	8		378	4.5	641	0.01	3.18	5.30
S371W28DS0096	6		219	2.2	249	0.01	5.78	5.78
S371W28DS0098	10	18	315	1,007.1	375	2.68	-0.27	2.93
S371W28DS0099	15		474	1,588.7	1,302	1.22	3.76	6.53
S371W28DS0100	12	18	482	2,300.3	2,120	1.09	3.74	6.12
S371W28DS0106	6		258	1.8	199	0.01	4.33	4.33
S371W28DS0107	8		214	5.8	1,166	0.01	3.97	3.97
S371W28DS0108	8		170	7.2	1,794	0.00	7.17	7.17
S371W28DS0109	10		257	1,421.4	947	1.50	6.34	6.34
S371W28DS0110	10		211	1,430.8	1,628	0.88	7.40	7.40
S371W28DS0111	8		174	5.4	1,076	0.01	8.01	8.01
S371W28DS0112	12		258	1,438.5	2,302	0.63	9.40	9.40
S371W28DS0113	8		289	1.8	1,794	0.00	6.06	6.06
S371W28DS0114	12		242	1,440.3	1,306	1.10	6.81	6.81
S371W28DS0115	12		161	1,441.6	1,332	1.08	6.44	6.44
S371W28DS0116	6		108	1.3	448	0.00	2.48	2.48
S371W28DS0117	8		195	2.7	1,345	0.00	4.46	4.46
S371W28DS0118	8		127	3.6	399	0.01	4.46	4.46
S371W28DS0119	8		150	4.9	411	0.01	4.95	4.95
S371W28DS0120	8		230	6.3	1,569	0.00	6.37	6.37
S371W28DS0121	6		204	2.2	448	0.01	5.97	5.98
S371W28DS0122	8		174	34.1	406	0.08	6.52	9.15
S371W28DS0123	15		495	1,588.7	1,275	1.25	5.59	8.27
S371W28DS0125	8		94	52.9	756	0.07	4.06	6.34
S371W28DS0127	12		442	1,557.7	2,404	0.65	6.44	6.44
S371W28DS0128	8		295	47.5	490	0.10	4.06	6.34
S371W28DS0129	8		69	0.4	448	0.00	6.44	6.44
S371W28DS0130	8		445	114.3	841	0.14	6.34	6.34
S371W28DS0131	8		66	1.3	673	0.00	6.34	6.34
S371W28DS0134	8		47	6.7	480	0.01	8.25	8.25
S371W28DS0135	6		94	0.4	224	0.00	6.29	6.29
S371W28DS0136	8		328	5.4	897	0.01	7.18	7.18
S371W28DS0137	8		10	0.4	448	0.00	7.18	7.18
S371W28DS0138	8		258	2.7	673	0.00	6.53	6.53
S371W28DS0139	12	18	194	2,341.5	1,623	1.44	3.74	4.89
S371W28DS0148	4		170	0.4	448	0.00	1.98	1.98
S371W28DS0150	4		166	0.0	0	0.00	1.28	1.28
S371W28DS0151	4		19	0.0	0	0.00	1.28	1.28
S371W28DS0154	4		164	0.9	179	0.01	0.74	0.74
S371W28DS0155	4		94	0.9	60	0.02	0.74	0.74
S371W28DS0156	6		41	0.9	448	0.00	5.37	5.37
S371W28DS0157	6		28	0.9	448	0.00	4.57	4.57
S371W28DS0160	8		480	110.3	882	0.13	6.29	6.29
S371W28DS0161	8	12	182	35.4	611	0.06	1.59	6.79
S371W28DS0166	8	15	56	827.7	507	1.63	-1.20	5.08
S371W28DS0168	8	15	66	830.0	644	1.29	3.18	5.30
S371W28DS0169	8	15	287	829.1	522	1.59	0.75	5.61

From: Rodney L. Grehn <Rodney.Grehn@cityofmedford.org>
Sent: Tuesday, December 12, 2017 11:28 AM
To: 'raul@csaplanning.net'
Cc: Eric C. Johnson
Subject: RE: zone change application for 371W27-1605
Attachments: SE Commercial Center Core Area - Water Facility Map - Dec 12 2017.pdf

RECEIVED

DEC 18 2017

PLANNING DEPT.

Raul,

MWC has a 16-inch water line in both E Barnett road and also in N Phoenix, and static water pressure is around 65 psi. The existing Medford Water Commission facilities in this area will adequately serve this proposed development. On site 12-inch water lines will be required to provide adequate fire protection, and water meters to each proposed building.

If you have any other questions please give me a call or email.

Thanks,

Rodney L. Grehn, P.E.
 Staff Engineer
 Medford Water Commission
 200 S. Ivy St. Rm. 177
 Medford, Oregon 97501
 Direct: 541-774-2448
 Fax: 541-774-2555
 Email: Rodney.Grehn@cityofmedford.org

From: raul@csaplanning.net [mailto:raul@csaplanning.net]
Sent: Tuesday, December 12, 2017 10:54 AM
To: Rodney L. Grehn
Subject: zone change application for 371W27-1605

Rodney:

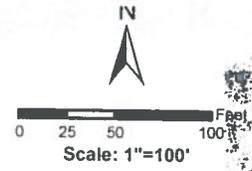
We are preparing to file an application to rezone the above referenced 8-acre lot (net; 9.03 acres gross) from SFR-00 to C-C. It is in the SE Plan area, and buildout is to be as per the City's adopted Master Plan for the SE Commercial Center Core Area (attached). The subject lot will accommodate Buildings 1 through 8 as shown on the master plan.

Could you please let me know if the MWC facilities are adequate to serve the proposed zone change area?

Very truly yours,

CSA Planning, Ltd.
 Raul Woerner

(541) 779-0569



Water Facility Map
for
PLA-17-093
(Parcel: 371W27-1605)

Legend

- Air Valve
 - Sample Station
 - Fire Service
 - Hydrant
 - Reducer
 - Blow Off
 - Plugs-Caps
- Water Meters:**
- Active Meter
 - On Well
 - Unknown
 - Vacant
- Water Valves:**
- Butterfly Valve
 - Gate Valve
 - Tapping Valve
- Water Mains:**
- Active Main
 - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line
- Boundaries:**
- Urban Growth Boundary
 - City Limits
 - Tax Lots
- MWC Facilities:**
- Control Station
 - Pump Station
 - Reservoir



This map is based on a digital elevation compiled by Medford Water Commission from a number of other sources. Medford Water Commission is not responsible for the accuracy or completeness of the data shown. There are no warranties, implied or otherwise.



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 1/24/2018
File Number: ZC-17-168

PUBLIC WORKS DEPARTMENT STAFF REPORT **Zone Change – North Phoenix Enterprises LLC** **North Phoenix Road at East Barnett Road** **(TL 1605)**

Project: Consideration of a zone change from SFR-00 (Single Family Residential – one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot.

Location: Located northeast of the intersection of North Phoenix Road and East Barnett Road in southeast Medford (371W271605).

Applicant: Applicant: North Phoenix Enterprises LLC; Agent: CSA Planning; Planner: Steffen Roennfeldt.

The Medford Land Development Code (MLDC), Section 10.227 (2) requires a zone change application demonstrate Category ‘A’ urban services and facilities are available or can and will be provided to adequately serve the subject property. The Public Works Department reviews zone change applications to assure the Category ‘A’ urban services and facilities under its jurisdiction meet those requirements. The Category urban services and facilities the Public Works Department manages are sanitary sewers within the City’s sewer service boundaries, storm drains, and the transportation system.

I. Sanitary Sewer Facilities

This site lies within the City of Medford Sewer Service area. The sanitary sewer stipulation listed in CSA Planning’s Findings of Fact and Conclusions of Law (under IV, 11.B) dated December 18th, 2017 is acceptable.

II. Storm Drainage Facilities

This site lies within the Larson Creek Drainage Basin. The City of Medford has existing storm drain facilities in the area. This site would be able to connect to these facilities at the time of

development. This site will be required to provide stormwater quality and detention at time of development in accordance with MLDC, Section 10.729 and/or 10.486.

III. Transportation System

Public Works received a Traffic Impact Analysis (TIA) from JRH Transportation Engineering for the Southeast Commercial Center Core Area located at the northeast and southeast corners of the intersection of N. Phoenix Rd and Barnett Rd. The project is comprised of tax lot 371W271605 and tax lot 371W340501. Lot 371W271605 being 8 acres, is currently zoned SFR-00 and is the subject of a Zone Change application to change to C-C (Community Commercial). Lot 371W340501, being 2.14 acres, is currently zoned C-C with a trip cap of 628 ADT and is the subject of a Traffic Impact Analysis to increase the trip cap.

