

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA JANUARY 18, 2019



Commission Members

Jim Quinn, Chair

Bill Chmelir, Vice Chair

Jeff Bender

Jim Catt

David Culbertson

Bob Neatham

Marcy Pierce

Milo Smith

Rick Whitlock

City Council Liaison - Dick Gordon

City Council Liaison Alt. - Tim D'Alessandro

Regular Commission meetings are held on
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380



Site Plan and Architectural Commission

Agenda

Public Hearing

January 18, 2019

12:00 noon

Council Chambers, City Hall, Room 300
411 West Eighth Street, Medford, Oregon

-
10. Roll Call.
 20. Consent Calendar.
 30. Minutes.
 - 30.1 Consideration for approval of minutes from the January 4, 2019, meeting.
 40. **Oral and Written Requests and Communications for Items Not on the Agenda.**
Comments will be limited to 3 minutes per individual or 5 minutes if representing a group or organization. **PLEASE SIGN IN.**
 50. **Public Hearings.**
Comments are limited to a total of 10 minutes for applicants and/or their representatives. You may request a 5-minute rebuttal time. All others will be limited to 3 minutes per individual or 5 minutes if representing a group or organization. **PLEASE SIGN IN.**

Old Business.

- 50.1 **AC-18-126** Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on a parcel totaling 0.76-acres, located at the corner of Garfield Street and Center Drive (1408 Center Drive) in the Regional Commercial (C-R) zoning district (371W32B TL 4801 & 3605); (Barry Thiriot, Applicant; Phillip Moss, Agent; Dustin Severs, Planner).
- 50.2 **AC-18-138
E-18-139** Consideration of the construction of steel buildings for a shop and office approximately 6,300 square feet with an exception to the General Design Requirements for Parking, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved in size on a 4.0 acre parcel located at 356 Bateman Drive approximately 450 feet west of Bierson Way zoned I-G (General Industrial) (362W36D TL 142), (Jeff and Alyson Fowler, Applicants; Liz Conner, Planner).

New Business.

- 50.3 **AC-18-147** Consideration to construct a 43,170 square foot self-storage facility on a 2.37 acres parcel zoned I-L (Light Industrial), located approximately 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62). (371W18AA TL 1200-1400). (Delta Waters Lenders, Applicant; Bill Philp, Agent; Liz Conner, Planner).

Meeting locations are generally accessible to persons with disabilities. To request interpreters for hearing impaired or other accommodations for persons with disabilities, please contact the ADA Coordinator at (541) 774-2074 or ada@cityofmedford.org at least three business days prior to the meeting to ensure availability. For TTY, dial 711 or (800) 735-1232.

- 60. **Written Communications. None**
- 70. **Unfinished Business. None**
- 80. **New Business. None**
- 90. **Report from the Planning Department.**
- 100. **Messages and Papers from the Chair.**
- 110. **Propositions and Remarks from the Commission.**
- 120. **City Council Comments.**
- 130. **Adjournment.**



Site Plan and Architectural Commission

Minutes

From Public Hearing on January 4, 2019

The regular meeting of the Site Plan and Architectural Commission was called to order at noon in the Council Chambers on the above date with the following members and staff in attendance:

Commissioners Present

Jim Quinn, Chair
Jeff Bender
Jim Catt
Dave Culbertson
Bob Neathamer
Marcy Pierce
Rick Whitlock
Dick Gordon, City Council Liaison

Staff Present

Kelly Evans, Assistant Planning Director
Katie Zerkel, Senior Assistant City Attorney
Debbie Strigle, Recording Secretary

Commissioners Absent

Bill Chmelir, Vice Chair, excused
Milo Smith, unexcused

10. Roll Call.
20. Consent Calendar/Written Communications. None.
30. Minutes.
 - 30.1 The minutes for the December 21, 2018, meeting, were approved as submitted.
Commissioner Whitlock pointed out
40. Oral and Written Requests and Communications. None.
50. Public Hearings.

Katie Zerkel, Senior Assistant City Attorney, read the rules governing the public hearings.

Continuance Request.

50.1 **AC-18-126** Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on a parcel totaling 0.76-acres, located at the corner of Garfield Street and Center Drive (1408 Center Drive) in the Regional Commercial (C-R) zoning district (371W32B TL 4801 & 3605); (Barry Thiriot, Applicant; Phillip Moss, Agent; Dustin Severs, Planner).

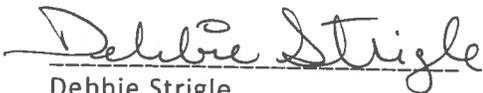
Motion: Continue this item to the January 18, 2019, meeting, per the applicant's request.

Moved by: Commissioner Whitlock Seconded by: Commissioner Culbertson

Roll Call Vote: Motion passed, 7-0.
60. Written Communications. None.
70. Unfinished Business. None.

80. New Business. None.
90. Report from the Planning Department.
- 90.1 Ms. Evans announced there is business scheduled for the January 18, 2019, meeting.
- 90.2 Ms. Evans reminded the Commissioners of the Boards and Commissions Luncheon on Friday, January 11, 2019.
- 90.3 Ms. Evans reported that City Council had reappointed Commissioners Catt and Bender to this Commission.
- 90.4 Ms. Evans stated that the City had received a notice of intent to appeal City Council's decision on their Transportation System Plan to LUBA.
100. Messages and Papers from the Chair. None
110. Propositions and Remarks from the Commission.
- 110.1 Commissioner Whitlock thanked Commissioners Catt and Bender for receiving their reappointments to this Commission. He said he appreciated their continued commitment to this Commission and to the individual members of this Commission.
120. City Council Comments.
130. Adjournment
- 130.1 The meeting was adjourned at approximately 12:07 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:



Debbie Strigle
Recording Secretary

Jim Quinn
Site Plan and Architectural Commission Chair

Approved: January 18, 2019



STAFF REPORT

for a Type-III quasi-judicial decision: **Site Plan and Architectural Review**

PROJECT KFC
Applicant: Barry Thiriot
Agent: Phillip Moss

FILE NO. AC-18-126

TO Site Plan and Architectural Commission

for January 18, 2019 hearing

FROM Dustin Severs, Planner III

REVIEWER Kelly Evans, Assistant Planning Director *W.*

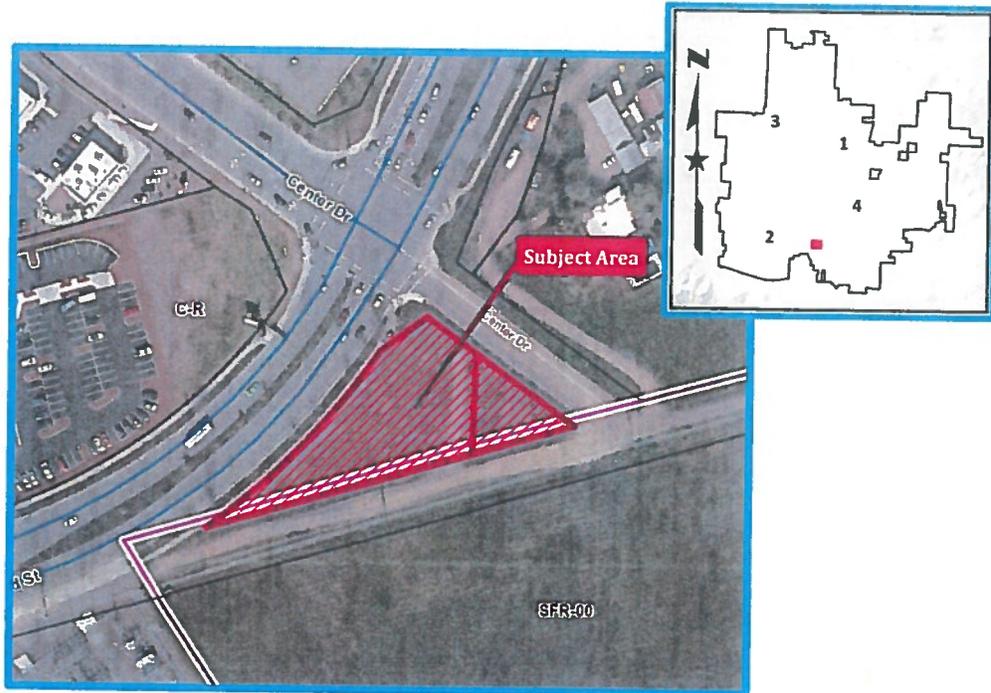
DATE January 11, 2018

BACKGROUND

Proposal

Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on two parcels totaling 0.76-acres, located at the corner of Garfield Street and Center Drive (1408 Center Drive) in the Regional Commercial (C-R) zoning district (371W32B TL 4801 & 3605).

Vicinity Map



Subject Site Characteristics

Zoning: C-R (Regional Commercial)
GLUP: CM (Commercial)
Overlay(s): None
Use: Vacant land

Surrounding Site Characteristics

<i>North</i>	Zone:	C-R
	Use(s):	Rogue Valley Credit Union parking lot
<i>South</i>	Zone:	SFR-00 (Single Family Residential, one dwelling per lot)
	Use(s):	Vacant
<i>East</i>	Zone:	C-R
	Use(s):	South Center shopping center
<i>West</i>	Zone:	C-R
	Use(s):	Pacific Power substation

Related Projects

SV-17-039	Partial vacation of Belknap Road
LDP-17-131	Partition
PA-18-014	Pre-application to discuss subject project

Applicable Criteria

MLDC Section: 10.200(E)(1) – Site Plan & Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

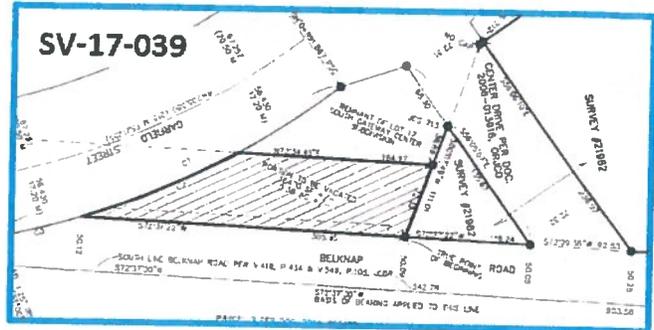
- (a) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (b) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.186.*

ISSUES AND ANALYSIS

Project Summary

Site History

The subject parcels were approved by City Council for a partial vacation of Belknap Road on December 7, 2017, which added 0.38 acres to parcel 4801. The applicant initially requested that all of Belknap Road fronting the subject site be vacated by the City; however, the applicant of the vacation request (Southside Center, LLC.) was unable to gain the necessary signatures from the adjoining properties of the area, as required per ORS 271.130. Consequently, the Southside Center revised their request to only include the vacation of the northernmost portion of right-of-way totaling 0.38 acres, while removing the 50-foot wide portion of Belknap Road as part of the vacation request. This request was ultimately approved, giving the site its current layout.



The subject parcels were part of a three-lot partition (LDP-17-131), which received tentative plat approval on February 22, 2018. The partition was required as a condition of approval for the South Side Center Phase I development (AC-16-138). While only the partitioning/consolidation of Parcel 1 was required, the applicant (Galpin Gang) elected to incorporate the subject parcels into the partition, as they had also been identified by the City Surveyor as being illegally created (Exhibit P). At the time of this writing, final plat approval has not been granted for LDP-17-131.



Current proposal

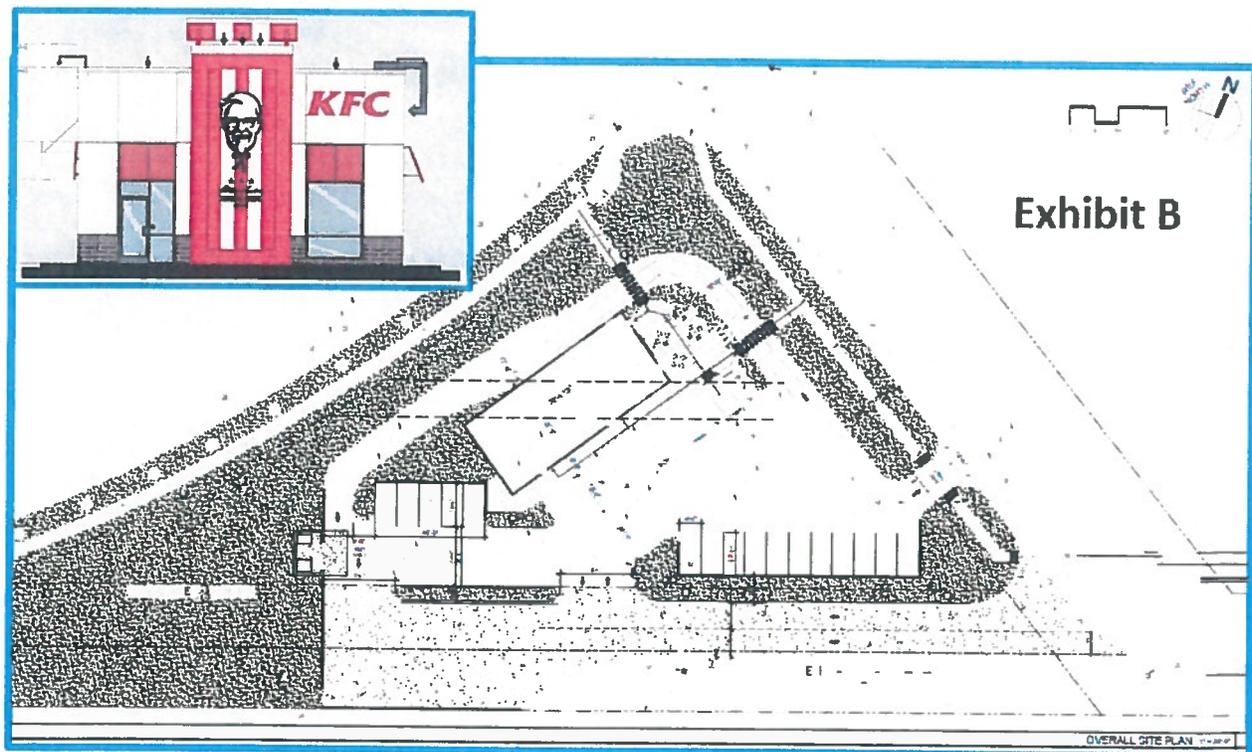
The applicant submitted the subject request concurrently with a request for the vacation of Belknap Road (SV-18-125), which runs along the southerly boundary of the site. The approval of the vacation of Belknap Road would preclude the applicant from constructing public improvements along this right-of-way, and would likewise preclude the applicant from providing frontage landscaping along this future constructed street. Accordingly, the original plans that were submitted for the subject application (e.g., site plan, landscape plan, etc.)



were based on the assumption that the vacation would be approved, with the plans showing a layout which excluded both the improvement of Belknap Road as public right-of-way and full frontage landscaping running along the length of the right-of-way. At the time of this writing, the vacation application has not yet been deemed complete by planning staff, as the submitted application did not include the necessary signatures of adjoining properties as statutorily required by ORS.

At the Land Development meeting held on December 12, 2018, staff explained to the applicant's agent that City staff's review of the application would have to be based on the current layout of the surrounding area, which includes Belknap Road as a public right-of-way running parallel to the site along its southerly boundary. As such, the applicant was asked to submit a revised site plan showing Belknap Road on the site plan, and that all public improvements would have to be made to this right-of-way prior to the issuance of building permits. It was further explained to the applicant's agent that a revised landscape plan would also need to be submitted, as the original landscape plan – anticipating that an approved vacation of Belknap would eliminate this right-of-way – does not show the required frontage landscaping along the site's frontage with Belknap Road. A revised site plan and landscape plan was submitted by the applicant on January 9, 2019.

As shown on the revised site plan shown below (Exhibit B), the applicant is proposing to develop the subject site as the location for a KFC restaurant. The development will consist of a single 2,849 square foot building and a drive thru. The proposed restaurant will also include 42 indoor seats, and 16 seats to be located outdoors at the front patio of the building.



In addition to the standard SPAC approval for the request, the applicant has also formally requested relief from the following:

- Relief from meeting minimum required parking spaces, as required per MLDC 10.743-1.
- Relief to exceed 15 seats for the proposed outdoor seating area, as limited per MLDC 10.833.

Site Development Standards

SITE DEVELOPMENT TABLE

	Allowed/Required	Proposed
Height	35 feet	23'-6"
Lot Coverage	40%	9%
Setback (front)	10 feet	15'
Setback (sides)	None Except ½ feet for each foot in building height over 20 feet	67' & 50'-7"
Setback (rear)	None Except ½ feet for each foot in building height over 20 feet	39'-4"

As shown in the Site Development Table above, it can be found that the proposed structure identified on the submitted site plan meets the bulk standards for the C-R zoning district as found in Article V of the Medford Land Development Code.

Parking

PARKING TABLE (10.743-751)

	Required	Provided
Total Spaces	34	26
Accessible Spaces	2	2
Bicycle Spaces	3	6

The submitted site plan identifies a total of 26 parking spaces, including the required number of accessible and bicycle spaces; however, the total number of spaces shown on the site plan is short eight spaces.

(3) **Exceptions to Required Off-Street Parking for Non-Residential Uses.** The approving authority may allow exceptions to the number of parking spaces in Table 10.743-1 for specific uses without complying with Section 10.186 if they find that the applicant's detailed description of the proposed use demonstrates that the number of needed parking spaces is less than the minimum required or more than the maximum allowable based upon one or both of the following:

- (a) An explanation why the characteristics of the proposed use require a different off-street parking standard than what is otherwise required.
- (b) An analysis providing parking data for the same business or a similar use within the city that demonstrates a need for a different off-street parking standard than what is otherwise required.

Pursuant to MLDC 10.743(3) cited above, the applicant has requested relief from meeting the minimum number of parking spaces required of the site. The applicant's findings state the following:

There is a 7-car drive thru lane that will relieve the need for the additional parking. Approximately 50% of a KFC business come thru the drive thru.

The guidelines for parking stalls for KFC restaurants by the corporation is 24 parking stalls should be the goal for an new restaurants. Allowing the reduction would eliminate the additional unneeded 1,068 square feet of hard surface area that would not be required for parking and instead be used for landscaping.

Landscaping

LANDSCAPE TABLE – Frontage Landscaping

Center Drive	Required	Shown
Trees	7	5
Shrubs	44	+44

Garfield Street	Required	Shown
Trees	13	10
Shrubs	85	+85

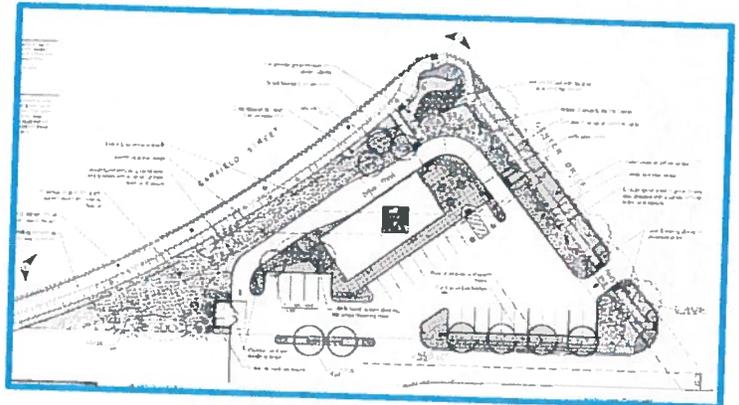
Belknap	Required	Shown
Trees	14	8
Shrubs	90	+90

LANDSCAPE TABLE – Parking Area Planters (10.746[3])

	Required		Shown
	34 spaces	26 spaces	
Trees	4	3	2
Shrubs	8	6	+8
Coverage area	705 s.f.	540 s.f.	+705 s.f.

The submitted landscape plan (Exhibit H), shows landscaping along all three street frontages of the site, and located within the parking area; however, the plan is deficient the minimum number of trees required to be located within the parking area and along all three street frontages.

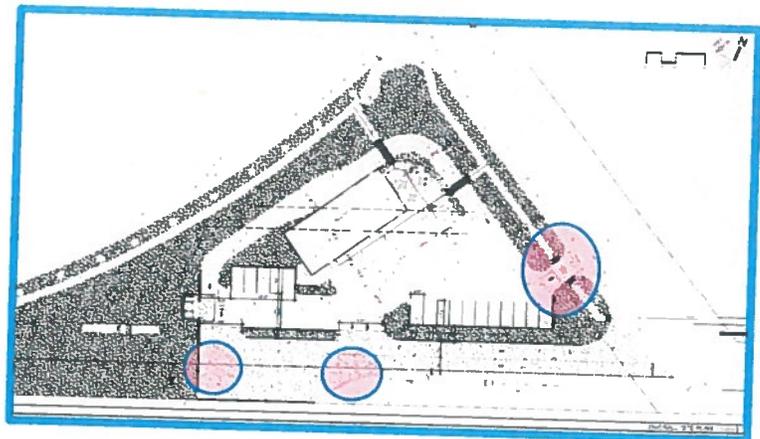
As a condition of approval, the applicant will be required to submit a revised site plan showing the required number/amount of trees, shrubs, and coverage area, pursuant to MLDC 10.746 and MLDC 10.797.



Access

Vehicular Access

The site plan shows a vehicular entrance to the site via a driveway off of the future Belknap Road. The site plan also shows a second drive off of the future Belknap Road, identified as *exit-only*, and another *exit-only* drive off of Center Drive



Pedestrian Access

The site plan shows pedestrian walkways connecting the proposed building to the public sidewalks running along Garfield Street and Center Drive, consistent with the Pedestrian connectivity standards found in MLDC 10.772-776. However, no such connection is shown between the building and the public sidewalk (not shown on the site plan) required to be located along Belknap Road.



As a condition of approval, the applicant will be required to show a pedestrian walkway connecting the proposed building to the public sidewalk running along Belknap Road, consistent with the pedestrian connectivity standards found in MLDC 10.722-776, prior to the issuance of building permits for vertical construction.

Outdoor seating

MLDC 10.833 – Restaurants-Outdoor Eating Areas – reads as follows:

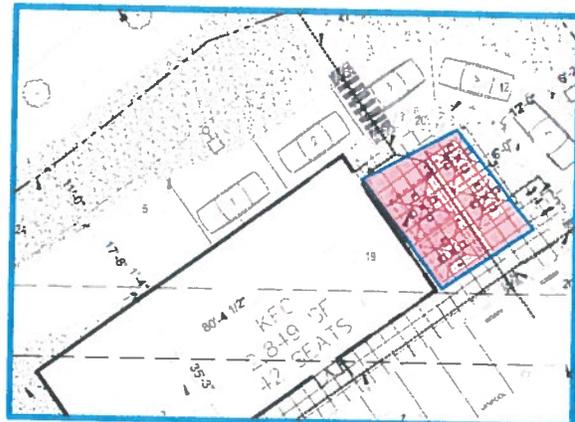
10.833 Restaurants - Outdoor Eating Areas.

Outdoor eating areas shall be allowed for restaurants in all commercial and industrial zoning districts subject to the following:

- (1) Compliance with all other provisions of this Chapter.
- (2) Historic Review or Site Plan and Architectural Review as applicable and approval when the outdoor eating area includes seating for more than 15 patrons.
- (3) Where adjacent or abutting a residential zone, outdoor activity shall only be allowed between the hours of 8:00 a.m. and 9:00 p.m.

The submitted site plan shows four sets of four seats under umbrellas to be located outdoors within the front patio of the building.

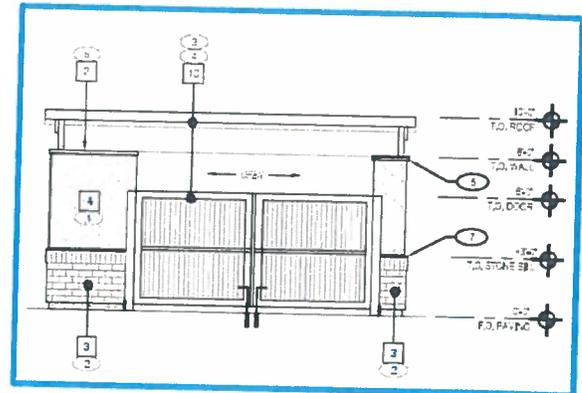
Staff is supportive of the applicant's request to be approved for up to 16 seats on the restaurant's exterior patio, as it is staff's view that the submitted site plan shows sufficient space to accommodate the additional seating.



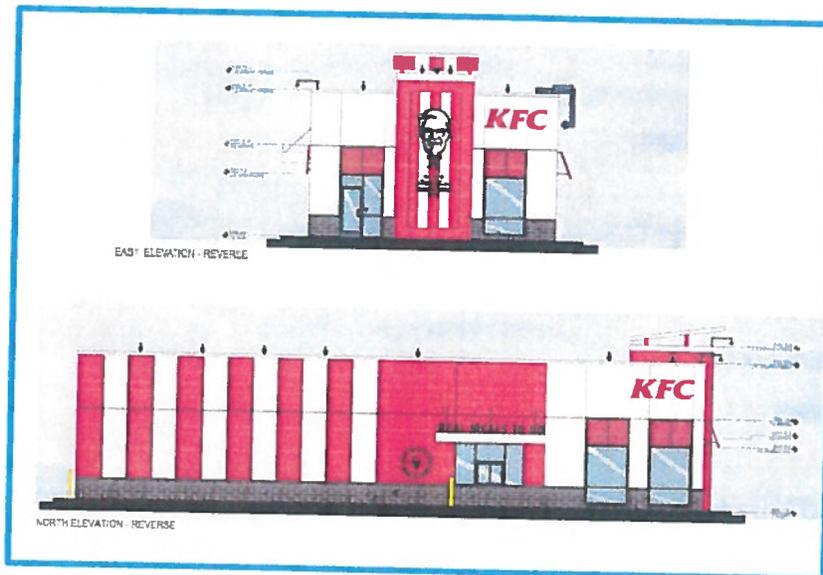
Concealments

Trash Enclosure (Exhibit I)

The trash enclosure plan (Exhibit I) identifies 10-foot high gated trash enclosure with brick veneer/cement plaster exterior and a metal roof, and the site plans shows the trash enclosure located to the southwest corner of the site, consistent with the requirements found in MLDC 10.781.



Architecture



The applicant's submitted narrative (Exhibit J) describes the building's proposed architecture and how it fits with and complements adjacent buildings, as the following:

The building is a modern design commercial building with red accent and a brick wainscot. It is similar in style to the Panda Express directly across the street.

The Exterior is broken up by white and red vertical stripping and brick wainscot. At the front there is a red and white stripped tower that breaks up the front façade.

Lot legality

Per comments received by the City Surveyor (Exhibit P), the two subject parcels were unlawfully created. The issue of lot legality is being addressed through the land partition initiated by Galpin Gang, LLC, which includes the subject parcels, and received tentative plat approval on February 22, 2018 (LDP-17-131); however, at the time of this writing, said partition has not received final plat approval through the Planning Director.

As a condition of approval, the final plat approval for LDP-17-131 must be completed prior to the issuance of building permits.

Existing Public Utility Easement

The topographic survey submitted by the applicant (Exhibit Q) identifies an existing 15-foot Public Utility Easement (PUE), which runs through the middle of the site. The extinguishment of a PUE is subject to ORS Chapter 271, and therefore must be removed in accordance with the City's vacation procedures.



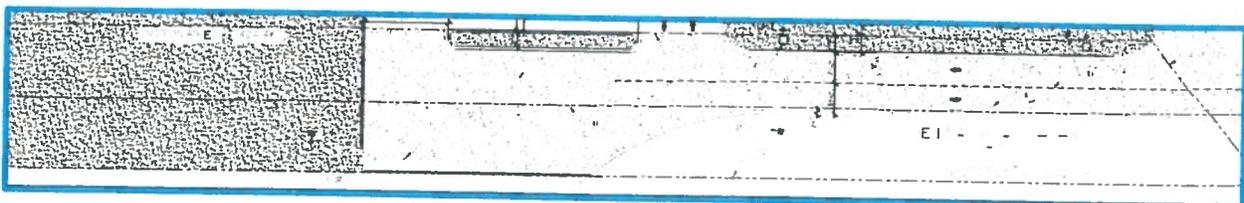
As a condition of approval, the applicant will be required to obtain approval for the vacation of the PUE identified on the property prior to issuance of building permits.

Public Improvements (Belknap Road)

As per staff report submitted by Public Works, the applicant is required to construct/improve the north half plus 2-feet south of the centerline along the site's frontage with Belknap Road to Commercial Street standards as outlined in MLDC 10.429(1), including the installation of curb/gutter, sidewalks, and a planter strip.

The submitted site plan shows Belknap Road stubbed at the site's westerly drive – short of the property's full southerly boundary – and does not show sidewalks or a planter strip along the roadway. The applicant's agent explained to staff that it is their intent to request relief; however, at the time of this writing, the applicant has not submitted an exception application nor has staff received findings from the applicant addressing the request.

As a condition of approval, the applicant will be required to comply with all requirements of the Public Works department, including constructing full street improvements along the north half plus 2-feet of the center line along the entire frontage of Belknap Road.



Facility Adequacy

Per the agency comments submitted to staff (Exhibits K-N), including the Rogue Valley Sewer Services (RVSS), it can be found that there are adequate facilities to serve the proposed development.

Other Agency Comments

Oregon Department of Transportation (ODOT) (Exhibit O)

Per the email received by staff, the applicant will be required to submit any utility work to ODOT, and the ODOT right-of-way cannot be used to meet the landscaping requirements of the City.

Rogue Valley Sewer Services (RVSS) (Exhibit N)

The subject site is located within the RVSS service area. Per the report submitted to staff, the applicant will be required to submit all sewer improvements to RVSS, and all sewer system development charges will be owed to RVSS.

The applicant will be required to comply with all requirement of both ODOT and RVSS prior to the issuance of building permits for vertical construction.

Committee Comments

No comments were received from committees such as BPAC.

FINDINGS OF FACT

MLDC 10.200(E)(1)

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the Applicant's Questionnaire and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit J) and recommends the Commission adopt the findings as recommended by staff.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the Final Order for approval of AC-18-126 per the staff report dated January 11, 2019, including Exhibits A through Q.

EXHIBITS

- A Conditions of Approval, drafted January 11, 2019.
- B Site Plan, received January 9, 2019.
- C Utility Plan, received October 19, 2018.
- D Drainage Plan, received October 19, 2018.
- E Roof Plan/floor plan, received October 19, 2018.
- F Elevation Plans, received October 19, 2018.
- G Color elevation Plan, received October 19, 2018.
- H Landscape Plan, received January 9, 2019.
- I Trash Enclosure elevations, October 19, 2018.
- J Applicant's Narrative, Questionnaire, and Findings of Fact, received October 19, 2018.
- K Public Works staff report, received January 10, 2019.
- L Medford Water Commission report & map, received December 12, 2018.
- M Medford Fire Department report, received December 12, 2018.
- N Rogue Valley Sewer Services (RVSS), received December 5, 2018.
- O Oregon Department of Transportation email, received December 12, 2018.
- P City Surveyor comments, email received December 18, 2018.
- Q Topographic Survey, received October 19, 2018.
Vicinity map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA

January 18, 2019

EXHIBIT A

KFC
AC-18-126
Conditions of Approval
January 18, 2019

CODE REQUIRED CONDITIONS

Prior to the issuance of the first building permit, the following shall be completed:

1. The final plat for LDP-18-131 must be approved by the Planning Director in accordance with MLDC 10.162, and the approved final plat recorded with the Jackson County Recorder.
2. The applicant shall obtain approval by City Council for the vacation of the 15-foot Public Utility Easement (PUE) identified on the submitted Topographic Survey.

Prior to the issuance of a building permit for vertical construction, the applicant shall:

3. Comply with all conditions stipulated by the Public Works Department, including the construction of full street improvements along the north half plus 2-feet of the center line along the property's entire frontage of Belknap Road (Exhibit K).
4. Comply with all conditions stipulated by the Medford Water Commission (Exhibit L).
5. Comply with all requirements of the Medford Fire Department (Exhibit M).
6. Comply with all requirements of the Rogue Valley Sewer Services (RVSS) (Exhibit N).
7. Comply with all requirements of the Oregon Department of Transportation (Exhibit O).
8. Submit a revised landscape plan showing the number of trees identified on the plan as meeting the minimum as required per MLDC 10.746(3).
9. Submit a revised site plan showing a pedestrian walkway connecting the proposed building to the public sidewalk running along Belknap Road, consistent with the pedestrian connectivity standards found in MLDC 10.722-776.

CITY OF MEDFORD
EXHIBIT # A
File # AC-18-126

**PHILLIP
MOSS
ARCHITECT
NCARB - AIA**

637 FIFTH AVE
SAN RAFAEL, CA 94903
415-451-2543 x 114
mossarchitect@aia.com

VMI JOB NUMBER
1701



EXHIBIT 6-10-19



18215 72ND AVE SUITE SOUTH
FREMONT, CA 94532
(475)251-8222
(475)251-8182 FAX
CIVIL, MECHANICAL, LAND PLANNING,
SURVEYING, ENVIRONMENTAL SERVICES
M.E. JOB # 20115

RECORD DATES
PREP SUBMIT TOTAL 06-18
DATE SUBMIT 06-18

REVISIONS

NO.	DATE	DESCRIPTION

CLIENT
Lanol Corporation
4400 Mission Park
Medford, Oregon 97504
Phone: (541) 776-1500

PROJECT
KFC RESTAURANT
1408 Cardon Dr
Medford, Oregon 97501
15081787



AS-K

PRELIMINARY
DRAINAGE PLAN
PM-1

- 1 PROPOSED PRIVATE STORMWATER CATCH BASIN WITH 24"x24" PRET GRAFE, TYPICAL.
- 2 PROPOSED LOCATION OF CONNECTION TO EXISTING STORMWATER CURB DRET
- 3 PROPOSED LOCATION OF STORMWATER DETENTION CHAMBERS WITH INFLATOR ROW
- 4 PROPOSED PRIVATE STORMWATER EDWETANCE LINE, TYPICAL
- 5 PROPOSED LOCATION OF STORMWATER FLOW CONTROL MANHOLE
- 6 PROPOSED PRIVATE STORMWATER CATCH BASIN WITH 24"x24" WITH SOLID LID

REVISIONS



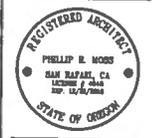
Curve #	Radius	Length	Data	Chord Direction	Chord Length
C2	761.81	114.79	B 38.00'	N 36.33 45° E	714.60
L1	761.81	129.22	B 38.00'	N 40.52 00° E	758.91

8" x 8" TRASH ENCLOSURE

KFC RESTAURANT
2,840 S.F. (CROSS)
42 SEATS

PHILLIP MOSS ARCHITECT NCARB - AIA
 837 FIFTH AVE
 SAN RAFAEL, CA 94901
 415 451-2500 x 114
 mossarchitect@aol.com

VM JOB NUMBER
1701



RECORD DATES
 PREPP SUBMITTAL 08/10
 DD SUBMITTAL 08/10

REVISIONS

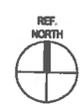
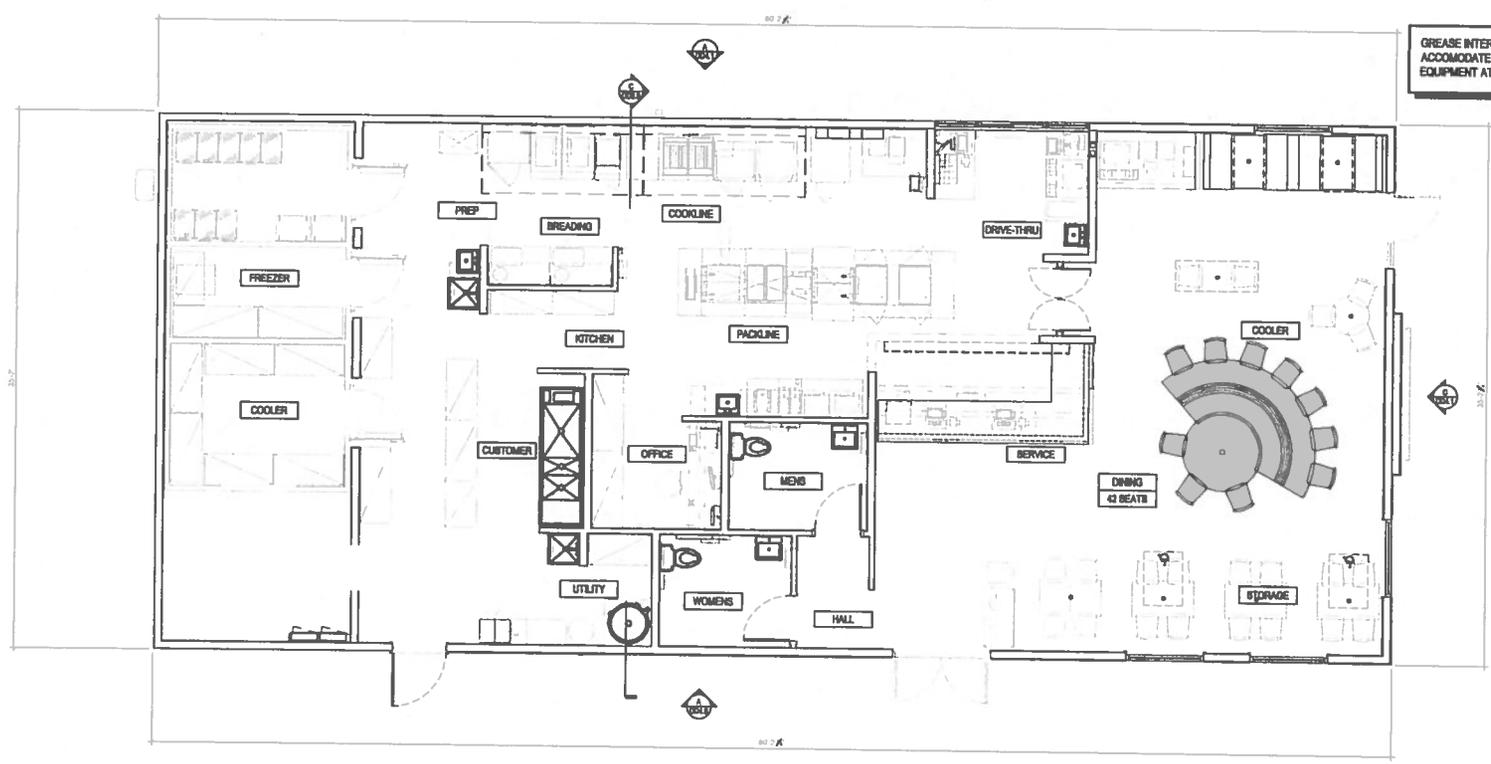
NO.	DESCRIPTION

CLIENT
 Loriel Corporation
 4800 Lakeside Plaza
 Medford, Oregon 97504
 Phone 541/776-8288

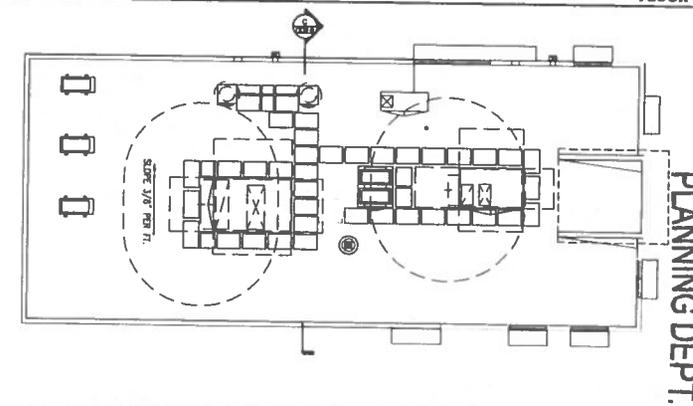
PROJECT
 KFC RESTAURANT
 1408 Conder Dr
 Medford, Oregon 97501
 1-0811787



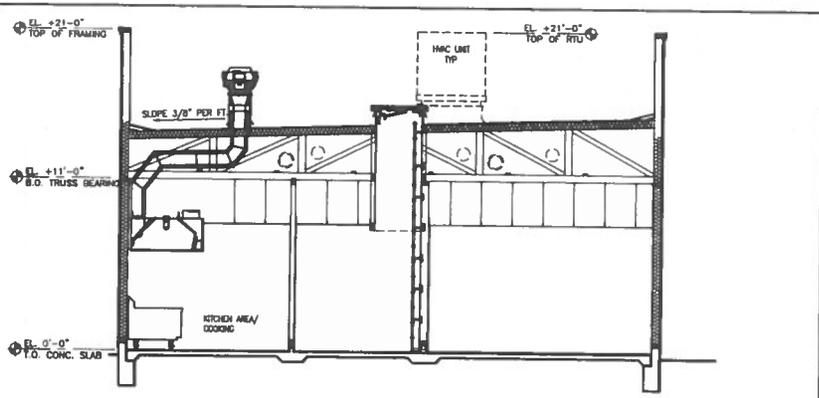
DD2.0



FLOOR PLAN 1/8" = 1'-0" A



ROOF PLAN 1/8" = 1'-0" B



BLDG. SECTION 1/8" = 1'-0" C

PHILLIP MOSS ARCHITECT NCARB - AIA

437 FIFTH AVE
SAN RAFAEL, CA 94901
415-451-2500 x 114
moosarchitect@gmail.com

VMI JOB NUMBER
1701

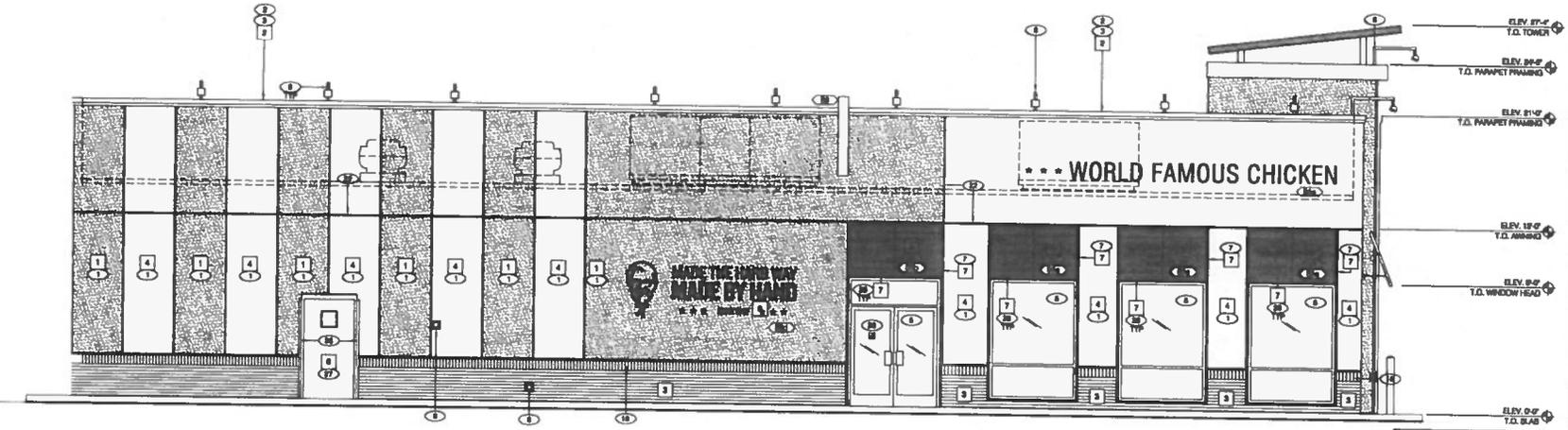


RECORD DATES
PREREQ SUBMITTAL 08/19
CD SUBMITTAL 08/19

REVISIONS

CLIENT
Lined Corporation
6200 Mendocino Place
Medford, Oregon 97504
Phone: (541) 778-0288

PROJECT
KFC RESTAURANT
1408 Center Dr.
Medford, Oregon 97501
1-0818177



SOUTH ELEVATION 1-1-17 A

NO.	EXT. FINISH AREA	AMOUNT (SQ. FT.)	NOTE	FINISH NAME	ALTERNATE COLORS	CONTACT
1	EXIST. FIN. WALL AREA					
1	EXIST. FIN. PANEL	DE FLAMMABLE	ALPHA EXTENSION PAINT BROWN PEBBLE 8088-10 BATH (801) 100% ACRYLIC HYBRID RESIN SELF PRIMING			KURT MOORELAND P: (800) 888-8147 / C: (800) 840-1888 kurt.mooreland@compconusa.com
2	EXIST. FIN. PANEL	DE FLAMMABLE	COMMON POLYURETHANE URETHANE 70 VISIONS W/CL, 6088 GLOSS CATALYST			KURT MOORELAND P: (800) 888-8147 / C: (800) 840-1888 kurt.mooreland@compconusa.com
3	BACK UP BRICK WALLS BRANCOLO / CLUMS	1411 SQ. FT.	BRICK BLACK HERITAGE GROUT: GLENSHIRE GRSS	GROUT: LATORITE GILT	FIN. (LOCAL) MORTAR REQUIREMENTS	
4	EXIST. FIN. PANEL	DE FLAMMABLE	ALPHA EXTENSION PAINT BLACK HORIZON 8188-02 BATH (801) 100% ACRYLIC HYBRID RESIN SELF PRIMING			KURT MOORELAND P: (800) 888-8147 / C: (800) 840-1888 kurt.mooreland@compconusa.com
5	TERMINATED AWNING	150 - 4 SQUARE FT.	POLYURETHANE CUSTOM MATCH BRANCOLO MOORE 8088-10 EXCITIC RED BATH P88H			
6	DEK LAYERS		PAINT 2 COAT WITH GLENSHIRE GRSS			
7	PAINT (2) STAIR CASE PANELS		ALPHA EXTENSION PAINT BLACK HORIZON 8188-02 BATH (801) 100% ACRYLIC HYBRID RESIN SELF PRIMING	DE FLAMMABLE		KURT MOORELAND P: (800) 888-8147 / C: (800) 840-1888 kurt.mooreland@compconusa.com

EXTERIOR FINISH SCHEDULE F

NO.	QTY	DESCRIPTION	UNIT
1	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
2	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
3	0	NO FINISHES TO BE APPLIED TO THIS AREA	
4	2	NO. 10 CHAIRS 11111111111111111111	
5	0	NO. 10 CHAIRS 11111111111111111111	
6	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
7	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
8	0	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
9	0	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
10	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
11	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
12	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
13	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
14	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
15	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
16	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
17	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
18	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
19	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
20	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
21	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
22	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
23	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
24	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
25	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
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27	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
28	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
29	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
30	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
31	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
32	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
33	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
34	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
35	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
36	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
37	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
38	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
39	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
40	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
41	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
42	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
43	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
44	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
45	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
46	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
47	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
48	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.
49	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1541 FT. FT.	FT. FT.
50	1	WRITE PAINTS/GRAPHICS FOR SIGNAGE PANEL 1181 FT. FT.	FT. FT.