According to TIA, the transportation system cannot accept the potential trip generation from the proposed zone change without mitigating the significant impact of the development traffic. The engineer proposes two separate trip caps on the north and south side properties, mitigation of the intersection of Barnett Rd and N. Phoenix Rd, and installation of a southbound left turn lane on N Phoenix Rd at Michael Park Dr.

Staff does not concur with the conclusions drawn regarding driveway H on Barnett Rd. The applicant makes the case that the driveway is depicted in the Southeast Village Commercial Center Core Area Master Plan and it was the intent of the master plan to allow this driveway. However, the master plan specifically states that any depicted access onto Barnett Rd shall not be approved until justified by a TIA. Driveway H is proposed approximately 195 feet east of the intersection with N. Phoenix Rd. The TIA shows the westbound left turn queue, from Barnett Rd to southbound N Phoenix Rd, anticipated to be 75 feet long and the eastbound left turn queue, from Barnett Rd into the development at driveway H, anticipated to be 25 feet long in 2023 under the studied traffic conditions. As proposed, the deceleration and transition area for these movements will overlap with each other, resulting in higher potential for head-on conflicts. As the southeast area continues to develop, the queues will get longer and the number of conflicts will increase. The best solution is to provide more space between N Phoenix Rd and the first access point by denying driveway H and concentrating access on Barnett to driveway I. Staff recommends denial of the north side access at driveway H, and approval of driveway I as the only north side site access allowed on E Barnett Rd. Staff recommends approval of the south side access at driveway H

Public Works recommends the following conditions be imposed or the zone change be denied:

1. Trips generated by the north side property, comprised of lot 371W271605, shall be capped at 431 P. M. peak hour trips. Trips generated by the south side property, comprised of lot 371W340501, shall be capped at 145 P. M. peak hour trips. Both of these trip caps were proposed by the engineer in the TIA and include allowable reductions for Transit Oriented District and pass-by trips. The developer shall submit trip accountings for approval by the Public Works Department prior to issuance of permits for vertical construction.

2. The intersection of Barnett Rd and N. phoenix Rd will require mitigation to operate acceptably through the planning horizon. Consistent with the recommendations of the TIA, the applicant shall make necessary improvements to convert the existing “protected” north and south left turn phasing to “protected-permissive” left turn phasing. The report says that this mitigation will allow the intersection to operate at level of service C, which meets City of Medford requirements. This mitigation shall be completed prior to the issuance of permits for vertical construction.

3. The applicant shall construct a southbound left turn lane at the intersection of N. Phoenix Rd and Michael Park Dr. This mitigation shall be completed prior to the issuance of permits for vertical construction.

4. Driveway H, as identified in the TIA, shall not be included in future development plans for the north side property (tax lot 371W271605).

If the commission grants access to the north side properties at driveway H, staff recommends that the applicant be required to enter into a Deferred Improvement Agreement (DIA) to build a median to restrict the access to right-in/right-out only for the north side properties and right-in/right-out/left-in only on the south side property when the intersection of Stanford Ave and Barnett Rd (planned as a roundabout) is constructed. The DIA shall be in accordance with Medford land Development Code section 10.432 and be in place prior to issuance of permits for vertical construction.

Prepared by: Doug Burroughs

The above report is based on the information provided with the Zone Change Application submittal and is subject to change based on actual conditions, revised plans and documents or other conditions. A full report with additional details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection shall be provided with a Development Permit Application.



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: ZC-17-168

PARCEL ID: 371W27 TL 1605

PROJECT: Consideration of a zone change from SFR-00 (Single Family Residential – one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot located northeast of the intersection of North Phoenix Road and East Barnett Road in southeast Medford (371W271605). Applicant: North Phoenix Enterprises LLC; Agent: CSA Planning; Planner: Steffen Roennfeldt.

DATE: January 24, 2018

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

COMMENTS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. The MWC system does have adequate capacity to serve this property.
4. Off-site water facility construction may be required depending on future land development review.
5. On-site water facility construction may be required depending on future land development review.
6. MWC-metered water service does not exist to this property.
7. Access to MWC water lines for connection is available. There is an existing 16-inch water line located in both Barnett Road, and also in N Phoenix Road.



Page 193



0 37.5 75 150 Feet
Scale: 1"=150'

Water Facility Map for ZC-17-168

January 24, 2018

Legend

- ⬇ Air Valve
- ⊙ Sample Station
- ⊠ Fire Service
- ⬆ Hydrant
- ▲ Reducer
- ⊠ Blow Off
- ⊕ Plugs-Caps
- Water Meters:**
 - ⊙ Active Meter
 - ⊙ On Wall
 - ⊙ Unknown
 - ⊙ Vacant
- Water Valves:**
 - ⊙ Butterfly Valve
 - ⊙ Gate Valve
 - ⊙ Tapping Valve
- Water Mains:**
 - Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line
- Boundaries:**
 - ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots
- MWC Facilities:**
 - C Control Station
 - P Pump Station
 - R Reservoir



This map is based on a digital orthophoto composite by Aerialcam.com. It is not a true color photograph. It is a composite of many individual photographs. It is not a true color photograph. It is a composite of many individual photographs. It is not a true color photograph. It is a composite of many individual photographs.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
E-mail www.fire@ci.medford.or.us

LAND DEVELOPMENT REPORT - PLANNING

To: Steffen Roennfeldt

LD Meeting Date: 01/24/2018

From: Fire Marshal Kleinberg

Report Prepared: 01/22/2018

File #: ZC - 17 - 168

Site Name/Description:

Consideration of a zone change from SFR-00 (Single Family Residential - one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot located northeast of the intersection of North Phoenix Road and East Barnett Road in southeast Medford (371W271605). Applicant: North Phoenix Enterprises LLC; Agent: CSA Planning; Planner: Steffen Roennfeldt.

DESCRIPTION OF CORRECTIONS

REFERENCE

Approved as Submitted

Meets Requirement: No Additional Requirements

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

CITY OF MEDFORD

EXHIBIT # E

File # ZC-17-168

Memo



To: Steffen Roennfeldt, Planning Department
From: Mary Montague, Building Department
CC: Raul Woerner, Agent: CSA Planning
Date: January 24, 2018
Re: ZC-17-168; North Phoenix Enterprises

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit may be required; see attached.
4. A separate demolition permit will be required for demolition of any structures.
5. This property is located in an area where a geotech soils report will be required.

CITY OF MEDFORD
EXHIBIT # 7
File # ZC-17-168



City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Legal Description
 File no. ZC-17-168
 To Jon Proud, Engineering
 From Steffen Roennfeldt, Planning Department
 Date January 10, 2018

Please verify the attached legal description covering the below subject at your earliest convenience. See attached map.

1. ZC-17-168- North Phoenix Enterprises LLC- Applicant- CSA Planning, Raul Woerner, Agent.

STEFFEN, the description describes tax lot 1405 together with the 1/2 widths (30, 245) OF BARNETT & N. PHOENIX ROADS.

THANKS, Jon
 1/17/18

CITY OF MEDFORD
 EXHIBIT # 6
 File # ZC-17-168

EXHIBIT " "

EXHIBIT RECEIVED

DEC 18 2017

PLANNING DEPT.

OUTLINE DESCRIPTION OF PROPOSED ZONING AREA

All that real property located in the Southwest One-quarter of Section 27, Township 37 South, Range 1 West of the Willamette Meridian, in the City of Medford, Jackson County, Oregon, being more particularly described as follows:

Beginning at the northeast corner of Tract 1 as described in Instrument Number 2009-001973, of the Official Records of Jackson County, Oregon, and as depicted on Survey Number 20296, as filed in the office of the Jackson County Surveyor; thence along the easterly boundary of said Tract 1, South 00°03'51" East, 503.52 feet to the northerly right-of-way of Barnett Road; thence leaving the easterly boundary of said Tract 1 and along the southerly extension thereof, South 00°03'51" East, 30.00 feet to the centerline of said Barnett Road; thence along the centerline thereof, North 89°38'05" West, 732.91 feet to intersect the centerline of North Phoenix Road; thence leaving the centerline of Barnett Road and along the centerline of said North Phoenix Road, North 00°03'51" West, 540.33 feet to intersect the westerly extension of the northerly boundary of said Tract 1; thence leaving the centerline of said North Phoenix Road, South 89°06'09" East, 45.01 feet to the easterly right-of-way of said North Phoenix Road, and the northwest corner of said Tract 1; thence leaving said right-of-way and along the northerly boundary of said Tract 1, South 89°06'09" East, 687.98 feet to the Point of Beginning.