IMAGE COMPONENTS SCHEDULE D

GENERAL NOTES

1. NOT USED.
2. PAINT COLORS SHOWN ARE COLOR REFERENCES FOR THE E.L.F. & SUPPLIER.

3. SEALING (REFER TO SPEC):
A. SEALANT AT ALL WALL AND ROOF PENETRATIONS.
B. SEALANT AT ALL WINDOW AND DOOR FRAMES AT HEAD AND JAMB. DO NOT SEAL IN AT WINDOWS.

4. EXTERIOR FINISH PROTECTION:
A. EXTERIOR BUILDING PAINT COLORS SHALL RECEIVE A UV PROTECTION FINISH COAT OF SHEEN-WEATHER-RESISTANT CLEAR COAT OR 8088-02 OR 8088-03 OR 8088-04 OR 8088-05 OR 8088-06 OR 8088-07 OR 8088-08 OR 8088-09 OR 8088-10 OR 8088-11 OR 8088-12 OR 8088-13 OR 8088-14 OR 8088-15 OR 8088-16 OR 8088-17 OR 8088-18 OR 8088-19 OR 8088-20 OR 8088-21 OR 8088-22 OR 8088-23 OR 8088-24 OR 8088-25 OR 8088-26 OR 8088-27 OR 8088-28 OR 8088-29 OR 8088-30 OR 8088-31 OR 8088-32 OR 8088-33 OR 8088-34 OR 8088-35 OR 8088-36 OR 8088-37 OR 8088-38 OR 8088-39 OR 8088-40 OR 8088-41 OR 8088-42 OR 8088-43 OR 8088-44 OR 8088-45 OR 8088-46 OR 8088-47 OR 8088-48 OR 8088-49 OR 8088-50 OR 8088-51 OR 8088-52 OR 8088-53 OR 8088-54 OR 8088-55 OR 8088-56 OR 8088-57 OR 8088-58 OR 8088-59 OR 8088-60 OR 8088-61 OR 8088-62 OR 8088-63 OR 8088-64 OR 8088-65 OR 8088-66 OR 8088-67 OR 8088-68 OR 8088-69 OR 8088-70 OR 8088-71 OR 8088-72 OR 8088-73 OR 8088-74 OR 8088-75 OR 8088-76 OR 8088-77 OR 8088-78 OR 8088-79 OR 8088-80 OR 8088-81 OR 8088-82 OR 8088-83 OR 8088-84 OR 8088-85 OR 8088-86 OR 8088-87 OR 8088-88 OR 8088-89 OR 8088-90 OR 8088-91 OR 8088-92 OR 8088-93 OR 8088-94 OR 8088-95 OR 8088-96 OR 8088-97 OR 8088-98 OR 8088-99 OR 8088-100.

5. HIGH IMPACT E.L.F.:
A. AS REQUIRED FOR SITE-SPECIFIC DESIGN, E.L.F. & BASE COAT WITH 50 G. HIGH IMPACT MESH TO BE APPLIED FROM STARTER TRACK TO 7'-0" A.F.F. REFER TO SPECIFICATION MANUAL FOR E.L.F. & MANUFACTURER'S ASSEMBLY DETAILS.

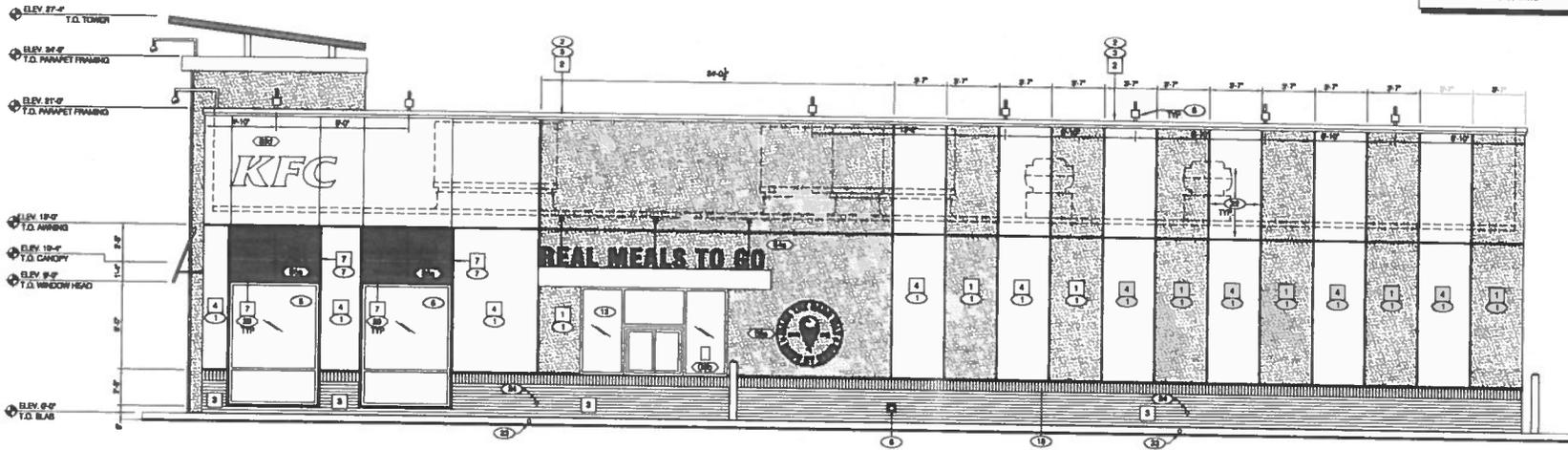
6. CRITICAL FINISH DETAIL:
BUILDING FINISH OPENINGS THAT ARE DESIGNATED AS "CRITICAL" SHALL BE MAINTAINED TO PROTECT FOR THE REMOVAL/INSTALLATION OF STANDARD SIGNAGE / BUILDING ELEMENTS. SEE GENERAL NOTE FOR WINDOW PROFILES / WINDOW INSTALLED ELEMENTS.

7. BRICK INSTALLATION:
THE GENERAL CONTRACTOR TO ENSURE THAT THE BRICK, CENTER FLASH AND LEVEL, AND THE BRICK FACER ALIGNED AND FLUSH.

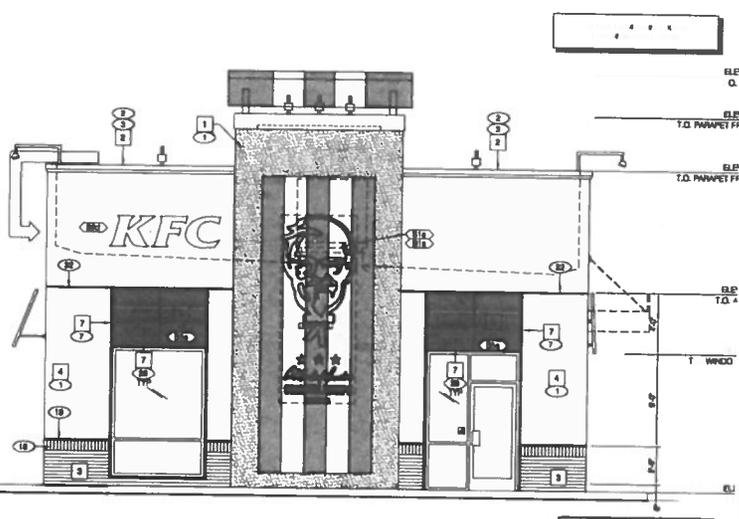
8. WINDOW SUPPLIED / INSTALLED ELEMENTS:
GC TO COORDINATE WITH WINDOW PROVIDER / WINDOW INSTALLED SIGNAGE AND BUILDING ELEMENTS.

1. PAINT WALL SURFACE BEHIND NEW ANNINGS
2. PRE-FINISH METAL LOUVERED ANNINGS (ANNINGS ABOVE DOORS TO HAVE FINISH DETAIL)
3. PARAPET FINISH CAP.
4. STAND OFF METAL PANEL SYSTEM BY VINYL GRAPHICS BY VENDOR
5. ALUMINUM STOREFRONT WINDOW / DOOR SYSTEM.
6. WALL LIGHTING - LIGHTING WINDOW SUPPLIED / GC INSTALLED.
7. JTD CORP. IT-DRAWABLE E.L.F. REFER TO SPECIFICATION MANUAL FOR MANUFACTURER'S ASSEMBLY DETAILS, FIELD SUPPORT SERVICES AND CONTACTS.
8. HOME BIL.
9. COX FILLER VALVE & COVER.
10. METAL PANEL SYSTEM ATTACHED TO AND HELD OFF TOWER TOP OF PARAPETS TO ALIGN WITH BOTTOM OF TOWER CORNICE.
11. SWITCHGEAR.
12. INDICATES TOP OF ROOF DECK.
13. DRIVE THRU WINDOW.
14. GAS METER. DO NOT PAINT METER.
15. STEEL BOLLARD W/ PVC BLEND. SEE SITE DETAILS.
16. TOP OF RTUL COORDINATE WITH MANUFACTURER'S SPECIFICATION.
17. EXHAUST FANS ON ROOF.
18. THIN BRICK BOLDER COURSE.
19. TRANSITION FROM THIN BRICK -> E.L.F.
20. THIN BRICK FLASHING BOND.
21. LINE OF PAINT COLOR CHANGE.
22. "V" GROOVE IN E.L.F.
23. ROOF ACCESS LADDER.
24. THRU WALL OVERFLOW DRAIN DRAINAGE.
25. STAIRWELL STEEL CORNER GUARDS.
26. HANDICAP SIGNAGE. MOUNT AT 8'-0" A.F.F.
27. SECURITY DOOR. OWNER-SUPPLIED / B.C. INSTALL.
28. PRE-FINISH METAL LOUVERED ANNINGS (ANNINGS ABOVE DOORS TO HAVE FINISH DETAIL).
29. PAINT WALL SURFACE BEHIND NEW ANNINGS.

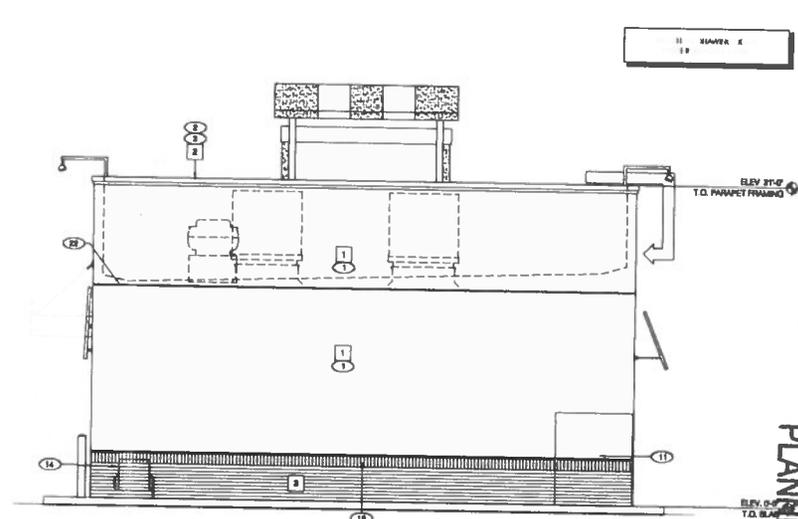
KEY NOTES



NORTH ELEVATION A



EAST ELEVATION C



WEST ELEVATION B

PHILLIP MOSS ARCHITECT NCARB - AIA
 637 FIFTH AVE
 SAN RAFAEL, CA 94001
 415-451-2500 x 114
 mossarchitect@icloud.com

VMI JOB NUMBER
1701



RECORD DATES
 PREP SUBMITTAL 08/16
 CD SUBMITTAL 08/16

REVISIONS

CLIENT
 Loriot Corporation
 4810 Industrial Pkwy
 Medford, Oregon 97504
 Phone: (503) 776-0288

PROJECT
 KFC RESTAURANT
 1409 Center Dr
 Medford, Oregon 97501
 1-0881727





EAST ELEVATION - REVERSE

LEGEND

- 1. BENJAMIN MOORE 6495-18 "RED TIC TAC" EXTERIOR WALLS/ACIAL PAINT
- 2. BENJAMIN MOORE 4215-10 "WEDDING VEE" EXTERIOR WALLS/ACIAL PAINT
- 3. CLEAN GARY "BRICK HERITAGE" BRICK BACK OF HOUSE WALLS & WARESCOT CURB
- 4. BENJAMIN MOORE 42132-20 "BLACK HORSEMAN" PAINTED BRICK

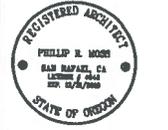


NORTH ELEVATION - REVERSE

EXTERIOR COLOR ELEVATIONS

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 637 FIFTH AVE.
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 415-451-2500 x 114
 mossarchitect@gmail.com

VMI JOB NUMBER
1701



RECORD DATED
 PREPARED BY: 08/10
 CD SUBMITTED: 08/10

REVISIONS

CLIENT
 Lorick Corporation
 4000 Innovation Place
 Medford, Oregon 97504
 Phone: (541) 775-4200

PROJECT
 KFC RESTAURANT
 1400 Center Dr
 Medford, Oregon 97504
 97504

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 07 28 2010
 PLANNING DEPT.
 DD5.0

CITY OF MEDFORD
 EXHIBIT # 5
 File # AC-18-126

PHILLIP MOSS ARCHITECT
 NCARB - AIA
 637 FIFTH AVE
 SAN RAFAEL, CA 04901
 415-451-2500 x 114
 mossarchitects@aol.com

VIM JOB NUMBER
1701



RECORD DATES
 PREPARED FOR: 08/18
 BY: 08/18
 DATE: 08/18

REVISIONS

NO.	DATE	DESCRIPTION

CLIENT
 Loriot Corporation
 4885 Mitchell Place
 Medford, Oregon 97504
 Phone: (541) 779-0000

PROJECT
 KFC RESTAURANT
 1488 Center Dr
 Medford, Oregon 97501
 1-888-1787



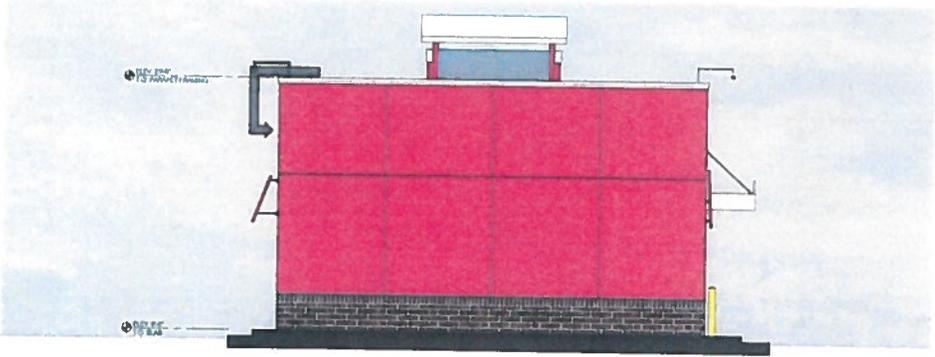
AS-K

COLOR ELEVATIONS

DD5.1

LEGEND

- 1. BENJAMIN MOORE
 #488 'N' NOBEC 'HALZ'
 EXTERIOR WALL/CEILING PAINT
- 2. IN SLASH LACING
 #125 'F2' 'WEDDING VEG'
 EXTERIOR WALL/CEILING PAINT
- 3. CLEM LERY
 'BLACK HERITAGE' BRICK
 BACK OF FRASE WALLS 'FRAMING' CURB
- 4. BENJAMIN MOORE
 #132 'N' 'BLACK HORIZON'
 PAINTED BORDER



WEST ELEVATION - REVERSE



SOUTH ELEVATION - REVERSE

RECEIVED
 SEP 07 2018
 PLANNING DEPT

COLOR ELEVATIONS 1/8" SCALE

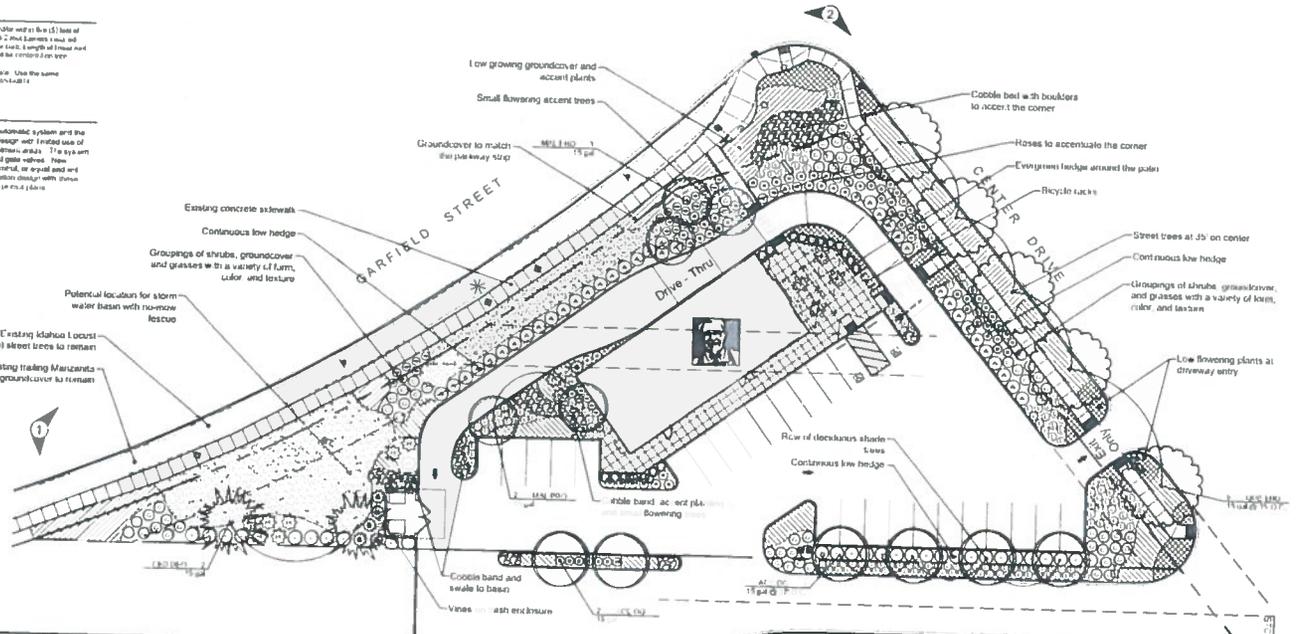
Tree Root Barriers
 All trees planted in this site shall be installed with root barriers (R) that are 12 inches wide and 18 inches deep. The length of the root barrier shall be 18 inches for all plants and shall be 24 inches for trees.

Plant Selection (R) may be used in all of the above. Use the same quantity per tree unless noted otherwise. (L) See Appendix A.

Irrigation
 The entire site will be irrigated using a fully automatic system and the irrigation system will be designed to be efficient. The system shall be designed to provide adequate water to all plants. The system shall be designed to provide adequate water to all plants. The system shall be designed to provide adequate water to all plants.

Landscape Areas

Turf Area	1,100 sq. ft.
Planting Area	1,100 sq. ft.
Other Area	1,100 sq. ft.
Total	3,300 sq. ft.



PLAN SCHEDULE

ITEM NO.	DESCRIPTION	QUANTITY	UNIT	WATER	PLANT
001	Japanese Laurus	15	gal	6	Medium
002	Japanese Laurus	15	gal	6	Medium
003	Japanese Laurus	15	gal	6	Medium
004	Japanese Laurus	15	gal	6	Medium
005	Japanese Laurus	15	gal	6	Medium
006	Japanese Laurus	15	gal	6	Medium
007	Japanese Laurus	15	gal	6	Medium
008	Japanese Laurus	15	gal	6	Medium
009	Japanese Laurus	15	gal	6	Medium
010	Japanese Laurus	15	gal	6	Medium
011	Japanese Laurus	15	gal	6	Medium
012	Japanese Laurus	15	gal	6	Medium
013	Japanese Laurus	15	gal	6	Medium
014	Japanese Laurus	15	gal	6	Medium
015	Japanese Laurus	15	gal	6	Medium
016	Japanese Laurus	15	gal	6	Medium
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039	Japanese Laurus	15	gal	6	Medium
040	Japanese Laurus	15	gal	6	Medium
041	Japanese Laurus	15	gal	6	Medium
042	Japanese Laurus	15	gal	6	Medium
043	Japanese Laurus	15	gal	6	Medium
044	Japanese Laurus	15	gal	6	Medium
045	Japanese Laurus	15	gal	6	Medium
046	Japanese Laurus	15	gal	6	Medium
047	Japanese Laurus	15	gal	6	Medium
048	Japanese Laurus	15	gal	6	Medium
049	Japanese Laurus	15	gal	6	Medium
050	Japanese Laurus	15	gal	6	Medium

Landscape Concept

The landscape design concept for this site is to provide a high quality landscape that is functional, aesthetically pleasing, and sustainable. The design will incorporate a variety of plant species, including native plants, to create a diverse and resilient landscape. The design will also incorporate sustainable practices, such as water conservation and soil erosion control, to ensure the long-term health and vitality of the landscape.

Project Location



PHILLIP MOSS ARCHITECT NCARB - AIA
 637 FIFTH AVE
 SAN RAFAEL, CA 94901
 415-451-2500 x 114
 moss@pmoa.com

VW JOB NUMBER
1701

RECORD DATES
 1/20/18
 1/20/18

REVISIONS

NO.	DATE	DESCRIPTION

CLIENT
 Larkal Corporation
 1400 Rockwood Dr
 Foster City, CA 94404
 Phone: (415) 776-6600

PROJECT
 NCF RESTAURANT
 1400 Center Dr
 Medford, Oregon 97501
 02/18/17

AS-K
 Preliminary Landscape Plan
LO

PHILLIP MOSS ARCHITECT NCARB - AIA
 637 FIFTH AVE
 SAN RAFAEL, CA 04901
 415-451-2500 x 114
 mossarchitect@gmail.com

VMI JOB NUMBER
1701



RECORD DATES
 PRELIMINARY 2016
 COMPLETED 2016

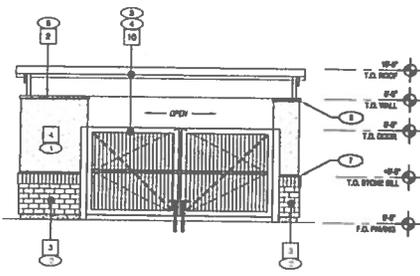
REVISIONS

CLIENT
 Lariat Corporation
 4000 Wilshire Plaza
 Medford, Oregon 97504
 Phone: (541) 756-0000

PROJECT
 KFC RESTAURANT
 1408 Center Dr
 Medford, Oregon 97501
 1-2017/87

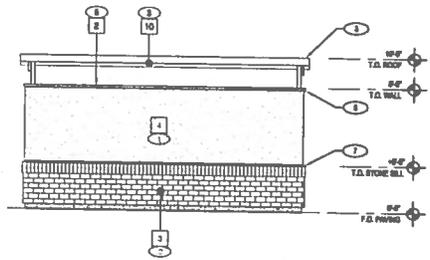
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 SEP 27 2017
 PROPOSED TRASH ENCLOSURE
 D 2688
 PLANNING DEPT

REFER TO DRAWING(S) FOR PHYSICAL NOTES



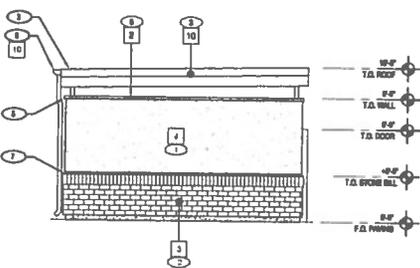
EAST ELEVATION 1/4" = 1'-0" D

REFER TO DRAWING(S) FOR PHYSICAL NOTES



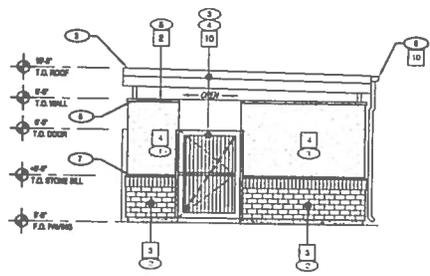
WEST ELEVATION 1/4" = 1'-0" A

REFER TO DRAWING(S) FOR PHYSICAL NOTES



SOUTH ELEVATION 1/4" = 1'-0" E

REFER TO DRAWING(S) FOR PHYSICAL NOTES



NORTH ELEVATION 1/4" = 1'-0" D

- 1 DR. CEILING FINISH SYSTEM
- 2 DR. HATCH FINISH
- 3 DR. ML. FIN. CRN.
- 4 DR. ML. FIN. CRN.
- 5 DR. LAST 1/4"
- 6 DR. FINISH & TRIM SYSTEM

NOTE: NOT ALL KEY NOTES APPLY TO THIS SHEET. KEY NOTES C

RECEIVED

SEP 07 2018

PLANNING DEPT.

Compatibility Statement KFC Restaurant 1408 Center Drive

- A. The property is surrounded on three sides by newer retail and restaurants. All appear to have been built in the last 7 years except for the bank that has been there longer. To the west is an undeveloped street right of way (Belknap) and further west is open space.
- B. The building is a modern design commercial building with red accent and a brick wainscot. It is similar in style to the Panda Express directly across the street.
- C. The exterior is broken up by white and red vertical stripping and brick wainscot. At the front there is a red and white stripped tower that breaks up the front façade.
- D. The placement of the building is parallel to Garfield Street with the front of the building facing the corner of Garfield Street and Center Drive. There is a large serpentine sidewalk along the streets and a 15' landscape buffer. There is a direct path from the Center Drive to the front of the building.
 - a. The sites very edge is within 600 feet of a transit stop. The building can not move any closer to the street due to a PUE easement.
- E. We are proposing an outdoor plaza with seats and umbrellas surrounded by landscape buffer from the drive thru lane at the front of the building.
- F. Vehicle access is from Center Drive. We were asked in our pre-app to move the entrance to our site as far away from the Center / Garfield intersection. The site across Center Drive from the subject property also access their site from Center Drive. Center Drive / Garfield Street intersection has stop lights and turn arrows.
- G. There are not trees on this site nor significant native vegetation to protect.
- H. We have designated a large area as a surface storm water retention area. The landscaping can be seen on the LO sheet. The area designated for drainage is being proposed to have no-mow fescue and is surround by trees and shrubs on three sides. The designated area sets off to the side on the site where there will be little if any pedestrian traffic.
- I. The proposed landscaping is made up of shrubs that are design to ring the hard surfaces around the building. Where there is landscape against the building the landscape is layered in different heights to break the hard diffraction between the building and ground.
- J. The lighting on the building is angled down with shields on top. All light is directed down and not up. The pole site lights are at a maximum of 25' with LED heads. The light is directed down with cutoff shields keeping the light from going up.
- K. The signage shows the single occupant of the building. From the provided color elevations, I don't think anyone will have a problem identifying the occupant as a KFC restaurant.
- L. There is no fencing being proposed.
- M. Given the location of the site adjacent to a major arterial street and far from any residential sound from the proposed project or any future commercial projects on this site would be an issue.
- N. The proposed project is similar in scope and use with the surrounding commercial projects in scale and density. The proposed project has substantially more landscape on the site when compared to other similar restaurants in the immediate area.
- O. We are requesting a reduction in the parking requirement for a restaurant. KFC Corporation recommends 24-26 parking stalls as the optimum number. We are asking for a reduction of 4 stalls. We do not like to use compact stalls for a couple of reasons. First, we like our customers to park with the least amount of squeezing into tight spaces. Second, to reduce the conflict

CITY OF MEDFORD
EXHIBIT # 3
File # AC-18-126

between drivers having difficulty parking in compact spaces, or their inability to park in compact spaces.

P. We are not asking for any relief from the landscape requirements.

Thank you for your consideration,

Philip Moss
architect

RECEIVED

SEP 07 2018

PLANNING DEPT.

Narrative:

The proposed project is a new 2,849 square foot 42 seat KFC Restaurant with a drive thru lane. The property is 32,978 square feet (.80 AC) triangular shaped parcel. The parcel is bordered by Garfield Street on one side and Center Drive on the other. Belknap Road Right of way makes up the third side of the triangular shaped property.

Presently we are not including any of the Belknap Road right of way for access nor are we asking to improve the exist Belknap right of way for access of development.

The building design is a rectangle with a tower element on the front of the building. We are proposing 16 additional seats and 4 umbrellas at the front patio of the building.

The drive lane is known as a reverse drive thru where the direction of the cars is front to back and not the more normal back to front. The configuration will allow the front of the building to face the intersection of Garfield Street and Center Drive.

The proposed development is compatible with uses and development that exist on adjacent land:

The proposed KFC with a drive thru fits into the fabric with the surrounding commercial retail development in the area. There is other quick service restaurant with drive thru pickup in the immediate area including a Panda Express, Taco Now and Starbucks directly across Garfield Street.

The proposed KFC Restaurant meets all the City provisions and ordinances except for the parking requirement. We are proposing 27 spaces where 31 spaces are required. No compact parking space are being used since they are more difficult to maneuver into and out off when parking.

There is a 7-car drive thru lane that will relieve the need for the additional parking. Approximately 50% of a KFC business come thru the drive thru lane.

The guidelines for parking stalls for KFC Restaurants by the corporation is 24 parking stalls should be the goal for any new restaurants. Allowing the reduction would eliminate the additional unneeded 1,068 square feet of hard surface area that would not be required for parking and instead be used for landscaping.

Philip Moss
architect



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 12/12/2018

Revised Date: 1/10/2019

File Numbers: AC-18-126

Reference: PA-18-014, ZC-16-077, LDP-17-131, SV-17-039

PUBLIC WORKS DEPARTMENT STAFF REPORT

KFC Restaurant

Center Drive (TL 4801 & 3605)

- Project:** Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on a parcel totaling 0.76-acres.
- Location:** Located at the corner of Garfield Street and Center Drive (1408 Center Drive) in the Regional Commercial (C-R) zoning district (371W32B TL 4801 & 3605).
- Applicant:** Applicant, Barry Thirirot; Agent, Phillip Moss; Planner, Dustin Severs.

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of vertical building permits. Construction plans for the improvements will need to be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas.
- Certification by the design engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

P:\Staff Reports\AC\2018\AC-18-126 Garfield St at Center Dr (TLs 4801 & 3605) KFC Restaurant\AC-18-126 Staff Report-Revised.docx Page 1 of 10

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

CITY OF MEDFORD
EXHIBIT # K
File # AC-18-126

A. STREETS

1. Dedications

Garfield Street is classified as a Major Arterial street, in accordance with the Medford Land Development Code (MLDC) 10.428, and is under Oregon Department of Transportation (ODOT) jurisdiction. Garfield Street has been fully improved along the subject property's frontage, but the Applicant should consult with ODOT regarding any additional right-of-way dedication requirements.

Belknap Road is classified as a Commercial street within the MLDC, Section 10.429. This section of Belknap Road has recently gone through a vacation process in which a portion of the northern right-of-way was vacated (SV-17-039). **No additional right-of-way will be required.**

Center Drive is classified as a Commercial street, in accordance with MLDC Section 10.429. **No additional right-of-way will be required.**

In accordance with MLDC 10.471, **the property owner shall dedicate a 10-foot wide public utility easement (PUE)** adjacent to the right-of-way line along this Developments frontage to Garfield Street, Belknap Road and Center Drive, unless a minimum 10-foot PUE already exists.

The right-of-way and easement dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: the right-of-way and easement dedication, including an exhibit map; a copy of a current Lot Book Report, Preliminary Title Report, or Title Policy; a mathematical closure report (if applicable), and the Planning Department File Number; for review and City Engineer acceptance signature prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the right-of-way and PUE area.

2. Public Improvements

a. Public Streets

Garfield Street is classified as a Major Arterial street, in accordance with MLDC 10.428, and is under ODOT jurisdiction. The Applicant shall consult with ODOT regarding any additional improvements that may be required.

Belknap Road shall be improved to Commercial street standards in accordance with MLDC 10.429. The Developer shall improve the north half plus 2-feet south of the centerline along the frontage of this development. If this is to be a dead-end street, then the Developer shall also improve a portion of a cul-de-sac turnaround in accordance with MLDC 10.450. The design orientation of the cul-de-sac may be either centered on the street or offset or as otherwise approved by the City Engineer. If Belknap right-of-way is to be vacated with SV-18-125, then no improvements will be required along the vacated section of frontage.

Center Drive is currently improved in close conformance to Commercial street standards. **The Developer shall construct a 5-foot sidewalk with planter strip along the frontage of this development in accordance with MLDC 10.429.**

NOTE: All projects subject to Site Plan and Architectural Review shall be required per MLDC 10.501 (6), as a condition of approval, to repair all frontage sidewalks as determined by the Engineering Division. When attached as a condition of approval of a Site Plan and Architectural Review application the sidewalk maintenance procedures set forth in Chapter 3, Section 3.025, Notice on Hearing of City Repair of Sidewalks, through 3.035, Notice of Sidewalk Repair, are hereby superseded.

b. Street Lights and Signing

The Developer shall provide and install in compliance with Section 10.495 of the Medford Municipal Code (MMC). Based on the preliminary plan submitted, the following number of street lights and signage will be required:

Street Lighting – Developer Provided & Installed:

- A. 2 – Type R-150
- B. 1 – Base Mounted Cabinet (BMC)
 - a. The existing BMC at the corner of Garfield Street and Center Drive may be able to be used; Developer’s Engineer to verify.
 - b. Please submit conduit, wire, and load calculations along with the Public Improvement Plans.

Signs and Devices – City Installed, paid by the Developer:

- A. As required.

Numbers are subject to change if changes are made to the plans. All street lights shall be installed per City standards and be shown on the public improvement plans. Public Works will provide preliminary street light locations upon request. All street lights shall be operating and turned on at the time of the final “walk through” inspection by the Public Works Department.

The Developer shall pay for City installed signage required by the development. City installed signs include, but are not limited to, street name signs, stop signs, speed signs, school signs, dead end signs, and dead end barricades. Sign design and placement shall be per the Manual on Uniform Traffic Control Devices (MUTCD). All signs shall be shown on the public improvement plans and labeled as City installed.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer’s contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided by the Developer.

c. Pavement Moratoriums

The Applicant shall consult with ODOT regarding any moratorium(s) currently in effect along this frontage to Garfield Street.

There are no pavement cutting moratoriums currently in effect along this frontage to Center Drive or Belknap Road.

The Developer shall be responsible for notifying by certified letter all utility companies, as well as all current property owners of parcels which are adjacent to any Public Street being constructed or paved as part of this project. The letter shall inform the utility companies and property owners of the City's street moratorium policy with respect to pavement cutting for future utility services. The utility companies and property owners shall be given the opportunity to install utility services within the right-of-way prior to paving and the subsequent moratorium. Notifications shall be mailed by the Developer at least 6 months before a street is resurfaced or rebuilt per MMC, Section 3.070. Copies of the certifications shall be submitted to the City Engineer with the submittal of the preliminary construction drawings.

d. Soils Report

The Developer's engineer shall obtain a soils report to determine if there is shrink-swell potential in the underlying soils in this development. If they are present, they shall be accounted for in the roadway and sidewalk design within this Development. The soils report shall be completed by a licensed Geotechnical Engineer in the state of Oregon.

e. Access to Public Street System

Driveway access to the proposed development site shall comply with MLDC 10.550.

No driveway access shall be allowed to Garfield Street.

The driveway locations as shown on the site plan are acceptable which consist of the exit only access to Center Drive and the two driveways accessing Belknap Road.

f. Transportation System

A portion of this parcel, Tax lot 3605 was included in ZC-16-077. The Applicant submitted a trip accounting showing that the proposed development does not exceed the 367 PM peak hour trip cap established by ZC-16-077. Their trip accounting states that the South Side Center trips assigned to the approved buildings to date is 266 PM peak hour trips. The prorated trips for this development is 10 PM peak hour trips, for a total of 276 PM peak hour trips. This leaves 91 PM peak hour trips remaining from ZC-16-077.

Corner radius standards shall be in accordance with MLDC 10.445.

Dead-end streets shall be in accordance with MLDC 10.439 and 10.450.

3. Section 10.668 Analysis

To support a condition of development that an applicant dedicates land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and storm drains to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining "rough proportionality" have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Belknap Road and Center Drive:

Belknap Road and Center Drive will be one of the primary routes for pedestrians traveling to and from this development. The development shall construct approximately 680 linear feet of sidewalk along the frontage of the property. All developments in Medford are required to construct their frontage sidewalk and therefore this is roughly proportional.

The additional street lighting will provide the needed illumination to meet current MLDC requirements.

Local street construction requirements identified by the Public Works Department and required by the City are the minimum required to protect the public interest and are necessary for additional or densification of development in the City without detracting from the common good enjoyed by existing properties. Developments are required to provide all internal local streets and half-street improvements to abutting streets, including associated right-of-way dedications, to ensure that new development and density intensification provides the current level of urban services and adequate street circulation is maintained.

Dedication of the Public Utility Easements (PUEs) will benefit development by providing public utility services, which are out of the roadway and more readily available to each lot or building being served. The additional traffic of all modes of travel generated by this proposed development supports the dedication and improvements for all modes of travel and utilities. As indicated above, the area required to be dedicated for this development is necessary and roughly proportional to that required in similar developments to provide a transportation system that meets the needs for urban level services.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

A Site/Utility Plan shall be submitted with the building permit application to show the location of existing or proposed stormdrain lateral/s for the site.

All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. GENERAL CONDITIONS

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the

Engineering Division of Medford Public Works Department for approval. Construction drawings for public improvements shall be submitted only for the improvements to be constructed with each phase. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the governing commission's Final Order, together with all pertinent details and calculations. A checklist for public improvement plan submittal can be found on the City of Medford, Public Works web site (<http://www.ci.medford.or.us/Page.asp?NavID=3103>). The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

In order to properly maintain an updated infrastructure data base, the Surveyor of Record shall submit an as-built survey prior to the Final Inspection and, the Engineer of Record shall submit mylar "as-constructed" drawings to the Engineering Division within sixty (60) calendar days of the Final Inspection (walk through). Also, the engineer shall coordinate with the utility companies, and show all final utility locations on the "as built" drawings.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

Contractors proposing to do work on public streets, sewers, or storm drains shall 'prequalify' with the Engineering Division prior to starting work. Contractors shall work off a set of public improvement drawings that have been approved by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and storm drain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges (SDC)

Buildings in this development are subject to SDCs fees. All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Jodi K Cope
Reviewed by: Doug Burroughs
Revised by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

KFC Restaurant

Center Drive (TL 4801 & 3605)

AC-18-126

A. Streets

1. Street Dedications to the Public:

- **Garfield Street** – Consult with ODOT.
- **Belknap Road** – No additional right-of-way required.
- **Center Drive** – No additional right-of-way required.
- Dedicate 10-foot Public Utility Easement (PUE) along all frontages as required.

2. Improvements:

Public Streets

- **Garfield Street** – Consult with ODOT.
- **Belknap Road** – Construct to Commercial street standards (half plus 2-feet) with a turnaround as required, unless the street vacation is approved.
- **Center Drive** – Construct sidewalk with planter strip.

Lighting and Signing

- Developer supplies and installs all street lights at own expense.
- City installs traffic signs and devices at Developer's expense.

Access and Circulation

- Driveway access to the proposed development site shall comply with MLDC 10.550.
- No driveway access shall be allowed to Garfield Street.

Other

- There is no pavement moratorium currently in effect on Center Drive. Applicant shall consult with ODOT regarding any moratorium(s) currently in effect along this frontage to Garfield Street.
- Provide pavement moratorium letters.
- Provide soils report.
- Comply with Transportation System conditions.

B. Sanitary Sewer:

- The site is situated within the RVSS area.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers certification of stormwater facility construction.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.
- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.

P:\Staff Reports\AC\2018\AC-18-126 Garfield St at Center Dr (TLs 4801 & 3605) KFC Restaurant\AC-18-126 Staff Report-Revised.docx Page 10 of 10

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

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FAX (541) 774-2552



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-18-126

PARCEL ID: 371W32B TL's 3605 & 4801

PROJECT: Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on a parcel totaling 0.76-acres, located at the corner of Garfield Street and Center Drive (1408 Center Drive) in the Regional Commercial (C-R) zoning district (371W32B TL 4801 & 3605); Applicant, Barry Thiriort; Agent, Phillip Moss; Planner, Dustin Severs.

DATE: December 12, 2018

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of an 8-inch water line is required off the existing 24-inch water line south of this site. Applicants civil engineer shall coordinate with MWC Engineering staff for water facility layout.
4. Applicant shall coordinate with Medford Fire Department for location of required fire hydrant.
5. Installation of a domestic water meter is required. Applicants civil engineer shall coordinate with MWC Engineering staff for approved locations of proposed domestic, and irrigation water meter.
6. Dedication of a 10-foot-wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement shall be submitted to MWC for review and recordation prior to construction.
7. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

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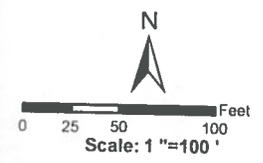
CITY OF MEDFORD
EXHIBIT # L
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Continued from Previous Page

COMMENTS

1. Off-site water line installation is required. (See Condition 3 above)
2. On-site water facility construction is not required.
3. Static water pressure is approximately 75 psi.
4. MWC-metered water service does not exist to this property. (See Condition 5 above)
5. Access to MWC water lines is available. There is an existing 24-inch welded steel water line south of the proposed site.



Water Facility Map
City of Medford
Planning Application:
AC-18-126
(37-1W-32-B- 4801 & 3605)
November 28, 2018

Legend

- ⊕ Air Valve
- Sample Station
- Fire Service
- ◆ Hydrant
- ▲ Reducer
- Blow Off
- ⊕ Plugs-Caps

- Water Meters:**
- ⊕ Active Meter
 - On Well
 - Unknown
 - Vacant

- Water Valves:**
- ⊕ Butterfly Valve
 - ⊕ Gate Valve
 - ⊕ Tapping Valve

- Water Mains:**
- Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line

- Boundaries:**
- ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots

- MWC Facilities:**
- C** Control Station
 - P** Pump Station
 - R** Reservoir



This map is based on a digital elevation model by Geo Data Systems, Inc. and other data. Medford Water Commission is not responsible for any errors or omissions. It is the user's responsibility to verify the accuracy of the information shown on this map. © 2018 Medford Water Commission. All rights reserved.