Said outlined area contains 9.03 acres, more or less, including the area within the right-of-ways for North Phoenix Road and Barnett Road.

This description is prepared for the distinct purposes of outlining an area to be re-zoned in the City of Medford, Oregon, and is not sufficient for the conveyance of real property, determining or creating property boundaries.

Prepared By: Neathamer Surveying, Inc.
3126 State Street, Suite 203
PO Box 1584
Medford, Oregon 97501
Phone: (541) 732-2869
Project Number: 16062

Date: November 28, 2016

REGISTERED
PROFESSIONAL
LAND SURVEYOR

Robert V. Neathamer

OREGON
JULY 19, 1994
ROBERT V NEATHAMER
2675

RENEWAL DEC. 31, 2016

CITY OF MEDFORD
EXHIBIT #
File # ZC-17-168

12



JACKSON COUNTY

Roads

Roads Engineering

Kevin Christiansen
Construction Manager

200 Antelope Road
White City, OR 97503
Phone (541) 774-6255
Fax (541) 774-6295
christke@jacksoncounty.org
www.jacksoncounty.org

January 17, 2018

Attention: Steffen Roennfeldt
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Zone change off North Phoenix Road- in the City maintained portion and Barnett Road East- a county maintained road.
Planning File: ZC-17-168.

Dear Dustin:

Thank you for the opportunity to comment on the consideration of a zone change from SFR-00 (Single Family Residential – one dwelling unit per existing lot) to C-C (Community Commercial) on an 8.00 acre lot located at the northeast of the intersection of North Phoenix Road and Barnett Road East in southeast Medford (37-127 TL 1605). Jackson County Roads has the following comments:

1. Any new or improved road approaches off North Phoenix Road or Barnett Road East shall be permitted and inspected by the City of Medford.
2. The applicant shall submit construction drawings to Jackson County Roads and obtain county permits if required.
3. North Phoenix Road (south of the intersection of North Phoenix Road and Barnett Road East) is a County Minor Arterial and is county-maintained.
4. The Average Daily Traffic Count was 7,610 ADT on July 15, 2014, 225 feet south of Coal Mine Road. As a comparison of capacity for North Phoenix Road, the capacity of a two lane suburban road with ten foot lanes and no shoulders is 8,832 ADT.
5. If frontage improvements are required, they shall be permitted and inspected by the City of Medford.
6. We would like to be notified of future development proposals, as county permits may be required.
7. According to our records, Barnett Road East at the section in front of the subject property is a local access road within the City Limits of Medford, and as per ORS 368.031, not under the jurisdiction of Jackson County.
8. Barnett Road East from Mile Post 0.21 to Mile Post 0.92 is a local access road outside of the City Limits but within the Medford Urban Growth Boundary and is maintained by

CITY OF MEDFORD
EXHIBIT # 4
FILE # ZC-17-168

January 17, 2018

Page 2 of 2

the adjacent property owners. The road is substandard having a sixty-foot right-of-way. Roads has concerns about the inadequate transportation facilities. We have concerns that this section of Barnett Road East will not hold up to frequent heavy loads. The residents along this section of the local access road may request a barricade closing the road to through traffic.

9. Storm water should meet City of Medford requirements that also include water quality.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Kevin Christiansen
Construction Manager

Steffen K. Roennfeldt

From: David Haight <David.R.Haight@state.or.us>
Sent: Thursday, January 18, 2018 1:13 PM
To: Steffen K. Roennfeldt
Subject: ZC-17-168

Steffen,

The Oregon Department of Fish and Wildlife has no objection to the proposal to change the zoning of the property at 371W271605 from SFR-00 to C-C.

David R. Haight
Fisheries Biologist
Oregon Department of Fish and Wildlife
1495 East Gregory Road
Central Point, OR 97502
541-826-8774, ext 224

CITY OF MEDFORD
EXHIBIT # 1
File # ZC-17-168

EXHIBIT A

Rogue Credit Union Zone Change

ZC-15-041

Conditions of Approval

June 4, 2015

DISCRETIONARY CONDITIONS

1. Comply with the Public Works Department Memo dated May 15, 2015 (Exhibit G). The applicant stipulates that development will be limited to that which would generate a total of 628 total daily trips. Said stipulation will remain in effect until a traffic impact analysis shows that additional trips will have no significant impact to the transportation system.



City of Medford

Planning Department

Vicinity
Map

File Number:

ZC-17-168

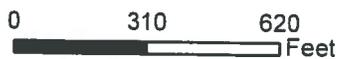


Project Name:

North Phoenix ENterprises LLC Zone Change

Map/Taxlot:

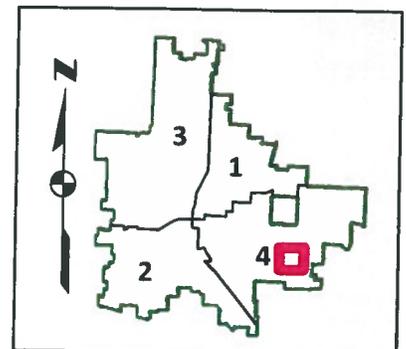
371W27 TL 1605



12/21/2017

Legend

-  Subject Area
-  Medford Zoning
-  Tax Lots





City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT

for a Type-C quasi-judicial decision: **Zone Change**

Project Stylus Development LLC
Applicant: Stylus Development; Agent: ORW Architecture

File no. ZC-18-008

To Planning Commission *for 03/22/2018 hearing*

From Steffen Roennfeldt, Planner III

Reviewer Kelly Akin, Assistant Planning Director *ka*

Date March 13, 2018

BACKGROUND

Proposal

Consideration of a zone change on a 1.06 acre parcel located south of East Barnett Road, approximately 530 feet east of Ellendale Drive from MFR-20 (Multi-Family Residential – 15 to 20 dwelling units per gross acre) to MFR-30 (Multi-Family Residential – 20 to 30 dwelling units per gross acre)(371W32AB500).

Vicinity Map



Subject Site Characteristics

Zoning	MFR-20	Multi-Family Residential – 15 to 20 dwelling units per gross acre
GLUP	U-H	Urban High Density Residential
Use	Single-Family Residential	

Surrounding Site Characteristics

<i>North</i>	Zone:	MFR-20 & C-C (Community Commercial)
	Use:	Residential & Parking Lot
<i>South</i>	Zone:	MFR-20
	Use:	Multi-Family Residential
<i>East</i>	Zone:	MFR-30
	Use:	Townhouses
<i>West</i>	Zone:	C-C
	Use:	Chiropractic and Wellness Center

Related Projects

None.

Applicable Criteria

ZONE CHANGE APPROVAL CRITERIA – MEDFORD LAND DEVELOPMENT CODE SECTION 10.227

The zone change criteria that are not relevant to this particular application are hereby omitted from the following citation and noted by ***.

The approving authority (Planning Commission) shall approve a quasi-judicial zone change if it finds that the zone change complies with subsections (1) and (2) below:

- (1) The proposed zone is consistent with the Transportation System Plan (TSP) and the General Land Use Plan Map designation. A demonstration of consistency with the acknowledged TSP will assure compliance with the Oregon Transportation Planning Rule.) Where applicable, the proposed zone shall also be consistent with the additional locational standards of the below sections (1)(a), (1)(b), (1)(c), or (1)(d). Where a special area plan requires a specific zone, any conflicting or additional requirements of the plan shall take precedence over the locational criteria below.

- (2) It shall be demonstrated that Category A urban services and facilities are available or can and will be provided, as described below, to adequately serve the subject property with the permitted uses allowed under the proposed zoning, except as provided in subsection (c) below. The minimum standards for Category A services and facilities are contained in the MLDC and Goal 3, Policy 1 of the *Comprehensive Plan* "Public Facilities Element."
- (a) Storm drainage, sanitary sewer, and water facilities must already be adequate in condition, capacity, and location to serve the property or be extended or otherwise improved to adequately serve the property at the time of issuance of a building permit for vertical construction.
- (b) Adequate streets and street capacity must be provided in one of the following ways:
- (i) Streets which serve the subject property, as defined in Section 10.461(2), presently exist and have adequate capacity; or
- (ii) Existing and new streets that will serve the subject property will be improved and/or constructed, sufficient to meet the required condition and capacity, at the time building permits for vertical construction are issued; or
- (iii) If it is determined that a street must be constructed or improved in order to provide adequate capacity for more than one proposed or anticipated development, the Planning Commission may find the street to be adequate when the improvements needed to make the street adequate are fully funded. A street project is deemed to be fully funded when one of the following occurs:
- (a) the project is in the City's adopted capital improvement plan budget, or is a programmed project in the first two years of the State's current STIP (State Transportation Improvement Plan), or any other public agencies adopted capital improvement plan budget; or
- (b) when an applicant funds the improvement through a reimbursement district pursuant to the MLDC. The cost of the improvements will be either the actual cost of construction, if constructed by the applicant, or the estimated cost. The "estimated cost" shall be 125% of a professional engineer's estimated cost that has been approved by the City, including the cost of any right-of-way acquisition. The method described in this paragraph shall not be used if the Public Works Department determines, for reasons of public safety, that the improvement must be constructed prior to issuance of building permits.

- (iv) When a street must be improved under (b)(ii) or (b)(iii) above, the specific street improvement(s) needed to make the street adequate must be identified, and it must be demonstrated by the applicant that the improvement(s) will make the street adequate in condition and capacity.
- (c) In determining the adequacy of Category A facilities, the approving authority (Planning Commission) may evaluate potential impacts based upon the imposition of special development conditions attached to the zone change request. Special development conditions shall be established by deed restriction of covenant, which must be recorded with proof of recordation returned to the Planning Department, and may include, but are not limited to the following:
 - (i) Restriction of uses by type or intensity; however, in cases where such a restriction is proposed, the Planning Commission must find that the resulting development pattern will not preclude future development, or intensification of development, on the subject property or adjacent parcels. In no case shall residential densities be approved which do not meet minimum density standards,
 - (ii) Mixed-use, pedestrian-friendly design which qualifies for the trip reduction percentage allowed by the Transportation Planning Rule,
 - (iii) Transportation Demand Management (TDM) measures which can be reasonably quantified, monitored, and enforced, such as mandatory car/van pools.

Corporate Names

Stylus Development, LLC is the owner of this property. The Oregon Secretary of State Business Registry lists Jennifer Nicholls as the Registered Agent and Dana C. Ing, Jeffrey J. Bender, Andrew L. Owen, and James C. Roemer as Members.

ISSUES AND ANALYSIS

Project Summary

The subject property consists of one parcel totaling 1.06 acres located on the south side of East Barnett Road between Ellendale Drive and Hilldale Avenue. The site is developed with a single-family home with an accessory structure.

Transportation System

The Public Works Staff Report (Exhibit B) states that a Traffic Impact Analysis will not be required, and recommends no conditions pertaining to streets, street capacity, or access.

Sanitary Sewer Facilities

The Public Works Staff Report (Exhibit B) states there is capacity in the existing sanitary sewer system to allow for this zone change.

Storm Drainage Facilities

The subject site lies within the Larson Creek Drainage Basin. Per the Public Works Staff Report (Exhibit B), there are sufficient storm drain facilities to support the proposed zoning designation. Stormwater quality and detention will be required at the time of development in accordance with the Medford Land Development Code.

Water Facilities

Medford Water Commission currently has infrastructure and adequate capacity to serve the subject site (Exhibit C).

Other Agencies

Comments from the Building Department (Exhibit E), the Floodplain Coordinator (Exhibit F) and the Surveyor (Exhibit G) are for information only at this point and will be applicable at time of development.

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit B) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of ZC-18-008 per the staff report dated March 13, 2018, including Exhibits A through J.

EXHIBITS

- A Applicant's Findings of Facts, received January 23, 2018
- B Public Works Staff Report, received February 21, 2018
- C Medford Water Commission Memo, received February 21, 2018

- D Medford Fire Department Report, received February 21, 2018
- E Medford Building Department Memo, received February 21, 2018
- F Floodplain Coordinator Memo, received February 13, 2018
- G City Surveyor Memo, received February 7, 2018
- H Assessors Map, received January 23, 2018
- I Legal Description, received January 23, 2018
Vicinity map

PLANNING COMMISSION AGENDA:

MARCH 22, 2018

RECEIVED
JAN 23 2010
PLANNING DEPT

ORW
O'ROURKE ROEHLER WILKINSON
ARCHITECTURE
WWW.ORWARCHITECTURE.COM
2950 EAST BARNETT ROAD
MEDFORD OR 97504
P 5 4 1 7 7 9 5 2 3 7
F 5 4 1 7 7 2 8 4 7 2

2100 EAST BARNETT ROAD

ZONE CHANGE APPLICATION - FINDINGS OF FACT

January 19, 2018

10.227 Zone Change Criteria

The approving authority (Planning Commission) shall approve a quasi-judicial zone change if it finds that the zone change complies with subsections (1) and (2) below:

(1) The proposed zone is consistent with the Transportation System Plan (TSP) and the General Land Use Plan Map designation. A demonstration of consistency with the acknowledged TSP will assure compliance with the Oregon Transportation Planning Rule.) Where applicable, the proposed zone shall also be consistent with the additional locational standards of the below sections (1)(a), (1)(b), (1)(c), or (1)(d). Where a special area plan requires a specific zone, any conflicting or additional requirements of the plan shall take precedence over the locational criteria below.

(a) For zone changes to SFR-2, the zoning shall be approved under either of the following circumstances:

(i) if at least seventy percent (70%) of the area proposed to be rezoned exceeds a slope of fifteen percent (15%),

(ii) if other environmental constraints, such as soils, geology, wetlands, and flooding, restrict the capacity of the land to support higher densities.

(b) For zone changes to SFR-6 or SFR-10 where the permitted density is proposed to increase, one (1) of the following conditions must exist:

(i) At least one (1) parcel that abuts the subject property is zoned the same as the proposed zone, either SFR-6 or SFR-10 respectively; or

(ii) The area to be rezoned is five (5) acres or larger; or

(iii) The subject property, and any abutting parcel(s) that is(are) in the same General Land Use Plan Map designation and is(are) vacant, when combined, total at least five (5) acres.

(c) For zone changes to any commercial zoning district, the following criteria shall be met for the applicable zoning sought:

(i) The overall area of the C-N zoning district shall be three (3) acres or less in size and within, or abutting on at least one (1) boundary, residential zoning. In determining the overall area, all abutting property(s) zoned C-N shall be included in the size of the district.

(ii) The overall area of the C-C zoning district shall be over three (3) acres in size and shall front upon a collector or arterial street or state highway. In determining the overall area, all abutting property(s) zoned C-C shall be included in the size of the district.

PROJECT: 2100 East Barnett Road Zone Change

DATE: January 19, 2018

(iii) The overall area of the C-R zoning district shall be over three (3) acres in size, shall front upon an arterial street or state highway, and shall be in a centralized location that does not otherwise constitute a neighborhood shopping center or portion thereof. In determining the overall area, all abutting property(s) zoned C-R shall be included in the size of the district. The C-R zone is ordinarily considered to be unsuitable if abutting any residential zones, unless the applicant can show it would be suitable pursuant to (1)(e) below.

(iv) The C-H zone shall front upon an arterial street or state highway. The C-H zone may abut the General Industrial (I-G), Light Industrial (I-L), and/or any commercial zone. The C-H zone is ordinarily considered to be unsuitable if abutting any residential and I-H zones, unless the applicant can show it would be suitable pursuant to (1)(e) below.

(d) For zone changes to any industrial zoning district, the following criteria shall be met for the applicable zoning sought:

(i) The I-L zone may abut residential and commercial zones, and the General Industrial (I-G) zone. The I-L zone is ordinarily considered to be unsuitable when abutting the Heavy Industrial (I-H) zone, unless the applicant can show it would be suitable pursuant to (1)(e) below.

(ii) The I-G zone may abut the Heavy Commercial (C-H), Light Industrial (I-L), and the Heavy Industrial (I-H) zones. The I-G zone is ordinarily considered to be unsuitable when abutting the other commercial and residential zones, unless the applicant can show it would be suitable pursuant to (1)(e) below.

(iii) The I-H zone may abut the General Industrial (I-G) zone. The I-H zone is ordinarily considered to be unsuitable when abutting other zones, unless the applicant can show it would be suitable pursuant to (1)(e) below.

(e) For purposes of (1)(c) and (1)(d) above, a zone change may be found to be "suitable" where compliance is demonstrated with one (1) or more of the following criteria:

(i) The subject property has been sited on the General Land Use Plan Map with a GLUP Map designation that allows only one (1) zone;

(ii) At least fifty percent (50%) of the subject property's boundaries abut zones that are expressly allowed under the criteria in (1)(c) or (1)(d) above;

(iii) At least fifty percent (50%) of the subject property's boundaries abut properties that contain one (1) or more existing uses which are permitted or conditional uses in the zone sought by the applicant, regardless of whether the abutting properties are actually zoned for such existing uses; or

(iv) Notwithstanding the definition of "abutting" in Section 10.012 and for purposes of determining suitability under Section (1) (e), the subject property is separated from the "unsuitable" zone by a public right-of-way of at least sixty (60) feet in width.