Medford Fire-Rescue Land Development Report

Review/Project Information

Reviewed By: Kleinberg, Greg

Review Date: 12/6/2018
Meeting Date: 12/12/2018

LD File #: AC18126

Planner: Dustin Severs

Applicant: Barry Thiriot

Project Location: Corner of Garfield Street and Center Drive (1408 Center Drive)

Project Description: Consideration of a proposal for the construction of a 2,849 square foot restaurant with a drive-thru (KFC) on a parcel totaling 0.76-acres

Specific Development Requirements for Access & Water Supply

Table with 3 columns: Reference, Comments, and Conditions Description. Reference: OFC 508.5. Comments: One fire hydrant is required. The location is approved as submitted on the plans. Conditions Description: Fire hydrants with reflectors will be required for this project. The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site. Plans and specifications for fire hydrant system shall be submitted to Medford Fire-Rescue for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).

Construction General Information/Requirements

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300

www.medfordfirerescue.org

CITY OF MEDFORD
EXHIBIT # M
File # AC-18-126



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 7502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

December 5, 2018

City of Medford Planning Department
200 S. Ivy Street
Medford, Oregon 97501

Re: AC-18-126, KFC (Map 371W32B, Tax Lots 4801 & 3605)
Ref: LDP-17-131, PA-18-014

ATTN: Dustin,

The subject properties are within RVSS service area. There is a 21" inch sewer along Garfield Street. Sewer service for the proposed development can be had by tapping the main along Garfield Street or by a short 8" main extension along Center Drive from the manhole at the intersection of Garfield Street and Center Drive.

Rogue Valley Sewer Services requests that approval of this application be subject to the following conditions:

1. Sewer improvements shall be designed and constructed in accordance with RVSS standards and submitted for approval.
2. The applicant must pay sewer system development charges owed to Rogue Valley Sewer Service prior to issuance of building permits.

Sincerely,

Nicholas R. Bakke

Nicholas R. Bakke, P.E.
District Engineer

Dustin J. Severs

From: HORLACHER Ian K <Ian.K.HORLACHER@odot.state.or.us>
Sent: Wednesday, December 12, 2018 11:05 AM
To: Dustin J. Severs
Cc: CUTSFORTH DeLanie; SCRUGGS Julee Y; WANG Wei * Michael; MCDONALD John
Subject: RE: City of Medford

Dustin – looks like we need to see the hydraulic calculations from the applicant it needs to be submitted to Delanie Cutsforth, ODOT Hydraulics Specialist. I have cc'ed her on this email. She can be reached at 541-774-6326.

Access shall be taken from Center Drive, there shall be no direct access to Garfield from the turn lane. The applicant may not use ODOT ROW to meet landscaping requirements for the City of Medford.

As far as everything else, any misc/utility work within the ODOT ROW needs to be submitted and permitted by Julee Scruggs, ODOT permit specialist, and she can be reached at 541.864.8811.

If you have any questions, please feel to contact myself or John McDonald when he returns on the 17th.

From: Dustin J. Severs [mailto:Dustin.Severs@cityofmedford.org]
Sent: Tuesday, December 04, 2018 3:43 PM
To: HORLACHER Ian K
Cc: WANG Wei * Michael
Subject: RE: City of Medford

Attached is a PDF.



Dustin Severs, Planner III

City of Medford – Planning Department
200 South Ivy Street, Lausmann Annex
Medford, OR 98501 Phone: 541-774-2389

From: HORLACHER Ian K [mailto:Ian.K.HORLACHER@odot.state.or.us]
Sent: Tuesday, December 4, 2018 3:17 PM
To: Dustin J. Severs
Cc: WANG Wei * Michael
Subject: FW: City of Medford

Any possible way to get a cleaner copy of the site plan? Most of the numbers are unreadable or hard to read.

Dustin J. Severs

From: Jon M. Proud
Sent: Tuesday, December 18, 2018 4:25 PM
To: Dustin J. Severs
Cc: Matt H. Brinkley; Kelly A. Akin; Lori J. Cooper
Subject: Yellow Paper FW: KFC (AC-18-126)
Attachments: KFClegal.pdf

Dustin, Just to clarify, I never received yellow paper for this AC #.

It is general knowledge from the staff report for LDP-17-131 that the property in question was created unlawfully. Planning's failure to review units of land for lot legality before development seems to be a continuing problem and building on its self. If I had not contacted you last week I'm sure this application would have been approved on an unlawful unit of land as were the sites across the street. That would have been a double whammy! First it was brought up in LDP-17-131 but would have been allowed to proceed in AC-18-126 for the second whammy. I have attached pertinent parts of ORS 92 but basically the complete chapter falls under planning's purview and in my opinion should be understood and enforced according to MC 10.005, and 10.061. Furthermore I believe ORS 92.176 and 92.177 is totally misunderstood and used erroneously by COM planning department. In my opinion those sections of the law were added to ORS as a reminder by the state legislation that land division laws need to be followed. I also added a portion of ORS 93 to also show that the planning department is needed when conveyances of fee title transfers, since real property is one of the publics most cherished possessions and the state legislators are mandating that Planning departments are the publics "go to" people for lot legality.

To answer your question specifically now:

Both parcels included within the attached deed are unlawfully created in my opinion.

Parcel 1 of JCOR 2018-014412

Is a "portion" of lot 17 in South Gateway subdivision. It was created without benefit of partition in violation of city and state laws. Specifically old MC 10.021 that was in affect at time of creation and ORS 92.010(9)(d). (Center Drive created per JCOR 2006-13916, and Garfield Street created by JCOR 2005-55426 does not act to divide land per ORS 92.010(9)(d))

Parcel 2 of JCOR 2018-014412

Is a portion of JCOR 2016-15304, which was a property line adjustment deed (planning #'s 16-010,16-058) approved by the city. The survey completed as a requirement of the pla clearly shows the "portion" described in said parcel 2 as **being a single unit of land** attached to the larger unit of land east of Center dr. (Center Drive created per JCOR 2006-13916 does not act to divide land per ORS 92.010(9)(d)) so until which time that single unit of land is further dived by an approved land division the land cannot be divided as was done in the attached deed.

It is my opinion that the land developer orchestrating the underlying development is running afoul of the law and making a mockery of the cities oversite and regulatory function as required by the municipal code and state laws. I would recommend this application be denied and LDP-17-131 to be found in fault of the law and null and void. Then begin the process all over under 92.176/177 and until which time a permit for validation is secured and perfected with mapping as required per the partition process and the ownership document describes the property per the land division (parcel X of partition plat xx) before the proposed development project be allowed to go forward. But this is not my decision just my opinion the Planning director is responsible for the enforcement of the land development chapter.

Thanks, Jon

Jon Proud, L.S.
City Surveyor
200 S. Ivy Street
Medford, Or, 97501
jon.proud@ci.medford.or.us
p.541-774-2126
f.541-774-2552

SOME PERTINENT LAWS (NOT AN EXHAUSTIVE LIST)

93.040 Mandatory statements for sales agreements, earnest money receipts or other instruments for conveyance of fee title to real property; liability of drafter and recorder. (1) The following statement shall be included in the body of an instrument transferring or contracting to transfer fee title to real property except for owner's sale agreements or earnest money receipts, or both, as provided in subsection (2) of this section: "BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON TRANSFERRING FEE TITLE SHOULD INQUIRE ABOUT THE PERSON'S RIGHTS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010. THIS INSTRUMENT DOES NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY THAT THE UNIT OF LAND BEING TRANSFERRED IS A LAWFULLY ESTABLISHED LOT OR PARCEL, AS DEFINED IN ORS 92.010 OR 215.010, TO VERIFY THE APPROVED USES OF THE LOT OR PARCEL, TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES, AS DEFINED IN ORS 30.930, AND TO INQUIRE ABOUT THE RIGHTS OF NEIGHBORING PROPERTY OWNERS, IF ANY, UNDER ORS 195.300, 195.301 AND 195.305 TO 195.336 AND SECTIONS 5 TO 11, CHAPTER 424, OREGON LAWS 2007, SECTIONS 2 TO 9 AND 17, CHAPTER 855, OREGON LAWS 2009, AND SECTIONS 2 TO 7, CHAPTER 8, OREGON LAWS 2010."

92.010 Definitions for ORS 92.010 to 92.192. As used in ORS 92.010 to 92.192, unless the context requires otherwise:

(3)(a) "Lawfully established unit of land" means:

(A) A lot or parcel created pursuant to ORS 92.010 to 92.192; or

(B) Another unit of land created:

(i) In compliance with all applicable planning, zoning and subdivision or partition ordinances and regulations; or

(ii) By deed or land sales contract, if there were no applicable planning, zoning or subdivision or partition ordinances or regulations.

(b) "Lawfully established unit of land" does not mean a unit of land created solely to establish a separate tax account.

(4) "Lot" means a single unit of land that is created by a subdivision of land.

(6) "Parcel" means a single unit of land that is created by a partition of land.

(7) "Partition" means either an act of partitioning land or an area or tract of land partitioned.

(8) "Partition plat" includes a final map and other writing containing all the descriptions, locations, specifications, provisions and information concerning a partition.

(9) "Partitioning land" means dividing land to create not more than three parcels of land within a calendar year, but does not include:

- (a) Dividing land as a result of a lien foreclosure, foreclosure of a recorded contract for the sale of real property or the creation of cemetery lots;
- (b) Adjusting a property line as property line adjustment is defined in this section;
- (c) Dividing land as a result of the recording of a subdivision or condominium plat;
- (d) Selling or granting by a person to a public agency or public body of property for state highway, county road, city street or other right of way purposes if the road or right of way complies with the applicable comprehensive plan and ORS 215.213 (2)(p) to (r) and 215.283 (2)(q) to (s). However, any property sold or granted for state highway, county road, city street or other right of way purposes shall continue to be considered a single unit of land until the property is further subdivided or partitioned; or
- (e) Selling or granting by a public agency or public body of excess property resulting from the acquisition of land by the state, a political subdivision or special district for highways, county roads, city streets or other right of way purposes when the sale or grant is part of a property line adjustment incorporating the excess right of way into adjacent property. The property line adjustment shall be approved or disapproved by the applicable local government. If the property line adjustment is approved, it shall be recorded in the deed records of the county where the property is located.

(11) "Property line" means the division line between two units of land.

(15) "Sale" or "sell" includes every disposition or transfer of land or an interest or estate therein.

92.012 Compliance with ORS 92.010 to 92.192 required. No land may be subdivided or partitioned except in accordance with ORS 92.010 to 92.192. [1973 c.696 §2; 1975 c.643 §24]

92.014 Approval of city or county required for specified divisions of land. (1) A person may not create a street or road for the purpose of subdividing or partitioning an area or tract of land without the approval of the city or county having jurisdiction over the area or tract of land to be subdivided or partitioned.

92.016 Sale or negotiation to sell lot or parcel prior to approval of tentative plan. (1) No person shall sell any lot in any subdivision with respect to which approval is required by any ordinance or regulation adopted under ORS 92.044 and 92.048 until such approval is obtained. No person shall negotiate to sell any lot in a subdivision until a tentative plan has been approved.

(2) A person may negotiate to sell any parcel in a partition with respect to which approval of a tentative plan is required by any ordinance or regulation adopted under ORS 92.044 or 92.046, respectively, prior to the approval of the tentative plan for the partition, but no person may sell any parcel in a partition for which approval of a tentative plan is required by any ordinance or regulation adopted under ORS 92.044 or 92.046, respectively, prior to such approval. [1955 c.756 §24; 1973 c.696 §5; 1974 c.74 §1; 1977 c.809 §5; 1991 c.763 §5; 2003 c.14 §34]

92.025 Prohibition of sale of lot or parcel prior to recordation of plat; waiver. (1) A person may not sell a lot in a subdivision or a parcel in a partition until the plat of the subdivision or partition has been acknowledged and recorded with the recording officer of the county in which the lot or parcel is situated.

(2) A person may not sell a lot in a subdivision or a parcel in a partition by reference to or exhibition or other use of a plat of the subdivision or partition before the plat for the subdivision or partition has been so recorded. In negotiating to sell a lot in a subdivision or a parcel in a partition under ORS 92.016 (1) and (2), a person may use the approved tentative plan for the subdivision or partition.

(3) Notwithstanding subsections (1) and (2) of this section, the governing body of a city or county may enact an ordinance waiving the requirement that parcels created in excess of 80 acres be shown on a partition plat. Nothing in this subsection shall exempt a local government from minimum area requirements established in acknowledged comprehensive plans and land use regulations. [1955 c.756 §6 (enacted in lieu of 92.020 and 92.030); 1973 c.696 §6; 1977 c.809 §6; 1989 c.772 §4; 1991 c.763 §6; 2005 c.399 §3]

92.027 Deed reference to creation of unit of land. A person who conveys or contracts to convey fee title to a lot or parcel, or another unit of land resulting from a lien foreclosure or foreclosure of a recorded contract for the sale of real property, created or established on or after January 1, 2008, must include in the deed or other instrument conveying or contracting to convey fee title:

- (1) A reference to the recorded subdivision plat or partition plat for the lot or parcel;
- (2) A reference to or exhibit of the final land use decision that approved the subdivision or partition if a subdivision plat or partition plat is not required by law; or
- (3) A reference to or exhibit of a final judgment or other document that evidences a lien foreclosure or a foreclosure of a recorded contract for the sale of the real property. [2007 c.866 §3]

92.176 Validation of unit of land not lawfully established. (1) A county or city may approve an application to validate a unit of land that was created by a sale that did not comply with the applicable criteria for creation of a unit of land if the unit of land:

- (a) Is not a lawfully established unit of land; and
 - (b) Could have complied with the applicable criteria for the creation of a lawfully established unit of land in effect when the unit of land was sold.
- (2) Notwithstanding subsection (1)(b) of this section, a county or city may approve an application to validate a unit of land under this section if the county or city approved a permit, as defined in ORS 215.402 or 227.160, respectively, for the construction or placement of a dwelling or other building on the unit of land after the sale. If the permit was approved for a dwelling, the county or city must determine that the dwelling qualifies for replacement under the criteria set forth in ORS 215.755 (1)(a) to (e).
- (3) A county or city may approve an application for a permit, as defined in ORS 215.402 or 227.160, respectively, or a permit under the applicable state or local building code for the continued use of a dwelling or other building on a unit of land that was not lawfully established if:
- (a) The dwelling or other building was lawfully established prior to January 1, 2007; and
 - (b) The permit does not change or intensify the use of the dwelling or other building.
- (4) An application to validate a unit of land under this section is an application for a permit, as defined in ORS 215.402 or 227.160. An application to a county under this section is not subject to the minimum lot or parcel sizes established by ORS 215.780.
- (5) A unit of land becomes a lawfully established parcel when the county or city validates the unit of land under this section if the owner of the unit of land causes a partition plat to be recorded within 90 days after the date the county or city validates the unit of land.
- (6) A county or city may not approve an application to validate a unit of land under this section if the unit of land was unlawfully created on or after January 1, 2007.
- (7) Development or improvement of a parcel created under subsection (5) of this section must comply with the applicable laws in effect when a complete application for the development or improvement is submitted as described in ORS 215.427 (3)(a) or 227.178 (3)(a). [2007 c.866 §2]

Note: 92.176 was added to and made a part of 92.010 to 92.192 by legislative action but was not added to any smaller series therein. See Preface to Oregon Revised Statutes for further explanation.

92.177 Creation of parcel by less than all owners of lawfully established unit of land. When a unit of land was sold before January 1, 2007, but was not a lawfully established unit of land, the governing body of the city or county or its designee shall consider and may approve an application for the creation of a parcel pursuant to ORS 92.176, notwithstanding that less than all of the owners of the existing lawfully established unit of land have applied for the approval. [1993 c.436 §2; 1995 c.595 §14; 2007 c.866 §6]

10.005 Purpose

It is the purpose of this chapter to promote the public health, safety, and general welfare of all residents of the City of

Medford through the regulation and management of the use and development of land within the city. It is the specific intent of this chapter to: (2) Comply with all applicable Oregon Revised Statutes as pertains to the development and use of land.

10.012 Definitions, Specific

Approving Authority. The designated official or official body charged with the duty of investigating and reporting on the design, improvement and use of proposed developments of real property, the imposing of requirements or conditions thereon and the authority to approve, conditionally approve or disapprove development permits and land use reviews as per this chapter.

Boundary line. A line bounding all contiguous lands held in common ownership. The term "boundary line" shall be synonymous with property line.

Development. The improvement of a parcel of land; including partitioning or subdividing of any improved or unimproved real property, for any purpose, and by any person, association, or other entity.

Development permit. The written acknowledgment by the city that a specific development proposal has complied with all required land use reviews determined necessary for development.

Lawfully established unit of land. A lot or parcel created pursuant to ORS 92.010 to 92.192 or another unit of land created in compliance with all applicable planning, zoning, and subdivision or partition ordinances and regulations or by deed or land sales contract, if there were no applicable planning, zoning, subdivision, or partition ordinances or regulations. Lawfully established unit of land does not mean a unit of land created solely to establish a separate tax account.

Partition. The act of dividing an area of land into two or three parcels within a calendar year when such area or tract of land exists as a unit or contiguous units of land under single ownership. Partition does not include divisions of land resulting from lien foreclosures, divisions of land resulting from foreclosure of recorded contracts for the sale of real property, and divisions of land resulting from the creation of cemetery lots; and does not include any adjustment of a lot line by the relocation of a common boundary where an additional parcel is not created and where the existing parcel reduced in size by the adjustment is not reduced below the minimum lot size established by this code for the district within which it is located. Partition does not include the sale of a lot in a recorded subdivision, even though the lot may have been acquired prior to the sale with other contiguous lots or property by a single owner.

10.032 Nonconformities

Except as otherwise provided in this section, any lot, use, sign, or structure lawfully existing on the effective date of this chapter or subsequent amendments thereto which does not comply with the standards of this chapter shall be defined as legal nonconforming and may be continued so long as it remains otherwise lawful. If the taking by eminent domain action of a portion of a lot reduces the lot dimensions or area below minimums prescribed in this chapter, the lot shall then be considered to be a legal nonconforming lot. All nonconformities shall be encouraged to convert to conformity wherever possible and shall be further subject to the requirements of Section 10.033, Continuation of Nonconforming Development, through 10.037, Completion of Nonconforming Development.

[Amd. Sec. 1, Ord. No. 7593, March 17, 1994.]

10.033 Continuation of Nonconforming Development

Except as otherwise provided in Sections 10.034, Criteria for Nonconformity Expansion or Change through 10.037, Completion of Nonconforming Development, a legal nonconforming structure or use may continue but shall only be changed, while continuing to be nonconforming, as authorized by this chapter.

(6) A lot of record, or a parcel of land for which a deed or other instrument dividing the land was recorded with Jackson County prior to May 5, 1980, which has an area or dimension less than required by this code, shall be considered legal nonconforming and may be developed and occupied by a permitted use subject to compliance with the minimum standards of this code.

10.035 Nonconformity Burden of Proof

The owner, not the city, has the burden of proving that any nonconforming lot, use, sign or structure was lawfully established as provided in Section 10.032, Nonconformities.

10.061 Duty to Enforce

The approving authority or City Council, on appeal, shall refuse approval of any development permit application not in conformity herewith, and subject to the provisions of state law. All officials, departments, officers, and employees of the city vested with the authority to issue permits or certificates shall not issue any such permits or certificates which conflict with any provisions of this chapter, or which purport to authorize or permit the development of any land where compliance with all applicable provisions of this chapter have not been met. Any such permit or authorization which may be issued in conflict herewith shall be void and of no force or effect. (Effective Dec. 1, 2013.)

Other than as above set forth, the Planning Director or his designee shall be the official responsible for the enforcement of this chapter.

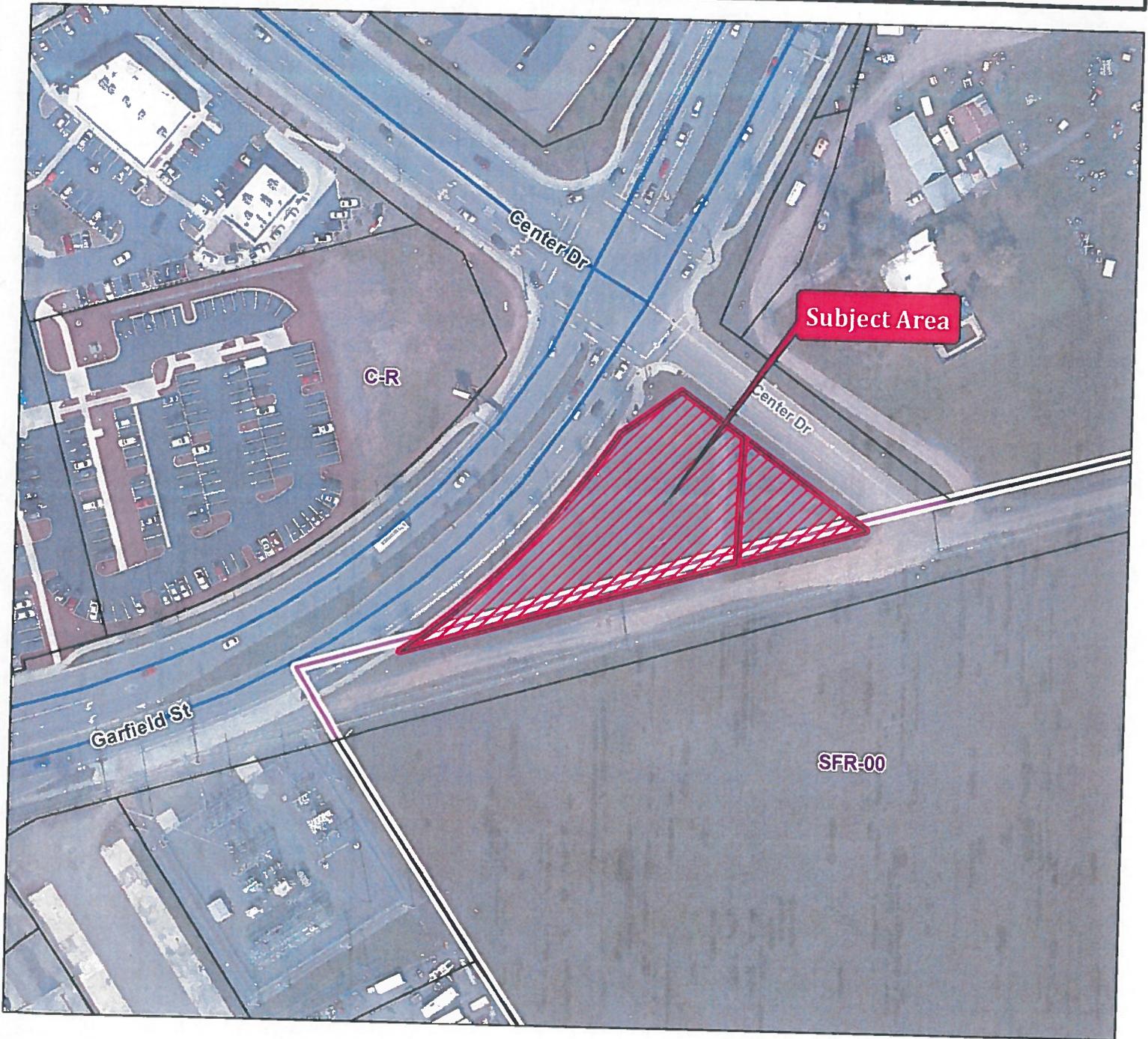
From: Dustin J. Severs
Sent: Tuesday, December 18, 2018 10:24 AM
To: Jon M. Proud
Subject: KFC (AC-18-126)

Jon,
Sorry I didn't get this to you before. As stated on the phone last week, we will be adding a condition of approval that the partition (LDP-17-131) gets final plat approval prior to the issuance of building permits for the proposed KFC on this site. Could you email me the yellow paper confirming that these lots were illegally created, so that I can add it as an exhibit in the record. Thank you.



Dustin Severs, Planner III

City of Medford – Planning Department
200 South Ivy Street, Lausmann Annex
Medford, OR 98501 Phone: 541-774-2389



Project Name:

KFC

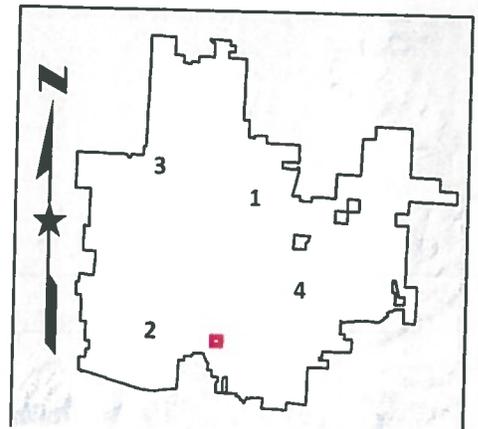
Map/Taxlot:

371W32B TL 3601 & 4801

Legend

-  Subject Area
-  Zoning Districts
-  Tax Lots

12/21/2018





City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT

for a Type-III quasi-judicial decision: **Site Plan & Architectural Review**

Project Pilot Rock Excavation
Applicant: Jeff & Alyson Fowler; Agent: Jim Higday

File no. AC-18-138/E-18-139

To Site Plan & Architectural Commission *for January 18, 2019 hearing*

From Liz Conner, Planner II

Reviewer Kelly Evans, Assistant Planning Director *lv*

Date January 11, 2019

BACKGROUND

Proposal

Consideration of the construction of a steel building for a shop and office approximately 6,300 square feet with an exception to the General Design Requirements for Parking, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved in size on a 4.0 acre parcel located at 356 Bateman Drive approximately 450 feet west of Bierson Way zoned I-G (General Industrial) (362W36D TL 142).

Vicinity Map



Subject Site Characteristics

Zoning	I-G	General Industrial
GLUP	GI	General Industrial
Use	Vacant	

Surrounding Site Characteristics

North	Zone:	I-L Light Industrial
	Use:	Vacant/Trucking Company
South	Zone:	I-L Light Industrial
	Use:	Pressure Point Roofing, Inc.
East	Zone:	I-L Light Industrial
	Use:	Bierson Corporation, manufacturing
West	Zone:	I-G
	Use:	Vacant

Related Projects

PA-18-050 Pilot Rock Excavation Inc.

Applicable Criteria

Medford Municipal Code §10.200(E) Site Plan and Architectural Review Approval Criteria.

(1) The Site Plan and Architectural Commission shall approve a site plan and architectural review application for a commercial or industrial development, if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (a) The proposed development is compatible with uses and development that exist on adjacent land, and*
- (b) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in Section 10.186.*

Medford Municipal Code §10.186(B) Criteria for an Exception.

No exception, in the strict application of the provisions of this chapter, shall be granted by the approving authority having jurisdiction over the land use review unless it finds that all of the following criteria and standards are satisfied. The power to authorize an exception from the terms of this code shall be sparingly exercised. Findings must indicate that:

- (1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the*

exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met.

- (2) *The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.*
- (3) *There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.*
- (4) *The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.*

Corporate Names

Alyson Fowler is listed as the registered agent for Pilot Rock Excavation, Inc. according to the Oregon Secretary of State Business Registry.

ISSUES AND ANALYSIS

Background

The subject site contains approximately 4 acres and is located adjacent to the west of Bierson Industrial Park on the south side of Bateman Drive. It is currently vacant. Bateman Drive is classified as a commercial street.

The applicants are owners of Pilot Rock Excavation Inc., which owns and operates more than 30 pieces of excavation and construction equipment. The equipment includes but is not limited to excavators, loaders, back-hoes, skid steers and dozers, many of which are tracked vehicles.

Project Summary

The request includes the construction of new steel framed office, a covered wash rack facility and maintenance bays. A request for relief from the paving requirement in Medford Land Development Code (MLDC) Section 10.746 is also included.

Site Plan (Exhibit B)

Access

The subject property has approximately 260 feet of frontage along Bateman Drive. A paved commercial street. The site plan identifies two 36 foot wide driveways that provide direct access to the public right of way. The site plan also shows a five foot sidewalk that runs between the parking area and the building.

Parking

Per the site plan the applicant is providing nine parking spaces. The applicant's findings (Exhibit E) state that a total number of employees on its largest shift is approximately 20. The applicant is requesting the Commission to allow an exception to the number of required spaces. The applicant's findings state that the bulk of the employees will report to the job site while only three employees will report to the office location. There will not be 20 employees at the business office.

The applicant is requesting that the Commission allow parking for the site at the Industrial and Warehouse rate, which is 0.2 spaces per 1,000 square feet with one additional space per employee. A total of nine spaces would allow for future expansion.

Parking 10.721	Proposed	Required
Automobiles	9	22
Bicycle Parking (based on 9)	0	2

The site plan does not indicate where bicycle parking facilities will be located. A condition of approval (Exhibit A) has been included requiring that the standards in 10.748-10.751 are met.

Concealment

The site plan does not indicate where a trash enclosure would be located or where the Heating, Ventilation, Air Conditioning (HVAC) equipment will be located. A condition of approval has been included to require adequate concealment is provided pursuant to MLDC 10.781-10.782 (Exhibit A).

Landscape Plan (Exhibit C)

The subject property has approximately 188 feet of frontage along Bateman Drive. Per the MLDC 10.797 a minimum of six trees and 29 shrubs are required. The applicant's landscape plan shows a total of seven trees and 31 shrubs along the frontage and parking area buffer.

Landscaping 10.797 188 feet frontage	Proposed	Required
Trees	7	6
Shrubs	31	29

Parking areas that abut a public street shall be buffered by a minimum of 10 feet landscaped area. The applicant's landscape plan complies with MLDC 10.746(9).

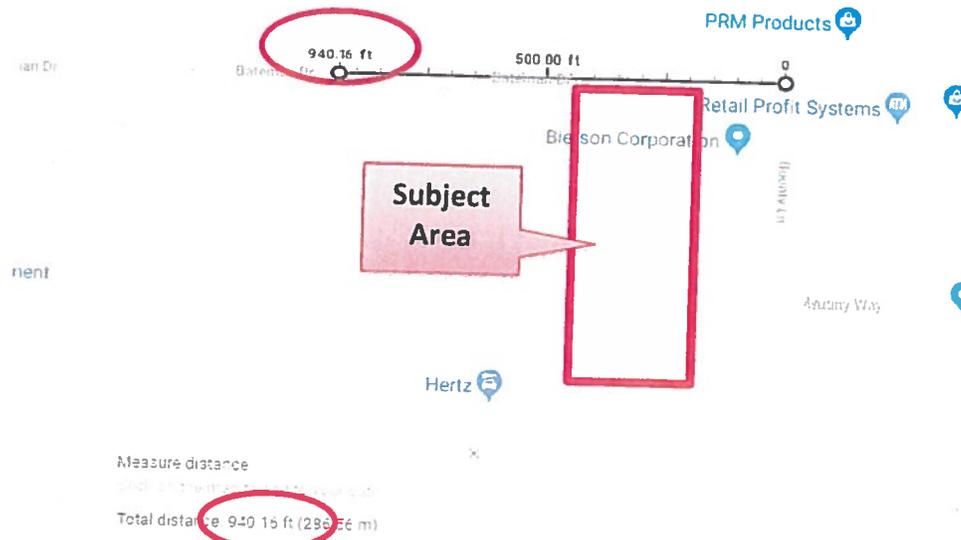
Block Length

The maximum block length required for Industrial Zones by MLDC Section 10.426C-1 is 940 feet and perimeter length of 3,760 feet.

MAXIMUM BLOCK LENGTH AND PERIMETER LENGTH Table 10.426-1		
Zone or District	Block Length	Block Perimeter Length
a. Residential Zones	660'	2,100'
b. Central Business Overlay District	600'	1,800'
c. Transit Oriented Districts (Except SE Plan Area)	600'	1,800'
d. Neighborhood, Community, and Heavy Commercial Zones; and Service Commercial-Professional Office Zones	720'	2,880'
e. Regional Commercial and Industrial Zones	940'	3,760'

The approving authority may find proposed blocks that exceed maximum and/or perimeter standards are acceptable when it is demonstrated that future development on adjoining property or reserve acreage can feasibly satisfy the block or perimeter standards.

Per the applicant's supplemental findings (Exhibit F), block length can be met with future development to the west. The images below depict the distance from Bounty Lane west approximately 940 feet further illustrating the Industrial Zone Block Length standards.



Architecture (Exhibit D)

The applicant's submitted narrative (Exhibit E) describes the building's proposed architecture and how it fits with and complements the adjacent buildings, as the following:

The architectural style of the proposed building is standard for the metal building industry and is similar in nature to the adjacent developments. The proposed building will be two-toned per the attached color samples. The façade and roof will be corrugated metal with the bottom eight feet of the siding, and all trim is Burnished slate, the remainder 16 feet of siding will be fox gray.

Facility Adequacy

Per the agency comments submitted to staff (Exhibits G-L), including the Rogue Valley Sewer Services (RVSS), it can be found that there are adequate facilities to serve the proposed development.

Agency Comments

Jackson County Roads (Exhibit O and Q)

Jackson County Road's report provided an itemized list of comments, including the requirement that a traffic analysis at the intersection of Bateman Drive and Table Rock Road be completed and requiring any recommended mitigations.

The applicant provided a Traffic Study (Exhibit P) pursuant to the Jackson County Roads requirements. The study was provided to both the City of Medford Traffic Engineer and Jackson County Roads.

The study concluded that the transportation system can accommodate the subject application without adverse impact. No mitigation recommendations were made.

Both the City Traffic Engineer and Jackson County Roads Department reviewed the study and have no comment moving forward (Exhibits Q and R).

Oregon Department of Aviation (ODA) (Exhibit M)

Oregon Department of Aviation provided comments to staff stating that the warehouse is considered a compatible use according to the ODA's land use compatibility guidebook, and due to the distance and FAR Part 77 surface, ODA does not find that the proposed development is a hazard to air navigation. No FAA Form 7460-1 will be required by ODA.

Jackson County Airport Authority (Exhibit N)

Rogue Valley International Airport requests an Avigation, Noise and Hazard Easement to be required as part of the permit process. In the 2010 LUBA decision on Michelle Barnes vs. City of Hillsboro and the Port of Portland, Nollan/Dolan findings are required to support the request (LUBA No. 2010-011). None were provided; therefore, a condition requiring compliance with the airport's request for an Avigation, Noise and Hazard Easement has not been included.

In addition, the Airport also requires the applicant to contact the FAA regarding submittal of a 7460-1 form.

Exception Request

The applicant proposes to park wheeled vehicles in the parking area provided in front of the building. Per the site plan provided, the area to the north of the building is proposed as parking and shall be paved.

The shop is intended to be used for maintenance of the fleet equipment and storage of tracked vehicles. The purpose of the exception request is to reduce damage to the pavement that heavy tracked vehicles would cause. The site plan (Exhibit B) identifies the front portion of the property as paved parking and maneuvering areas and gravel behind the gate where the tracked vehicles will be parking and maneuvering.

The applicant's findings (Exhibit E and F) address the criteria found in MLDC Section 10.186, and are as follows;

(1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met.

It can be found that granting the exception to paving requirements for tracked vehicles is harmonious with the general purpose and intent of the regulations imposed by the code. Per the applicant's findings, the use of gravel surrounding the shop is not unlike the

ground cover that currently exists on site therefore it will not be injurious to the general area or detrimental to the health, safety, or general welfare or adjacent natural resources.

In addition to the applicant's findings, the Commission can find that the proposed storm water catch basin and detention and water quality facility will treat any storm water concerns for the gravel area.

(2) The granting of an exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.

This exception request does allow a use that is not permitted. The I-G zone allows establishments that are primarily engaged in special trade construction and the use of tracked vehicles. The request meets the exception criterion.

(3) There are unique or unusual circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.

The unique circumstance in this instance is the proposed use of heavy equipment and tracked vehicles. MLDC 10.746 specifically mentions that parking and vehicle maneuvering areas for wheeled-vehicle sales lots shall be paved. This section does not mention tracked vehicles. Per the applicant, 'the pavement will be destroyed with bulldozers, excavators, and other tracked machinery running over paved surface.' Staff agrees with the applicant's findings and the Commission can find that the strict application of the paving requirement per MLDC 10.746 could result in unusual hardship for the owner.

(4) The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.

Allowing the Exception would not result in greater profit for the applicant nor is it the result of an illegal act; it is simply a matter of practicality.

The Public Works Department supports the proposed exception on the condition that the unpaved portion of the development is limited to heavy equipment only (Exhibit G).

No other issues were identified by staff.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit E and F) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-18-138 and E-18-139 per the staff report dated January 11, 2019, including Exhibits A through R.

EXHIBITS

- A. Conditions of Approval, dated November 9, 2018
- B. Site Plan received November 5, 2018
- C. Landscape Plan received September 21, 2018
- D. Architectural Plan received September 21, 2018
 - Color Palette
- E. Applicants findings and conclusions received September 21, 2018
- F. Applicant's supplemental findings received November 7, 2018
- G. Public Works Department Report revised December 4, 2018
- H. Medford Fire Department report received October 31, 2018
- I. Medford Building Department memo received October 31, 2018
- J. Medford Surveyor comments received October 25, 2018
- K. Medford Water Commission memo received October 31, 2018
- L. Rogue Valley Sewer Services comments received October 23, 2018
- M. Oregon Department of Aviation comments received October 22, 2018
- N. Jackson County Airport Authority comments received October 23, 2018
- O. Jackson County Road comments received October 19, 2018
- P. Traffic Study received December 24, 2018
- Q. Comments from Jackson County Roads re Traffic Study received January 3, 2019
- R. Comments from City Traffic Engineer received January 3, 2019
Vicinity map

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA:

**NOVEMBER 16, 2018
DECEMBER 18, 2018
JANUARY 18, 2019**

EXHIBIT A
Pilot Rock Excavation
AC-18-138 / E-18-139
Conditions of Approval
January 11, 2019

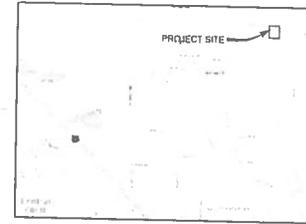
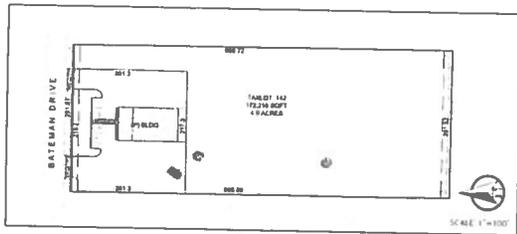
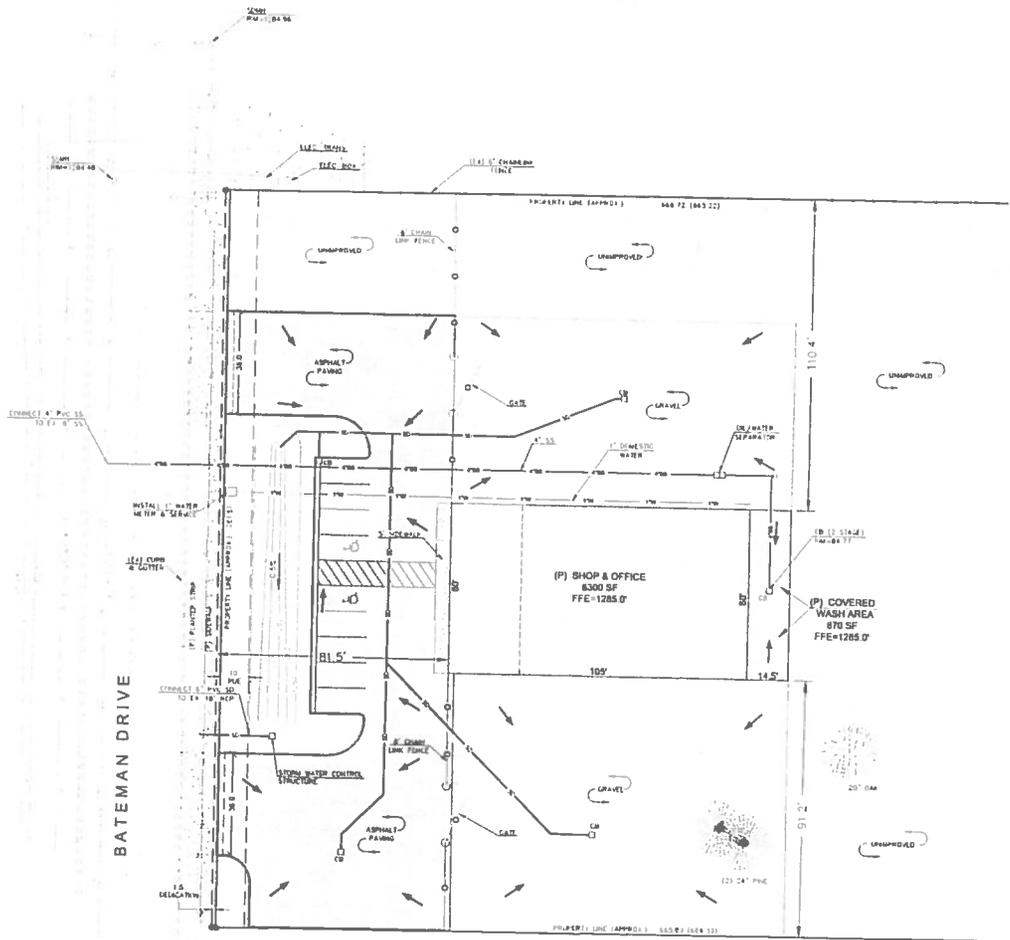
DISCRETIONARY CONDITIONS

Upon granting approval of paving requirement, limit the unpaved portion of the development to heavy equipment only.

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit G).
2. Comply with all requirements of the Medford Fire Department (Exhibit H).
3. Comply with all requirements of the Medford Building Department memo (Exhibit I).
4. Comply with all conditions stipulated by the Medford Water Commission (Exhibit K).
5. Comply with all requirements of the Rogue Valley Sewer Services (RVSS) (Exhibit L).
6. Comply with Jackson County Airport Authority's condition to file any forms required by FAA (Exhibit N).
7. Comply with concealment requirements for all Heating, Ventilation, Air Conditions equipment pursuant to MLDC 10.781-10-782.
8. Comply with required bicycle parking pursuant to MLDC 10.748-10.751.



VICINITY MAP
RECEIVED
NOV 08 2018
Planning Dept.

PROJECT:
PROPOSED SHOP & OFFICE
T.36.19.29W. SEC.36D
TAXLOT 142

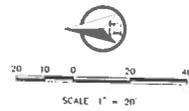
OWNER:
JEFF & ALYSON FOWLER
368 BATEMAN DRIVE

TOTAL AREA: 174,246 SQ. FT.
4.00 ACRES

DEVELOPMENT AREA: 47,220 SQ. FT.

ZONING: RLI - RURAL LIMITED INDUSTRIAL

PARKING:
7 STANDARD SPACES
2 HC SPACES



LEGEND - NEW CONSTRUCTION

AC	ASPHALT	SEMI	STORM DRAIN MANHOLE
CB	CATCH BASIN	SEMI	SEMI
TOG	TOP OF GATE	SDC	STORM DRAIN, CLEAN OUT
LF	LINEAR FEET	FL	FLOW LINE
KE	RIVER ELEVATION	(E+)	EXTINGUISH
RC	TOP OF CURB	(P)	PROPOSED
GD	GRADE	(U)	UTILITY LOCATION
(E+)	EXISTING SURFACE	(100.00)	EXISTING GRADE
←	SURFACE DRAINAGE		

PRELIMINARY
THIS IS A PRELIMINARY PLAN FOR INFORMATION ONLY. IT IS NOT TO BE USED FOR CONSTRUCTION. A PERMIT IS REQUIRED FOR ANY WORK.