(f) For zone changes to apply or remove the overlay zones (Limited Industrial, Exclusive Agricultural, Freeway, Southeast, Historic) the criteria can be found in the applicable overlay section (Sections 10.345 through 10.413).

PROJECT: 2100 East Barnett Road Zone Change

DATE: January 19, 2018

The proposed Zone Change of this property from MFR-20 to MFR-30 fits with Medford's General Land Use Plan (GLUP). The GLUP map shows this property with a designation of Urban High Density Residential (UH), which permits residential uses ranging from 15 units per acre to 30 units per acre.

The current zoning is MFR-20. This application requests a zone change to MFR-30. According to the General Land Use Plan element of Medford's Comprehensive Plan, the UH designation permits both the MFR-20 and MFR-30 zoning districts. Therefore, the proposed zone change from MFR-20 to MFR-30 fits within the constraints of the GLUP and supports the direction of the Comprehensive Plan.

The proposed use of the property (to be submitted separately under a Land Use Development application) calls for the development of thirty-two dwelling units that will be rented as apartments. This use is permitted as-of-right with an MFR-30 zone.

The proposed Zone Change will allow multi-family residential development, which is in a dire shortage within the City of Medford. The site abuts several existing multi-family developments, with several others in the immediate vicinity. The site is situated on a fully developed Major Arterial street which can support the more intense level of development that this project will provide.

Additionally, the proposed Zone Change is consistent with Medford's adopted Transportation System Plan (TSP). The site is situated on a fully developed Major Arterial street which can support the slight increase in vehicle trips that this project will generate.

(2) It shall be demonstrated that Category A urban services and facilities are available or can and will be provided, as described below, to adequately serve the subject property with the permitted uses allowed under the proposed zoning, except as provided in subsection (c) below. The minimum standards for Category A services and facilities are contained in Section 10.462 and Goal 2 of the Comprehensive Plan "Public Facilities Element" and Transportation System Plan.

(a) Storm drainage, sanitary sewer, and water facilities must already be adequate in condition, capacity, and location to serve the property or be extended or otherwise improved to adequately serve the property at the time of issuance of a building permit for vertical construction.

Storm drainage, sanitary sewer, and water facilities are already in place in East Barnett Road. They are in adequate condition and have sufficient capacity to serve the subsequent development after the zone change with no upgrades.

(b) Adequate streets and street capacity must be provided in one (1) of the following ways:

(i) Streets which serve the subject property, as defined in Section 10.461(2), presently exist and have adequate capacity; or

PROJECT: 2100 East Barnett Road Zone Change

DATE: January 19, 2018

This property is served by East Barnett Road, which is a fully developed Major Arterial street. The street has adequate capacity for this zone change and subsequent development of the property with no further improvements required.

(ii) Existing and new streets that will serve the subject property will be improved and/or constructed, sufficient to meet the required condition and capacity, at the time building permits for vertical construction are issued; or

(iii) If it is determined that a street must be constructed or improved in order to provide adequate capacity for more than one (1) proposed or anticipated development, the Planning Commission may find the street to be adequate when the improvements needed to make the street adequate are fully funded. A street project is deemed to be fully funded when one (1) of the following occurs:

(a) the project is in the City's adopted capital improvement plan budget, or is a programmed project in the first two (2) years of the State's current STIP (State Transportation Improvement Plan), or any other public agencies adopted capital improvement plan budget; or

(b) when an applicant funds the improvement through a reimbursement district pursuant to the MLDC. The cost of the improvements will be either the actual cost of construction, if constructed by the applicant, or the estimated cost. The "estimated cost" shall be 125% of a professional engineer's estimated cost that has been approved by the City, including the cost of any right-of-way acquisition. The method described in this paragraph shall not be used if the Public Works Department determines, for reasons of public safety, that the improvement must be constructed prior to issuance of building permits.

(iv) When a street must be improved under (b)(ii) or (b)(iii) above, the specific street improvement(s) needed to make the street adequate must be identified, and it must be demonstrated by the applicant that the improvement(s) will make the street adequate in condition and capacity.

(c) In determining the adequacy of Category A facilities, the approving authority (Planning Commission) may evaluate potential impacts based upon the imposition of special development conditions attached to the zone change request. Special development conditions shall be established by deed restriction or covenant, which must be recorded with proof of recordation returned to the Planning Department, and may include, but are not limited to the following:

(i) Restriction of uses by type or intensity; however, in cases where such a restriction is proposed, the Planning Commission must find that the resulting development pattern will not preclude future development, or intensification of development, on the subject property or adjacent parcels. In no case shall residential densities be approved which do not meet minimum density standards,

(ii) Mixed-use, pedestrian-friendly design which qualifies for the trip reduction percentage allowed by the Transportation Planning Rule,

(iii) Transportation Demand Management (TDM) measures which can be reasonably quantified, monitored, and enforced, such as mandatory car/van pools.



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 2/21/2018
File Number: ZC-18-008

PUBLIC WORKS DEPARTMENT STAFF REPORT **2100 East Barnett Road** **(TL 500)**

Project: Consideration of a zone change on 1.06 acre parcel.

Location: Located south of East Barnett Road, approximately 530 east of Ellendale Drive from MFR-20 (Multi Family Residential – 15 to 20 dwelling units per gross acre) to MFR-30 (Multi-Family Residential – 20 to 30 dwelling units per gross acre) (371W32AB500).

Applicant: Applicant: Stylus Development LLC; Agent: ORW Architecture; Planner: Steffen Roennfeldt.

The Medford Land Development Code (MLDC), Section 10.227 (2) requires a zone change application demonstrate Category ‘A’ urban services and facilities are available or can and will be provided to adequately serve the subject property. The Public Works Department reviews zone change applications to assure the services and facilities under its jurisdiction meet those requirements. The services and facilities that Public Works Department manages are sanitary sewers within the City’s service boundary, storm drains, and the transportation system.

I. Sanitary Sewer Facilities

This site lies within the City of Medford Sewer Service area. There is an existing 21-inch sanitary sewer main in East Barnett Road. There is capacity in the existing sanitary sewer system to allow this Zone Change.

II. Storm Drainage Facilities

This site lies within the Larson Creek Drainage Basin. The subject property currently drains to the west. The City of Medford has existing storm drain facilities in the area. This site would be able to connect to these facilities at the time of development. This site may be required to provide stormwater quality and detention at time of development in accordance with MLDC, Section 10.729 and/or 10.486.

III. Transportation System

No traffic impact analysis (TIA) will be required for this zone change. The proposed application doesn't meet the requirements for a TIA, per Medford Municipal Code (MMC), Section 10.461 (3).

No conditions pertaining to streets, street capacity, or access are requested by Public Works at this time.

Prepared by: Doug Burroughs

The above report is based on the information provided with the Zone Change Application submittal and is subject to change based on actual conditions, revised plans and documents or other conditions. A full report with additional details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection shall be provided with a Development Permit Application.



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: ZC-18-008

PARCEL ID: 371W32AB TL 500

PROJECT: Consideration of a zone change on 1.06 acre parcel located south of East Barnett Road, approximately 530 east of Ellendale Drive from MFR-20 (Multi Family Residential – 15 to 20 dwelling units per gross acre) to MFR-30 (Multi-Family Residential – 20 to 30 dwelling units per gross acre) (371W32AB500). Applicant: Stylus Development LLC; Agent: ORW Architecture; Planner: Steffen Roennfeldt.

DATE: February 21, 2018

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

COMMENTS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. The MWC system does have adequate capacity to serve this property.
4. Off-site water facility construction is not required.
5. On-site water facility construction may be required depending on future land development review.
6. MWC-metered water service does exist to this property. There is a ¾" water copper service line approximately mid-lot along E Barnett Road.
7. Access to MWC water lines for connection is available. There is an existing 12-inch water line in E Barnett Road, and an 8-inch water line loop that extends both east and west along the south property line of this parcel.

CITY OF MEDFORD
EXHIBIT # C
File # ZC-18-008



0 25 50 100 Feet
Scale: 1"=100"

Water Facility Map for ZC-18-008

February 16, 2018

Legend

- ◆ Air Valve
- Sample Station
- Fire Service
- ◆ Hydrant
- ▲ Reducer
- Blow Off
- ◆ Plugs-Caps

Water Meters:

- Active Meter
- On Well
- Unknown
- Vacant

Water Valves:

- Butterfly Valve
- Gate Valve
- Tapping Valve

Water Mains:

- Active Main
- - - Abandoned Main
- Reservoir Drain Pipe
- Pressure Zone Line

Boundaries:

- ▭ Urban Growth Boundary
- ▭ City Limits
- ▭ Tax Lots

MWC Facilities:

- C** Control Station
- P** Pump Station
- R** Reservoir



This map is based on the MWC GIS data as of 1/15/18. It is not a warranty of accuracy. The MWC is not responsible for any errors or omissions. This map is for informational purposes only. © 2018 Medford Water Commission. All rights reserved.