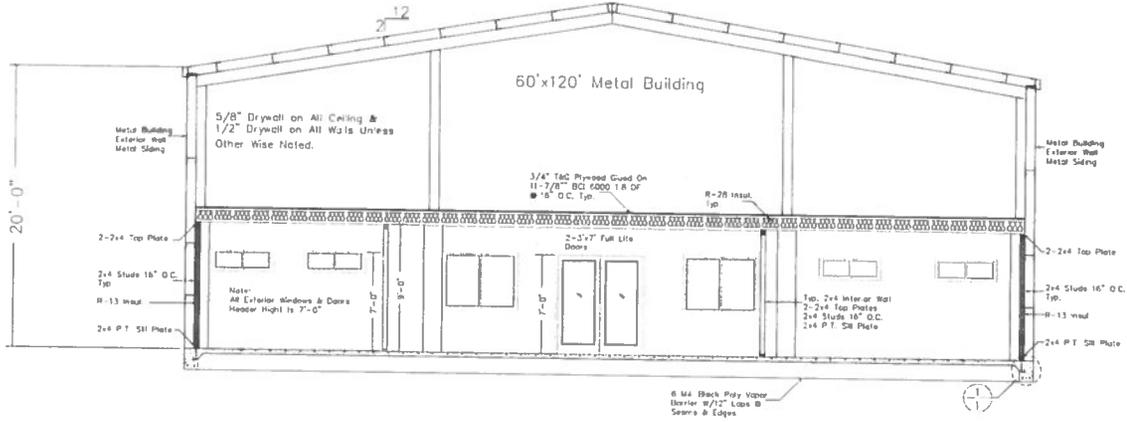
JEFF & ALYSON FOWLER
SHOP & OFFICE
368 BATEMAN DRIVE
MEDFORD, OREGON

CIVIL SITE PLAN

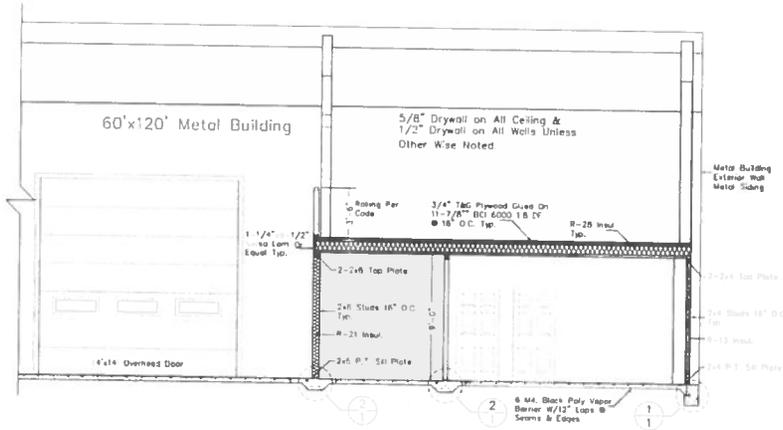
DATE	BY	SCALE
FILE NAME	CAD	
DATE	REV	DESCRIPTION

C1

CITY OF MEDFORD
 EXHIBIT # D of 6
 FILE # AC-18-138 / E-18-139

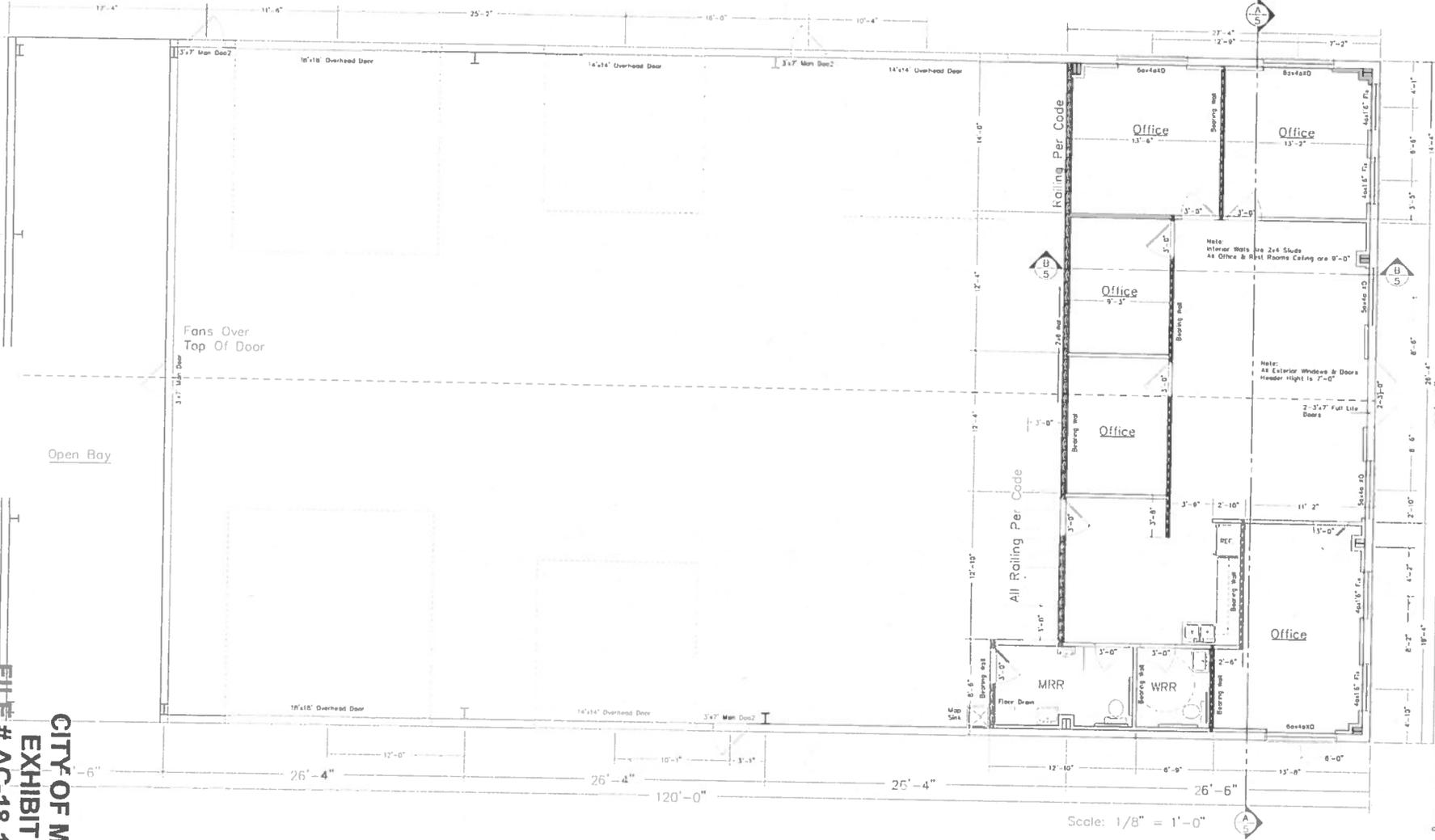


Section AA



Section BB

CONTRACTOR: Ken Brown Construction P.O. Box 858 Glod Hill, Oregon 97525 (541) 855-1846 Fax (541) 855-8641		LICENSE # 102440 DATE REVISED:
OWNER: Jeff Fowler (Pilot Rock Excavation) 356 Batement Drive Medford, Oregon 97504		DATE REVISED:
<i>CustomCADGraphics</i> 949 East Vijas Road Central Point, Oregon 97502 Cell: 541-621-5217		
Building late	Interior Office Layout Batement Drive Medford, Oregon	SECTIONS
RECEIVED SEP 21 2018 PLANNING DEPT		



Scale: 1/8" = 1'-0"

PROJECT: New 60'x120' Metal Building With Interior Office Layout 356 Batement Drive Medford, Oregon	CONTRACTOR: Ken Brown Construction P.O. Box 858 Glod Hill, Oregon 97525 (541) 855-1846 Fax (541) 855-8641	LICENSE # 102440 DATE REVISION:
	OWNER: Jeff Fowler (Pilot Rock Excavation) 356 Batement Drive Medford, Oregon 97504	CUSTOMER NAME: CustomCAD Graphics 949 East Vilas Road Central Point, Oregon 97502 Cell: 541-621-5217

FLOOR PLAN

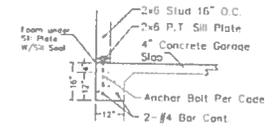
CITY OF MEDFORD
EXHIBIT # D 5 of 6
FILE # AC-18-138 / E-18-139

FOR METAL BUILDING
FOUNDATION SEE
METAL BUILDING
ENGINEERING

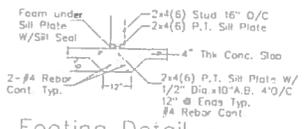
120'-0"

FOUNDATION/SLAB NOTES

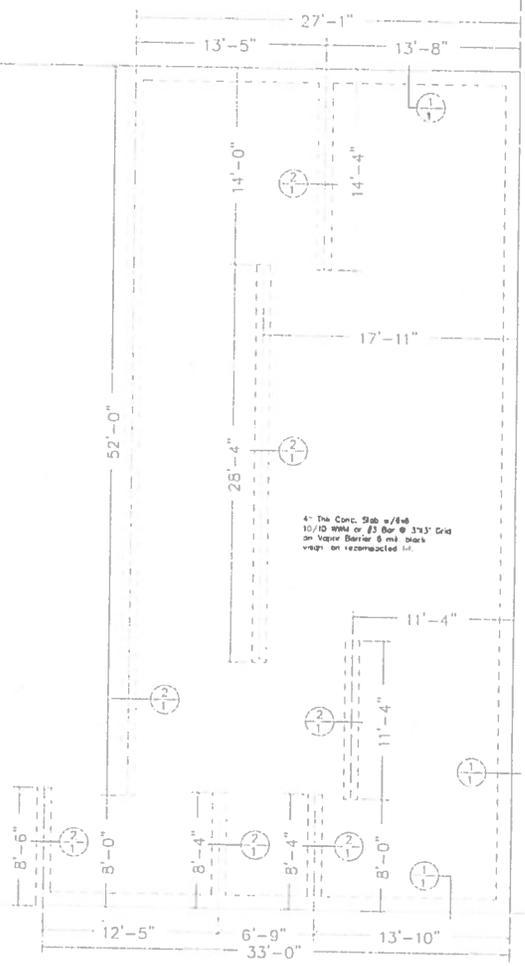
1. ALL FOOTINGS TO BEAR ON SOLID UNDISTURBED, NON-EXPANSIVE SOIL OR COMPACTED STRUCTURAL FILL.
2. CONCRETE SLABS TO BE PLACED OVER 4" OF 3/4" WASH CRUSHED ROCK AND STRUCTURAL FILL AS REQUIRED. ALL COMPACTED TO 95% DENSITY.
3. CONCRETE TO DEVELOP FULL STRENGTH OF 2500 PSI IN 28 DAYS. ALL EXTERIOR EXPOSED SLAB WORK TO BE AIR ENTRAINED.
4. REINFORCING BARS TO BE DEFORMED CONFORMING TO ASTM SPEC. A-625 WELDED WIRE FABRIC TO CONFORM TO ASTM SPEC. 333.
5. VERIFY LOCATIONS OF UTILITY SERVICE ENTRANCES. PROVIDE SLEEVES AND BLOCK OUTS AS REQUIRED FOR ELECTRICAL, TELEPHONE, TV, GAS, WATER, SEWER, FURNACE, DRYER AND RANGE VENTS, ETC. SEE FLOOR PLAN, MECH. AND ELEC. PLANS FOR DETAILS.
6. REINFORCING BARS IN FOOTINGS TO HAVE MIN 3" CONCRETE COVERAGE.
7. ALL NAIL SAILS TO HAVE MIN (2) ANCHOR BOLTS PER BOARD AND BE NO MORE THAN 12" FROM THE END OF BOARD.
8. Use 3"x3 1/4" Galv. Washer 1/2" A/B



Footing Detail 1



Footing Detail 2



60'-0"

LICENSE # 10240 DATE REVISED

CONTRACTOR:
Ken Brown Construction
P.O. Box 858 Glad Hill, Oregon 97525
(541) 855-1846 Fax (541) 855-8641

OWNER:
Jeff Fowler (Pilot Rock Excavation)
356 Batemans Drive
Medford, Oregon 97504

CustomCADGraphics

949 East Vilas Road
Central Point, Oregon 97502
Cell: 541-621-5217

60'x120' Metal Building
with Interior Office Layout
356 Batemans Drive
Medford, Oregon

FOUNDATION PLAN

RECEIVED
SEP 21 2018
PLANNING DEPT

Nucor Standard Panel Paint Systems

WALL

Nucor Classic Wall™



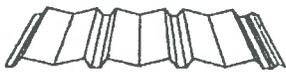
36" Panel Coverage / 26 Gauge*

Nucor Reverse Classic Panel™



36" Panel Coverage / 26 Gauge*

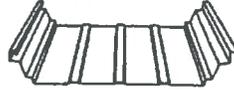
Nucor Accent Panel™



36" Panel Coverage / 26 Gauge*

ROOF

Nucor CFR™



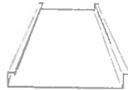
24" Panel Coverage / 24 Gauge*

Nucor Classic Roof™



36" Panel Coverage / 26 Gauge*

Nucor VR16 II™



16" Panel Coverage / 24 Gauge*

Our Silicone Polyester paint is a two-coat system that utilizes cool coating technology and offers superior quality and durability.

Colors shown are representative of actual colors offered and are NOT intended for matching purposes. Exact color match should be made from metal color chip samples.

Initial Solar Reflectance (IR) is the fraction of the total solar energy that is reflected away from a surface. To be considered "cool", products must have a Solar Reflectance of at least .25.

Initial Thermal Emittance (IE) is the relative ability of the roof panel to radiate absorbed heat.

Solar Reflectance Index (SRI) is calculated by using the values of solar reflectance, thermal emittance, and a medium wind coefficient. The higher the SRI value, the lower its surface temperature and consequently, the heat gain into the building.

Galvalume™ gutters, rake, and downspouts are available as an upcharge. Galvalume® ratings are .680 Initial Solar Reflectance (IR), .10 Initial Thermal Emittance (IE), and 56 Solar Reflectance Index (SRI).

Base angle flash is available in Burnished Slate and Polar White only.

All Standard Silicone Polyester colors shown on this chart feature a 25 year finish warranty. Unpainted Galvalume® panels feature a 25 year finish warranty. See Warranty Guide for specific warranty information. (Warranties apply only to the finish coat of exterior mounted panels. Backer side primer colors may vary.)

The term "TBS" on the Nucor Order Document refers to "To Be Selected from Standard Nucor Silicone Polyester Colors" as shown on this chart.

In keeping with a continuing program of product improvement, all information contained herein is subject to change without notice.

*Other gauges are available with extended lead-times and additional charges.
† Galvalume® is a registered trademark of BIEC International, Inc.

BURNISHED SLATE (BS)
IR: .31 IE: .86 SRI: 32

EVERGREEN (EG)
IR: .26 IE: .86 SRI: 25

AZTEC BLUE (AB)
IR: .25 IE: .86 SRI: 24

BRICK RED (BR)
IR: .32 IE: .85 SRI: 53

SAGEBRUSH TAN (SB)
IR: .47 IE: .85 SRI: 53

FOX GRAY (FG)
IR: .43 IE: .85 SRI: 47

LIGHTSTONE (LS)
IR: .59 IE: .87 SRI: 70

POLAR WHITE (PW)
IR: .66 IE: .86 SRI: 79



www.nucorbuildingsystems.com

"D"
6 of 6

Applicant – Jeff & Alyson Fowler

356 BATEMAN DRIVE – 362W36D 142(Map-taxlot)

City of Medford Planning – Site Plan and Architectural Review Application

PROJECT NARRATIVE:

Jeff and Alyson Fowler request to build on vacant parcel a new “steel” framed office attached to an Industrial style “steel” shop building with steel siding and roof; used for construction equipment and dump truck maintenance. Shop design is anticipated to have a covered wash rack facility on concrete slab per DEQ code requirements, including oil/water separator. Property will be securely fenced with gates to rear of property. Proposed fence/gates will be installed at Northern corner of office/shop allowing for open access to office frontage, and fence set-backs of approximately 90’ from Bateman Drive street frontage. The street frontage and access to office will be paved and all city drainage improvements, including detention systems as required. Landscaping will be implemented between street and office. Area around shop will be graveled due to equipment storage needs and track mounted excavators, dozers, etc which will have grousers; not allowing for HMAC surfacing.

- A) The existing uses and developments adjacent to said property is currently used as Truck Fleet storage yard/shops, Industrial/Commercial buildings used for manufacturing, and other contractors of the like reside their place of business in this local area.
- B) The proposed shop/office building including architecture and exterior façade will match all other buildings in the local area. Our proposed building will be two-toned per attached color samples provided in application. The building façade and roof will be corrugated metal, with the bottom 8’ of siding and all trim “burnished slate” in color, and the remaining 16’ height “fox gray” in color.
- C) The proposed two-toned siding, color choice, and landscaping will provide greater curbside visual and improve the existing development.
- D) The street frontage will include full width city sidewalks which will improve the pedestrian access on frontage street as well as accessing proposed building. The civil design layout for employee parking and handi-cap parking allows for easy local access too.
- E) Building frontage sidewalk installed per plans to run along the entire front of building, 5’x60’
- F) There are two driveways leading to the parking lot that each allow for ingress and egress from the site. The parking area is directly in front of the entrance into the front office.
- G) The site plan is not sensitive to retaining any existing trees and there is no significant native vegetation on the site.
- H) The civil plan allows for surface pond detention facility which includes landscaping along street frontage which will enhance local curb appeal.
- I) The landscape plan will improve the proposed and thus adjacent properties by beautifying the site.

J) All exterior building lighting will be LED and provide glare protection shields as necessary to protect neighboring properties from unwanted glare.

K) A 3' x 4' commercial sign with our Pilot Rock Excavation logo will be placed among the landscaping in the front of the building adjacent to the road.

L) Fencing will be installed across front of building and surrounding south, east, and west property lines in an effort to reduce liability and theft to said property.

M) The proposed use for this property will suffice the noise ordinance consistent with the standards of sections 10.752-10.761.

O) We are requesting a modification to the code regarding the number of parking spaces required. Our project proposes the allowance for 9 parking spaces. The number of required spaces outlined in Table 10.743-1 state for Industrial or Warehouse application:

One space per employee on the largest shift, plus 0.2 space per 1,000 square feet of gross floor area.

10.743 section (3) states, "The approving authority may allow exceptions to the number of parking spaces in Table 10.743-1 for specific uses without complying with Section 10.186 if they find that the applicant's detailed description of the proposed use demonstrates that the number of needed parking spaces is less than the minimum required".

Pilot Rock Excavation employs at the height of the construction season upwards to 20 employees. The reason for the request of reduction in parking spaces is because the functionality of our business at this location will not require a space for each employee. The bulk of our employees meet and start their work day at the job site. We have three employees that report to the office each day, the remainder travel directly to the job sites to report to work. The office we are building will have five office spaces. If we expanded our business and employed two more office personnel, thus filling all five offices, nine parking spaces will still more than meet our needs at this location.

P) N/A

MEDFORD LAND DEVELOPMENT CODE SECTION 10.186, EXCEPTION

10.746 General Design Requirements for Parking

With the exception of storage of trailered items and recreational vehicles at single-family residences, all parking, loading, driveway, and vehicle maneuvering areas, including but not limited to, wheeled-vehicle sales lots, truck trailer parking areas, and on-site single-family residential driveways etc., shall be paved and improved pursuant to the following minimum design requirements of this section.

(A) Exception, Purpose.

The applicants are the owners of a 4 acre industrial piece of land. The site will be used to build a new office and shop for their excavation business. Our excavation business owns 5 highway dump trucks and a fleet of more than 30 pieces of equipment, including but not limited to excavators, loaders, back-hoes, skid steers, dozers, etc. The shop will serve as an area for maintenance of heavy equipment and trucks. The applicants wish to pave the street frontage, front parking area and access to office, and all city drainage improvements including required detention systems. The applicants propose the area around the shop be graveled due to equipment storage needs and track mounted excavators, dozers, etc. which have grousers. This exception is appropriate for reasons of:

(4) if strict applications of the public improvement or site development standards in the above-referenced Articles would result in peculiar, exceptional, and undue hardship on the owner.

(B) Criteria for an Exception.

(1) The granting of the exception shall be in harmony with the general purpose and intent of the regulations imposed by this code for the zoning district in which the exception request is located, and shall not be injurious to the general area or otherwise detrimental to the health, safety, and general welfare or adjacent natural resources. The approving authority shall have the authority to impose conditions to assure that this criterion is met.

The use of gravel around the shop area is not unlike the ground cover that currently exists on the property and will not be injurious to the general area or detrimental to the health, safety, or general welfare or adjacent natural resources.

(2) The granting of the exception will not permit the establishment of a use which is not permitted in the zoning district within which the exception is located.

The use of gravel around the shop area will not permit us a use which is not permitted in the zoning district within which the property is located.

(3) There are unique circumstances which apply to this site which do not typically apply elsewhere in the City, and that the strict application of the standard(s) for which an exception is being requested would result in peculiar, exceptional, and undue hardship on the owner.

The strict application of the standard to pave *all parking, loading, driveway, and vehicle maneuvering areas* would result in the pavement being heavily damaged by the tracks of our many pieces of equipment, large and small as the pieces are moved in and out of the shop area for maintenance. HMAC surfacing and/or concrete surfacing as outlined in **9.550 Parking Lot Specifications (3) and (4)** will not hold up to the grousers of the equipment that will be serviced and maintained in the shop building.

(4) The need for the exception is not the result of an illegal act nor can it be established on this basis by one who purchases the land or building with or without knowledge of the standards of this code. It must result from the application of this chapter, and it must be suffered directly by the property in question. It is not sufficient proof in granting an exception to show that greater profit would result.

The purpose of this exception is not the result of an illegal act.

"E"
4 of 5

RECEIVED

Jackson County Official Records 2018 SEP 25 2018
R-WD 04/10/2018 11:59 AM
Str=10 SHINGLJS \$10.00 \$10.00 \$8.00 \$11.00 \$20.00 \$59.00
PLANNING DEPT



After recording return to:
Jeff Fowler and Alyson Fowler
1716 Magnolia Ave.
Medford, OR 97501

Until a change is requested all tax
statements shall be sent to the
following address:
Jeff Fowler and Alyson Fowler
1716 Magnolia Ave.
Medford, OR 97501

File No.: 161-3033497 (SDB)
Date: April 10, 2018

THIS SPACE RE
I, Christine Walker, County Clerk for Jackson County, Oregon, certify
that the instrument identified herein was recorded in the Clerk
records.
Christine Walker - County Clerk

STATUTORY WARRANTY DEED

Franklin Bruce Bateman and Anna W. Bateman, as tenants by the entirety, Grantor, conveys
and warrants to **Jeff Fowler and Alyson Fowler as tenants by the entirety**, Grantee, the following
described real property free of liens and encumbrances, except as specifically set forth herein:

LEGAL DESCRIPTION: Real property in the County of Jackson, State of Oregon, described as follows:

**PARCEL 3, AS SHOWN ON THE PARTITION PLAT FILED IN THE OFFICE OF THE JACKSON
COUNTY OREGON SURVEYOR AS NO. 19560, AND RECORDED AS PARTITION PLAT NO. P-18-
2007 OF "RECORD OF PARTITION PLATS" IN JACKSON COUNTY, OREGON.**

Subject to:

1. Covenants, conditions, restrictions and/or easements, if any, affecting title, which may appear in the public record, including those shown on any recorded plat or survey.

The true consideration for this conveyance is **\$400,000.00**. (Here comply with requirements of ORS 93.030)

RECEIVED

NOV 07 2018

Planning Dept.

BLOCK LENGTH FINDINGS

10.426 Street Circulation Design and Connectivity

C. Maximum Block Length and Block Perimeter Length

1. Block lengths and block perimeter lengths shall not exceed the following dimensions as measured from centerline to centerline of through intersecting streets, except as provided in Subsections 10.426 C.2. When measured from the intersection of Bounty Lane West along Bateman Drive a distance of 940 feet falls within an undeveloped parcel thus showing that the block length for an industrial zone can be met with future development of the nearby and adjoining property.

Zone or District	Block Length	Block Perimeter Length
Industrial Zones	940'	3460'

C. 2. The approving authority may find that proposed blocks that exceed the maximum block and/or perimeter standards are acceptable when it is demonstrated by the findings that one or more of the constraints, conditions or uses listed below exists on, or adjacent to the site:

f. Future development on adjoining property or reserve acreage can feasibly satisfy the block or perimeter standards,

The attached graphic shows a map of 356 Bateman and adjacent properties. The blue line indicates a perimeter of a Maximum Block Perimeter Length, starting at the existing intersection of Bateman and Bounty Ln. Drawing on the map Block Lengths of 940', a Block Perimeter of 3460' was achieved.

356 Bateman sits within the Block Perimeter lines on all sides. 940' west of the corner of Bateman and Bounty Lane runs through a larger parcel of land that is two lots away from 356 Bateman. We refer to the Municipal Code 10.426 C. 2. f. that states future development on adjoining property can feasibly satisfy the block or perimeter standards.

CITY OF MEDEORD
EXHIBIT # F 1 of 3
FILE # AC-18-138 / E-18-139

11' F 11
2 of 3



EX

PLANNING EXHIBIT BLOCK LENGTH

NO. FOR AS. BUILT	
DATE	
PREP NAME	CSTE
DATE	06/11/09
DRAWN	REC
CHECKED	ESH

JEFF & ALYSON FOWLER
 SHOP & OFFICE
 256 BAYTEMAN DRIVE
 MEDFORD OREGON

MARQUES CONSULTING
 CIVIL & PROFESSIONAL ENGINEERING CONSULTANTS
 1000 MARSHALL BLVD
 SUITE 100
 MEDFORD, OR 97504
 503.754.4477

PRELIMINARY
 IT IS NOT GUARANTEED TO BE ACCURATE FOR CONSTRUCTION. CONSULT WITH THE ENGINEER FOR A FINAL DESIGN AND CONSTRUCTION OF A PROJECT.

Distance: 941.9 Feet

Bateman Dr

Bounty Ln

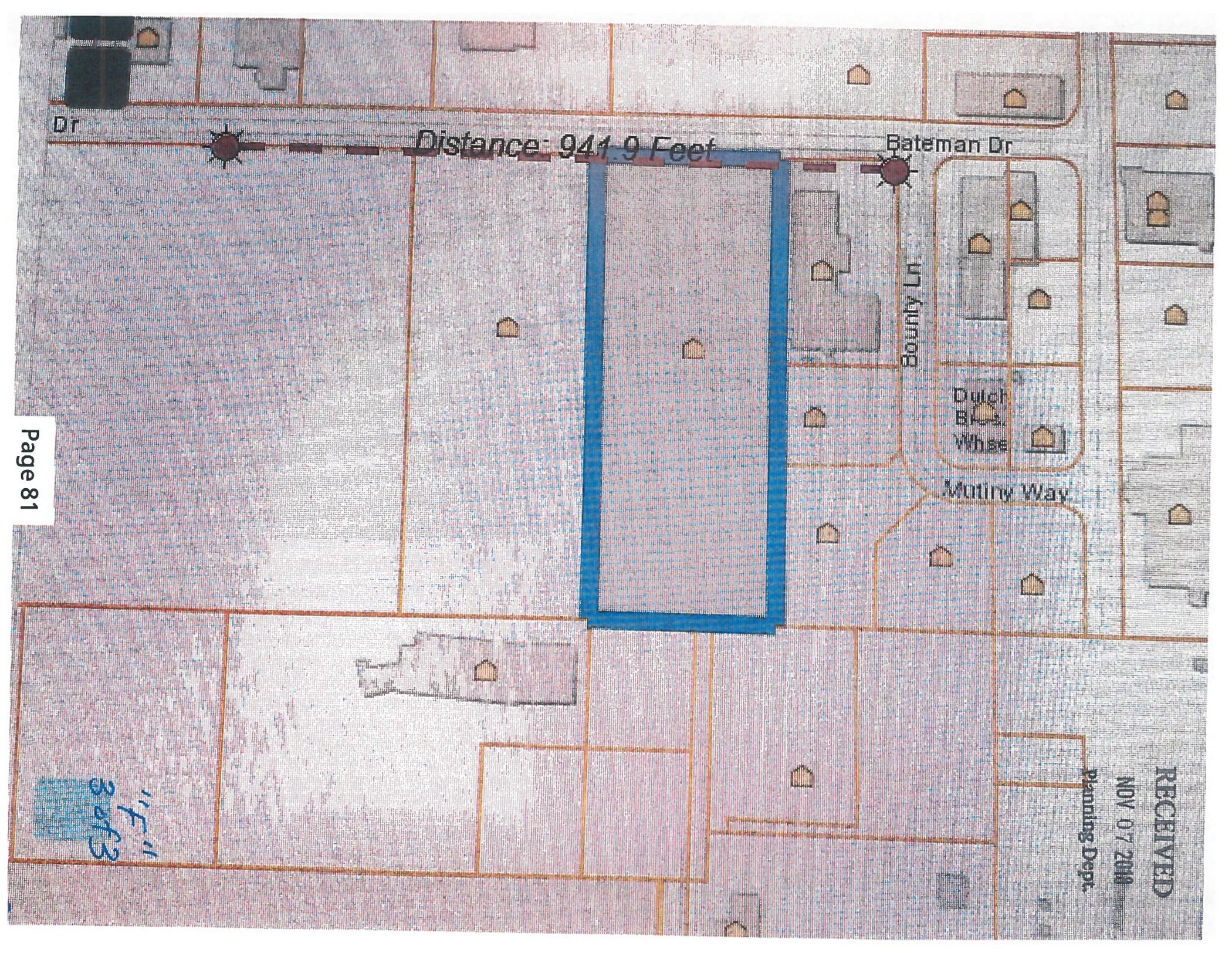
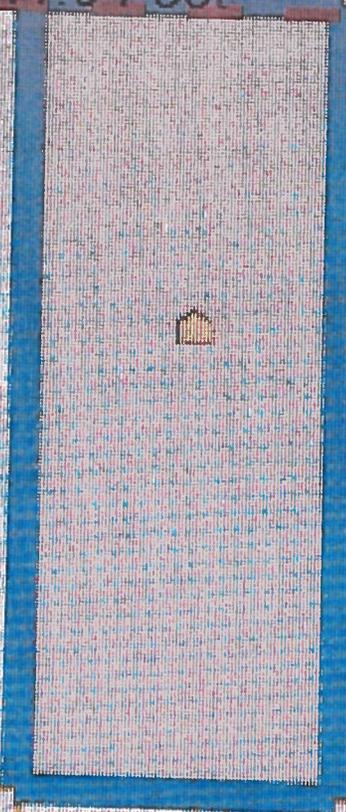
Dutch
Bldg
Whse

Mutiny Way

RECEIVED
NOV 07 2004
Planning Dept.

11 Feet
3 of 3

Dr





Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 10/31/2018

Revised: 12/04/2018

File Number: AC-18-138/E-18-139

PUBLIC WORKS DEPARTMENT STAFF REPORT

356 Bateman Drive (TL 142)

Pilot Rock Excavation, Inc.

- Project:** Consideration of the construction of a steel buildings for a shop and office approximately 6,300 square feet with an exception to the General Design Requirements for Parking, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved in size on a 4.0 acre parcel.
- Location:** located at 356 Bateman Drive approximately 450 feet west of Bierson Way zoned I-G (General Industrial) (362W36D TL 142).
- Applicant:** Applicant: Jeff and Alyson Fowler; Planner; Liz Conner
Related Application(s): PA-18-050.

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of vertical building permits. Construction plans for the improvements will need to be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas.
- Certification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

A. STREETS

1. Dedications

Bateman Drive is classified as Commercial Street within the Medford Land Development Code (MLDC) 10.429. **This section of Bateman Drive along this frontage is under jurisdiction of Jackson County.** Right-of-way has already been dedicated with partition plat no. P-18-2007 (Survey #19560). **No additional right-of-way is required.**

There is currently a 10-foot PUE along the frontage of Bateman Drive that was originally dedicated with the final plat for Table Rock Industrial Park (Survey #9673)

2. Public Improvements

a. Public Streets

Standard street section improvements have been completed on **Bateman Drive**, including pavement, curb and gutter and partial sidewalk as part of the Road & SS Construction Improvements (P1285D). **This section of Bateman Drive along this frontage is under jurisdiction of Jackson County, and shall continue to be maintained by Jackson County.** No additional improvements are required except for sidewalk with a planter strip.

b. Street Lights and Signing

The Developer shall provide and install in compliance with Section 10.495 of the Medford Municipal Code (MMC). Based on the preliminary plan submitted, the following number of street lights and signage will be required:

Street Lighting & Signage – Developer Provided & Installed:

A. 1 – Type R-150*

**NOTE – There is a street light going in across from this development. It is Old Dominion Freight Line - (AC-18-016). Once that is installed, this fixture for Pilot Rock would no longer be needed for lighting requirements.*

Traffic Signs and Devices – City Installed, paid by the Developer:

A. N/A

Numbers are subject to change if changes are made to the plans. All street lights shall be installed per City standards. Public Works will provide preliminary street light locations upon request. All street lights shall be operating and turned on at the time of the final "walk through" inspection by the Public Works Department.

The Developer shall pay for City installed signage required by the development. City installed signs include, but are not limited to, street name signs, stop signs, speed signs, school signs, dead end signs, and dead end barricades. Sign design and placement shall be per the Manual on Uniform Traffic Control Devices (MUTCD). All signs shall be shown on the public

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improvement plans and labeled as City installed.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer's contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this frontage to Bateman Drive.

d. Access and Circulation

The existing block length and block perimeter length do not comply with MLDC 10.426. The applicant has provided findings adequately addressing these requirements and the requirements of MLDC 10.464.

The driveways shown on the site plan are approximately 40-feet wide, which does not comply with MLDC 10.550. The driveways shall be revised to comply with this code section, which allows up to 36-feet wide driveway with either a curb cut or 20-foot radius approach.

3. Section 10.668 Analysis

To support a condition of development that an applicant dedicate land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and

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supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and storm drains to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining "rough proportionality" have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Bateman Drive:

The additional improvements will provide a planter strip and sidewalk on Bateman Drive. The planter strip moves pedestrians a safe distance from the edge of the roadway. Bateman Drive will be the primary route for pedestrians traveling to and from this development. The development shall construct approximately 260 linear feet of sidewalk along the frontage of the property. All developments in Medford are required to construct their frontage sidewalk and therefore this is roughly proportional.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the project's impacted site with sufficient information to determine the direction of runoff to the existing or proposed drainage system, and also showing elevations of the proposed drainage system (if applicable), shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

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All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

If the proposed development is to be constructed in phases, then each phase will be required to have its own stormwater detention and water quality treatment. If the Developer desires to do so, a Stormdrain Masterplan may be submitted in lieu of requiring each phase to have separate stormwater detention and water quality treatment. The Stormdrain Masterplan shall be submitted and reviewed with each phase's construction plans and shall be constructed with any phase to be served by the facility.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

6. Storm Drainage Conditions

Make improvements to storm drainage along the south property line to convey the 10 year storm with a foot of freeboard.

Provide a minimum 15-foot SD easement along the south side of the property if the drainage is to remain a ditch.

Provide a minimum 10-foot SD easement along the south side of the property if the drainage is allowed to be piped.

If piped, provide a manhole (MH) near the southeast corner of the property and provide access easement across the lot to the MH.

D. GENERAL CONDITIONS

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional Engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Construction drawings for public improvements shall be submitted only for the improvements to be constructed with each phase. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the governing Commission's Final Order, together with all pertinent details and calculations. A checklist for public improvement plan submittal can be found on the City of Medford, Public Works web site (<http://www.ci.medford.or.us/Page.asp?NavID=3103>). The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

In order to properly maintain an updated infrastructure data base, the Surveyor of Record shall submit an as-built survey prior to the Final Inspection and, the Engineer of Record shall submit mylar "as-constructed" drawings to the Engineering Division within sixty (60) calendar days of the Final Inspection (walk through). Also, the Engineer shall coordinate with the utility companies, and show all final utility locations on the "as built" drawings.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the

Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans.

Contractors proposing to do work on public streets, sewers, or storm drains shall 'prequalify' with the Engineering Division prior to starting work. Contractors shall work off a set of public improvement drawings that have been approved by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and storm drain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the Developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

Public Works supports the proposed exception on the condition that the unpaved portion of the development is limited to heavy equipment, including bulldozers, excavators and other tracked machinery only. At no time shall wheeled vehicles and/or trailers be allowed to park within the unpaved area. Public Works also requests the Applicant stipulate to implement dust mitigation as needed, and required, per the Medford-Ashland Air Quality Maintenance Area Plan. The Developer shall install stormwater quality and detention facilities, in accordance with MLDC Section 10.481 and 10.729, for the gravel areas unless otherwise approved by the City Engineer.

If the unpaved portion of this development shall be used, or is observed to be used, by vehicles other than tracked machinery, then the Applicant will be required to pave the entire subject area per Medford Land Development Code (MLDC) 10.746.

The Developer shall either fence or pave the easterly gravel area.

5. System Development Charges (SDC)

Buildings in this development are subject to SDC fees. All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Jodi K Cope

Reviewed and Revised by: Doug Burroughs, Revised by: Jodi K Cope 12/04/2018

P:\Staff Reports\AC\2018\AC-18-138_E-18-139 356 Bateman Dr (TL 142) Shop_Office Bldg\AC-18-138_E-18-139 Staff Report-LD_REV3.docx

Page 7 of 8

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PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

SUMMARY CONDITIONS OF APPROVAL

356 Bateman Drive (TL 142)

Pilot Rock Excavation, Inc.

AC-18-138/E-18-139

A. Streets

1. Street Dedications to the Public:

- **Bateman Drive** – No additional right-of-way required.
- 10-foot Public Utility Easements have been dedicated.

2. Improvements:

Public Streets

- **Bateman Drive** – No improvements are required aside from 5-foot wide sidewalk with a planter strip.

Lighting and Signing

- Developer supplies and installs all street lights at own expense.
- City installs traffic signs and devices at Developer's expense.

Access and Circulation

- The Applicant has addressed the requirements of MLDC 10.426 and MLDC 10.464.
- The Applicant shall comply with the access standards of MLDC 10.550.

Other

- There is no pavement moratorium currently in effect on **Bateman Drive**.
- The Developer shall either fence or pave the easterly gravel area.

B. Sanitary Sewer:

- The site is situated within the RVSS area.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers certification of stormwater facility construction.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.
- Comply with Storm Drainage Conditions.
- Public improvement plans shall be submitted directly to the Public Works Engineering Division.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.

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Medford Fire-Rescue Land Development Report

Review/Project Information

Reviewed By: Kleinberg, Greg

Review Date: 10/29/2018
Meeting Date: 10/31/2018

LD #: AC18138 Associated File #1: E18139

Planner: Liz Conner

Applicant: Jeff and Alyson Fowler

Project Location: 356 Bateman Drive

Project Description: Consideration of the construction of a steel buildings for a shop and office approximately 6,300 square feet with an exception to the General Design Requirements for Parking, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved in size on a 4.0 acre parcel located at 356 Bateman Drive approximately 450 feet west of Bierson Way zoned I-G (General Industrial) (362W36D TL 142).

Specific Development Requirements for Access & Water Supply

Conditions

Reference	Comments	Description
OFC 3201	High-piled storage requirements.	A maximum of 500 sq. ft. of high-piled combustible storage area is allowed without additional requirements. High-piled storage constitutes storage heights over 12' for normal combustibles or storage heights over 6' for certain high-hazard commodities. If the threshold is exceeded, the additional requirements of Oregon Fire Code Chapter 32 must be met (Additional fire protection and building design features for high piled storage).
OFC 1016, 1(2) + OFC TABLE 1014.3 COMMON PATH		OF EGRESS TRAVEL MAY EXCEED 100 FT FOR I/S OCCUPANCY

Construction General Information/Requirements

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300

www.medfordfirerescue.org

CITY OF MEDFORD
EXHIBIT # 11
FILE # AC-18-138 / E-18-139

Memo



To: Planner; Liz Conner.
From: Chad Wiltrout, Building Department (541) 774-2363
CC: Applicant: Jeff and Alyson Fowler.
Date: October 29, 2018
Re: October 31, 2018, 2018 LDC Meeting: AC-18-138/E-18-139; Previous application PA-18-050

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit will be required if more than 50 cubic yards is disturbed and to install utilities.
4. A separate demolition permit will be required for demolition of any structures not shown on the plot plan.

Comments:

5. The proposed building and site shall comply with all of the applicable ADA requirements scoped from the 2014 OSSC and ANSI A117.1
6. The building and building appurtenances shall be designed by an Oregon licensed design professional in accordance with 107.1 and 107.3.4 OSSC.
7. A code analysis providing occupant load, type of construction, type of occupancy, separated/non-separated sprinkled/non-sprinkled, means of egress plan etc... will be required. Commercial truck repair facilities with a fire areas greater than 5000 square feet requires a fire suppression system per 903.2.9.1 OSSC

8. A geotechnical engineer shall provide a design for soils at building locations pursuant to 1803 of the Oregon Structural Specialty Code.
9. Com-check forms are required for interior and exterior lighting, exterior envelope, mechanical equipment and water heating equipment to show energy compliance with the 2014 OEESC.
10. Special inspections will be required for this project based on the type of construction and construction methods based on chapter 17 of the OSSC.

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City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Legal Description
 File no. AC-18-138- E-18-139
 To Jon Proud, Engineering
 From Liz Conner, Planning Department
 Date October 17, 2018

Please verify the attached legal description covering the below subject at your earliest convenience. See attached map.

1. AC-18-138/E-18-139
 Applicant: Jeff and Alyson Fowler
 Agent: Jim Higday

*Liz, yes, this is description
 for 'Subject Area' shown on
 vicinity map.*

*THANKS, JON
 10/25/18*

cp

Attachments:

Vicinity Map, Legal description

RECEIVED

Jackson County Official Records 2018 SEP 12 542 2018
R-WD
Stn=10 SHINGLJS 04/10/2018 11:15:50 AM
\$10.00 \$10.00 \$8.00 \$11.00 \$20.00 \$59.00
PLANNING DEPT



After recording return to:
Jeff Fowler and Alyson Fowler
1716 Magnolia Ave.
Medford, OR 97501

Until a change is requested all tax
statements shall be sent to the
following address:
Jeff Fowler and Alyson Fowler
1716 Magnolia Ave.
Medford, OR 97501

File No.: 161-3033497 (SDB)
Date: April 10, 2018

THIS SPACE RESERVED FOR THE COUNTY CLERK'S SIGNATURE

I, Christine Walker, County Clerk for Jackson County, Oregon, certify that the instrument identified herein was recorded in the Clerk records.
Christine Walker - County Clerk

STATUTORY WARRANTY DEED

Franklin Bruce Bateman and Anna W. Bateman, as tenants by the entirety, Grantor, conveys and warrants to Jeff Fowler and Alyson Fowler as tenants by the entirety, Grantee, the following described real property free of liens and encumbrances, except as specifically set forth herein:

LEGAL DESCRIPTION: Real property in the County of Jackson, State of Oregon, described as follows:

PARCEL 3, AS SHOWN ON THE PARTITION PLAT FILED IN THE OFFICE OF THE JACKSON COUNTY OREGON SURVEYOR AS NO. 19560, AND RECORDED AS PARTITION PLAT NO. P-18-2007 OF "RECORD OF PARTITION PLATS" IN JACKSON COUNTY, OREGON.

Subject to:

1. Covenants, conditions, restrictions and/or easements, if any, affecting title, which may appear in the public record, including those shown on any recorded plat or survey.

The true consideration for this conveyance is **\$400,000.00**. (Here comply with requirements of ORS 93.030)

CITY OF MEDFORD
EXHIBIT # J 2 of 3
FILE # AC-18-138 / E-18-139

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Project Name:

Pilot Rock Excavation

Map/Taxlot:

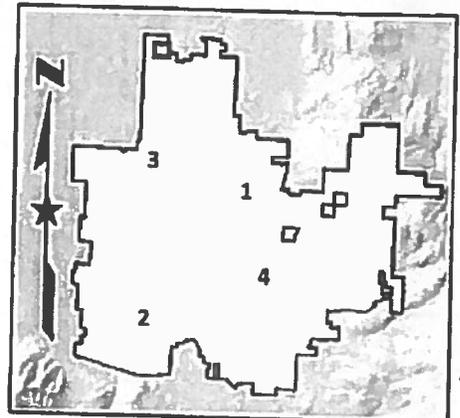
362W36D TL 142



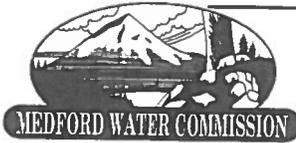
10/05/2018

Legend

-  Subject Area
-  Zoning Districts
-  Tax Lots



J
3 of 3



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-18-138/E-18-139

PARCEL ID: 371W30AC TL 2500

PROJECT: Community Consideration of the construction of a steel building for a shop and office approximately 6,300 square feet with an exception to the General Design Requirements for Parking., specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved in size on a 4.0 acre parcel located at 356 Bateman Road approximately 450 feet west of Bierson Way zoned I-G (General Industrial) (362W36D TL 142)

DATE: October 31, 2018

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. The installation of a water meter is required. The applicant shall coordinate with Medford Water Commission engineering staff for approved location, and payment for SDC's and installation of said water meter.
4. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

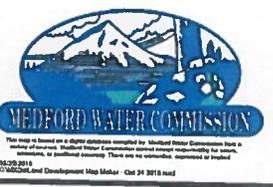
COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is not required.
3. MWC-metered water service does not exist to this property. (See Condition 3 above)
4. Access to MWC water lines is available. There's an existing 10-inch water line along the north side of Bateman Road.



Water Facility Map
City of Medford
Planning Application:
AC-18-138/E-18-139
October 31, 2018

- Legend**
- ⊙ Air Valve
 - ⊙ Sample Station
 - ⊙ Fire Service
 - ⊙ Hydrant
 - ▲ Reducer
 - ⊙ Blow Off
 - ⊙ Plugs-Caps
- Water Meters:**
- ⊙ Active Meter
 - ⊙ On Well
 - ⊙ Unknown
 - ⊙ Vacant
- Water Valves:**
- ⊙ Butterfly Valve
 - ⊙ Gate Valve
 - ⊙ Tapping Valve
- Water Mains:**
- Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line
- Boundaries:**
- ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots
- MWC Facilities:**
- C** Control Station
 - P** Pump Station
 - R** Reservoir



11/11
2018



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel: (541) 664-6300, Fax: (541) 664-7171 www.RVSS.us

October 23, 2018

City of Medford Planning Department
200 S. Ivy Street
Medford, Oregon 97501

Re: AC-18-0138/E-18-139, Pilot Rock Excavation (382W01A – 147)
Ref: PA-18-050

ATTN: Liz,

The subject property is within the RVSS service area. There are 8 inch sewer main along Bateman Drive. Sewer service for the proposed development can be had by tapping the main as shown on the preliminary site plan. Tap permits are issued by RVSS and the sewer connection permit will be issued by the City of Medford. There will be system development charges owed to Rogue Valley Sewer Services.

Rogue Valley Sewer Services requests that approval of the application be subject to the following conditions:

1. All sewer design and construction must be performed in accordance with RVSS standards.
2. The applicant must submit architectural plumbing plans for the calculation of system development charges.
3. The applicant must pay sewer system development charges to Rogue Valley Sewer Service prior to issuance of building permits.

Please feel free contact me with any questions.

Sincerely,

Nicholas R. Bakke

Nicholas R. Bakke, P.E.
District Engineer

K:\DATA\AGENCIES\MEDFORD\PLANNING\ARCH COMM\2018\AC-18-138_E-18-139_PILOT ROCK EXCAVATION.DOC

CITY OF MEDFORD
EXHIBIT # L
FILE # AC-18-138 / E-18-139



Oregon

Kate Brown, Governor

Oregon Department of Aviation

3040 25th Street SE
Salem, OR 97302-1125
Office: 503-378-4880
Fax: 503-373-1688



October 22, 2018

Elizabeth Conner
Planner
City of Medford
200 South Ivy St, Lausmann Annex Rm 240
Medford, OR 97501

Re: File No. AC-18-138/E-18-139 Pilot Rock Excavation

Dear Mr. Roennfeldt:

The Oregon Department of Aviation (ODA) appreciates the opportunity to review and comment on the proposed Pilot Rock Excavation building construction located at 356 Bateman Drive near Bierson Way (362W36D TL142).

The Oregon Department of Aviation would like to submit the following comments:

- Prior to issuance of a building permit the applicant must file and receive a determination from the Oregon Department of Aviation as required by OAR 738-070-0060 on FAA Form 7460-1 Notice of Proposed Construction or Alteration to determine if the structure will pose a hazard to aviation safety. A subsequent submittal may be required by the FAA due to its location to the Troutdale Airport.
- The height of the new structure should not penetrate FAA Part 77 Imaginary Surfaces, as determined by ODA and the FAA.
- Shields on any external lights should be designed as to not interfere with aircraft or airport operations.
- Marking Lights, per FAA design, may be needed to identify to structures.
- Coordination with the Rogue Valley Int'l Airport and the Medford Air Traffic Control tower may be needed to issue a NOTAM during the construction.

ODA appreciates the opportunity to comment on this application. The Department requests to be identified as a party of record for any future land use application.

If you have any questions or need further information or clarification on the comments, please feel free to contact me at 503-378-2529 or Jeff.Caines@aviation.state.or.us.

Sincerely,

Jeff Caines, AICP
Aviation Planner
Oregon Department of Aviation

*Oregon Department of Aviation aims to provide infrastructure, financial resources,
and expertise to ensure a safe and efficient air transport system*

CITY OF MEDFORD
EXHIBIT # M
FILE # AC-18-138 / E-18-139

Liz A. Conner

From: Amber Judd <JuddAJ@jacksoncounty.org>
Sent: Tuesday, October 23, 2018 7:02 PM
To: Liz A. Conner
Subject: File No. AC-18-138/E-18-139 Project Name: Pilot Rock Excavation

Elizabeth,

The Airport requests an Avigation, Noise and Hazard Easement be a requirement of this project. In addition, due to the proximity to the Airport, the applicant needs to contact the FAA regarding filing a 7460-1 Notice of Proposed Construction or Alteration. The FAA contact is: Paul Holmquist, phone (206) 231-2990.

I have inserted some information below from the FAA's website:

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference CFR Title 14 Part 77.9.

You must file with the FAA at least 45 days prior to construction if:

- Your structure will exceed 200 ft above ground level
- Your structure will be in proximity to an airport and will exceed the slope ratio
- Your structure involves construction of a traverseway (i.e. highway, railroad, waterway, etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- Your structure will emit frequencies, and does not meet the conditions of the FAA Co-location Policy
- Your structure will be in an instrument approach area and might exceed part 77 Subpart C
- Your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- Your structure will be on an airport or heliport
- Filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the Air Traffic Areas of Responsibility map for Off Airport construction, or contact the FAA Airport Region/District Office for On Airport construction.

Results

You exceed the following Notice Criteria:

Your proposed structure exceeds an instrument approach area by approximately 24 feet and aeronautical study is needed to determine if it will exceed a standard of subpart C of 14CFR Part 77. The FAA, in accordance with 77.9, requests that you file.

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

The FAA requests that you file.

Thank you,

Amber Judd

Deputy Director-Administration
Rogue Valley International-Medford Airport (MFR)



1000 Terminal Loop Parkway, Suite 201
Medford, Oregon 97504
541-776-7222

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JACKSON COUNTY

Roads

Roads
Engineering

Chuck DeJanvier
Construction Engineer

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
dejanvca@jacksoncounty.org

www.jacksoncounty.org

October 19, 2018

Attention: Liz Conner
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Shop/Yard/Office off Bateman Drive – a county maintained road.
Planning File: AC-18-138/E-18-139

Dear Liz:

Thank you for the opportunity to comment on the consideration of construction of steel buildings for a shop and office approximately 6,300 square feet with an exception to the General Design Requirement for Parling, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved on a 4.0 acre parcel located at 356 Bateman Drive approximately 450 feet west of Beirson Way the General Industrial (I-G) zoning district, (36-2W-36D Tax Lot 142). Jackson County Roads has the following comments:

1. If frontage improvements are required off Bateman Drive, they shall be permitted and inspected by the City of Medford.
2. Any new or improved road approaches off Bateman Drive shall be permitted and inspected by the City of Medford.
3. The applicant shall submit construction plans to Jackson County Roads, so we may determine if county permits will be required.
4. The County portion of Bateman Drive is a County Local Road and is maintained by the County. The Average Daily Traffic count was 1,389 on July 19, 2016, 50 feet east of Table Rock Road. As a comparison of capacity for Bateman Drive, the capacity of a two lane suburban road with ten foot lanes and no shoulders is 8,832 ADT.
5. Jackson County's General Administration Policy #1-45 sets forth the County's position as it relates to the management of County roads located within existing or proposed city limits or Urban Growth Boundaries (UGB). The County has no current plans for

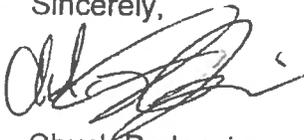
CITY OF MEDFORD
EXHIBIT # 01 of 2
FILE # AC-18-138 / E-18-139

improvements to Bateman Drive Road. Jackson County Roads recommends that the city request jurisdiction of this road.

6. Storm water should meet City of Medford requirements that also include water quality.
7. Jackson County Roads would like to review and comment on the hydraulic report including the calculations and drainage plan. Capacity improvements or on site detention, if necessary, shall be installed at the expense of the applicant. Upon completion of the project, the developer's engineer shall certify that construction of the drainage system was constructed per plan and a copy of the certification shall be sent to Jackson County Roads.
8. We require that the applicant prepare a traffic study that addresses impacts at the intersection of Bateman Drive and Table Rock Road. The study should recommend mitigation if necessary and be reviewed and approved by Jackson County. If mitigations are recommended, they shall be required.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Chuck DeJanvier
Construction Engineer

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2 of 2

SOUTHERN OREGON TRANSPORTATION ENGINEERING, LLC

319 Eastwood Drive - Medford, Or. 97504 – Phone (541) 608-9923 – Email: Kim.parducci@gmail.com

December 24, 2018

Mike Kuntz, County Engineer
Jackson County Roads Department
200 Antelope Road
White City, Oregon 97502

RECEIVED

DEC 24 2018

Planning Dept.

RE: Pilot Rock Excavation Office/Shop Development – Bateman Drive Intersection Analysis

Dear Mike,

Southern Oregon Transportation Engineering, LLC evaluated a proposed office and shop development for Pilot Rock Excavation, Inc., located on the south side of Bateman Drive approximately 450 feet west of Beirson Way on 362W36D tax lot 142 in Medford, Oregon. An analysis was required by Jackson County to evaluate development impacts at the intersection of Bateman Drive and Table Rock Road.

Background

The subject property is currently vacant. Proposed development includes an office and equipment repair shop for Pilot Rock Excavation, Inc. Access will be taken from Bateman Drive, which connects to Table Rock Road to the west with no other outlet at this time.

Bateman Drive is a County local road. It was shown in 2016 to carry 1,389 average daily trips (ADT) with a reported capacity of 8,832 ADT as a two lane suburban road with ten foot lanes and no shoulders.

Table Rock Road is classified as an urban minor arterial in the Jackson County Transportation System Plan (TSP). It's an improved five lane facility with curb, gutter, and sidewalk at Bateman Drive, and has a posted speed of 45 miles per hour.

Bateman Drive – looking west



Bateman Drive – looking east



Table Rock Road – looking south

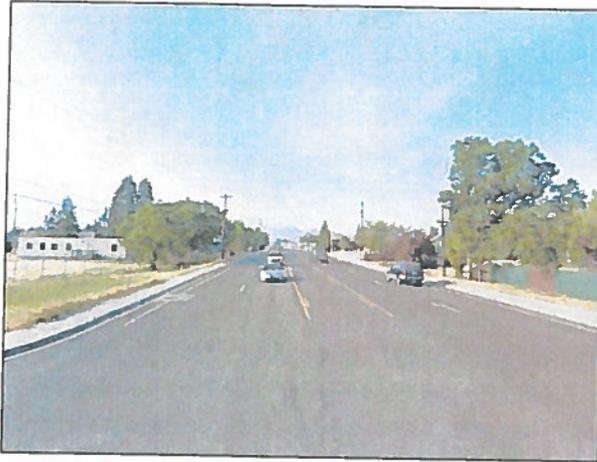
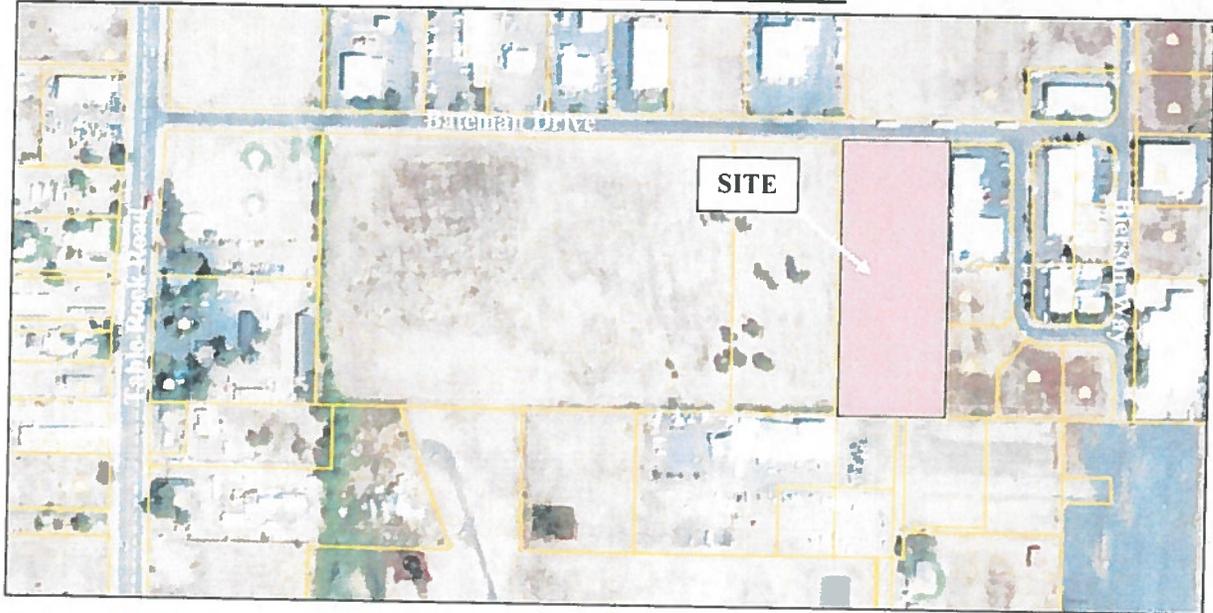


Table Rock Road – looking north



Proposed Pilot Rock Excavation Site



Traffic Data Development

Traffic counts were gathered in November of 2018 on Table Rock Road at Bateman Drive during the p.m. peak period. A seasonal adjustment was determined from the 2017 ODOT Seasonal Trend Table and applied to raw count data to develop year 2018 no-build volumes. Daily traffic volumes on Table Rock Road and Bateman Drive were gathered from Jackson County ADT web reports. Growth in the area was determined from comparing base year 2015 and future year 2038 traffic volumes at intersections near Bateman Drive in the Jackson County TSP. Refer to the attachments for manual traffic counts and volume development diagrams.

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Year 2018 No-Build Intersection Operations

Year 2018 no-build conditions were evaluated at the intersection of Bateman Drive and Table Rock Road during the p.m. peak hour to evaluate existing conditions. Results are summarized in Table 1.

Table 1 – Year 2018 No-Build Intersection Operations, PM Peak Hour

Intersection	Jurisdiction	Performance Standard	Traffic Control	Year 2018 No-Build PM Peak Hour
Bateman Drive / Table Rock Road	Jackson County	V/C 0.95	TWSC	V/C 0.39 WBL

V/C = volume-to-capacity ratio, TWSC = two-way stop controlled, WBL = westbound left
 Note: Exceeded performance standards are shown in bold, italic

Results of the analysis show the intersection of Bateman Drive and Table Rock Road operates within performance standards under existing conditions during the p.m. peak hour. The westbound left turn movement is shown to be the critical movement at the intersection, operating at a level of service (LOS) "C" with a v/c ratio of 0.39. Synchro output sheets are provided in the attachments.

Year 2018 No-Build Queuing and Blocking

Queuing is the stacking up of vehicles for a given lane movement, and it can have a significant effect on roadway safety and the overall operation of a transportation system. Long queue lengths in through lanes can block access to turn lanes, driveways, and minor street approaches, as well as spill back into upstream intersections. As a result of this, the estimation of queue lengths is an important aspect of the analysis process for determining how a study area operates.

Queue lengths are reported as the average, maximum, or 95th percentile queue length. The 95th percentile queue length is used for design purposes and is the queue length reported in this analysis. Five simulations were run and averaged in SimTraffic to determine 95th percentile queue lengths. Queue lengths were rounded up to the nearest 25 feet (single vehicle length) and reported in Table 2 during the a.m. and p.m. peak hours.

Table 2 – Year 2018 No-Build 95th Percentile Queue Lengths, PM Peak

Intersection Movement	Available Link Distance (feet)	95 th Percentile Queue Length (feet) PM	Exceeded Roadway
Table Rock Rd / Bateman Dr			
Southbound Left	250	50	None
Westbound Left / Right	2400	225	None

Note: Exceeded queue lengths are shown in bold, italic

Results of the queuing analysis show queue lengths stay within available link distances under year 2018 no-build conditions during the p.m. peak hour. The westbound queue length is fairly long at times during the peak hour, which reflects the difficulty for westbound turning movements. During a stop sign delay count, the maximum number of queued vehicles at any one time on Bateman Drive during the p.m. peak hour was eight vehicles. The average wait time per vehicle was 25.63 seconds, which is considered a level of service C and is consistent with synchro output. The maximum stopped time was 75 seconds. A full queuing and blocking report is provided in the attachments.

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Crash History

Crash data for the most recent five-year period was gathered from ODOT's online crash database. Results were gathered for the period of January 1, 2012 through December 31st, 2016. Crash data is gathered to identify crash patterns that could be attributable to geometric or operational deficiencies, or crash trends of a specific type that would indicate the need for further investigation. The crash rate at each intersection is also compared to a critical crash rate provided in the Highway Safety Manual (HSM). Intersections that exceed their respective critical crash rate are flagged for further review. Tables 3 and 4 provide intersection crash rates and types of collisions at study area intersections. Crash data is provided in the attachments.

Table 3 - Study Area Intersection Crash Rates, 2012-2016

Intersection	2012	2013	2014	2015	2016	Total Crashes	AADT	Crash Rate	Critical Crash Rate
Bateman Dr / Table Rock Rd	0	1	1	0	1	3	22,500	0.07	0.408

Table 4 - Crash History by Type, 2012-2016

Intersection	Collision Type					Severity		
	Rear-End	Turning /Angle	Fixed Object	Other	Ped/Bike	Non-Injury	Injury	Fatal
Bateman Dr / Table Rock Rd	0	2	1	0	0	2	1	0

There were three reported collisions at the intersection of Table Rock Road and Bateman Drive within a five-year period. Two of the three crashes were turning collisions and the remaining crash a single car collision with a fixed object. One resulted in injury, but it was not severe injury (injury C). The remaining two resulted in property damage only. There were no pedestrian or bicyclist related collisions.

The intersection crash rate was less than the HSM critical crash rate, which is used as a measure to determine whether further investigation should be taken. The number of collisions within a five-year period is not considered excessive, and there are no crash patterns that are attributable to geometric or operational deficiencies. This location is also not listed as a top 5% Statewide Priority Index System (SPIS) site. No further investigation is shown to be necessary.

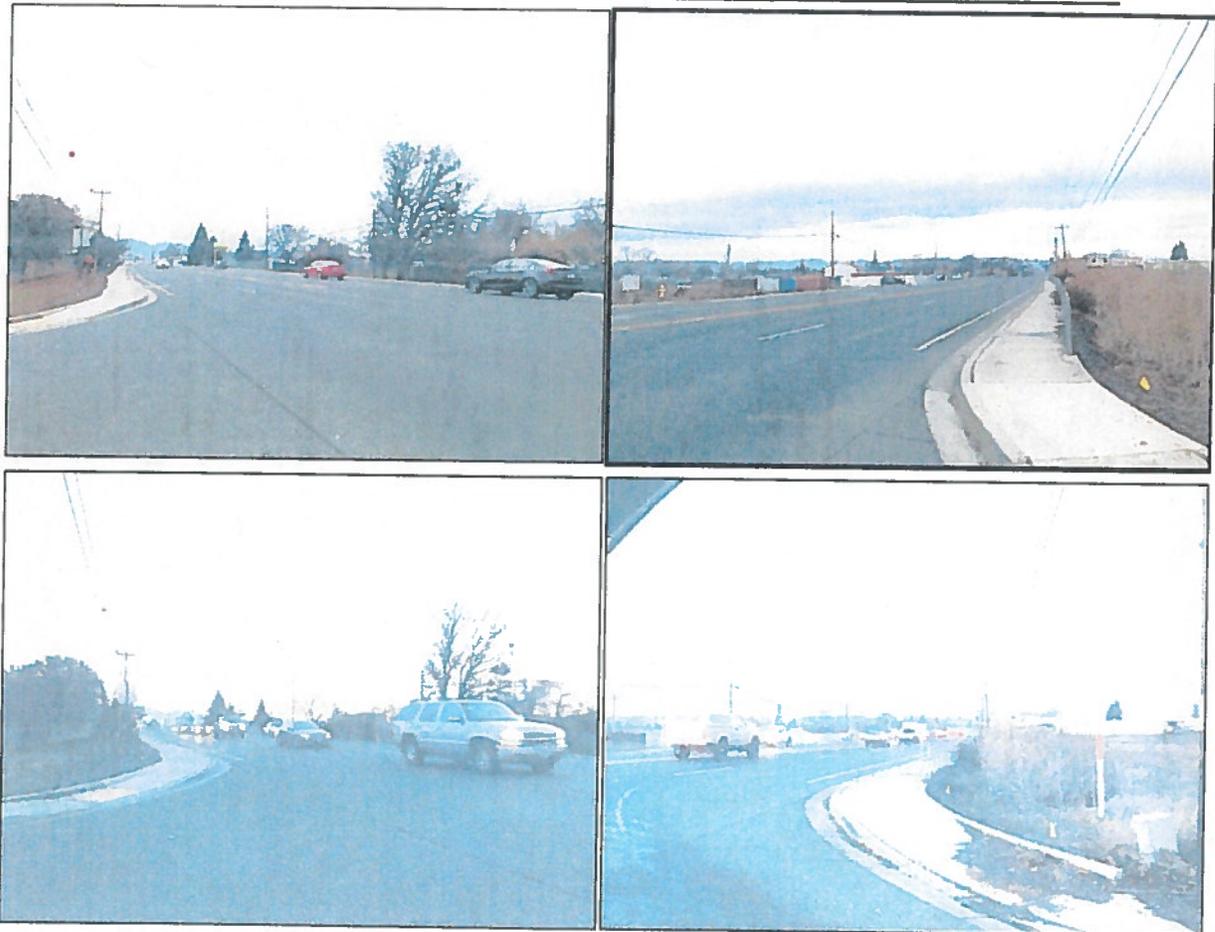
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Sight Distance

Access to Bateman Drive is provided from Table Rock Road. Sight distance in the field was evaluated from Bateman and shown to be adequate in both directions. Table Rock Road is a five lane facility at this intersection, and is fairly straight and flat with only slight horizontal curves to the north and south several hundred feet out. When traffic is less congested and free-flowing, sight distance is over 800 feet in both directions. During peak conditions, sight distance is restricted to the north based on the length of northbound queue from the signalized intersection of Wilson Road / Table Rock Road that blocks southbound traffic. When this occurs, it becomes more difficult to make the westbound left turn movement. Observing this in the field, it appears to be temporary and only lasts until the northbound queue releases. Visuals of both free-flow and more congested conditions are shown below.

Looking south from Bateman Drive

Looking north from Bateman Drive



The minimum stopping sight distance (SSD) recommended by American Association of State Highways and Transportation Officials (AASHTO) for a facility with a posted speed of 45 miles per hour is 360 feet. The desirable intersection sight distance (ISD) is 500 feet. Field measurements showed sight distance being greater than 800 feet in both directions, with the exception of times when the northbound queue backed up from Wilson Road. During these times, westbound left turning drivers wait for a gap in traffic from the south to pull out into a center refuge lane on Table Rock Road before pulling into southbound traffic (referred to as utilizing a two-stage gap process). Sight distance is, therefore, concluded to be adequate.

Design Year 2019 No-Build Intersection Operations

No-build conditions represent future conditions for a study area without consideration of proposed development trips. This condition is evaluated to determine how a study area will operate with background growth, but no traffic from proposed development. No-build conditions are evaluated in this analysis for the projected design year 2019. Growth, as stated previously, was determined to occur in accordance with the Jackson County TSP.

Design year 2019 no-build traffic volumes were evaluated at the intersection of Bateman Drive and Table Rock Road during the p.m. peak hour. Results are summarized in Table 5.

Table 5 – Design Year 2019 No-Build Intersection Operations, PM Peak Hour

Intersection	Jurisdiction	Performance Standard	Traffic Control	Design Year 2019 No-Build PM Peak Hour
Bateman Drive / Table Rock Road	Jackson County	V/C 0.95	TWSC	V/C 0.40 WBL

V/C = volume-to-capacity ratio, TWSC = two-way stop controlled, WBL = westbound left
 Note: Exceeded performance standards are shown in bold, italic

Results of the analysis show the intersection of Bateman Drive and Table Rock Road continues to operate acceptably under design year 2019 no-build conditions during the p.m. peak hour. A slight increase in the westbound v/c ratio occurs as a result of background growth. Synchro output sheets are provided in the attachments.

Design Year 2019 No-Build Queuing and Blocking

Five simulations were run and averaged in SimTraffic to determine 95th percentile queue lengths under design year 2019 no-build conditions. Queue lengths were rounded up to the nearest 25 feet (single vehicle length) and reported in Table 6 during the p.m. peak hour.

Table 6 – Design Year 2019 No-Build 95th Percentile Queue Lengths, PM Peak

Intersection Movement	Available Link Distance (feet)	95 th Percentile Queue Length (feet) PM	Exceeded Roadway
Table Rock Rd / Bateman Dr			
Southbound Left	250	50	None
Westbound Left / Right	2400	250	None

Note: Exceeded queue lengths are shown in bold, italic

Results of the queuing analysis show queue lengths continue to stay within available link distances under design year 2019 no-build conditions during the p.m. peak hour. The westbound queue length increases by 25 feet or the equivalent of one passenger car as a result of background growth. A full queuing and blocking report is provided in the attachments.

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Trip Generation

Trip generation calculations for Pilot Rock Excavation, Inc. were based on the Institute of Transportation Engineers (ITE) *Trip Generation*, 10th Edition. ITE rates were used for land use codes 712 – Small Office Building and 110 – General Light Industrial. The proposed 6,300 square foot (SF) building is broken up into a 1,500 SF office and 4,800 SF repair/shop for Pilot Rock Excavation equipment. All trips were considered new trips to the transportation system. A summary is provided in Table 7.

Table 7 – Development Trip Generations						
Land Use	Unit	Size	Daily Trips	PM Peak Hour		
				Total	In	Out
Pilot Rock Excavation						
712 – Single Office Building	1000 SF	1.5	24	4	1	3
110 – General Light Industrial	1000 SF	4.8	24	3	0	3
Total Trips			48	7	1	6

Trip Distribution and Assignment

Development trips were distributed to the transportation system based on existing traffic splits from manual traffic counts taken on Bateman Drive at Table Rock Road. Existing counts showed a little more than half of entering trips come from the south (57% from south, 43% from north), and a majority of exiting trips go to the south (72% to south, 28% to north). See below.

Existing Trip Splits at Table Rock Road / Bateman Drive, PM Peak Hour

				Southbound							
						43%	%				
				R ←	T ↓	L ↘	Ped				
				2	791	19	0				
Eastbound	%	Ped	0	Table Rock Rd / Bateman Dr				27	↑ R	28%	Westbound
		L ↗	3					0	← T	0%	
	0%	T →	0					69	↓ L	72%	
		R ↘	3					0	Ped	%	
				0	3	647	25				
				Ped	L ←	T ↑	R →				
				%			57%				
				Northbound							

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Design Year 2019 Build Conditions

Design year build conditions represent no-build conditions with the addition of proposed development trips. Proposed development trips in this analysis represent an office and shop development for Pilot Rock Excavation, Inc., estimated for completion by the design year 2019.

Design Year 2019 Build Intersection Operations

Design year 2019 build conditions were evaluated at the intersection of Bateman Drive and Table Rock Road during the p.m. peak hour. Results are summarized in Table 8.

Table 8 – Design Year 2019 Build Intersection Operations, PM Peak Hour

Intersection	Jurisdiction	Performance Standard	Traffic Control	Design Year 2019 Build PM Peak Hour
Bateman Drive / Table Rock Road	Jackson County	V/C 0.95	TWSC	V/C 0.42 WBL

V/C = volume-to-capacity ratio, TWSC = two-way stop controlled, WBL = westbound left
 Note: Exceeded performance standards are shown in bold, italic

Results of the analysis show the intersection of Bateman Drive and Table Rock Road continues to operate acceptably under design year 2019 build conditions during the p.m. peak hour. The westbound left turn movement v/c ratio shows another slight increase as a result of development traffic. Synchro output sheets are provided in the attachments.

Design Year 2019 No-Build Queuing and Blocking

Five simulations were run and averaged in SimTraffic to determine 95th percentile queue lengths under design year 2019 build conditions. Queue lengths were rounded up to the nearest 25 feet (single vehicle length) and reported in Table 9 during the p.m. peak hour.

Table 9 – Design Year 2019 Build 95th Percentile Queue Lengths, PM Peak

Intersection Movement	Available Link Distance (feet)	95 th Percentile Queue Length (feet) PM	Exceeded Roadway
Table Rock Rd / Bateman Dr			
Southbound Left	250	50	None
Westbound Left / Right	2400	275	None

Note: Exceeded queue lengths are shown in bold, italic

Results of the queuing analysis show queue lengths continue to stay within available link distances under design year 2019 build conditions during the p.m. peak hour. The westbound queue length increases by approximately 25 feet or the equivalent of one passenger car. A full queuing and blocking report is provided in the attachments.

Turn Lane Criterion

Criterion for a northbound right turn lane was evaluated under design year 2019 build conditions to determine whether a turn lane is necessary as a result of proposed development. Results showed

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criterion is not met during the p.m. peak hour. A turn graph is provided in the attachments for further reference.

Street Capacity

Bateman Drive is not shown to have capacity constraints at this time, but the intersection of Bateman Drive and Table Rock Road will fail operationally at some point in the future well before Bateman Drive exceeds its reported design capacity. In the event that this occurs, a traffic signal is not a viable option due to the close proximity of the Wilson Road / Table Rock Road traffic signal, which is only 600 feet to the north. As development continues to occur on Bateman Drive, a better solution will be to have an alternate outlet to relieve traffic at the intersection of Bateman Drive and Table Rock Road. Potential connections could occur through extensions of Bierson Way to the north to Judge Lane or to the south to Vilas Road. The County and City should encourage such connections if the opportunity arises.

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Conclusions

The findings of the traffic analysis conclude that the proposed Pilot Rock Excavation development can be accommodated on the transportation system without creating adverse impacts to the transportation system. The traffic analysis evaluated intersection operations, sight distance, crash history, and turn lane criterion at the intersection of Bateman Drive and Table Rock Road. Results of the analysis are summarized below.

- Intersection operations are shown to be within performance standards under existing and design year no-build and build conditions during the p.m. peak hour.
- 95th percentile queue lengths are shown to stay within available link distances.
- Criterion for a right turn lane on Table Rock Road at Bateman Drive was evaluated and shown not to be met as a result of proposed development during the p.m. peak hour.
- No trend or pattern of crashes was identified at the intersection of Bateman Drive and Table Rock Road that would indicate there is a safety concern.
- Sight distance is shown to be adequate in both directions from Bateman Drive at Table Rock Road.

It is noted that the intersection of Bateman Drive and Table Rock Road will fail operationally prior to Bateman Drive reaching its reported design capacity. The County and City are encouraged to pursue additional street connections through Bierson Way extensions to the north or south if the opportunity arises. This will help alleviate traffic on Bateman Drive as future development occurs.

Please feel free to contact me if you have any questions or concerns regarding this analysis.

Sincerely,



Kimberly Parducci PE, PTOE
SOUTHERN OREGON TRANSPORTATION ENGINEERING, LLC

Attachments: Site Plan
Manual Count Data
Crash Data
Traffic Volume Development
Synchro Output/SimTraffic Output
Turn Lane Graphs
Agency Requirements



Cc: City of Medford Public Works/Planning Departments
Client

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ATTACHMENTS

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SOUTHERNOREGON TRANSPORTATION ENGINEERING

Medford, Oregon 97504 | Kim.parducci@gmail.com | (541) 941-4148 cell

North-South: Table Rock Rd
 East-West: Bateman Dr
 Weather: Sunny, 55 deg
 Veh Type: All Vehicles

File Name : TR-Bateman
 Site Code : 00000001
 Start Date : 11/28/2018
 Page No : 1

Groups Printed- Unshifted

Start Time	Table Rock From North					Bateman Dr From East					Table Rock From South					Driveway From West					Int. Total
	Left	Thru	Right	Peds	App Total	Left	Thru	Right	Peds	App Total	Left	Thru	Right	Peds	App Total	Left	Thru	Right	Peds	App Total	
03:00 PM	1	145	0	0	146	14	0	10	0	24	2	165	12	0	179	0	0	2	0	2	351
03:15 PM	2	155	0	1	158	14	0	3	0	17	0	125	3	0	128	1	0	0	0	1	304
03:30 PM	1	181	0	0	182	21	0	10	0	31	1	156	7	0	164	0	0	0	0	0	377
03:45 PM	4	200	0	0	204	20	0	4	0	24	2	155	8	0	165	2	0	1	0	3	396
Total	8	681	0	1	690	69	0	27	0	96	5	601	30	0	636	3	0	3	0	6	1428
04:00 PM	3	185	1	0	189	18	0	6	0	24	0	164	5	0	169	1	0	1	0	2	384
04:15 PM	12	177	0	0	189	17	0	6	0	23	0	160	5	0	165	0	0	0	0	0	377
04:30 PM	0	229	1	0	230	14	0	11	0	25	1	168	7	0	176	0	0	1	0	1	432
04:45 PM	5	150	0	0	155	10	0	4	0	14	1	150	5	0	156	0	0	0	0	0	325
Total	20	741	2	0	763	59	0	27	0	86	2	642	22	0	666	1	0	2	0	3	1518
05:00 PM	0	177	0	0	177	21	0	8	0	29	0	170	4	1	175	1	0	1	0	2	383
05:15 PM	0	166	0	0	166	5	0	7	0	12	0	160	5	0	165	0	0	0	0	0	343
05:30 PM	1	131	0	0	132	10	0	5	0	15	0	137	3	0	140	0	0	0	0	0	287
05:45 PM	0	112	0	0	112	4	0	3	0	7	0	114	2	0	116	0	0	0	0	0	235
Total	1	586	0	0	587	40	0	23	0	63	0	581	14	1	596	1	0	1	0	2	1248
Grand Total	29	2008	2	1	2040	168	0	77	0	245	7	1824	66	1	1898	5	0	6	0	11	4194
Apprch %	1.4	98.4	0.1	0		68.6	0	31.4	0		0.4	96.1	3.5	0.1		45.5	0	54.5	0		
Total %	0.7	47.9	0	0	48.6	4	0	1.8	0	5.8	0.2	43.5	1.6	0	45.3	0.1	0	0.1	0	0.3	

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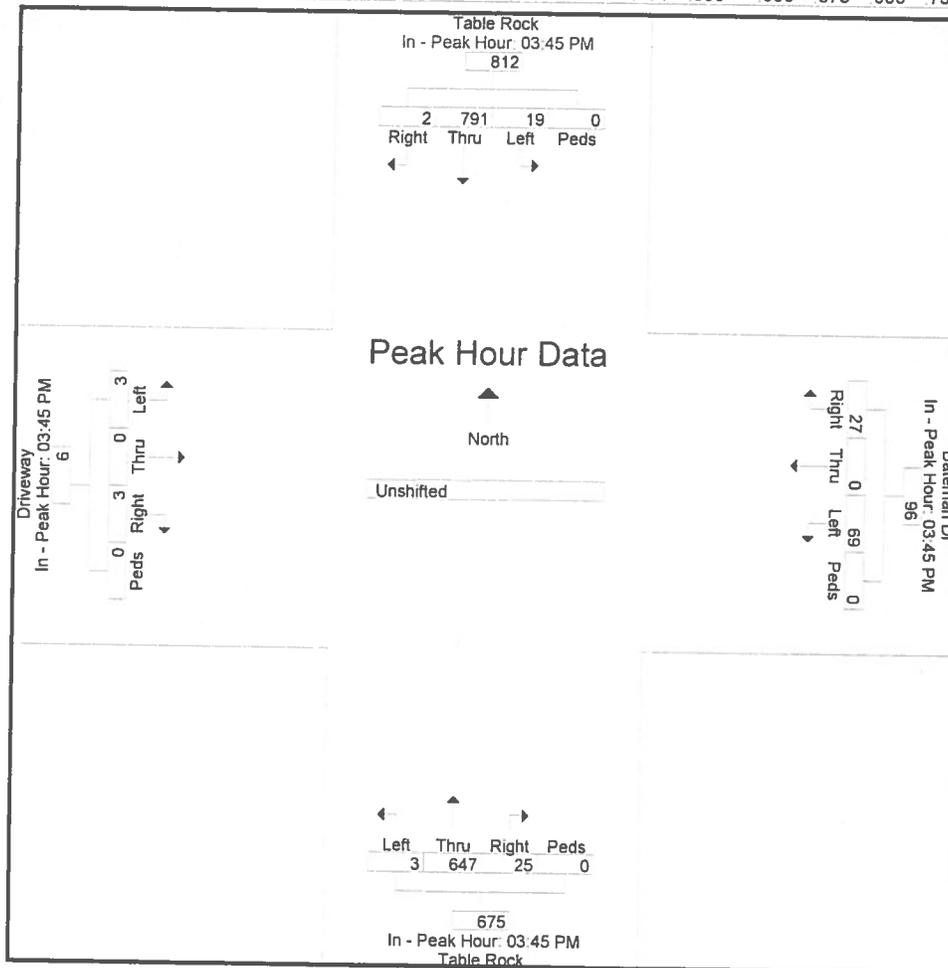
SOUTHERN OREGON TRANSPORTATION ENGINEERING

Medford, Oregon 97504 | Kim.parducci@gmail.com | (541) 941-4148 cell

North-South: Table Rock Rd
 East-West: Bateman Dr
 Weather: Sunny, 55 deg
 Veh Type: All Vehicles

File Name : TR-Bateman
 Site Code : 00000001
 Start Date : 11/28/2018
 Page No : 2

Start Time	Table Rock From North					Bateman Dr From East					Table Rock From South					Driveway From West					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 03:45 PM to 04:30 PM - Peak 1 of 1																					
Peak Hour for Each Approach Begins at:																					
	03:45 PM					03:45 PM					03:45 PM					03:45 PM					
+0 mins.	4	200	0	0	204	20	0	4	0	24	2	155	8	0	165	2	0	1	0	3	
+15 mins.	3	185	1	0	189	18	0	6	0	24	0	164	5	0	169	1	0	1	0	2	
+30 mins.	12	177	0	0	189	17	0	6	0	23	0	160	5	0	165	0	0	0	0	0	
+45 mins.	0	229	1	0	230	14	0	11	0	25	1	168	7	0	176	0	0	1	0	1	
Total Volume	19	791	2	0	812	69	0	27	0	96	3	647	25	0	675	3	0	3	0	6	
% App. Total	2.3	97.4	0.2	0		71.9	0	28.1	0		0.4	95.9	3.7	0		50	0	50	0		
PHF	396	864	500	000	883	863	000	614	000	960	375	963	781	000	959	375	000	750	000	500	



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SOUTHERNOREGON TRANSPORTATION ENGINEERING

Medford, Oregon 97504 | Kim.parducci@gmail.com | (541) 941-4148 cell

North-South: Table Rock Road
 East-West: Bateman Drive
 Weather: Sunny, 50 deg
 PM Peak Hour Stopped Delay

File Name : Bateman_Stop Delay_PM
 Site Code : 00000002
 Start Date : 11/29/2018
 Page No : 1

L n.	No.	Joined Queue	Released From Queue	Delay
1	1	3:40:01 PM	3:40:50 PM	49
1	2	3:41:17 PM	3:41:53 PM	36
1	3	3:41:27 PM	3:41:54 PM	27
1	4	3:42:11 PM	3:42:12 PM	1
1	5	3:43:30 PM	3:43:31 PM	1
1	6	3:45:28 PM	3:45:33 PM	5
1	7	3:45:37 PM	3:45:46 PM	9
1	8	3:46:25 PM	3:46:29 PM	4
1	9	3:47:01 PM	3:47:23 PM	22
1	10	3:47:25 PM	3:47:30 PM	5
1	11	3:47:28 PM	3:48:19 PM	51
1	12	3:48:09 PM	3:48:27 PM	18
1	13	3:48:22 PM	3:48:43 PM	21
1	14	3:48:33 PM	3:48:56 PM	23
1	15	3:48:43 PM	3:49:23 PM	40
1	16	3:49:06 PM	3:49:28 PM	22
1	17	3:49:36 PM	3:49:37 PM	1
1	18	3:49:41 PM	3:49:56 PM	15
1	19	3:50:11 PM	3:50:25 PM	14
1	20	3:50:14 PM	3:50:28 PM	14
1	21	3:50:23 PM	3:50:35 PM	12
1	22	3:50:37 PM	3:50:44 PM	7
1	23	3:50:40 PM	3:50:47 PM	7
1	24	3:53:06 PM	3:53:33 PM	27
1	25	3:53:44 PM	3:53:51 PM	7
1	26	3:54:13 PM	3:54:23 PM	10
1	27	3:55:27 PM	3:55:37 PM	10
1	28	3:55:36 PM	3:55:44 PM	8
1	29	3:56:16 PM	3:56:17 PM	1
1	30	3:57:22 PM	3:57:24 PM	2
1	31	3:57:35 PM	3:57:44 PM	9
1	32	3:57:41 PM	3:57:51 PM	10
1	33	3:59:02 PM	3:59:36 PM	34
1	34	4:01:34 PM	4:01:49 PM	15
1	35	4:01:47 PM	4:01:55 PM	8
1	36	4:02:30 PM	4:02:34 PM	4
1	37	4:03:48 PM	4:04:11 PM	23
1	38	4:08:04 PM	4:08:33 PM	29
1	39	4:09:38 PM	4:10:09 PM	31
1	40	4:09:58 PM	4:10:19 PM	21
1	41	4:10:01 PM	4:10:22 PM	21
1	42	4:12:16 PM	4:12:26 PM	10
1	43	4:15:09 PM	4:15:56 PM	47
1	44	4:15:21 PM	4:16:04 PM	43
1	45	4:15:58 PM	4:16:10 PM	12
1	46	4:17:04 PM	4:17:13 PM	9

"P"
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SOUTHERNOREGON TRANSPORTATION ENGINEERING

Medford, Oregon 97504 | Kim.parducci@gmail.com | (541) 941-4148 cell

North-South: Table Rock Road
East-West: Bateman Drive
Weather: Sunny, 50 deg
PM Peak Hour Stopped Delay

File Name : Bateman_Stop Delay_PM
Site Code : 00000002
Start Date : 11/29/2018
Page No : 2