Medford Fire Department

200 S. Ivy Street, Room #180
Medford, OR 97501
Phone: 774-2300; Fax: 541-774-2514;
E-mail www.fire@ci.medford.or.us

LAND DEVELOPMENT REPORT - PLANNING

To: Steffen Roennfeldt

LD Meeting Date: 02/21/2018

From: Fire Marshal Kleinberg

Report Prepared: 02/21/2018

File #: ZC - 18 - 8

Site Name/Description:

Consideration of a zone change on 1.06 acre parcel located south of East Barnett Road, approximately 530 east of Ellendale Drive from MFR-20 (Multi Family Residential - 15 to 20 dwelling units per gross acre) to MFR-30 (Multi-Family Residential - 20 to 30 dwelling units per gross acre) (371W32AB500). Applicant: Stylus Development LLC; Agent: ORW Architecture; Planner: Steffen Roennfeldt.

DESCRIPTION OF CORRECTIONS

REFERENCE

Approved as Submitted

Meets Requirement: No Additional Requirements

Development shall comply with access and water supply requirements in accordance with the Fire Code in affect at the time of development submittal.

Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on the information provided only.

Design and installation shall meet the Oregon requirements of the IBC, IFC, IMC and NFPA standards.

CITY OF MEDFORD
EXHIBIT # D
File # ZC-18-008

Memo



To: Steffen Roennfeldt, Planning Department
From: Mary Montague, Building Department
CC: Stylus Development LLC, Agent, David
Date: February 21, 2018
Re: ZC-18-008; Stylus Development LLC Zone Change

Building Department:

Please Note: This is not a plan review. These are general notes based on general information provided. Plans need to be submitted and will be reviewed by a residential plans examiner to determine if there are any other requirements for this occupancy type. Please contact the front counter for fees.

1. Applicable Building Codes are 2017 ORSC; 2017 OPSC; and 2014 OMSC. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Go to "City Departments" at top of screen; click on "Building"; click on "ELECTRONIC PLAN REVIEW (ePlans)" for information.
3. Site Excavation permit required to develop, install utilities.
4. Demo Permit is required for any buildings being demolished.
5. Any properties located within the 100 year Flood Plain requires a permit. All buildings will require a flood elevation certificate.

CITY OF MEDFORD
EXHIBIT # F
File # ZC-18-008



Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Stylus Development LLC
File no. ZC-18-008
To Steffen Roennfeldt, Planner III
From *W* Liz Conner, Planner II, Floodplain Coordinator
Date February 13, 2018

SITE CHARACTERISTICS

- SFR-20, Multiple family residential (15-20 dwelling units per gross acre)
- Southern third portion of parcel within 1% chance annual floodplain (100 year floodplain).
- FIRM panel 41029C 1978F & 41029C 1979F effective May 3, 2011

PROJECT DESCRIPTION

Consideration of a zone change on 1.06 acre parcel located south of East Barnett Road, approximately 530 feet east of Ellendale Drive from MFR-20 (Multi-family residential - 15-20 dwelling units per gross acre) to MFR-30 (Multi-family residential - 20-30 dwelling units per gross acre).

FLOODPLAIN REGULATIONS

The property is currently within a mapped AE zone with Base Flood Elevations and a designated floodway that is contained within the structure. Per the National Flood Insurance Program Regulations, any vertical structures and utilities shall comply with 44 CFR 60.3(a),(b),(c), and (d) and MMC Section 9.706.

The Medford Floodplain regulations are found in Sections 9.701-9.707 of the Municipal Code. The sections pertaining to areas of special flood hazard with Base Flood Elevations (BFE) need to be reviewed and adhered to specifically, along with other relevant sections.

CITY OF MEDFORD
EXHIBIT # F
File # ZC-18-008

A Floodplain Development Permit is required prior to development in the Special Flood Hazard Areas (SFHA). *Development is broadly defined and includes, but is not limited to, grading, filling, paving, and construction of buildings.*

Future tentative plans shall identify the special flood hazard areas. Structures shall be constructed a minimum of one-foot above the BFE.

Existing and proposed grades shall be provided and the effect of this earth movement on the floodplain shall be described in a narrative.

Floodplain Manager Comments

We advise that the design and layout of future development is done in a manner that retains the area within the Special Flood Hazard Area as natural and with little disturbance as possible and free from structures.

You might consider designating the area as open space or a playground with fewer structures.

Parking Areas may also be permitted for development within the SFHA.

Floodplain Permit

Submit a floodplain development application and fee (\$150) along with submittal requirements identified in Section 9.705 (C). An Elevation Certificate (EC) is required with the submittal of building permits for new commercial structures located in the special flood hazard area (one at the time of building permit submittal, one during construction, and one prior to certificate of occupancy).

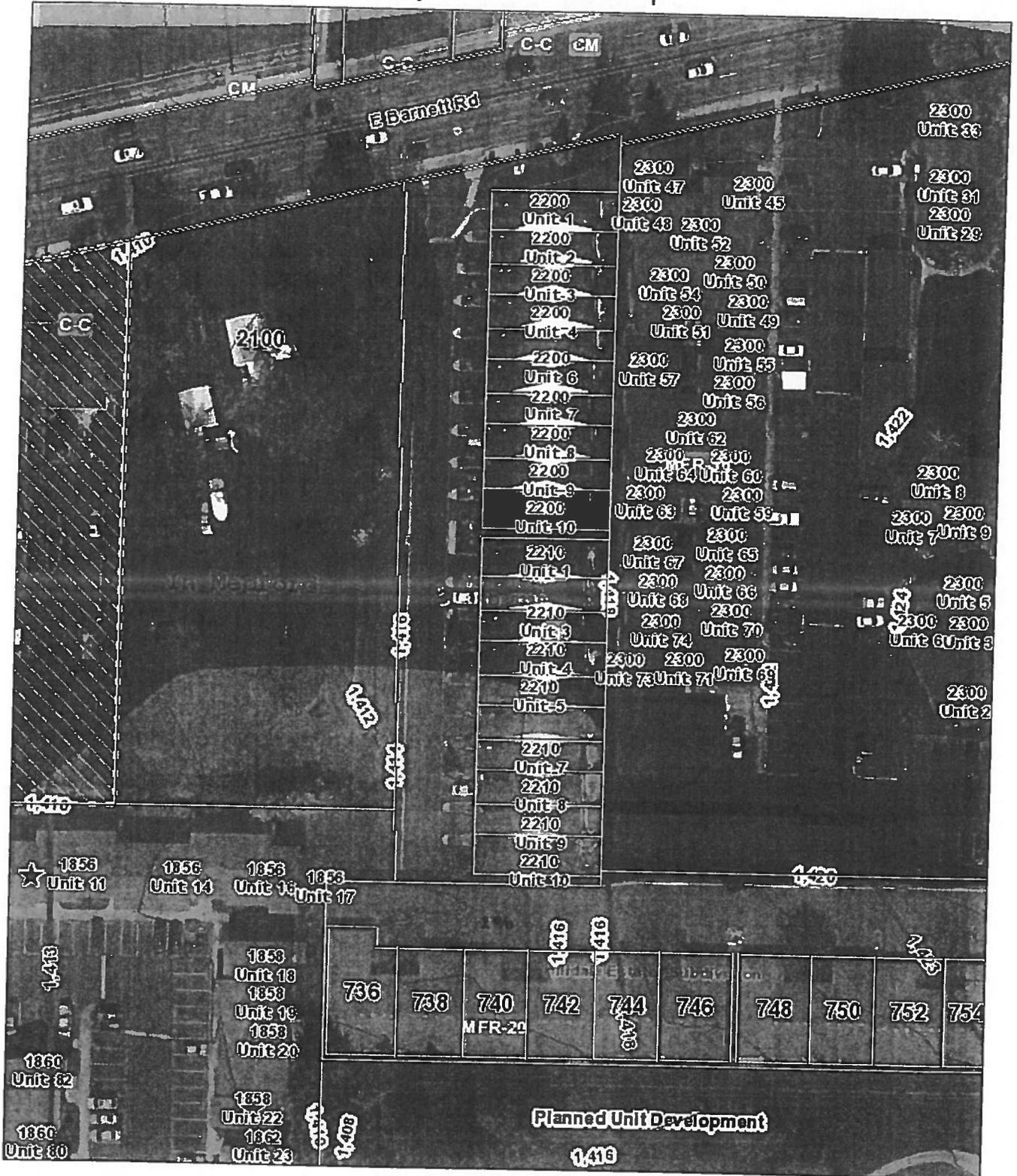
Submit copies of all necessary permits from other governmental agencies from which approval is required prior to start of construction.