L	No.	Joined Queue	Released From Queue	Delay
1	47	4:18:17 PM	4:18:20 PM	3
1	48	4:18:37 PM	4:18:39 PM	2
1	49	4:19:20 PM	4:19:34 PM	14
1	50	4:19:48 PM	4:19:50 PM	2
1	51	4:19:56 PM	4:20:33 PM	37
1	52	4:21:32 PM	4:22:02 PM	30
1	53	4:24:06 PM	4:24:08 PM	2
1	54	4:25:39 PM	4:26:03 PM	24
1	55	4:26:07 PM	4:26:12 PM	5
1	56	4:26:08 PM	4:27:00 PM	52
1	57	4:26:18 PM	4:27:06 PM	48
1	58	4:26:21 PM	4:27:10 PM	49
1	59	4:26:34 PM	4:27:36 PM	62
1	60	4:26:51 PM	4:27:40 PM	49
1	61	4:27:05 PM	4:27:43 PM	38
1	62	4:27:05 PM	4:27:49 PM	44
1	63	4:27:12 PM	4:27:59 PM	47
1	64	4:27:25 PM	4:28:40 PM	75
1	65	4:27:31 PM	4:28:44 PM	73
1	66	4:27:53 PM	4:28:47 PM	54
1	67	4:28:42 PM	4:28:59 PM	17
1	68	4:29:14 PM	4:29:16 PM	2
1	69	4:30:24 PM	4:30:41 PM	17
1	70	4:31:48 PM	4:31:58 PM	10
1	71	4:31:51 PM	4:31:59 PM	8
1	72	4:31:53 PM	4:32:20 PM	27
1	73	4:32:12 PM	4:32:58 PM	46
1	74	4:32:30 PM	4:33:35 PM	65
1	75	4:32:39 PM	4:33:37 PM	58
1	76	4:32:43 PM	4:33:45 PM	62
1	77	4:32:46 PM	4:33:50 PM	64
1	78	4:32:52 PM	4:33:52 PM	60
1	79	4:33:01 PM	4:33:57 PM	56
1	80	4:33:30 PM	4:34:08 PM	38
1	81	4:33:34 PM	4:34:11 PM	37
1	82	4:33:49 PM	4:34:14 PM	25
1	83	4:34:07 PM	4:34:17 PM	10
1	84	4:34:35 PM	4:34:35 PM	0
1	85	4:34:58 PM	4:35:34 PM	36
1	86	4:35:55 PM	4:36:15 PM	20
1	87	4:36:09 PM	4:36:37 PM	28
1	88	4:36:36 PM	4:36:40 PM	4
1	89	4:38:02 PM	4:38:26 PM	24
1	90	4:39:20 PM	4:40:07 PM	47
1	91	4:39:45 PM	4:40:17 PM	32
1	92	4:39:59 PM	4:40:59 PM	60

*11p"
11/30*

**SOUTHERN OREGON
TRANSPORTATION ENGINEERING**

Medford, Oregon 97504 | Kim.parducci@gmail.com | (541) 941-4148 cell

North-South: Table Rock Road
East-West: Bateman Drive
Weather: Sunny, 50 deg
PM Peak Hour Stopped Delay

File Name : Bateman_Stop Delay_PM
Site Code : 00000002
Start Date : 11/29/2018
Page No : 3

Summary Information:

3:40:00 PM - 4:41:00 PM	WBLR
Total Vehicle Count:	92
Delayed Vehicle Count:	92
Through Vehicle Count:	0
Average Stopped Time:	25.63
Maximum Stopped Time:	75
Min. Secs. for Delay:	0
Average Queue:	0.64
Queue Density:	1.85
Maximum Queue:	8
Delay in Vehicle Hour:	0.64
Total Delay:	2358

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ADT Web Report Jackson County - November 2017

Road Name	MilePost	Location Description	Date	Count
1st ST	0.21	450 FT. WEST OF MILL CREEK DRIVE	08/15/17	835
Adams RD	0.02	100 FT. WEST OF COLVER ROAD	05/25/16	462
Agate RD	0.09	50 FT. SOUTH GREGORY ROAD	08/17/15	4610
	0.19	450 FT. NORTH OF GREGORY ROAD	07/05/17	1778
	1.75	900 FT. NORTH OF AVENUE "H"	07/05/17	4217
	3.13	50 FT. NORTH OF NICK YOUNG ROAD	07/05/17	2645
	3.99	50 FT. NORTH OF LINN ROAD	07/05/17	2650
	7.00	200 FT. NORTH OF MOUNTAIN VIEW DRIVE	08/08/17	1872
Alla Vista RD	0.98	200 FT. EAST OF BIGHAM-BROWN ROAD	08/22/17	3224
	2.00	450 FT. WEST OF RILEY ROAD	08/22/17	616
Anderson Creek RD	0.03	100 FT. WEST OF WAGNER CREEK ROAD	05/25/16	642
Antelope RD	0.09	450 FT. EAST OF TABLE ROCK ROAD	09/05/17	13997
	1.45	300 FT. WEST OF AGATE ROAD	08/29/17	14691
	1.53	100 FT. EAST OF AGATE ROAD	08/29/17	11000
	1.86	150 FT. EAST OF HIGHWAY 62	07/05/17	11916
	2.03	200 FT. EAST OF DIVISION ROAD	08/29/17	8289
	2.48	150 FT. EAST OF HALE WAY	07/05/17	6019
	3.39	175 FT. WEST OF ATLANTIC AVENUE	06/22/17	2420
	3.97	275 FT. WEST OF KERSHAW ROAD	06/22/17	2107
Antioch RD	4.07	275 FT. EAST OF KERSHAW ROAD	06/22/17	2760
	0.03	150 FT. NORTHWEST OF MODOC ROAD	07/05/17	2123
	2.78	300 FT. NORTH OF HIGHWAY 234	08/02/17	2224
	4.00	300 FT. NORTH OF DODGE ROAD	08/02/17	1561
Applegate RD	4.98	500 FT. NORTH OF BEAGLE ROAD	08/02/17	925
	0.20	1050 FT. SOUTH OF HIGHWAY 238	07/18/16	2698
	1.76	150 FT. SOUTH OF HAMILTON ROAD	07/18/16	2102
	2.91	550 FT. SOUTH OF LITTLE APPLGATE ROAD	07/18/16	1366
	9.13	300 FT. NORTH OF (SOUTH INTERSECTION) EASTSIDE RD	07/18/16	791
Arnold LN	0.04	200 FT. SOUTH OF WEST MAIN STREET	06/21/16	1287
	1.05	125 FT. SOUTH OF BELLINGER LANE	06/21/16	721
Atlantic AV	0.20	100 FT. SOUTH OF ANTELOPE ROAD	06/22/17	595
	0.31	100 FT. NORTH OF ANTELOPE ROAD	06/27/17	2083
	0.93	150 FT. SOUTH OF AVENUE "G"	06/27/17	1329
Avenue A	0.98	120 FT. NORTH OF AVENUE "G"	06/22/17	921
	0.05	250 FT. EAST OF HIGHWAY 62	06/27/17	2860
	0.99	100 FT. EAST OF LAKEVIEW DRIVE	06/22/17	1574
	1.57	300 FT. WEST OF ATLANTIC AVENUE	06/22/17	913
Avenue G	1.66	150 FT. EAST OF ATLANTIC AVENUE	06/27/17	716
	2.11	450 FT. WEST OF HIGHWAY 62	07/05/17	5849
	2.25	250 FT. EAST OF HIGHWAY 62	07/05/17	5375
	2.38	250 FT. EAST OF DIVISION	06/27/17	3053
Avenue H	3.30	150 FT. WEST OF ATLANTIC AVENUE	06/27/17	680
	0.50	450 FT. EAST OF ATLANTIC AVENUE	09/05/17	783
	1.40	200 FT. EAST OF DIVISION	06/27/17	1229
Ball RD	3.05	200 FT. EAST OF HIGHWAY 62	08/23/17	770
Baleman DR	0.01	50 FT. EAST OF TABLE ROCK ROAD	07/19/16	1389
Beagle RD	0.07	375 FT. NORTH OF DODGE ROAD	08/08/17	666

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119011

ADT Web Report Jackson County - November 2017

Road Name	MilePost	Location Description	Date	Count
Riley RD	0.08	400 FT. NORTH OF HIGHWAY 140	06/22/17	1214
	1.28	1200 FT. NORTH OF ALTA VISTA ROAD (S)	08/28/17	701
	2.80	100 FT. SOUTH OF STEVENS ROAD	08/22/17	719
Rogue River DR	0.03	150 FT. NORTH OF HIGHWAY 234	08/08/17	2349
	5.58	700 FT. SOUTH OF LONG BRANCH ROAD	08/07/17	1359
	6.16	100 FT. WEST OF DEER PARK LANE	08/07/17	1664
	7.22	100 FT. WEST OF SHADY COVE PARK	08/07/17	2417
	7.29	100 FT. WEST OF HIGHWAY 62	08/07/17	2656
	1.21	350 FT. WEST OF HANLEY ROAD	08/02/16	3894
Ross LN	2.23	100 FT. EAST OF HILLSIDE DRIVE	06/07/16	3380
	2.27	100 FT. WEST OF HILLSIDE DRIVE	05/31/16	738
	0.01	75 FT. NORTH OF HIGHWAY 99	07/11/17	855
Sardine Creek RD	0.11	600 FT. SOUTH OF OLD HIGHWAY 99	08/14/17	702
Savage Creek RD	0.76	200 FT. WEST OF HIGHWAY 99	06/06/16	2214
Scenic AV	1.14	175 FT. WEST OF SEVEN OAKS ROAD	06/06/16	1478
South Stage RD	2.54	50 FT. WEST OF TOLO ROAD	06/06/16	1306
	0.04	225 FT. WEST OF HIGHWAY 99	07/11/16	6750
	0.30	300 FT. WEST OF VOORHIES ROAD	07/11/16	6300
	1.89	225 FT. EAST OF KINGS HIGHWAY	07/05/16	6141
	2.48	100 FT. EAST OF COLUMBUS AVENUE	06/27/16	6000
	2.90	150 FT. EAST OF ORCHARD HOME DRIVE	06/27/16	4685
	3.40	225 FT. EAST OF GRIFFIN CREEK ROAD	06/28/16	4215
	3.52	75 FT. EAST OF FAIRLANE DRIVE	07/22/14	4016
	3.70	100 FT. WEST OF HULL ROAD	06/30/14	4091
	4.83	250 FT. WEST OF ARNOLD LANE	06/30/14	3236
	5.81	300 FT. WEST OF BELLINGER LANE	06/30/14	5151
	0.65	200 FT. NORTH OF EAST ASHLAND LANE	06/20/16	2629
	Sterling Creek RD	0.01	50 FT. SOUTHEAST OF CADY ROAD	08/02/16
Stevens RD	1.27	100 FT. SOUTH OF POORMAN CREEK ROAD	06/28/16	750
	4.76	100 FT. EAST OF RILEY ROAD	08/22/17	731
Suncrest RD	4.80	100 FT. WEST OF RILEY ROAD	08/22/17	1259
	0.83	300 FT. SOUTH OF PAYNE ROAD	07/25/16	640
	0.91	100 FT. EAST OF PAYNE ROAD	07/25/16	487
Sunset DR	2.33	150 FT. NORTH OF WEST VALLEY VIEW ROAD	07/25/16	650
	0.25	150 FT. WEST OF THOMAS ROAD	07/05/16	2776
	0.29	50 FT. EAST OF THOMAS ROAD	07/05/16	1191
Table Rock RD	2.42	750 FT. NORTH OF BIDDLE ROAD	08/03/16	19356
	3.07	150 FT. NORTH OF EAST VILAS ROAD	08/01/16	20349
	3.73	925 FT. NORTH OF WILSON ROAD	08/08/16	18648
	4.59	225 FT. NORTH OF WEST GREGORY ROAD	08/01/16	15869
	5.87	1060 FT. NORTH OF ANTELOPE ROAD	07/10/17	9867
	6.36	800 FT. NORTH OF KIRTLAND ROAD	07/10/17	7238
	7.52	325 FT. WEST OF MODOC ROAD	07/10/17	2764
	11.94	300 FT. SOUTH OF HIGHWAY 234	08/02/17	1875
	Takelma DR	0.06	300 FT. NORTH OF HIGHWAY 62	08/07/17
Tami LN	0.03	150 FT. WEST OF OLD STAGE ROAD	05/31/16	291
Taylor RD	0.84	100 FT. WEST OF GRANT ROAD (WEST, NORTH LEG)	07/22/14	1287

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11P"

Bateman - Table Rock
PM Peak Hour (3:45-4:45)

2018 PM Raw

		812		677		
		2	791	19		
5	3	Bateman_Table Rock			27	96
	0				0	
6	3	Bateman_Table Rock			69	44
					3	647
		863		675		

Bateman - Table Rock
PM Peak Hour (3:45-4:45)
Seasonal Adjustment

2018 Adj PM

1.144

		929		774		
		2	905	22		
6	3	Bateman_Table Rock			31	110
	0				0	
7	3	Bateman_Table Rock			79	50
					3	740
		987		772		

Bateman - Table Rock
PM Peak Hour (3:45-4:45)

2019 Design Year No Build

		919		790		
		2	894	22		
5	3	Bateman_Table Rock			31	111
Driveway	0				0	
6	3	Bateman_Table Rock			80	51
					3	756
		977		788		

Bateman - Table Rock
PM Peak Hour (3:45-4:45)

PM Dev

		0		2		
		0	0	0		
0	0	Bateman_Table Rock			2	6
Driveway	0				0	
0	0	Bateman_Table Rock			4	1
					0	0
		4		1		

Bateman - Table Rock
PM Peak Hour (3:45-4:45)

2019 PM Build

		919		792		
		2	894	22		
5	3	Bateman_Table Rock			33	117
	0				0	
6	3	Bateman_Table Rock			84	52
					3	756
		981		789		

"P"
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HCM Unsignalized Intersection Capacity Analysis

115: Table Rock Rd & Bateman Drive

12/19/2018

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↗	↖		↗	↖	
Traffic Volume (veh/h)	3	0	3	79	0	31	3	740	29	22	905	2
Future Volume (Veh/h)	3	0	3	79	0	31	3	740	29	22	905	2
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	0	3	86	0	34	3	804	32	24	984	2
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								TWLTL			None	
Median storage (veh)								2				
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	1475	1875	493	1369	1860	418	986			836		
vC1, stage 1 conf vol	1033	1033		826	826							
vC2, stage 2 conf vol	442	842		543	1034							
vCu, unblocked vol	1475	1875	493	1369	1860	418	986			836		
tC, single (s)	7.5	6.5	6.9	7.7	6.5	7.2	4.8			4.5		
tC, 2 stage (s)	6.5	5.5		6.7	5.5							
tF (s)	3.5	4.0	3.3	3.6	4.0	3.5	2.5			2.4		
p0 queue free %	99	100	99	67	100	94	99			97		
cM capacity (veh/h)	223	234	527	264	240	546	536			699		
Direction, Lane #	EB 1	WB 1	NB 1	NB 2	NB 3	SB 1	SB 2	SB 3				
Volume Total	6	120	3	536	300	24	656	330				
Volume Left	3	86	3	0	0	24	0	0				
Volume Right	3	34	0	0	32	0	0	2				
cSH	313	309	536	1700	1700	699	1700	1700				
Volume to Capacity	0.02	0.39	0.01	0.32	0.18	0.03	0.39	0.19				
Queue Length 95th (ft)	1	44	0	0	0	3	0	0				
Control Delay (s)	16.7	23.8	11.8	0.0	0.0	10.3	0.0	0.0				
Lane LOS	C	C	B			B						
Approach Delay (s)	16.7	23.8	0.0			0.2						
Approach LOS	C	C										
Intersection Summary												
Average Delay			1.6									
Intersection Capacity Utilization			42.9%	ICU Level of Service	A							
Analysis Period (min)			15									

"P"
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SimTraffic Simulation Summary
 Year 2018 No-Build - PM Peak Hour

12/19/2018

Summary of All Intervals

Run Number	1	2	3	4	5	Avg
Start Time	3:40	3:40	3:40	3:40	3:40	3:40
End Time	4:45	4:45	4:45	4:45	4:45	4:45
Total Time (min)	65	65	65	65	65	65
Time Recorded (min)	60	60	60	60	60	60
# of Intervals	3	3	3	3	3	3
# of Recorded Intervals	2	2	2	2	2	2
Vehs Entered	1990	1925	1986	1993	1987	1975
Vehs Exited	1994	1933	1982	1983	1977	1974
Starting Vehs	32	38	33	30	28	32
Ending Vehs	28	30	37	40	38	35
Travel Distance (mi)	1060	1030	1064	1060	1063	1055
Travel Time (hr)	29.6	29.1	32.1	30.8	32.0	30.7
Total Delay (hr)	2.8	2.9	4.8	3.7	5.0	3.8
Total Stops	128	136	148	143	149	141
Fuel Used (gal)	29.9	29.1	30.5	30.1	30.4	30.0

Interval #0 Information Seeding

Start Time 3:40
 End Time 3:45
 Total Time (min) 5
 Volumes adjusted by PHF, Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 3:45
 End Time 4:00
 Total Time (min) 15
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	501	505	495	467	469	487
Vehs Exited	499	508	491	470	463	485
Starting Vehs	32	38	33	30	28	32
Ending Vehs	34	35	37	27	34	32
Travel Distance (mi)	264	269	266	248	251	259
Travel Time (hr)	7.4	7.8	7.5	6.7	7.0	7.3
Total Delay (hr)	0.7	1.0	0.8	0.4	0.6	0.7
Total Stops	34	42	32	28	30	34
Fuel Used (gal)	7.6	7.8	7.6	6.9	7.0	7.4

"P"
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SimTraffic Simulation Summary
 Year 2018 No-Build - PM Peak Hour

12/19/2018

Interval #2 Information Recording

Start Time 4:00
 End Time 4:45
 Total Time (min) 45
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	1489	1420	1491	1526	1518	1490
Vehs Exited	1495	1425	1491	1513	1514	1488
Starting Vehs	34	35	37	27	34	32
Ending Vehs	28	30	37	40	38	35
Travel Distance (mi)	796	761	798	811	812	796
Travel Time (hr)	22.3	21.3	24.5	24.0	25.1	23.4
Total Delay (hr)	2.1	1.9	4.1	3.2	4.4	3.1
Total Stops	94	94	116	115	119	108
Fuel Used (gal)	22.3	21.3	22.9	23.1	23.3	22.6

"P"
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Queuing and Blocking Report
 Year 2018 No-Build - PM Peak Hour

12/19/2018

Intersection: 115: Table Rock Rd & Bateman Drive

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	L	L
Maximum Queue (ft)	35	264	21	55
Average Queue (ft)	7	118	1	14
95th Queue (ft)	28	236	9	44
Link Distance (ft)	159	2410		
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)			300	250
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 0

"P"
25/36

HCM Unsignalized Intersection Capacity Analysis
 115: Table Rock Rd & Bateman Drive

12/19/2018

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↖	↗		↖	↗	
Traffic Volume (veh/h)	3	0	3	80	0	31	3	756	29	22	894	2
Future Volume (Veh/h)	3	0	3	80	0	31	3	756	29	22	894	2
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	0	3	87	0	34	3	822	32	24	972	2
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								TWLTL			None	
Median storage (veh)								2				
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	1472	1881	487	1381	1866	427	974			854		
vC1, stage 1 conf vol	1021	1021		844	844							
vC2, stage 2 conf vol	451	860		537	1022							
vCu, unblocked vol	1472	1881	487	1381	1866	427	974			854		
tC, single (s)	7.5	6.5	6.9	7.7	6.5	7.2	4.8			4.5		
tC, 2 stage (s)	6.5	5.5		6.7	5.5							
tF (s)	3.5	4.0	3.3	3.6	4.0	3.5	2.5			2.4		
p0 queue free %	99	100	99	66	100	94	99			97		
cM capacity (veh/h)	225	234	532	260	240	539	542			687		

Direction, Lane #	EB 1	WB 1	NB 1	NB 2	NB 3	SB 1	SB 2	SB 3
Volume Total	6	121	3	548	306	24	648	326
Volume Left	3	87	3	0	0	24	0	0
Volume Right	3	34	0	0	32	0	0	2
cSH	317	304	542	1700	1700	687	1700	1700
Volume to Capacity	0.02	0.40	0.01	0.32	0.18	0.03	0.38	0.19
Queue Length 95th (ft)	1	46	0	0	0	3	0	0
Control Delay (s)	16.6	24.5	11.7	0.0	0.0	10.4	0.0	0.0
Lane LOS	C	C	B			B		
Approach Delay (s)	16.6	24.5	0.0			0.3		
Approach LOS	C	C						

Intersection Summary		
Average Delay		1.7
Intersection Capacity Utilization	42.6%	ICU Level of Service
Analysis Period (min)		15
		A

11 P
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SimTraffic Simulation Summary
 Design Year 2019 No-Build - PM Peak Hour

12/19/2018

Summary of All Intervals

Run Number	1	2	3	4	5	Avg
Start Time	3:40	3:40	3:40	3:40	3:40	3:40
End Time	4:45	4:45	4:45	4:45	4:45	4:45
Total Time (min)	65	65	65	65	65	65
Time Recorded (min)	60	60	60	60	60	60
# of Intervals	3	3	3	3	3	3
# of Recorded Intervals	2	2	2	2	2	2
Vehs Entered	1954	1923	1928	1988	2005	1960
Vehs Exited	1950	1924	1932	1978	2012	1960
Starting Vehs	20	31	30	27	33	28
Ending Vehs	24	30	26	37	26	29
Travel Distance (mi)	1043	1032	1039	1062	1079	1051
Travel Time (hr)	29.3	29.0	31.3	32.1	32.0	30.7
Total Delay (hr)	2.8	2.7	4.8	5.0	4.5	3.9
Total Stops	141	149	146	153	149	148
Fuel Used (gal)	29.4	29.2	29.9	30.5	30.9	30.0

Interval #0 Information Seeding

Start Time 3:40
 End Time 3:45
 Total Time (min) 5
 Volumes adjusted by PHF, Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 3:45
 End Time 4:00
 Total Time (min) 15
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	498	480	475	483	530	493
Vehs Exited	492	478	483	483	522	492
Starting Vehs	20	31	30	27	33	28
Ending Vehs	26	33	22	27	41	29
Travel Distance (mi)	265	259	259	256	281	264
Travel Time (hr)	7.6	7.6	7.9	7.7	8.8	7.9
Total Delay (hr)	0.8	0.9	1.3	1.3	1.6	1.2
Total Stops	35	44	28	39	37	36
Fuel Used (gal)	7.5	7.5	7.5	7.5	8.0	7.6

"P"
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SimTraffic Simulation Summary
 Design Year 2019 No-Build - PM Peak Hour

12/19/2018

Interval #2 Information Recording

Start Time 4:00
 End Time 4:45
 Total Time (min) 45
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	1456	1443	1453	1505	1475	1465
Vehs Exited	1458	1446	1449	1495	1490	1467
Starting Vehs	26	33	22	27	41	29
Ending Vehs	24	30	26	37	26	29
Travel Distance (mi)	778	773	780	806	798	787
Travel Time (hr)	21.7	21.4	23.4	24.4	23.2	22.8
Total Delay (hr)	2.0	1.8	3.5	3.7	2.9	2.8
Total Stops	106	105	118	114	112	111
Fuel Used (gal)	22.0	21.7	22.4	23.1	22.9	22.4

"P"
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Queuing and Blocking Report
 Design Year 2019 No-Build - PM Peak Hour

12/19/2018

Intersection: 115: Table Rock Rd & Bateman Drive

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	L	L
Maximum Queue (ft)	35	287	33	62
Average Queue (ft)	6	119	2	13
95th Queue (ft)	26	245	14	43
Link Distance (ft)	159	2410		
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)			300	250
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 0

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HCM Unsignalized Intersection Capacity Analysis
115: Table Rock Rd & Bateman Drive

12/22/2018

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔			↔		↖	↗		↖	↗	
Traffic Volume (veh/h)	3	0	3	84	0	33	3	756	30	22	894	2
Future Volume (Veh/h)	3	0	3	84	0	33	3	756	30	22	894	2
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	0	3	91	0	36	3	822	33	24	972	2
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								TWTL			TWTL	
Median storage veh								2			2	
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	1474	1882	487	1382	1866	428	974			855		
vC1, stage 1 conf vol	1021	1021		844	844							
vC2, stage 2 conf vol	453	861		537	1022							
vCu, unblocked vol	1474	1882	487	1382	1866	428	974			855		
tC, single (s)	7.5	6.5	6.9	7.7	6.5	7.2	4.8			4.5		
tC, 2 stage (s)	6.5	5.5		6.7	5.5							
tF (s)	3.5	4.0	3.3	3.6	4.0	3.5	2.5			2.4		
p0 queue free %	99	100	99	65	100	93	99			97		
cM capacity (veh/h)	225	234	532	259	240	538	542			687		

Direction, Lane #	EB 1	WB 1	NB 1	NB 2	NB 3	SB 1	SB 2	SB 3
Volume Total	6	127	3	548	307	24	648	326
Volume Left	3	91	3	0	0	24	0	0
Volume Right	3	36	0	0	33	0	0	2
cSH	316	304	542	1700	1700	687	1700	1700
Volume to Capacity	0.02	0.42	0.01	0.32	0.18	0.03	0.38	0.19
Queue Length 95th (ft)	1	49	0	0	0	3	0	0
Control Delay (s)	16.6	25.1	11.7	0.0	0.0	10.4	0.0	0.0
Lane LOS	C	D	B			B		
Approach Delay (s)	16.6	25.1	0.0			0.3		
Approach LOS	C	D						

Intersection Summary			
Average Delay		1.8	
Intersection Capacity Utilization	41.2%		ICU Level of Service
Analysis Period (min)		15	A

11p
30/36

SimTraffic Simulation Summary
 Design Year 2019 Build - PM Peak Hour

12/22/2018

Summary of All Intervals

Run Number	1	2	3	4	5	Avg
Start Time	3:40	3:40	3:40	3:40	3:40	3:40
End Time	4:45	4:45	4:45	4:45	4:45	4:45
Total Time (min)	65	65	65	65	65	65
Time Recorded (min)	60	60	60	60	60	60
# of Intervals	3	3	3	3	3	3
# of Recorded Intervals	2	2	2	2	2	2
Vehs Entered	1965	2018	1939	1966	1958	1967
Vehs Exited	1963	1999	1937	1953	1963	1964
Starting Vehs	31	24	23	27	32	28
Ending Vehs	33	43	25	40	27	34
Travel Distance (mi)	1051	1078	1038	1061	1049	1056
Travel Time (hr)	31.5	31.0	29.5	32.7	30.9	31.1
Total Delay (hr)	4.7	3.6	2.9	5.6	4.1	4.2
Total Stops	148	156	147	166	137	150
Fuel Used (gal)	30.2	30.7	29.4	30.7	29.6	30.2

Interval #0 Information Seeding

Start Time 3:40
 End Time 3:45
 Total Time (min) 5
 Volumes adjusted by PHF, Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 3:45
 End Time 4:00
 Total Time (min) 15
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	508	500	488	444	527	493
Vehs Exited	500	493	488	438	521	488
Starting Vehs	31	24	23	27	32	28
Ending Vehs	39	31	23	33	38	33
Travel Distance (mi)	266	263	263	241	280	263
Travel Time (hr)	7.8	7.2	7.9	8.5	7.9	7.8
Total Delay (hr)	1.0	0.6	1.1	2.2	0.7	1.1
Total Stops	32	42	43	49	34	39
Fuel Used (gal)	7.6	7.3	7.5	7.3	7.9	7.5

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SimTraffic Simulation Summary
 Design Year 2019 Build - PM Peak Hour

12/22/2018

Interval #2 Information Recording

Start Time 4:00
 End Time 4:45
 Total Time (min) 45
 Volumes adjusted by PHF, Growth Factors.

Run Number	1	2	3	4	5	Avg
Vehs Entered	1457	1518	1451	1522	1431	1475
Vehs Exited	1463	1506	1449	1515	1442	1476
Starting Vehs	39	31	23	33	38	33
Ending Vehs	33	43	25	40	27	34
Travel Distance (mi)	785	815	776	820	769	793
Travel Time (hr)	23.8	23.8	21.6	24.3	23.0	23.3
Total Delay (hr)	3.7	3.0	1.8	3.4	3.3	3.1
Total Stops	116	114	104	117	103	111
Fuel Used (gal)	22.6	23.5	21.9	23.5	21.8	22.6

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Queuing and Blocking Report
 Design Year 2019 Build - PM Peak Hour

12/22/2018

Intersection: 115: Table Rock Rd & Bateman Drive

Movement	EB	WB	NB	SB
Directions Served	LTR	LTR	L	L
Maximum Queue (ft)	35	283	37	54
Average Queue (ft)	5	126	2	12
95th Queue (ft)	25	258	17	38
Link Distance (ft)	159	2410		
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)			300	250
Storage Blk Time (%)				
Queuing Penalty (veh)				

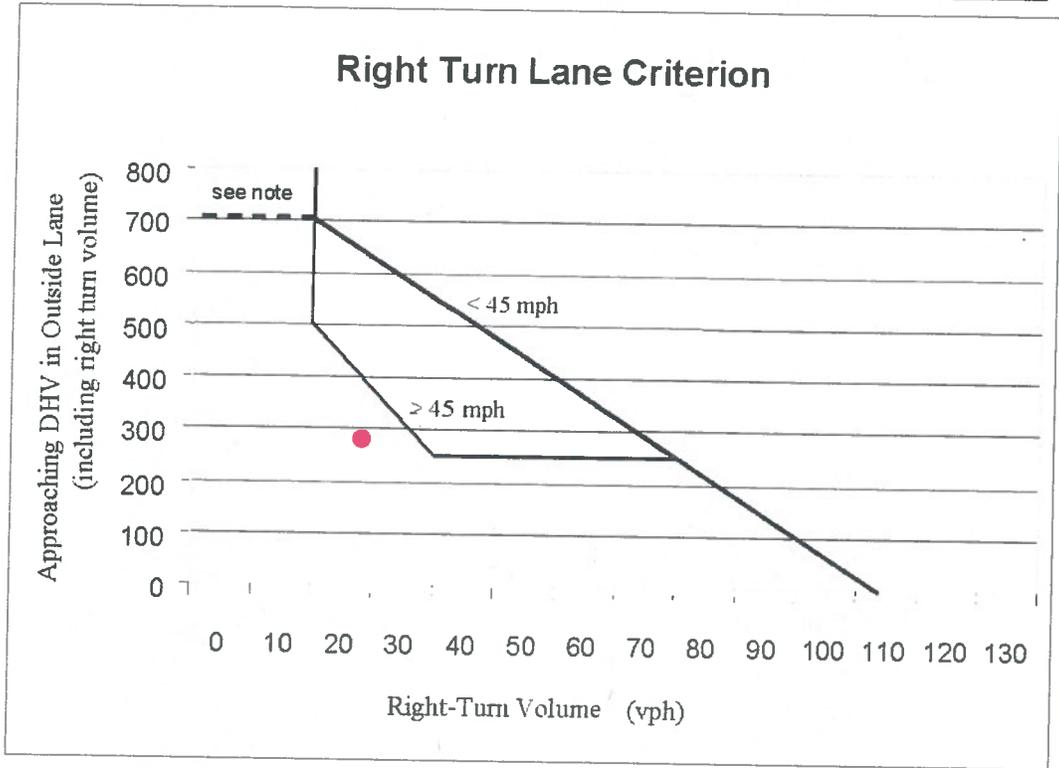
Network Summary

Network wide Queuing Penalty: 0

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Exhibit 7-2 Right Turn Lane Criterion

Year 2019 Build, PM Peak Hour



Note: If there is no right turn lane, a shoulder needs to be provided. If this intersection is in a rural area and is a connection to a public street, a right turn lane is needed.

X = 30 VPH
 Y = 280 VPH
 Criterion: Not Met

"p"
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JACKSON COUNTY

Roads

Roads
Engineering

Chuck DeJanvier
Construction Engineer

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
dejanvca@jacksoncounty.org

www.jacksoncounty.org

October 19, 2018

Attention: Liz Conner
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Shop/Yard/Office off Bateman Drive – a county maintained road.
Planning File: AC-18-138/E-18-139

Dear Liz:

Thank you for the opportunity to comment on the consideration of construction of steel buildings for a shop and office approximately 6,300 square feet with an exception to the General Design Requirement for Parling, specifically the requirement that all parking, loading, and vehicle maneuvering areas shall be paved and improved on a 4.0 acre parcel located at 356 Bateman Drive approximately 450 feet west of Beirson Way the General Industrial (I-G) zoning district, (36-2W-36D Tax Lot 142). Jackson County Roads has the following comments:

1. If frontage improvements are required off Bateman Drive, they shall be permitted and inspected by the City of Medford.
2. Any new or improved road approaches off Bateman Drive shall be permitted and inspected by the City of Medford.
3. The applicant shall submit construction plans to Jackson County Roads, so we may determine if county permits will be required.
4. The County portion of Bateman Drive is a County Local Road and is maintained by the County. The Average Daily Traffic count was 1,389 on July 19, 2016, 50 feet east of Table Rock Road. As a comparison of capacity for Bateman Drive, the capacity of a two lane suburban road with ten foot lanes and no shoulders is 8,832 ADT.
5. Jackson County's General Administration Policy #1-45 sets forth the County's position as it relates to the management of County roads located within existing or proposed city limits or Urban Growth Boundaries (UGB). The County has no current plans for

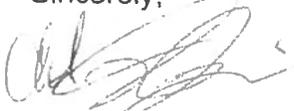
11P"
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improvements to Bateman Drive Road. Jackson County Roads recommends that the city request jurisdiction of this road.

6. Storm water should meet City of Medford requirements that also include water quality.
7. Jackson County Roads would like to review and comment on the hydraulic report including the calculations and drainage plan. Capacity improvements or on site detention, if necessary, shall be installed at the expense of the applicant. Upon completion of the project, the developer's engineer shall certify that construction of the drainage system was constructed per plan and a copy of the certification shall be sent to Jackson County Roads.
8. We require that the applicant prepare a traffic study that addresses impacts at the intersection of Bateman Drive and Table Rock Road. The study should recommend mitigation if necessary and be reviewed and approved by Jackson County. If mitigations are recommended, they shall be required.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Chuck DeJanvier
Construction Engineer

"P"

36 of 36

Liz A. Conner

From: Mike Kuntz <KuntzM@jacksoncounty.org>
Sent: Thursday, January 3, 2019 3:33 PM
To: 'Jeff Fowler'; 'Kim Parducci'
Cc: Liz A. Conner; Karl H. MacNair; Nancy Coates; Charles DeJanvier
Subject: TIS: Pilot Rock Excavation Office/Shop - Bateman Drive

Kim,

Jackson County has received and reviewed the subject TIS, dated December 24, 2018. We find the TIS to be acceptable and offer no comment.

Mike Kuntz
County Engineer
Jackson County Roads

CITY OF MEDFORD
EXHIBIT # Q
FILE # AC-18-138 / E-18-139

Liz A. Conner

From: Karl H. MacNair
Sent: Thursday, January 3, 2019 4:03 PM
To: 'Mike Kuntz'; 'Jeff Fowler'; 'Kim Parducci'
Cc: Liz A. Conner; Nancy Coates; Charles DeJanvier; Peter T. Mackprang; Douglas E. Burroughs; Jodi K. Cope
Subject: RE: Pilot Rock Excavation Office/Shop - Bateman Drive

The City of Medford has no comment either.

Karl

From: Mike Kuntz [mailto:KuntzM@jacksoncounty.org]
Sent: Thursday, January 03, 2019 3:33 PM
To: 'Jeff Fowler' <jeff.pilotrock@gmail.com>; 'Kim Parducci' <kim.parducci@gmail.com>
Cc: Liz A. Conner <Elizabeth.Conner@cityofmedford.org>; Karl H. MacNair <Karl.MacNair@cityofmedford.org>; Nancy Coates <CoatesN@jacksoncounty.org>; Charles DeJanvier <DeJanvCA@jacksoncounty.org>
Subject: TIS: Pilot Rock Excavation Office/Shop - Bateman Drive

Kim,

Jackson County has received and reviewed the subject TIS, dated December 24, 2018. We find the TIS to be acceptable and offer no comment.

Mike Kuntz
County Engineer
Jackson County Roads

CITY OF MEDFORD
EXHIBIT # R
FILE # AC-18-138 / E-18-139



Project Name:

Pilot Rock Excavation

Map/Taxlot:

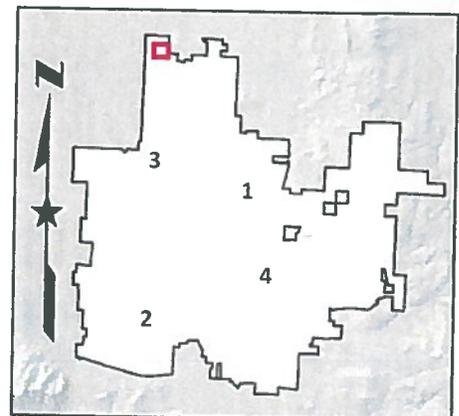
362W36D TL 142



10/05/2018

Legend

-  Subject Area
-  Zoning Districts
-  Tax Lots





City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

STAFF REPORT

for a Type III quasi-judicial decision: **Site Plan & Architectural Review**

Project Delta Waters Self Storage
Applicant: Delta Waters Lenders, LLC; Agent: Bill Philp

File no. AC-18-147

To Site Plan & Architectural Commission *for 01/18/2019 hearing*

From Liz Conner, Planner II

Reviewer Kelly Evans, Assistant Planning Director *ke*

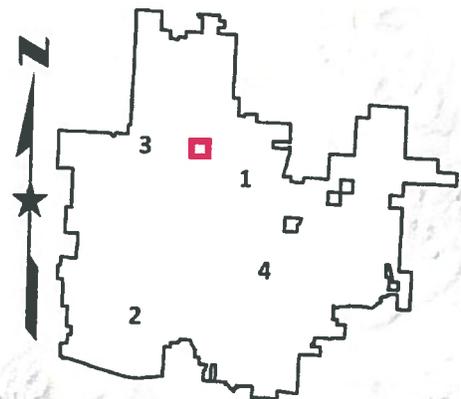
Date January 11, 2019

BACKGROUND

Proposal

Consideration to construct a 43,170 square foot self-storage facility on 2.37 acres zoned I-L (Light Industrial), located approximately 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62) 1884-1862 Delta Waters Road (371W18AA TL 1200-1400).

Vicinity Map



Subject Site Characteristics

Zoning	I-L	Light Industrial
GLUP	GI	General Industrial
Use	Outdoor RV Storage/Office	

Surrounding Site Characteristics

<i>North</i>	Zone:	I-L
	Use:	Stove, Pool and Sauna sales
<i>South</i>	Zone:	I-L, MFR-15 (Multi Family Residential – 15 dwelling units per gross acre)
	Use:	Vacant
<i>East</i>	Zone:	I-L
	Use:	Curtius-Huntley Plumbing
<i>West</i>	Zone:	I-L
	Use:	Custom fabrication shop

Related Projects

PA-17-055 Pre-Application
GF-18-096 Riparian Corridor Reduction
CUP-18-148 Stormwater Facilities within a Riparian Corridor

Applicable Criteria

**SITE PLAN AND ARCHITECTURAL REVIEW APPROVAL CRITERIA
FROM SECTION 10.200(E) OF THE MEDFORD LAND DEVELOPMENT CODE**

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land, and
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in Section 10.186.

Corporate Names

Patrick Huycke is listed as the Registered Agent for Delta Waters Lenders, LLC. according to the Oregon Secretary of State Business Registry.

ISSUES AND ANALYSIS

Background

The subject site is made up of three parcels located on Delta Waters Road between Crater Lake Avenue and Highway 62. The site is currently developed and is used as a graveled RV storage lot.

Lone Pine Creek runs along the southern boundary of the site with a mapped floodplain and riparian corridor. In August of 2018 the applicant requested a reduction in the riparian corridor for Lone Pine Creek. The Planning Director approved a 25 foot reduction along Lone Pine Creek along tax lots 1200 and 1400 (Exhibit P).



Site Plan (Exhibit B)

The site plan shows a total of seven buildings consisting of approximately 43,170 square feet. The applicant intends to secure the development with locking gates and a continuous fence.

Parking

Required off street parking for mini-storage facilities is typically calculated using a manager's residence, office and total number storage cubicles. The intent is to require parking for the customers that are accessing the office for the rental of units and other office duties. This facility will not have an on-site manager. The site plan shows a total of 24 individual parking spaces located in the drive aisles in front of each unit.

MLDC 10.743 Calculation
1 space/11 storage cubicles

Table 1

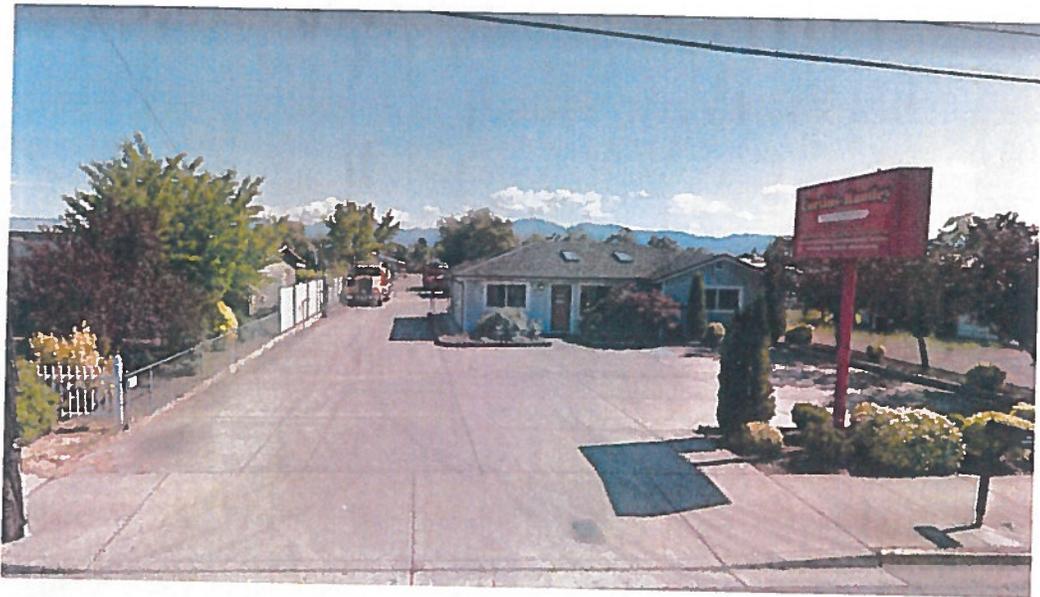
	Minimum	Maximum	Shown
Total Spaces	20	24	24
Accessible Spaces	1		2
Bicycle Spaces	4	5	0

The Commission can find that the use does not typically generate bicycle traffic, and in the event of customer accessing the self-storage facility by bicycle it would be similar to car traffic and would use the drive aisle in front of the individual unit therefore reducing the bicycle parking requirement to zero.

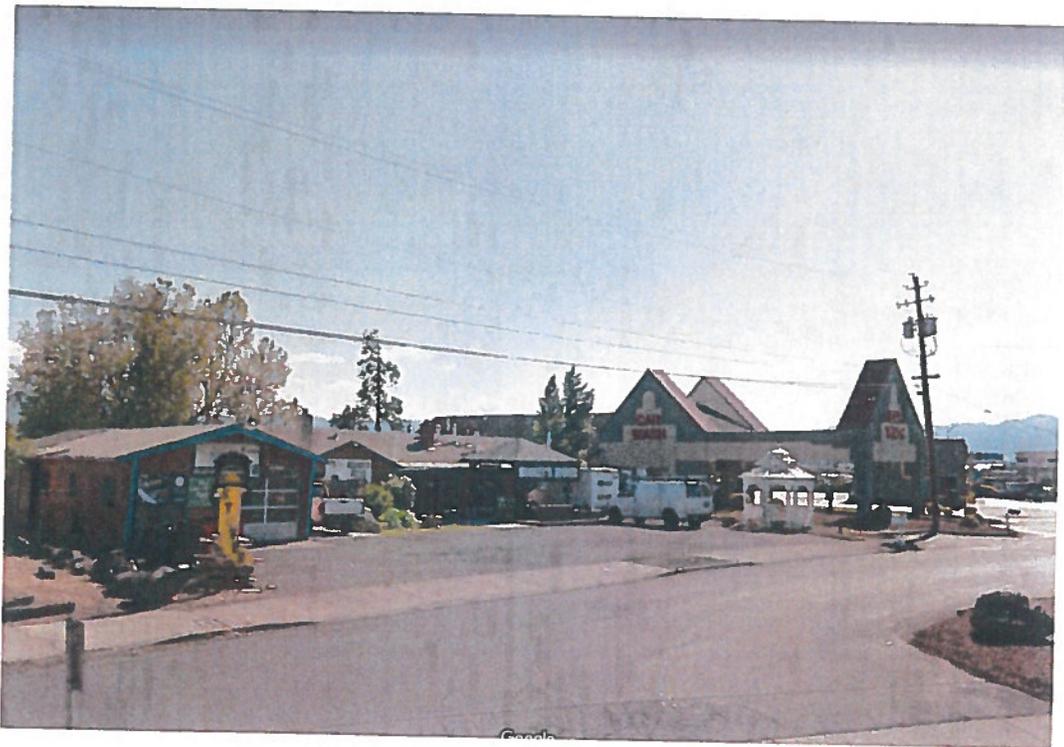
Architecture

The applicant's findings (Exhibit I) states "the architectural details of the different colored block as well as the use of gates, landscaping and other building and elevation changes in the project will give a pleasing drive by street view as well as break up the character of the use itself."

Property to the east shown in image below.



Property to the west shown in image below.



Access and Circulation

Driveway

The subject site has approximately 140 feet of frontage along Delta Waters Road. Per the submitted site plan (Exhibit B) there will be one access to the site located on the easterly portion of the site. Per the Public Works report (Exhibit L) the driveway access shall be limited to right-in/right-out movements.

Cross Access Easement

When driveway access is granted on an Arterial or Collector Street, the Medford Land Development Code Section 10.550 requires a cross access easement be granted to all contiguous parcels or tracts that do not front a lower order street. The site plan (Exhibit B), shows a future access drive that is the proposed as the cross access easement.

Per MLDC 10.746 all vehicle maneuvering areas are required to be paved.



City of Medford Attorney's Office Memo (Exhibit R)

The City of Medford's Deputy City Attorney provided a memo to address the applicant's communication regarding the requirement for a cross-access easement to adjoining

properties as well as the paving requirements for maneuvering areas within the required cross access easement.

The applicant originally stipulated to providing a cross-access easement to adjoining properties per the findings provided (Exhibit I). Per the City Attorney's memo the Applicant is challenging the establishment of any cross-access easements under the cases Nollan and Dolan and, if such easement is established the Applicant then challenges the paving of any such easement contending the easement is not a maneuvering area under the Medford Municipal Code where paving is required.

The City Attorney's memo provides a response with findings that addresses the applicant's challenge to the requirement of a cross-access easement as an unconstitutional taking under Nollan and Dolan.

The memo also provides a response to the challenge to require the cross-access easement be paved under the Medford Municipal Code Section 10.746.

The history of the site is visible through the various images provided through aerial photos (Exhibit V). Over the past several years it can be shown that the portion of the property intended for the cross-access easement as depicted on the site plan (Exhibit B) has been used to access the property to the west therefore MLDC 10.550 and 10.746 shall apply.

Block Length

The applicant submitted additional findings (Exhibit J) that request relief to the Block Length and Accessway requirements in the MLDC 10.426 and 10.464 respectively.

The approving authority may determine, based on evidence, that the construction of an accessway is infeasible or inappropriate per 10.464(1)(c) when the accessway would cross a natural area with significant natural habitat and construction would be incompatible with the protection of natural values.

The applicant's findings (Exhibit J), state that a connection south would require crossing Lone Pine Creek that has an established floodplain and designated riparian corridor.

Both the floodplain and riparian corridor have strict development restrictions for the protection of their natural functions. Therefore, requiring an accessway south would be incompatible with the protection of the natural values.

Floodplain

The subject site has obtained a Letter of Map Amendment (LOMA) for a portion of the property (Exhibit K). Per the exhibit map submitted with the LOMA the floodplain was removed on the property except in the portion noted.

Per the site plan (Exhibit B), the mapped floodplain does not contain vertical construction.

Storm Drainage

As mentioned previously, the subject site abuts Lone Pine Creek that has a mapped floodplain and riparian corridor. Per the site plan (Exhibit B), the applicant proposes to locate their stormwater facility within the riparian corridor. The MLDC Section 10.925 (1) permits water-related or water-dependent uses, such as drainage facilities as conditional uses. The applicant concurrently applied for a Conditional Use Permit (CUP) to allow the stormwater facilities within the riparian corridor. This application CUP-18-148, is scheduled for hearing before the Planning Commission on January 24, 2019. A condition of approval (Exhibit A) prior to vertical construction will be included to require Planning Commission approval of the stormwater facilities within the riparian corridor (CUP-18-148).

The applicant has provided a Riparian & Detention planting plan (Exhibit G) that details the planting requirements for the irrigated detention swale. This plan was approved by the Oregon Department of Fish and Wildlife (ODFW) with the Riparian Corridor reduction staff report (Exhibit P). A condition of approval has been included to comply with the planting plan submitted (Exhibit A).

Landscaping

Per the site plan (Exhibit B), the applicant has approximately 120 feet of frontage, excluding the driveway access.

Frontage Landscaping Requirements

	Delta Waters Road	
	Required	Shown
Minimum trees	4	6
Minimum shrubs	18	54

The landscape plan (Exhibit F) shows landscaping for the entire northern portion of the subject area, and within the proposed cross-access easement or "future access drive". As discussed previously, this future access drive is to be paved as vehicular maneuvering area.

The MLDC Section 10.746(9) Screening, a minimum of a 10 foot wide landscape buffer shall be required between all public right-of-way and parking and maneuvering areas.

The landscape plan will have to be revised to include the paved cross-access easement and the required 10 foot landscape buffer. A condition of approval to comply with MLDC 10.746 has been included (Exhibit A).

Bufferyard

Per MLDC Section 10.790 Bufferyards are utilized to minimize the potential conflicts caused by the types and intensity of uses on adjacent properties.

The table below determines the type of bufferyard and when it is required based on zoning.

Table 10.790-1. Bufferyard Standards—Zone to Zone

Subject Site Zoning	Zoning on Abutting Land										
	Vac	SFR	MFR	CS/P	C-N	C-C	C-R	C-H	I-L	I-G	I-H
SFR	-	-	-	-	-	-	-	-	-	-	-
MFR	1	A	-	-	-	-	-	-	-	-	-
CS/P	1	A	A	-	-	-	-	-	-	-	-
C-N	1	A	A	-	-	-	-	-	-	-	-
C-C	1	A	A	-	-	-	-	-	-	-	-
C-R	1	A	A	A	-	-	-	-	-	-	-
C-H	1	A	A	A	-	-	-	-	-	-	-
I-L	1	A	A	A	A	-	-	-	-	-	-
I-G	2	B	B	B	B	A	A	A	A	-	-
I-H	2	B	B	B	B	A	A	A	A	-	-

The table below shows the development standards for each bufferyard type.

Table 10.790-3. Bufferyard Types

Type	Width	Wall
A	10 feet	Six (6) foot concrete or masonry wall.
B	20 feet	Eight (8) foot concrete or masonry wall

The Site Plan and Architectural Commission can find that the approved riparian corridor reduction planting plan (Exhibit G), and Building "G" (Exhibit E) with the continuous wall with no openings, can be used in lieu of the required Type A Bufferyard.

Agency Comments

Jackson County Airport Authority (Exhibit N)

Rogue Valley International Airport requests an Avigation, Noise and Hazard Easement to be required as part of the permit process. In the 2010 LUBA decision on Michelle Barnes vs. City of Hillsboro and the Port of Portland, Nollan/Dolan findings are required to support the request (LUBA No. 2010-011). None were provided; therefore, a condition requiring compliance with the airport's request for an Avigation, Noise and Hazard Easement has not been included.

In addition, the Airport also requires the applicant to contact the FAA regarding submittal of a 7460-1 form.

No other issues were identified by staff.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit I and J) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-18-147 per the staff report dated January 11, 2019, including Exhibits A through W.

EXHIBITS

- A. Conditions of Approval, dated January 11, 2019
- B. Site Plan received October 4, 2018
- C. Grading and Drainage Plan received October 4, 2018
- D. Utility Plan received October 4, 2018
- E. Architectural Plans received October 4, 2018
 - Floor Plan
 - Building Elevations
- F. Frontage Landscape Plans received October 4, 2018
- G. Riparian Corridor Planting Plan received October 4, 2018
- H. Floodplain Exhibit Map received October 4, 2018
- I. Applicants findings and conclusions received October 4, 2018

- J. Applicant's additional findings received November 21, 2018
- K. Letter of Map Amendment received October 4, 2018
- L. Public Works Department report dated December 6, 2018
- M. Medford Fire Department report dated November 14, 2018
- N. Medford Building Department memo dated November 20, 2018
- O. Floodplain Managers Memo dated May 16, 2017
- P. Riparian Reduction Staff Report dated August 22, 2018
- Q. Medford Water Commission memo dated November 21, 2018
- R. City of Medford Legal Department memo dated January 10, 2019
- S. City of Medford Survey comments dated November 15, 2018
- T. Jackson County Roads comments dated November 16, 2018
- U. Jackson County Airport Authority email dated November 11, 2018
- V. Historic Aerial Photos
- W. Assessor Map received October 4, 2018
Vicinity map

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA:

JANUARY 18, 2019

EXHIBIT A
Delta Waters Lenders Mini Storage
AC-18-147
Conditions of Approval
January 11, 2019

DISCRETIONARY CONDITIONS

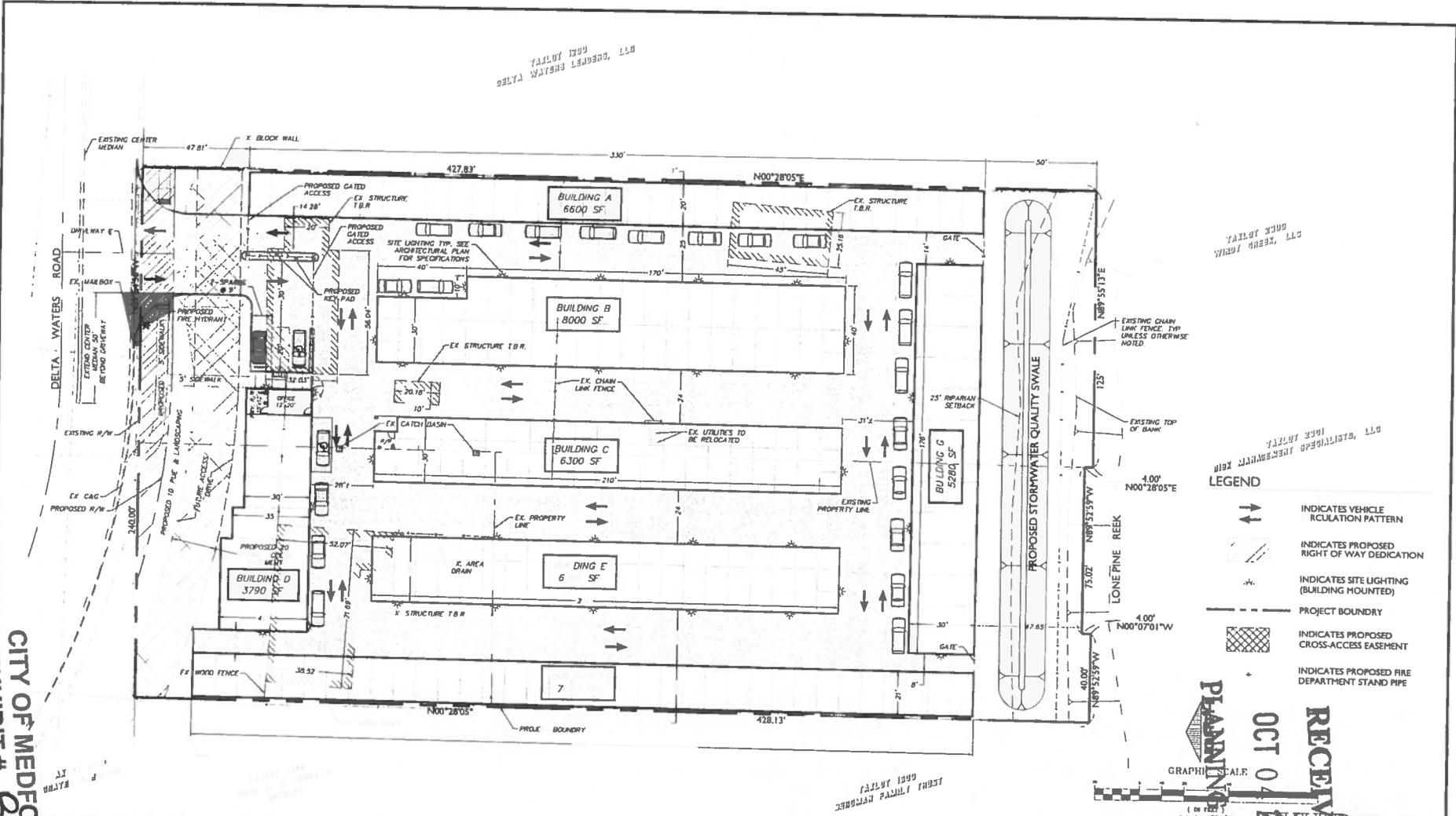
Prior to the issuance of a building permit for vertical construction, the applicant shall obtain approval for the Conditional Use Permit application CUP-18-148 for stormwater facilities within the Riparian Corridor.

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit L).
2. Comply with all requirements of the Medford Fire Department (Exhibit M).
3. Comply with all requirements of the Medford Building Department memo (Exhibit N).
4. Comply with all conditions stipulated by the Medford Water Commission (Exhibit Q).
5. Comply with Jackson County Airport Authority's condition to file any forms required by FAA (Exhibit U).
6. Comply with required landscaping pursuant to MLDC 10.746(9).
7. Comply with Riparian Corridor Planting Plan (Exhibit G).

CITY OF MEDFORD
EXHIBIT # 6



TALBOT TRUSS
DELTA WATERS LEASERS, LLC

TALBOT TRUSS
DELTA WATERS LEASERS, LLC

- LEGEND**
- INDICATES VEHICLE CIRCULATION PATTERN
 - INDICATES PROPOSED RIGHT OF WAY DEDICATION
 - INDICATES SITE LIGHTING (BUILDING MOUNTED)
 - PROJECT BOUNDARY
 - INDICATES PROPOSED CROSS-ACCESS EASEMENT
 - INDICATES PROPOSED FIRE DEPARTMENT STAND PIPE

PLANNING DEPT.
GRAPHIC UNIT

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P.O. BOX 1724 • MEDFORD, OREGON 97501
PH. (541) 778-5688

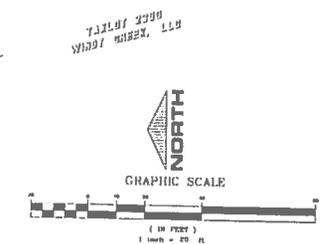
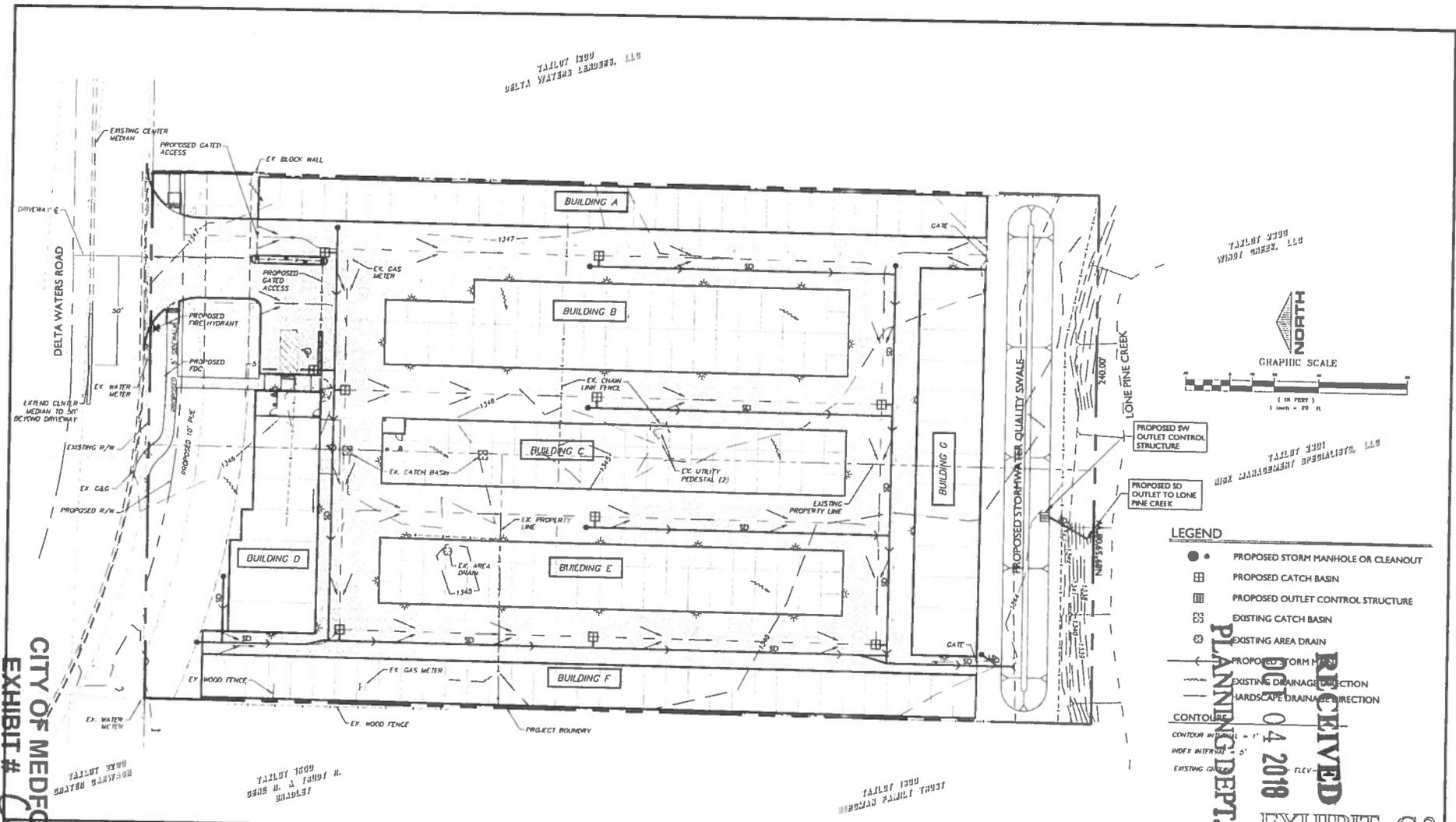
DRAWN BY: BRD	DATE: 02/18
CHECKED BY: MMK	DATE: 02/18
APPROVED RVS:	DATE:
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APPROVED:	DATE:
APPROVED:	DATE:

NO.	REVISION	DATE	BY



CONSTRUCTION ENGINEERING CONSULTANTS, INC.
HUYCKE MINI-STORAGE
CIVIL IMPROVEMENTS
CONCEPTUAL
SITE PLAN

CITY OF MEDFORD
EXHIBIT #



- LEGEND**
- PROPOSED STORM MANHOLE OR CLEANOUT
 - ▣ PROPOSED CATCH BASIN
 - ▣ PROPOSED OUTLET CONTROL STRUCTURE
 - ▣ EXISTING CATCH BASIN
 - ⊙ EXISTING AREA DRAIN
 - PROPOSED STORM DRAINAGE DIRECTION
 - EXISTING DRAINAGE DIRECTION
 - HARDSCAPE DRAINAGE DIRECTION
 - CONTOUR
 - CONTOUR INTERVAL = 1'
 - INDEX INTERVAL = 5'
 - EXISTING CENTER MEDIAN

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EXHIBIT C2



P.O. BOX 1724 • MEDFORD, OREGON 97501
TEL: (541) 779-5268

DRAWN BY: JND	DATE: 02/18
CHECKED BY: MWM	DATE: 02/18
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NO.	REVISION	DATE	BY

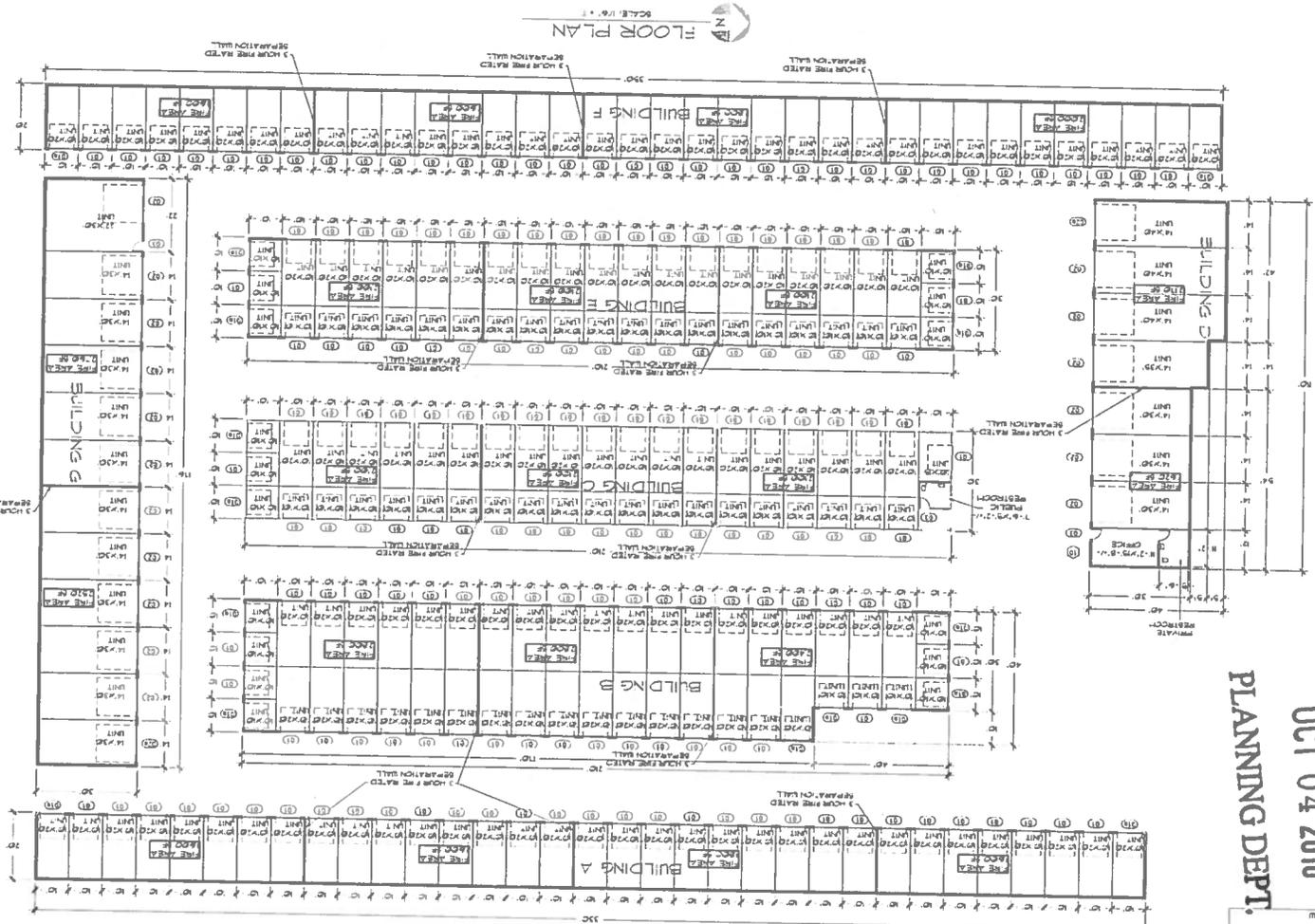


CONSTRUCTION ENGINEERING CONSULTANTS, INC.
 HUYCKE MINI-STORAGE CIVIL IMPROVEMENTS
 CONCEPTUAL GRADING & DRAINAGE PLAN

DOOR SCHEDULE

(01)	4000 SWDOOR
(02)	DOOR SCHEDULE
(03)	2000 DOOR
(04)	2000 BIRELL ROLL-UP DOOR
(05)	2000 BIRELL ROLL-UP DOOR
(06)	2000 BIRELL ROLL-UP DOOR
(07)	2000 BIRELL ROLL-UP DOOR
(08)	2000 BIRELL ROLL-UP DOOR
(09)	2000 BIRELL ROLL-UP DOOR
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FLOOR PLAN

SHEET NO. 1

HUYCKE
MINI-STORAGE

KAS & ASSOCIATES, INC.



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14



KAS
KAS ENGINEERING, INC.
CIVIL & STRUCTURAL PLANNING
1001 NE 11th St., Suite 100
Medford, OR 97504
Ph: 541-757-8887
Fax: 541-757-8888
www.kasinc.com

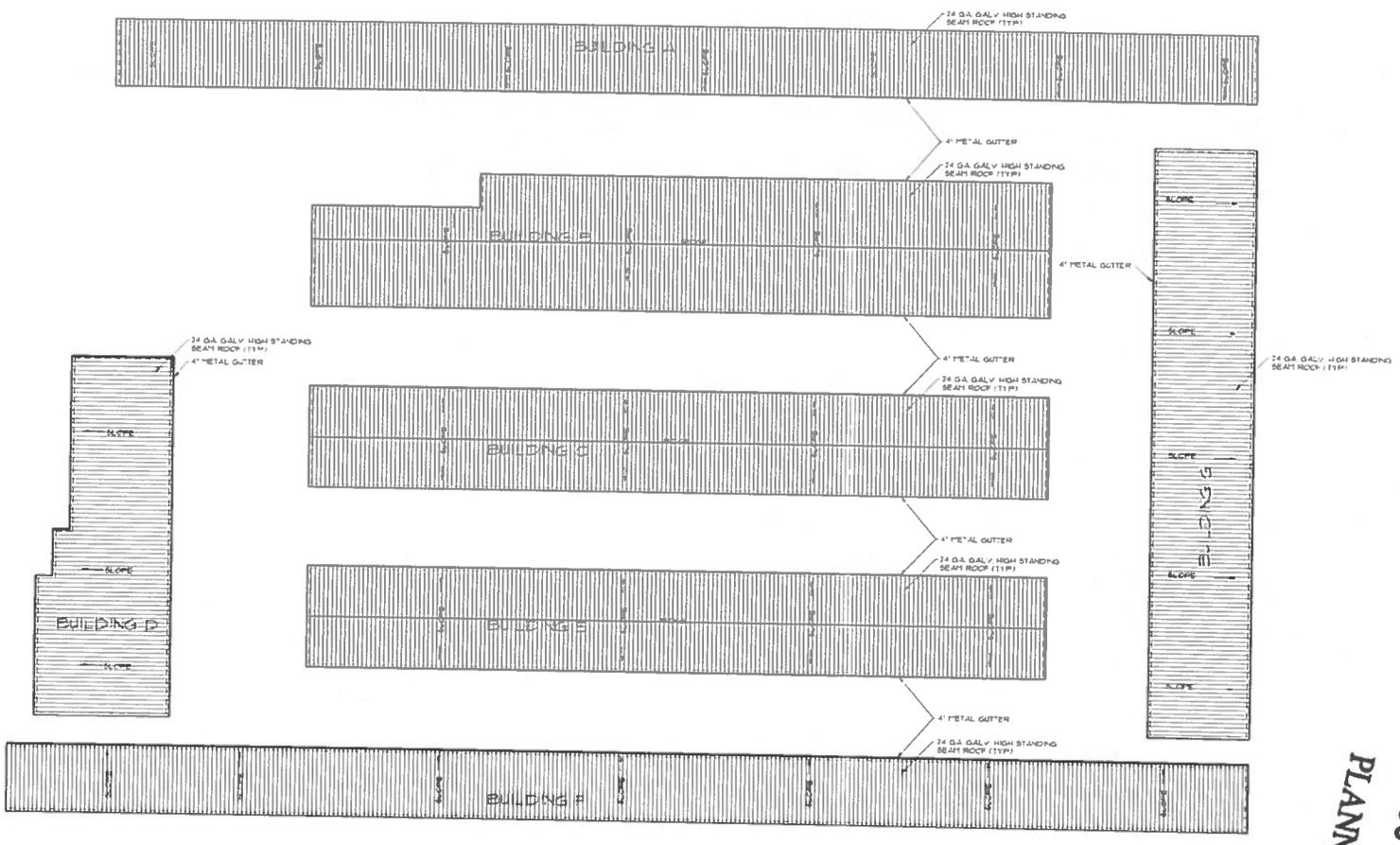
HUYCKE MINI-STORAGE
281-A ALABAMA ROAD
MEDFORD OREGON 97504

DATE	10/04/2018
PROJECT	HUYCKE MINI-STORAGE
DRAWN BY	DAVID HAYS
CHECKED BY	DAVID HAYS
SCALE	AS SHOWN
SHEET NO.	A2
TOTAL SHEETS	6

ROOF PLAN

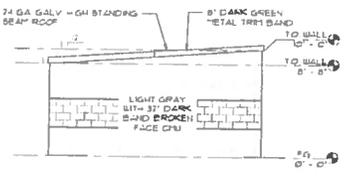
SHEET NO. A2

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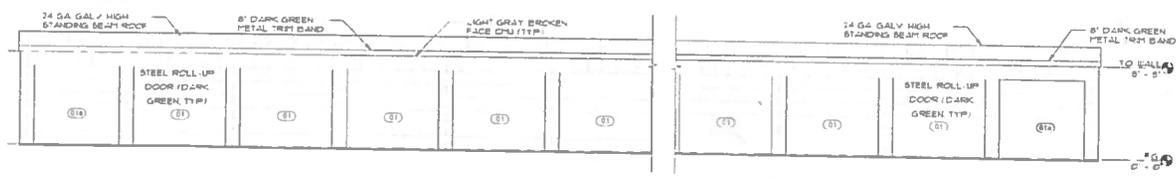


ROOF PLAN
SCALE 1/8" = 1'

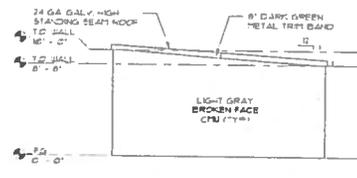
CITY OF MEDFORD
EXHIBIT # F-2
FILE # AC-18-147



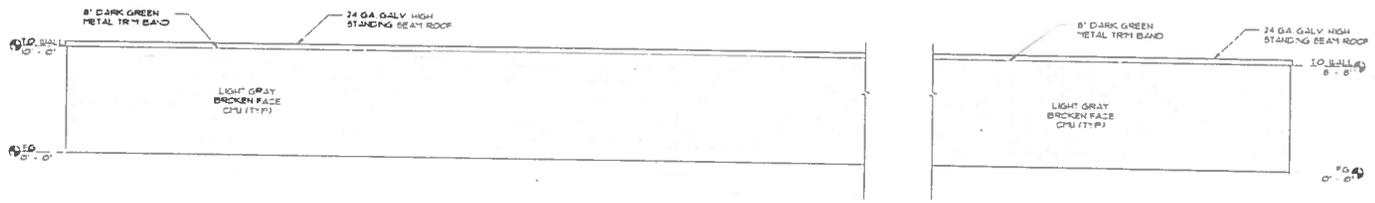
BUILDING F NORTH ELEVATION
SCALE: 3/16" = 1'



BUILDING F EAST ELEVATION
SCALE: 3/16" = 1'



BUILDING F SOUTH ELEVATION
SCALE: 3/16" = 1'



BUILDING F WEST ELEVATION
SCALE: 3/16" = 1'

BUILDING F ELEVATIONS
SCALE: 3/16" = 1'

NOTE:
FOR OVERALL BUILDING LENGTH SEE
SITE AND FLOOR PLAN

DOOR SCHEDULE

(E1)	8014 STEEL ROLL-UP DOOR
(E10)	8014 STEEL ROLL-UP DOOR



KAS
STRUCTURAL
PLANNING
1101 NW 10TH ST
PORTLAND, OR 97227
PHONE: 503-255-1111
FAX: 503-255-1112
WWW.KASSTRUCTURAL.COM

HUYCKE
MINI-STORAGE
1000 W. 13TH AVE. SUITE 100
PORTLAND, OREGON 97204

NO.	DATE	DESCRIPTION

NOTATIONS
BUILDING
ELEVATIONS

SHEET NO
A3.3

6 OF 6

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FILE # AC-18-147



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AUGUST 25 2018

SHEET L 10

MICHELLE STARR ALL PLANT LANDSCAPE CONTRACTOR 100160

WORK BY OTHERS
PROVIDE CLEAN SUBGRADE FREE OF CONSTRUCTION RUBBLE,
CONCRETE, STAKE & REEK.

NOTES
MINIMUM 6" INFILTRATED SOIL/COMPOST BLEND
IN ALL LANDSCAPE AREAS
MINIMUM 2" BARK MULCH IN ALL LANDSCAPE PLANTERS

2 EUROPEAN BEECH
FAGUS SYLVATICA 1 1/4" CAL.

KONIKIWIWICK
ARIZO STARBUCK UNITS
1 GAL 3" D.C.
90 PLANTS TOTAL

7 PINK PRINCESS
ESCALONIA FRAGR. PINK PRINCESS
1 GAL

6 GOLDEN ELDERBERRY
SAMBONIA ACIDA-ROSBURGIANA
1 GAL

YELLOW WINDMILL
GEOPHYTA FILOSA
1 GAL 3" D.C. 48 PLANTS

PEARLBRUSH
RUBUS CALY CANADICUS
1 GAL 3" D.C.
116 PLANTS TOTAL

AZALIA
AZALIA HYBRID COMPOSIT
1 GAL
KORONAN DOWNWOOD
CORNUS KOUSSA 1 1/4" CAL.

PINK ANGELIA
ANGELIA RED BRANCHES
1 GAL

5 CHINESE VAR.
DAVIDI VIBURNUM
1 GAL

3 RHODO.
RHODODENDRON
SAPPORO 10246
1 GAL

5 DWARF PINE
PINUS CHAMPECULS
1 1/4" CAL.

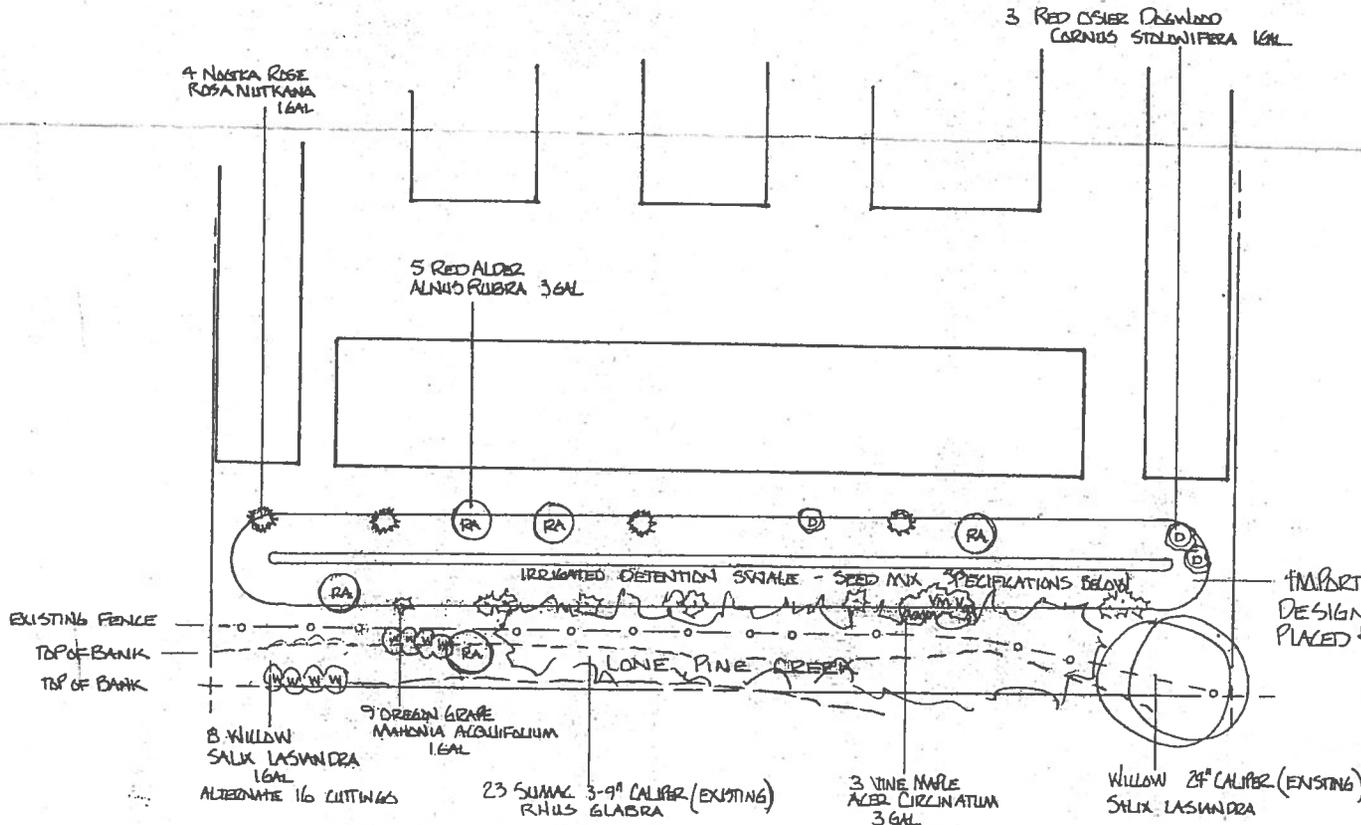
5 HOLLY LEAF OSMANTHUS
OSMANTHUS HEMISPHERICUS
TILICAPULUS
1 GAL

4 FRENCH LILAC
FRAXINUS LUSITANICA 2 GAL.

TOTAL LANDSCAPE AREA 7516 sq ft
HIGH WATER USE AREA 6 sq ft

SOIL PREPARATION SHALL MEET SECTION 10.0780 (6) (1) (4)

CITY OF MEDFORD
EXHIBIT # FE
FILE # AC-18-147



IMPORTED SOILS PER RVSS DESIGN MANUAL SPECIFICATIONS PLACED & GRADED BY OTHERS

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SHEET L 2.0

JULY 9, 2018
AUGUST 23, 2018

MICHAEL A. STARR, ALL PHASE
LANDSCAPE CONTRACTOR 10060

SUNMARK SEEDS INTERNATIONAL, INC.
PO Box 1210
Fairview OR 97024
503-241-7333
888-214-7333

CITY OF MEDFORD
EXHIBIT # 1
FILE # AC-18-147

Sunmark Native Riparian Mix

Acres: 1
Quantity: 43 lbs.

Botanical Name	Common Name	% by Weight	Seeds per lb. of mix	Seeds per sq. ft.	Actual % by Seed Size	PLS lbs. per acre	Proposed %
<i>Elymus glaucus</i>	Blue Wildrye	40%	44000	110,000	45.1%		25%
<i>Lolium brachytherum</i>	Meadow Brome	30%	25500	63,750	25.1%		70%
<i>Poa annua</i>	Annual Bluegrass	10%	9800	24,500	9.6%		5%
TOTALS:		100%	101300	258,250	100.00%	43.4	100%

Seeding Rate
1 PLS lbs. per 1000 sq. ft.
43 PLS lbs. per acre

Native Riparian provides a quick establishing native seed mix for riparian areas. This mix can withstand flooding and periods of drought.

SCALE
1" = 20'-0"

HUYCKE MINI-STORAGE
RIPARIAN & DETENTION POND

NATURAL SYSTEMS

779-2866

MEDFORD, OREGON

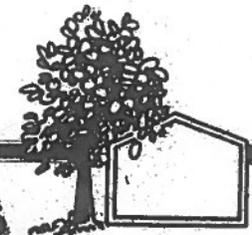
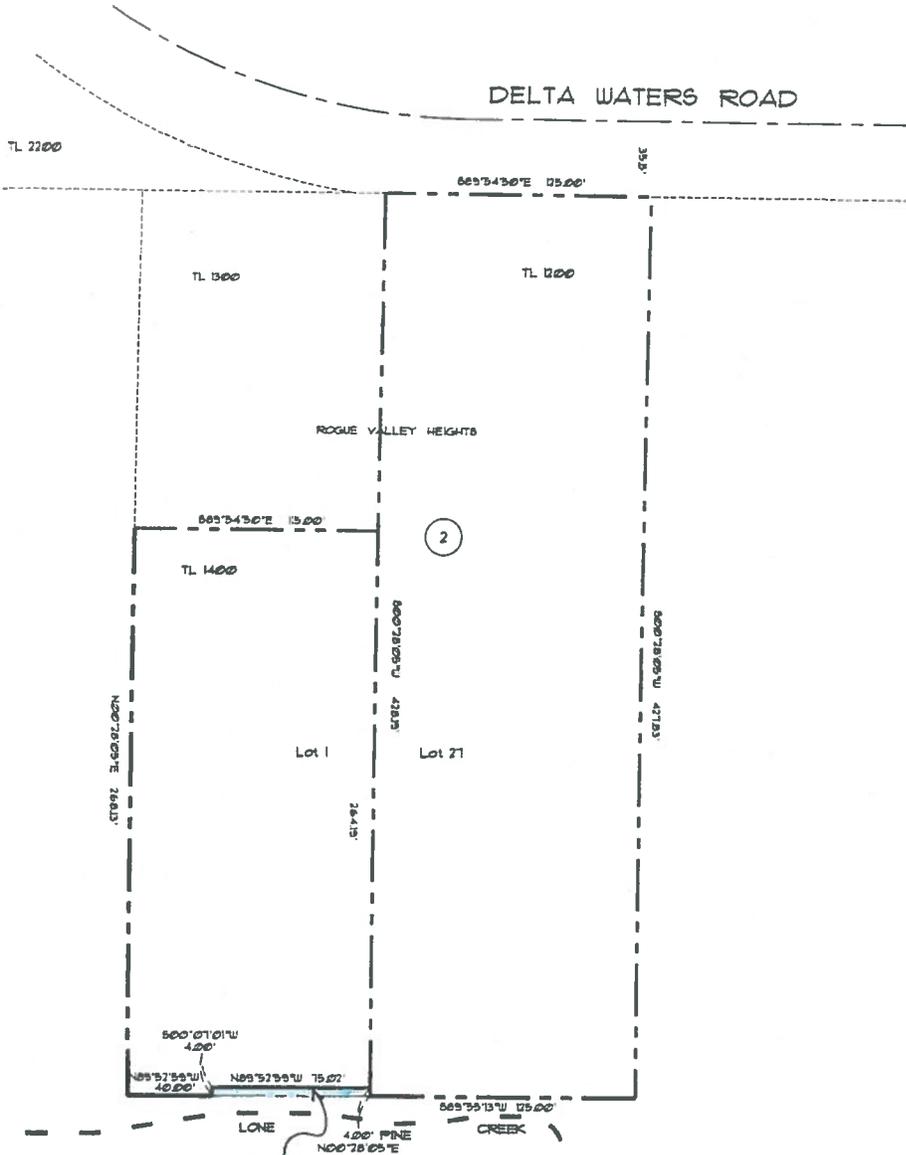




EXHIBIT MAP
 For:
Delta Waters Lenders, LLC
 823 Aldercreek Drive
 Medford, Oregon
 Located in:
 in the N.E. 1/4 of Sec. 18 (18AA)
 Township 37 South, Range 1 West, WM.,
 City of Medford, Jackson County, Oregon
 Taxlots 1200 & 1400

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Area Removed From Special Flood Hazard Areas
 SEE ATTACHED L.O. IN.A.

CITY OF MEDFORD
 EXHIBIT # H
 FILE # AC-18-147

REGISTERED
 PROFESSIONAL
 LAND SURVEYOR
 ELECTRONIC SIGNATURE
 OREGON
 2012, 2016
 DAVID M. MINNECI
 2348
 EXPIRES 12/31/18



BY: DAVID MINNECI	L5 2348
DATE	JULY 25, 2018
PROJECT	
PROJECT NO.	15-106
DRAWING FILE NO.	15106EXHIBIT
SCALE	1" = 40'
REVISION NO.	
REVISION DATE	
REVISION BY	
DATE	NAVD 88
DRAWN BY	DM
REVIEWED BY	

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APPLICANT'S QUESTIONNAIRE FINDINGS

Self Storage Facility

37-1w-18aa Tax Lots 1200, 1300, 1400

Section I- Narrative

Applicant proposes a 43,170sq. ft. +/-, 218 +/- unit self storage facility located on Delta Water Road. Presently the lots are being used for outside storage and have a few old buildings from bygone years.