Construction shall be in compliance with applicable building and fire codes and floodplain regulations.

Expiration of Floodplain Permit

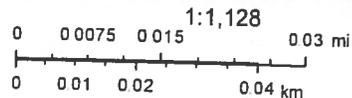
A floodplain Development Permit shall become invalid unless work is started within 180 days after its issuance. Extensions for periods of not more than 180 days each shall be requested in writing.

City of Medford Map



February 14, 2018

- Site Addresses
- Taxlots
- 2 Foot Contours-NGVD29



Sources Esri, HERE, DeLorme, Intermap, increment P Corp., GEBCO, USGS, FAO, NPS, NRCAN, Geobase, IGN, Kadaster NL, Ordnance Survey, Esri Japan, METI, Esri China (Hong Kong), swisstopo, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community
City of Medford



Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Legal Description
 File no. ZC-18-008
 To Jon Proud, Engineering
 From Steffen Roennfeldt, Planning Department
 Date February 7, 2018

Please verify the attached legal description covering the below subject at your earliest convenience. See attached map.

1. ZC-18-008-
 Applicant: Stylus Development LLC
 Agent: ORW Architecture- J. David Wilerkerson II

Steffen, The description provided does not account for the South 37.2' of the property being apparently illegally adjusted from the parent property in 1990. The description also calls for 500' along Barnett Road from the DLC 50 which appears to be a material discrepancy within the deed of approx 30'.

The applicant can find the last conveyance cp in J.C.O.R. as 2008-2530 attached.

Attachments: Legal description, Vicinity Map.

Planning might consider the need for "Validation" per ORS 92.176 if unit of land was unlawfully established(?)

WALES, JON

2/13/18
 CITY OF MEDFORD
 EXHIBIT # 6

File # ZC-18-008

LEGAL DESCRIPTION

Preliminary Report

Order No.: 7161-2973051
Page 7 of 8

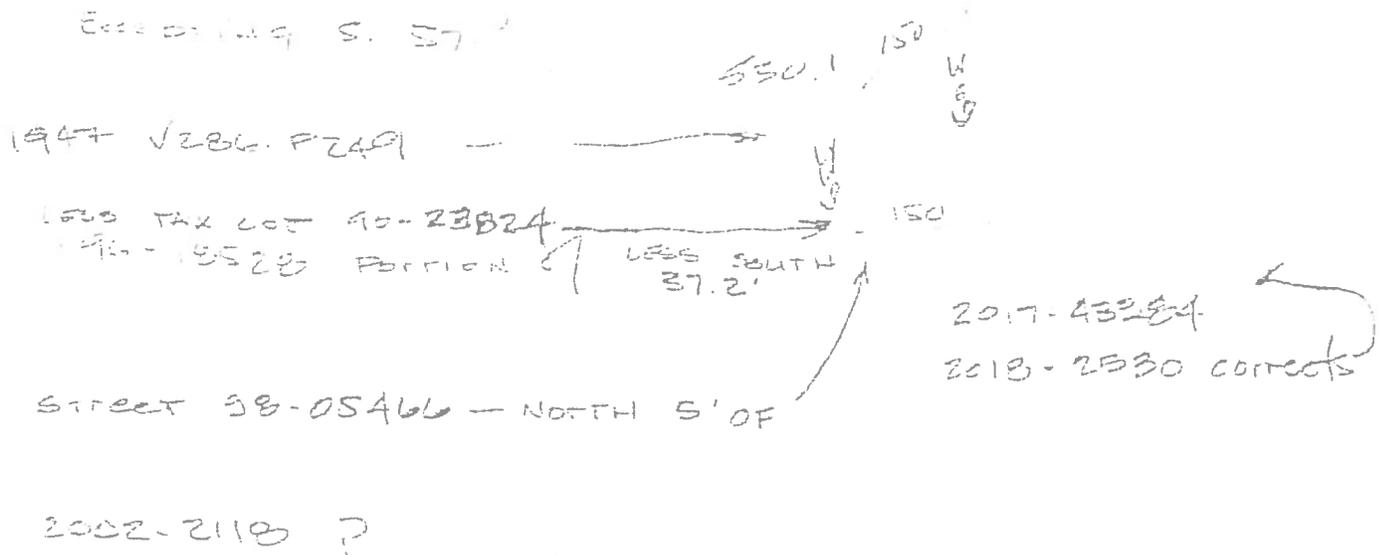
Exhibit "A"

Real property in the County of Jackson, State of Oregon, described as follows:

From the intersection of the east line of Donation Land Claim No. 50 in Township 37 South, Range 1 West of the Willamette Meridian in Jackson County, Oregon, with the Southerly right of way of County Road (Barnett Road), measure 500 feet Easterly along said right of way line to the point of beginning; thence South 339 feet; thence East 150 feet; thence North 369 feet to an intersection with the southerly line of said County Road; thence South 76°51' West, along said southerly line of said County Road, 150 feet to the point of beginning.

Excepting that portion of land conveyed to The City of Medford, an Oregon Municipal Corporation, by Deed recorded February 13, 1998 as Document No. 98-05466.

NOTE: This legal description was created prior to January 1, 2008.



CITY OF MEDFORD
EXHIBIT #
File # ZC-18-008

2



After recording return to:
Stylus Development LLC
2950 East Barnett
Medford, OR 97504

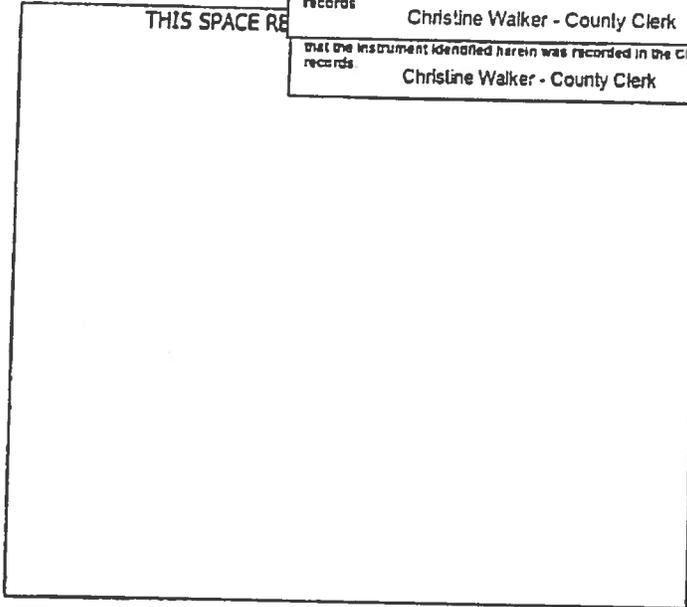
Until a change is requested all tax
statements shall be sent to the
following address:
Stylus Development LLC
2950 East Barnett
Medford, OR 97504

File No.: 7161-2973051 (PS)
Date: December 14, 2017

Jackson County Official Records **2018-002530**
R-WD
Stn=16 MORGANSS 01/24/2018 09:42:46 AM
\$15 00 \$10.00 \$8 00 \$11 00 \$20 00 **\$64.00**

I, Christine Walker, County Clerk for Jackson County, Oregon certify
that the instrument identified herein was recorded in the Clerk
records
Christine Walker - County Clerk

that the instrument identified herein was recorded in the Clerk
records
Christine Walker - County Clerk



STATUTORY WARRANTY DEED

Douglas J. Van Dyck and Gregory L. Van Dyck, co-Trustees of The Eileen M. Van Dyck Trust, Grantor, conveys and warrants to Stylus Development LLC, an Oregon limited liability company, Grantee, the following described real property free of liens and encumbrances, except as specifically set forth herein:

LEGAL DESCRIPTION: Real property in the County of Jackson, State of Oregon, described as follows:

From the Intersection of the east line of Donation Land Claim No. 50 in Township 37 South, Range 1 West of the Willamette Meridian in Jackson County, Oregon, with the Southerly right of way of County Road (Barnett Road), measure 500 feet Easterly along said right of way line to the point of beginning; thence South 339 feet; thence East 150 feet; thence North 369 feet to an intersection with the southerly line of said County Road; thence South 76°51' West, along said southerly line of said County Road, 150 feet to the point of beginning.

Excepting that portion of land conveyed to The City of Medford, an Oregon Municipal Corporation, by Deed recorded February 13, 1998 as Document No. 98-05466.

NOTE: This legal description was created prior to January 1, 2008.

Subject to:

1. Covenants, conditions, restrictions and/or easements, if any, affecting title, which may appear in the public record, including those shown on any recorded plat or survey.

The true consideration for this conveyance is **\$385,000.00**. (Here comply with requirements of ORS 93.030)

RE-RECORDED TO
CORRECT LEGAL

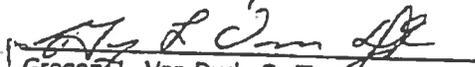
BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010. THIS INSTRUMENT DOES NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY THAT THE UNIT OF LAND BEING TRANSFERRED IS A LAWFULLY ESTABLISHED LOT OR PARCEL, AS DEFINED IN ORS 92.010 OR 215.010, TO VERIFY THE APPROVED USES OF THE LOT OR PARCEL, TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES, AS DEFINED IN ORS 30.930, AND TO INQUIRE ABOUT THE RIGHTS OF NEIGHBORING PROPERTY OWNERS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010.

Dated this 15 day of July, 2017.

Douglas J. Van Dyck and Gregory L. Van Dyck,
Successor Co-Trustees of the Eileen M. Van
Dyck Trust



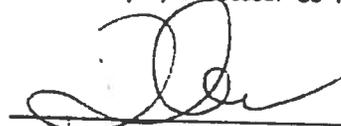
Douglas J. Van Dyck, Co-Trustee



Gregory L. Van Dyck, Co-Trustee

STATE OF Oregon)
County of Jackson)ss.
)

This instrument was acknowledged before me on this 15 day of Dec, 2017
by as of Douglas J. Van Dyck and Gregory L. Van Dyck, Successor Co-Trustees of the Eileen M. Van
Dyck Trust, on behalf of the .



Notary Public for Oregon
My commission expires: 8/19/18



CORRECTED LEGAL

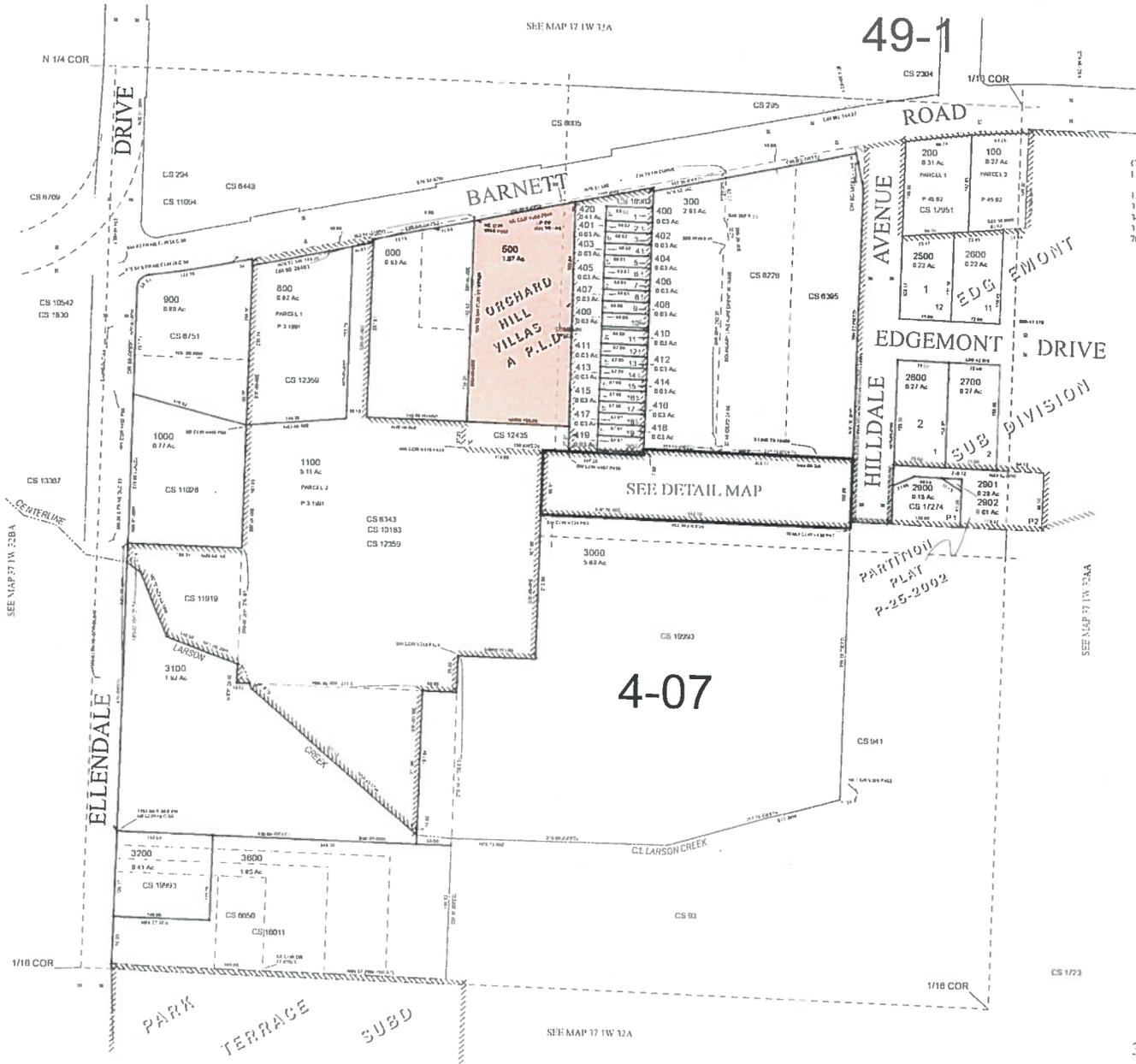
From the intersection of the east line of Donation Land Claim No. 50 in Township 37 South, Range 1 West of the Willamette Meridian in Jackson County, Oregon, with the Southerly right of way of County Road (Barnett Road), measure 500 feet Easterly along said right of way line to the point of beginning; thence South 339 feet; thence East 150 feet; thence North 369 feet to an intersection with the southerly line of said County Road; thence South 76°51' West, along said southerly line of said County Road, 150 feet to the point of beginning. EXCEPTING THEREFROM the South 37.20 feet as more fully described in Warranty Deed recorded as Document No. 90-23824, Official Records, Jackson County, Oregon. ALSO EXCEPTING THEREFROM that portion of land conveyed to The City of Medford, an Oregon Municipal Corporation, by Deed recorded February 13, 1998 as Document No. 98-05466.

NOTE: This legal description was created prior to January 1, 2008.

FOR ASSESSMENT AND TAXATION ONLY

N.W.1/4, N.E.1/4, SEC.32, T.37S., R.1W., W.M.
JACKSON COUNTY
1" = 100'

37 1W 32AB
MEDFORD



CANCELLED TAX
LOT NUMBERS
3200-2400 ON DETAIL MAP
3400 ADDED TO 3200
3400 ADDED TO 3600
3500 ADDED TO 3600
700 ADDED TO 600

49-1

4-07

RECEIVED
JAN 23 2010
PLANNING DEPT
37 1W 32AB
MEDFORD
NEW MAP JAN 23 2010
REV JAN 19 2006

RECEIVED
JAN 23 2010
PLANNING DEPT

LEGAL DESCRIPTION

Preliminary Report

Order No.: 7161-2973051
Page 7 of 8

Exhibit "A"

Real property in the County of Jackson, State of Oregon, described as follows:

From the intersection of the east line of Donation Land Claim No. 50 in Township 37 South, Range 1 West of the Willamette Meridian in Jackson County, Oregon, with the Southerly right of way of County Road (Barnett Road), measure 500 feet Easterly along said right of way line to the point of beginning; thence South 339 feet; thence East 150 feet; thence North 369 feet to an intersection with the southerly line of said County Road; thence South 76°51' West, along said southerly line of said County Road, 150 feet to the point of beginning.

Excepting that portion of land conveyed to The City of Medford, an Oregon Municipal Corporation, by Deed recorded February 13, 1998 as Document No. 98-05466.

NOTE: This legal description was created prior to January 1, 2008.



City of Medford Planning Department

Vicinity
Map

File Number:

ZC-18-008



Project Name:

**Stylus Development LLC
Zone Change**

Map/Taxlot:

371W32AB TL 500

0 120 240
Feet

02/01/2018

Legend

-  Subject Area
-  Tax Lots