- 1) Access will be taken from Delta Waters Road
- 2) A key code entry system is proposed to alleviate the need for an on-site manager.
- 3) Exteriors walls will be CMU Light grey with Dark green trim and doors.
- 4) An on-site restroom will be available during operating hours .
- 5) Paving will be used for all vehicular travel. Storm quality and detention shall be accomplished by the design of an Oregon licensed engineer.
- 6) All lots will be combined prior to final permits.
- 7) Trash cans or dumpsters will not be available for the clients use due to the amount of garbage that would be generated if they were.
- 8) Parking will be short term and in front of each individual rental unit. The spaces shown on the Site Plan are for demonstrative purposes only to show compliance with the LDO requirements.

At the request of the Planning Dept. and the Engineering Dept. a cross access easement will be provided at the North side of the property.

Additionally, a 10 foot P.U.E. will be provided at the north property line abutting Delta Waters Rd.

A Storm Water Quality feature will be built within the 50 foot riparian setback at the south property line. It will only use approximately 10 feet of the north side of the riparian.

Section II- Criterion No. 1

A) The property to the North is Delta Waters Rd. Across from that are numerous small retail businesses.

The property to the East is owned and used by Curtius/Huntley Plumbing Inc. as their place of business. The buildings are over thirty years old comprised of steel, block and wood.

The properties to the South are vacant.

The two properties to the West have two buildings on the northern lot. They are composed of a wood frame and wood sided building; both buildings are over thirty years old. The southern lot is vacant.

B) The project is consistent with numerous projects co-designed by Steel Building Systems and local design professionals. The use of block and steel provides a maintenance free exterior while prolonging the life of the project. The design more than complements the surrounding area by bringing a streetscape of colored block and metal design along with appealing street frontage landscape designed by Natural Systems Landscaping Co.

C) The architectural details of the different colored block as well as the use of gates, landscaping and other building and elevation changes in the project will give a pleasing drive by street view as well as break up the character of the use itself.

D) The entrance will utilize existing or install new street facilities. The project has one entrance to the facility which will be easily identifiable to a pedestrian looking for the entrance. Although pedestrians are historically not expected to use the facility, the vehicular traffic that the project will generate will be able to locate the entrance easily.

E) Due to the nature of the business, pedestrians are not expected to roam about the facility. Most customers will access their storage unit by a vehicle eliminating the need for defined pedestrian facilities or amenities. A sidewalk is provided to Delta Waters Rd. that will connect to the manager's office during business hours.

F) Pedestrian and vehicular traffic would access the site through a locked key coded access gate. The general public would not be able to access this project. Once inside the client would drive or walk to his storage unit. The secured premises provide an area that would prohibit the storage clients from entering, or a need to access adjacent sites. A pedestrian gate will remain locked during business hours; access will be by code or by the manager sending a signal to the gate to open.

"I"
2 of 4

- G) The property is void of any retainable vegetation at this time.
- H) The storm detention system/water quality treatment will be designed by CEC Engineering. Landscaping will be provided by Natural Systems Landscaping and will comply with the L.D.O. requirements of planting material.
- I) The landscaping design will provide a more than adequate system which will buffer the buildings from the street views.
- J) "Night Sky" Certified wall mount L.E.D. fixtures will be utilized throughout the project. The light will be mounted on buildings within the project therefore eliminating any light glare on neighboring properties.
- K) All signs will meet the current code. None are proposed at this time.
- L) Fencing will be utilized based on the gaps created by the building layout at the south property line. They will be chain link, eight feet tall with security wire located at the top. A gate will be provided to access the storm quality area. A block wall eight feet tall will be utilized at the building gap at the northwest corner.
- M) By nature a mini storage complex does not create any noise other than that of muffled vehicles. The project expects to have a lower noise output than most industrial uses. The projects noise levels will comply with Section 10.752-10.761 of the LDO.
- N) This project will provide the large expanse of homes in the Northeast Medford area a close accessible mini storage facility, which will reduce vehicular trips to other complexes. The abundant landscaping as well as the low maintenance required by the building materials used shall create a longlasting positive effect on the surrounding area. This concept has been built out in Medford and the surrounding area for over forty years with few if any complaints as to the degradation of the neighborhood. This is a low traffic, non-pedestrian, low noise polluting business that will fit perfectly with the surrounding and future surrounding developments.
- O) The applicant requests an exception to Section 10.426 Street Circulation Design and Connectivity. This request is based on Section 10.426 C) 2) b) "Environmental constraints including the presence of wetland or other body of water." Due to the presence of Lone Pine Creek at the south property line and it being in a Riparian area, recognized as a fish bearing creek and within the Floodplain, relief is requested from 10.426. Additionally, due to the North property lines' proximity to Delta Water Rd. it would not easily permit a newly created road extension at any point along Delta Waters Rd., relief is requested per 10.426 C) 2)d.

"T"
3 of 4

These properties are within a floodplain and a riparian area. Currently the applicant is finalizing the findings and applications for:

1. A Floodplain Development Permit
2. A LOMA Application.
3. A reduction of the riparian area per 10.927 B.
4. A C.U.P. for the Stormwater Quality system within a riparian area per 10.925

These applications and permits are extremely time consuming. Because of this, the applicant requests an approval of this project, as shown, with a stipulation that all required permits within the Flood Plain/Riparian area are approved prior to issuance of building permits.

P) A Type A Buffer Yard is required by code on half of the south property line due to the I-L /MFR zoning of the two abutting properties. A relief/adjustment to the Buffer yard requirements on the South East portion of the property is requested.

A building will be located approximately forty feet from the property line. It will utilize a continuous twelve foot tall block wall. Lone Pine Creek flows through the south property line which will add additional buffer area. The applicant has designed a complete landscaping plan that will be scrutinized and approved by R.V.S., the Engineering Dept. and Parks and Rec. for the required landscaping within the Water Quality Area. Additionally, the O.D.F.W. will also be in the approval loop for the landscaping within the Riparian area as well as the Planning Director being involved on the C.U.P. for the Reduction of the Riparian area. In all, a fifty foot strip of land will have five government agencies scrutinizing this section of the property for water quality, riparian enhancement and landscaping design.

The extent of buffering that will be created will exceed the intent of the L.D.O. as well as provide adequate buffering between the properties; therefore, approval of this adjustment is requested.

"I"
4 of 4

EXCEPTION REQUEST

RE: AC-18-147 / C.U.P. -18-148

MLDC 10.464

November 21, 2018

From: Bill Philp, Agent

To: Liz Conner, Planner

Dear Liz,

At the request of the Public Works Department I am submitting this request for an exception to the MLDO Sec. 10.464 Accessways.

The first paragraph of this section lists the purpose of the accessway is to provide safe and convenient pedestrian and bicycle accessway within and from new subdivisions, planned unit developments, shopping centers and industrial parks. This project and application is none of the above mentioned. It is a permitted use within an industrial zone. It is my belief that an accessway is not a requirement for this project.

However, I am submitting the following findings based on allowed exceptions to the code. Although Public Works did not clarify at which point or where the accessway should be located I assume they meant the Southern boundary in the Riparian area of Lone Pine Creek., due to the fact that this development will provide East/ West access by means of a sidewalk on the Northern property area. Again, I make the assumption that they are requesting the Riparian Area.

(1) (A)

To be able to build a bridge capable of pedestrian and bicycles across Lone Pine Creek would require numerous permits from multiple agencies. I doubt if a permit is even obtainable due to the nature of the access which would have to cross a fish bearing Riparian corridor that is. This along with the cost of construction, engineering, environmental studies and applications would deem this infeasible and inappropriate.

CITY OF MEDFORD
EXHIBIT # J of 2
FILE # AC-18-147

(1) (b)

Due to the nature of the abutting existing development (Lone Pine Creek) and private property there is no possibility to connect the proposed development to the abutting property without building within a Riparian Corridor or trespassing across private property.

(1) (c)

As mentioned above the only way feasible to connect to the property to the south would be to cross a natural area with significant natural habitat and construction would be incompatible with the protection of the natural values.

(1) (d)

The accessway would have to be built within and cross land that is designated in a flood way. Hoffbuhr and Assc did a thorough and comprehensive study of the Lone Pine Creek area within this project to identify the flood plain. A LOMA was submitted and approved by F.E.M.A. The creek itself is a flood channel. The designated use of the southern boundary is a flood way making a bridge incompatible with the use.

(1) (e)

If by chance a pedestrian bridge was not an option then creating a pathway within the Riparian Corridor would be an even poorer choice. In order to circumvent a minimum eight foot deep water channel a cross slope path would destroy significant Riparian area. Additionally, even if a cross slope path on both sides of the creek were built it would not eliminate the inability to cross the creek during times of a water flow event.

In conclusion, to provide a North/South Accessway at the southern end of this property would be infeasible and inappropriate as well as create an extreme financial hardship to the owners and destroy an existing Riparian Corridor.

Therefore, I am requesting an exception to Section 10.464 of the MLDO.

Respectfully,

Bill Philp

"J"
2 of 2

CITY



Federal Emergency Management Agency

Washington, D.C. 20472

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OCT 04 2018

PLANNING DEPT.

LETTER OF MAP AMENDMENT DETERMINATION DOCUMENT (REMOVAL)

COMMUNITY AND MAP PANEL INFORMATION		LEGAL PROPERTY DESCRIPTION
COMMUNITY	CITY OF MEDFORD, JACKSON COUNTY, OREGON	A portion of Lot 27, Block 2, Rogue Valley Heights, as described in the Sheriff's Deed recorded 2013-039430, in the Office of the County Clerk, Jackson County, Oregon The portion of property is more particularly described by the following metes and bounds:
	COMMUNITY NO.: 410096	
AFFECTED MAP PANEL	NUMBER: 41029C1976F	
	DATE: 5/3/2011	
FLOODING SOURCE: LONE PINE CREEK		APPROXIMATE LATITUDE & LONGITUDE OF PROPERTY: 42.359589, -122.857869 SOURCE OF LAT & LONG: LOMA LOGIC DATUM: NAD 83

DETERMINATION

LOT	BLOCK/SECTION	SUBDIVISION	STREET	OUTCOME WHAT IS REMOVED FROM THE SFHA	FLOOD ZONE	1% ANNUAL CHANCE FLOOD ELEVATION (NAVD 88)	LOWEST ADJACENT GRADE ELEVATION (NAVD 88)	LOWEST LOT ELEVATION (NAVD 88)
27	2	Rogue Valley Heights	1884 Delta Waters Road	Portion of Property	X (unshaded)	--	-	1342.7 feet

Special Flood Hazard Area (SFHA) - The SFHA is an area that would be inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood).

ADDITIONAL CONSIDERATIONS (Please refer to the appropriate section on Attachment 1 for the additional considerations listed below)

LEGAL PROPERTY DESCRIPTION
PORTIONS REMAIN IN THE SFHA

This document provides the Federal Emergency Management Agency's determination regarding a request for a Letter of Map Amendment for the property described above. Using the information submitted and the effective National Flood Insurance Program (NFIP) map, we have determined that the described portion(s) of the property(ies) is/are not located in the SFHA, an area inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood). This document amends the effective NFIP map to remove the subject property from the SFHA located on the effective NFIP map; therefore, the Federal mandatory flood insurance requirement does not apply. However, the lender has the option to continue the flood insurance requirement to protect its financial risk on the loan. A Preferred Risk Policy (PRP) is available for buildings located outside the SFHA. Information about the PRP and how one can apply is enclosed.

This determination is based on the flood data presently available. The enclosed documents provide additional information regarding this determination. If you have any questions about this document, please contact the FEMA Map Information eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, Engineering Library, 3601 Eisenhower Ave Ste 500, Alexandria, VA 22304-6426.

Luis V. Rodriguez, P.E., Director
Engineering and Modeling Division
Federal Insurance and Mitigation Administration

CITY OF MEDFORD

EXHIBIT # K 1 of 5
FILE # AC-18-147

X



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP AMENDMENT DETERMINATION DOCUMENT (REMOVAL)

ATTACHMENT 1 (ADDITIONAL CONSIDERATIONS)

LEGAL PROPERTY DESCRIPTION (CONTINUED)

BEGINNING at the Southwest corner of Lot 27 in Block 2 of ROGUE VALLEY HEIGHTS in Jackson County, Oregon, according to the Official Plat thereof, now of record, thence, along the Westerly line of said lot, North 00°28'05" East 428.19 feet (Record North 00°26'22" East 428.22 feet) to the Northwest corner of PARCEL 2 of that tract described in Document No. 2013-039430, Official Records of said Jackson County; thence, along the Northerly line of said tract, South 89°54'50" East 125.00 feet to the Northeast corner thereof; thence, along the Easterly line of said tract, South 00°26'22" West 428.25 feet to the Southeast corner thereof; thence, along the Southerly line of said Lot 27, North 89°53'14" West (Record North 89°52' West) 125.00 feet to the Point of Beginning.

PORTIONS OF THE PROPERTY REMAIN IN THE SFHA (This Additional Consideration applies to the preceding 1 Property.)

Portions of this property, but not the subject of the Determination/Comment document, may remain in the Special Flood Hazard Area. Therefore, any future construction or substantial improvement on the property remains subject to Federal, State/Commonwealth, and local regulations for floodplain management.

This attachment provides additional information regarding this request. If you have any questions about this attachment, please contact the FEMA Map Information eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, Engineering Library, 3601 Eisenhower Ave Ste 500, Alexandria, VA 22304-6426.

Luis V. Rodriguez, P.E., Director
Engineering and Modeling Division

"K"
2 of 5



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP AMENDMENT DETERMINATION DOCUMENT (NON-REMOVAL)

COMMUNITY AND MAP PANEL INFORMATION

LEGAL PROPERTY DESCRIPTION

COMMUNITY	CITY OF MEDFORD, JACKSON COUNTY, OREGON	A portion of Lot 1, Block 2, Rogue Valley Heights, as described in the Sheriff's Deed recorded 2013-039430, in the Office of the County Clerk, Jackson County, Oregon The portion of property is more particularly described by the following metes and bounds:
	COMMUNITY NO.: 410096	
AFFECTED MAP PANEL	NUMBER: 41029C1976F	
	DATE: 5/3/2011	

FLOODING SOURCE: LONE PINE CREEK	APPROXIMATE LATITUDE & LONGITUDE OF PROPERTY: 42.359382, -122.858323 SOURCE OF LAT & LONG: LOMA LOGIC DATUM: NAD 83
---	--

DETERMINATION

LOT	BLOCK/SECTION	SUBDIVISION	STREET	OUTCOME WHAT IS NOT REMOVED FROM THE SFHA	FLOOD ZONE	1% ANNUAL CHANCE FLOOD ELEVATION (NAVD 88)	LOWEST ADJACENT GRADE ELEVATION (NAVD 88)	LOWEST LOT ELEVATION (NAVD 88)
1	2	Rogue Valley Heights	1884 Delta Waters Road	Portion of Property	AE	1341.7 feet	-	1341.1 feet

Special Flood Hazard Area (SFHA) - The SFHA is an area that would be inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood).

ADDITIONAL CONSIDERATIONS (Please refer to the appropriate section on Attachment 1 for the additional considerations listed below.)

LEGAL PROPERTY DESCRIPTION

This document provides the Federal Emergency Management Agency's determination regarding a request for a Letter of Map Amendment for the property described above. Using the information submitted and the effective National Flood Insurance Program (NFIP) map, we have determined that the described portion(s) of the property(ies) is/are located in the SFHA, an area inundated by the flood having a 1-percent chance of being equaled or exceeded in any given year (base flood). Therefore, flood insurance is required for the property described above. The lowest adjacent grade elevation to a structure must be at or above the Base Flood Elevation for a structure to be outside of the SFHA.

This determination is based on the flood data presently available. The enclosed documents provide additional information regarding this determination and information regarding your options for obtaining a Letter of Map Amendment. If you have any questions about this document, please contact the FEMA Map Information eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, Engineering Library, 3601 Eisenhower Ave Ste 500, Alexandria, VA 22304-6426.

Luis V. Rodriguez, P.E., Director
 Engineering and Modeling Division
 Federal Insurance and Mitigation Administration

"K"
3 of 5



Federal Emergency Management Agency

Washington, D.C. 20472

LETTER OF MAP AMENDMENT DETERMINATION DOCUMENT (NON-REMOVAL)

ATTACHMENT 1 (ADDITIONAL CONSIDERATIONS)

LEGAL PROPERTY DESCRIPTION (CONTINUED)

Commencing at the Southeast corner of Lot 1 in Block 2 of ROGUE VALLEY HEIGHTS in Jackson County, Oregon, according to the Official Plat thereof, now of record; thence, along the Easterly line of said Lot, North 00°28'05" East 4.00 feet to the POINT OF BEGINNING; thence, leaving said Easterly line, North 89°52'59" West 75.02 feet; thence South 00°07'01" West 4.00 feet to the Southerly line of said Lot 1; thence, along said Southerly line, North 89°52'59" West (Record North 89°52' West) 40.00 feet to the Southwest corner of TRACT B of PARCEL 1 of that tract described in Document No. 2013-039430, Official Records of said Jackson County; thence, along the Westerly line of said tract, North 00°28'05" East 268.13 feet (Record North 00°26'22" East 268.10 feet) to the Northwest corner thereof; thence, along the Northerly line of said tract, North 89°54'50" East 115.00 feet to the Northeast corner thereof; thence, along the Easterly line of said tract, South 00°28'05' West (Record South 00°26'22" West) 264.19 feet to the Point of Beginning.

This attachment provides additional information regarding this request. If you have any questions about this attachment, please contact the FEMA Map Information eXchange (FMIX) toll free at (877) 336-2627 (877-FEMA MAP) or by letter addressed to the Federal Emergency Management Agency, Engineering Library, 3601 Eisenhower Ave Ste 500, Alexandria, VA 22304-6426.

Luis V. Rodriguez, P.E., Director
Engineering and Modeling Division
Federal Insurance and Mitigation Administration

"K"
4 of 5



EXHIBIT MAP

For:

Delta Waters Lenders, LLC

823 Aldercreek Drive

Medford, Oregon

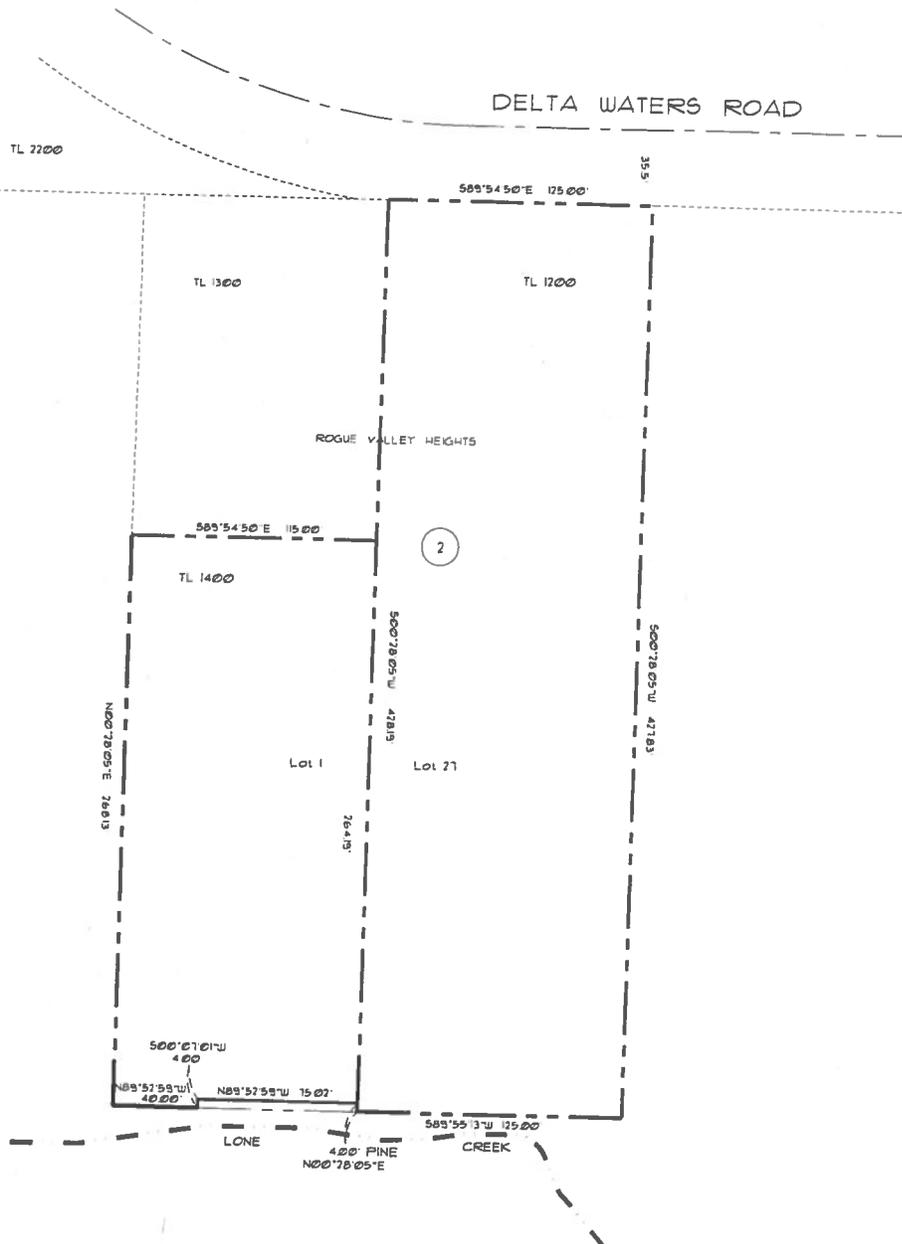
Located in:

in the N.E. 1/4 of Sec. 18 (18AA)

Township 37 South, Range 1 West, WM.,

City of Medford, Jackson County, Oregon

Taxlots 1200 & 1400

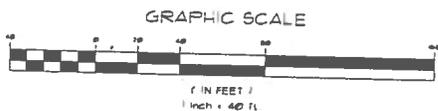


REGISTERED PROFESSIONAL LAND SURVEYOR

ELECTRONIC SIGNATURE

OREGON
JULY 21, 1988
DAVID M. MINNECI
2349

EXPIRES 12/31/18



37 1W 18AA - TL 1200 & 1400

BY	DAVID MINNECI	LS 2349
DATE		JULY 25, 2018
PROJECT		
PROJECT NO		15-106
DRAWING FILE NO		1506EXDUG
SCALE		1" = 40'
REVISION NO		
REVISION DATE		
BASIS OF BEARING		
DATUM		NAVD 88
DRAWN BY		DMM
REVIEWED BY		

"K"
50FS



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

Revised Date: 1/10/2019
File Number: AC-18-147/CUP-18-148
Reference: PA-17-055

PUBLIC WORKS DEPARTMENT STAFF REPORT 1862/1884 Delta Waters Road Self-Storage Units

- Project:** AC-18-147: Consideration to construct a 43,170 square foot self-storage facility on a 2.37 acres parcel zoned I-L (Light Industrial).
CUP-18-148: A Conditional Use Permit to allow storm water facilities within the Riparian Corridor of Lone Pine Creek.
- Location:** AC-18-147/CUP-18-148: Located approximately 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62) (371W18AA TL 1200-1400).
- Applicant:** Delta Waters Lenders; Agent; Bill Philp; Planner: Liz Conner.

NOTE:

The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The Applicant may provide security for 120% of the improvements prior to issuance of building permits. Construction plans for the improvements will need to be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas.
- Certification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

P:\Staff Reports\AC\2018\AC-18-147_CUP-18-148 Delta Waters Road - Mini-Storage Units (TLs 1200, 1300 & 1400)\AC-18-147_CUP-18-148 Staff Report-Revised2.docx Page 1 of 8

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

CITY OF MEDFORD
EXHIBIT # L
FILE # AC-18-147

A. STREETS

1. Dedications

Delta Waters Road is classified as a Major Arterial street, and in accordance with Medford Land Development Code (MLDC) Section 10.428, requires a total right-of-way width of 100-feet. The developer shall dedicate for public right-of-way, sufficient width of land along the entire frontage of this development to comply with the half width of right-of-way, which is 50-feet. **The Developer's surveyor shall verify the amount of additional right-of-way required.**

The developer will receive SSDC (Street System Development Charge) credits for the public right-of-way dedication on **Delta Waters Road**, per the methodology established by the MLDC 3.815. **Should the developer elect to have the value of the land be determined by an appraisal, a letter to that effect must be submitted to the City Engineer within sixty (60) calendar days of the date of the Final Order of the Planning Commission. The City will then select an appraiser, and a cash deposit will be required as stated in Section 3.815.**

In accordance with MLDC 10.471, **the property owner shall dedicate a 10-foot wide public utility easement (PUE) adjacent to the new right-of-way line along this Developments frontage.**

The right-of-way and easement dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: the right-of-way and easement dedication, including an exhibit map; a copy of a current Lot Book Report, Preliminary Title Report, or Title Policy; a mathematical closure report (if applicable), and the Planning Department File Number; for review and City Engineer acceptance signature prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the right-of-way and PUE area.

2. Public Improvements

a. Public Streets

Delta Waters Road – All street section improvements, with the exception of a planter strip and sidewalk, have been completed in close conformance with current standards, including pavement, curb and gutter. **A 5-foot wide sidewalk with a 10-foot planter strip will be required along this developments frontage, MLDC Section 10.428. Sidewalk shall transition to be curbtight on east end to align with sidewalk on adjacent property.**

b. Street Lights and Signing

No additional street lights are required.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer's contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing

signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this frontage.

3. Access and Circulation

Driveway access to the proposed development site shall comply with MLDC 10.550. The driveway access on Delta Waters Road shall be restricted to right-in/right-out only by extending traffic separator median at least 50-feet beyond the driveway in both directions.

Cross-access easements shall be required to be granted to tax lots 2200, 1500, and 1100 in accordance with MLDC 10.550. Site design shall accommodate the future use of such accesses.

The cross access easement shall be paved in order to accommodate use of the easement and in accordance with MLDC 10.746, which requires all vehicle maneuvering areas to be paved.

The applicant has provided acceptable findings addressing the constraints that allow block lengths to exceed maximum standards listed in MLDC 10.426.C.2. Also, the applicant has provided acceptable findings addressing how a public accessway is infeasible or inappropriate in accordance with section 10.464.

4. Section 10.668 Analysis

To support a condition of development that an applicant dedicate land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and

supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and storm drains to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining "rough proportionality" have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Delta Waters Road:

The additional right-of-way on Delta Waters Road will provide the needed width for a future planter strip and sidewalk. Delta Waters Road is a 30 mile per hour facility, which currently carries approximately 13,200 vehicles per day. The 10-foot planter strip moves pedestrians a safe distance from the edge of the roadway. Delta Waters Road will be a primary route for pedestrians traveling to and from this development. The development shall construct approximately 140 linear feet of sidewalk along the frontage of the property. All developments in Medford are required to construct frontage sidewalk.

The City assesses System Development Charges (SDCs) to help pay for acquisition of right-of-way and construction of additional Arterial & Collector Street capacity required as a result of new development. Because a mechanism exists in the form of SDC credit for right-of-way dedication and street improvements in accordance with Medford Municipal Code (MMC) 3.815 and other applicable parts of the Code, to fairly compensate the applicant, the conditions of MLDC, Section 10.668 are satisfied.

Dedication of the Public Utility Easements (PUE) will benefit development by providing public utility services, which are out of the roadway and more readily available to each lot or building being served. The additional traffic of all modes of travel generated by this proposed development supports the dedication and improvements for all modes of travel and utilities. As indicated above, the area required to be dedicated for this development is necessary and roughly proportional to that required in similar developments to provide a transportation system that meets the needs for urban level services.

Cross Access Easements:

Additional information addressing the nexus and proportionality of the cross access easements are addressed in a separate Memorandum from the City Attorney's Office dated January 9, 2019. Public Works concurs with this memorandum.

B. SANITARY SEWERS

The proposed development is situated within the Medford Sewer Service area. The Developer shall provide one separate individual service lateral to the site or ensure that the site is served by an individual service lateral. All unused laterals adjacent and stubbed to the development shall be capped at the main.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Storm Drainage Conditions

Developer shall make improvements to their side of Lone Pine Creek to convey the 25-year storm with one foot of freeboard, or provide calculations showing this condition now exists.

Developer shall provide a 30-foot easement for Lone Pine Creek measured from the centerline of the Creek.

Developer shall provide riparian plantings meeting Oregon Department of Fish and Wildlife (ODFW) standards within the Creek easement.

3. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

"2"

4. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

5. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design Engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

6. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. General Conditions

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

3. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

P:\Staff Reports\AC\2018\AC-18-147_CUP-18-148 Delta Waters Road - Mini-Storage Units (TLs 1200, 1300 & 1400)\AC-18-147_CUP-18-148 Staff Report-Revised2.docx Page 6 of 8

PUBLIC WORKS DEPARTMENT
ENGINEERING & DEVELOPMENT DIVISION

200 S. IVY STREET
MEDFORD, OREGON 97501
www.ci.medford.or.us

TELEPHONE (541) 774-2100
FAX (541) 774-2552

4. System Development Charges (SDC)

Buildings in this development are subject to SDC fees. All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Jodi K Cope, Reviewed by: Doug Burroughs, Revised by: Jodi K Cope/Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

1862/1884 Delta Waters Road

Self-Storage Units

AC-18-147/CUP-18-148

A. Streets

1. Street Dedications to the Public:

- Delta Waters Road – Dedicate additional right-of-way.
- Dedicate 10-foot Public Utility Easement (PUE) along frontage.

2. Improvements:

Public Streets

- No public improvements are required along Delta Waters Road, aside from 5-foot wide sidewalk and 10-foot planter strip.

Lighting and Signing

- No additional street lights are required.

Access and Circulation

- Driveway access to the proposed development site shall comply with MLDC 10.550. The driveway access on Delta Waters Road shall be restricted to right-in/right-out only.
- Cross-access easements shall be required.
- The cross access easements shall be paved.

Other

- There is no pavement moratorium currently in effect on Delta Waters Road.

B. Sanitary Sewer:

- Ensure or construct separate individual sanitary sewer connection.
- Cap remaining unused laterals at the main.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Comply with Storm Drainage Conditions.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide Engineers certification of stormwater facility construction.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.

P:\Staff Reports\AC\2018\AC-18-147_CUP-18-148 Delta Waters Road - Mini-Storage Units (TLs 1200, 1300 & 1400)\AC-18-147_CUP-18-148 Staff Report-Revised2.docx Page 8 of 8

PUBLIC WORKS DEPARTMENT
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MEDFORD, OREGON 97501
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TELEPHONE (541) 774-2100
FAX (541) 774-2552

"L"

A 2



Medford Fire-Rescue Land Development Report

Review/Project Information

Reviewed By: Kleinberg, Greg

Review Date: 11/14/2018
Meeting Date: 11/21/2018

LD File #: AC18147/CUP18148

Planner: Liz Conner

Applicant: Delta Waters Lenders

Project Location: 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62).

Project Description: AC18147: Consideration to construct a 43,170 square foot self-storage facility on a 2.37 acres parcel zoned I-L (Light Industrial)

CUP18148: A Conditional Use Permit to allow storm water facilities within the Riparian Corridor of Lone Pine Creek

Specific Development Requirements for Access & Water Supply

CITY OF MEDFORD
EXHIBIT # M
FILE # AC-18-147

Reference	Comments	Description	Conditions
OFC 508.5	Fire hydrant location approved as submitted.	Fire hydrants with reflectors will be required for this project.	The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site. Plans and specifications for fire hydrant system shall be submitted to Medford Fire-Rescue for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).
OFC 104.9	Horizontal dry standpipe system requirements.	In lieu of internal fire hydrants, a horizontal standpipe system is required for this project. Prior to construction the proposed standpipe system shall be approved by the Fire Marshal (See Medford Handout) and meet NFPA 14 requirements.	The Fire Department Connection (FDC) shall be located away from the building (out of the collapse zone if possible) and within 75 feet of a fire hydrant. The fire hydrant and fire department connection shall be located on the same side of the fire department access route.
OFC 503.2.1	Fire apparatus access road/fire lane requirements.	Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 60,000 pounds. (See also OFC 503.4; D102.1)	The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).
OFC 503	Electric gate requirements.	Access control devices must be approved by Medford Fire Department. All gates shall have approved locking devices. Manual gates shall have a lock connected to a long length of chain. Automatic gates shall be equipped with an approved emergency services activated opening device (radio frequency microphone click from fire engines opens gate). OFC 503.1; 503.4; 503.5; 503.6	
OFC 903	A fire sprinkler system is required for a Group S-1 occupancy that is used for the storage of upholstered furniture or mattresses when the fire area exceeds 2,500 sq. ft.	Fire sprinkler system requirement information.	Where a fire sprinkler system is required, it shall meet the requirements of the Oregon Fire Code and the applicable National Fire Protection Association (NFPA) Standard.
	Consult the Medford Water Commission for proper water meter sizing for fire sprinkler systems.	Consult the Medford Water Commission for proper water meter sizing for fire sprinkler systems.	

Construction General Information/Requirements

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300

www.medfordfirerescue.org

Memo



To: Elizabeth Conner, Planning Department
From: Chad Wiltrout, Building Department (541) 774-2363
CC: Delta Waters Lenders, Applicant; Bill Philp, Agent
Date: November 20, 2018
Re: November 21, 2018 LDC Meeting Item #2: AC-18-147/CUP-18-148; (Previous app PA-17-055)

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit will be required if more than 50 cubic yards is disturbed and/or utilities installed.
4. A separate demolition permit will be required for demolition of any structures not shown on the plot plan.

Comments:

5. The building and building appurtenances shall be designed by an Oregon licensed design professional in accordance with 107.1 and 107.3.4 OSSC (for buildings over 4000 sqft.).
6. A code analysis providing means of egress plan, type of construction, occupancy classification, occupant load, notation of sprinkled or non-sprinkled, fire protection systems, fuel loading/fire areas, etc. shall be required.
7. Building construction shall comply with table 602 and section 705 OSSC for fire separation distance to adjacent buildings and property lines.
8. Modular buildings are required to comply with the Oregon Insignia provisions of OAR 918-674.

9. Self-service storage facilities shall provide accessible individual self-storage spaces in accordance with table 1108.3.OSSC.
10. A geotechnical engineer may be required to provide a design for soils at building locations pursuant to 1803 or the Oregon Structural Specialty Code (usually buildings over 4,000 sf.)
11. Each building - unit cluster - will be a separate permit.
12. The city provided special inspection statement form for inspections may be required by Chapter 17 of the OSSC.
13. Com-check forms shall be required for lighting, mechanical equipment and exterior envelope to show energy efficiency compliance with the 2014 Oregon Energy Efficiency Specialty Code (for any conditioned spaces).
14. Construction located in the flood hazard zone shall comply with ASCE7-10, ASCE 24, and OSSC Section 1612.



City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Delta Waters Secure RV Storage
File no. PA-17-055 (1862 and 1884 Delta Waters Drive)
To Praline McCormack, Planner II
From Carla Angeli Paladino, Principal Planner, Floodplain Coordinator
Date May 16, 2017

SITE CHARACTERISTICS

- Light Industrial (L-I) zoning district; General Industrial (GI) General Land Use Plan designation
- Lone Pine Creek along the southern property boundaries
- Base Flood Elevations established (approximately 1,341 to 1,342 feet)
- Riparian corridor established along the creek
- Portions of the south property lines contain both the 0.2% and 1% Annual Chance Flood Hazard Area (previously known as the 100-year and 500-year floodplains)
- FIRM panel 41029C 1976F effective May 3, 2011

FLOODPLAIN PERMIT REQUIREMENTS

Project Proposal

A pre-application conference to review the construction of a 97-space recreational vehicle storage lot (12 spaces enclosed and 85 covered) including a wash bay and clean out station.

Floodplain Regulations

The Medford Floodplain regulations are found in Sections 9.701-9.707 of the Municipal Code.

A Floodplain Development Permit is required prior to development in the Special Flood Hazard Areas.

The topographic survey provided shows the elevation for the top of bank of Lone Pine Creek. This survey seems to indicate the floodplain is contained near or within the banks of the creek. Verification of the floodplain boundaries on a survey map will help to identify these special hazard areas.

The riparian corridor regulations are found in Sections 10.920-10.928 of the Municipal Code. Future site plans shall identify this elevation information and identify the riparian corridor setback of 50 feet from the top of bank per Section 10.923. Existing fencing or structures within this area also need to be identified and noted if they will be removed or relocated outside of the riparian corridor. A landscape plan documenting the existing trees and vegetation along the creek is needed. The canopy structure proposed along the southern property line will need to be relocated outside of the riparian corridor setback. It appears that if the riparian corridor setback is maintained on the property that there will not be any structures proposed in the floodplain. Information from a licensed engineer or surveyor confirming this is requested.

The formal application shall identify what is proposed near the creek (grading, fill, additional vegetation, etc.).

Floodplain Permit

If structures are proposed in the Special Flood Hazard Area, submit a floodplain application and fee (\$150) along with submittal requirements identified in Section 9.705 (C). An elevation certificate is required at the time of building permit submittal, during construction, and prior to certificate of occupancy.

Submit copies of all necessary permits from other governmental agencies from which approval is required prior to start of construction.

Construction shall be in compliance with applicable building and fire codes.

Expiration of Floodplain Permit

A floodplain Development Permit shall become invalid unless work is started within 180 days after its issuance. Extensions for periods of not more than 180 days each shall be requested in writing.

"10"
2 of 2

20



Planning Department

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Date: August 22, 2018
To: Matt Brinkley, AICP, CFM, Planning Director
From: Liz Conner, CFM, Planner II
RE: 1862/1884 Delta Waters Road – Riparian Corridor Reduction (GF-18-096)

Request:

Consideration of a request to reduce or deviate from the riparian corridor boundary provisions, pursuant to MLDC 10.927, for a proposed development consisting of the construction of a mini storage facility and RV storage on three parcels totaling 2.36 acres located approximately 321 feet east of Crater Lake Highway on the south side of Delta Waters Road within an I-L/AA/A-C (Light Industrial, Airport Approach Overlay, Airport Area of Concern Overlay) zoning district (371W18AA1200, 1300, 1400).

Background:

The subject property consists of three contiguous parcels totaling approximately 2.3 acres, and is located on Delta Water Road between Crater Lake Hwy and Crater Lake Avenue. The southern portion of the property is encumbered by the riparian corridor of Lone Pine Creek which runs along the southern boundary of the parcels. Per MLDC 10.922, Lone Pine Creek is identified as a protected waterway within the City. As such, a 50-foot riparian corridor - measured horizontally from the top-of-bank on both sides of the creek - is applied to the section of Lone Pine Creek abutting the lot, restricting development within this established corridor. The creek's northerly top-of-bank encroaches slightly within the boundary of the subject lot, with the 50-foot riparian corridor area covering a significant portion of the lot.

Per MLDC 10.927, titled *Riparian Corridors, Reduction or Deviation*; a 50-foot riparian corridor may be reduced if a request to reduce the setback has been approved. MLDC 10.927 reads as follows:

A request to reduce or deviate from the riparian corridor boundary provisions of this section may be submitted to the Planning Director or designee for consideration. A deviation request may be approved as long as equal or better protection of the riparian area will be ensured through a plan for restoration, enhancement, or similar means. Such a plan shall be submitted to the Oregon Department of Fish and Wildlife for a habitat mitigation recommendation pursuant to O.A.R. 635-415 "Fish and Wildlife Habitat Mitigation Policy." In no case shall activities prohibited in Section 10.926 (1) through (3), "Prohibited

Activities within Riparian Corridors" be located any closer than 25 feet from the top-of-bank. The Planning Commission shall be kept advised of the outcome of deviation or reduction requests. Any decision of the Planning Director may be appealed to the City Council as provided in Chapter 10 of the Code of Medford.

Along with the letter for the riparian reduction request (Exhibit A), the applicant has included a letter of recommendation from Oregon Department of Fish and Wildlife (ODFW) (Exhibit B) and a Riparian Planting Plan prepared by a State of Oregon registered all-phase landscape contractor (Exhibit C) consistent with the requirements outlined in MLDC 10.927.

As stated in the submitted letter and illustrated in the submitted Riparian Planting Plan, the applicant is requesting a riparian reduction of 25 feet, which will allow some encroachment to accommodate a bio-swale for storm water treatment and a portion of the storage facility structure.

The applicant's submitted Riparian Planting Plan shows the remaining 25 feet of riparian area to be restored and enhanced within the top of bank. The bio-swale will be planted and irrigated to increase native vegetation within the Riparian Corridor.

Pursuant to MLDC 10.927 cited above, the applicant forwarded the Riparian Planting Plan to ODFW for a habitat mitigation recommendation.

Per the ODFW letter (Exhibit B), the proposed Riparian Planting Plan and addition of bio-swale will provide significant natural area to the current decomposed granite parking lot. It goes on to state that decomposed granite in large quantities can smother and kill incubating salmon and steelhead eggs.

ODFW believes that the proposed reduction to the riparian area will not negatively affect Lone Pine Creek. The addition of 54 native trees and shrubs will meet the planting density recommendations and enhance the present riparian area along Lone Pine Creek as the plantings will assist in cooling Lone Pine Creek and provide habitat for animals that seek refuge in this habitat. As proposed, ODFW does not recommend against the application.

GLUP / Statewide Planning Goals and Policies

The Environmental Element of the *Medford Comprehensive Plan* provides goals, policies, and implementation strategies for improving and maintaining environmental quality in Medford, while accommodating continued growth. The Environment Element is primarily guided by the provisions set forth in *Statewide Planning Goal 5: Open Spaces, Scenic and Historic Places, and Natural resources*, which oversees the protection and conservation of natural resources in Oregon. Included in Goal 5 is the requirement that riparian corridor regulations be applied to those waterways identified as being *fish-bearing streams*, and other waterways having riparian areas determined to be significant. The

August 22, 2018
Matt Brinkley, AICP, CFM, Planning Director
RE: 1862 & 1884 Delta Waters Riparian Reduction Request
Staff Report

means to achieve the objectives of Goal 5 must be set forth in Medford's land use guiding documents: the *Comprehensive Plan* and *Land Development Code*. Medford's Riparian Corridor ordinance, in keeping with the goals and policies established in the Environmental Element of the *Comprehensive Plan*, was adopted on June 1, 2000, to meet the requirements of Goal 5.

Agency Comments

Oregon Department of Fish and Wildlife (Exhibit B)

Peter Samarin, Rogue Assistant District Fisheries Biologist with ODFW, submitted a letter to the applicant in response to their request for their review of the proposed Riparian Planting Plan, with Mr. Samarin forwarding a favorable recommendation.

Recommendation

Staff recommends approval of the applicant's request for a riparian corridor reduction of 25 feet along the site's boundary abutting Lone Pine Creek. Staff has determined that the applicant has submitted the requisite materials as outlined in MLDC 10.927; is not proposing any activities prohibited in Sections 10.926 (1) through (3) to be located any closer than 25 feet from the top-of-bank; and has gained a favorable recommendation from ODFW. It is further staff's view that the submitted Riparian Planting Plan, together with the proposed irrigation system, will provide better protection of the riparian corridor from what currently exists. Therefore, the granting of the riparian corridor reduction request can be made in keeping with the purpose and spirit of both the *Medford Land Development Code*, and the Environmental Element of the *Medford Comprehensive Plan* in its goals to protect and restore Medford's waterways.

Exhibits

- A Applicant's request letter to Planning Director, drafted July 20, 2018
- B Letter of recommendation from ODFW, drafted July 17, 2018
- C Riparian Planting Plan, received August 16, 2017

Planning Director Decision:

Approval per the Staff Report dated August 22, 2018

Denial



Matt Brinkley, AICP, CFM
Planning Director

8-23-18

Date

"P"
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BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-18-147 & CUP-18-148

PARCEL ID: 371W18AA TL 1200 & 1400

PROJECT: Consideration to construct a 43,170 square foot self-storage facility on a 2.37 acres parcel zoned I-L (Light Industrial), located approximately 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62). (371W18AA TL 1200-1400).

A Conditional Use Permit to allow storm water facilities within the Riparian Corridor of Lone Pine Creek, located approximately 300 feet from the intersection of Delta Waters Road and Crater Lake Highway (HWY 62). (371W18AA TL 1200-1400). Applicant: Delta Waters Lenders; Agent; Bill Philp; Planner: Liz Conner

DATE: November 21, 2018

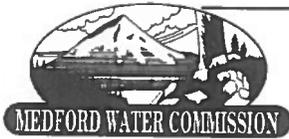
I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of a Fire Hydrant off the existing 6-inch water line stub located between Lots 1200 & 1300 is required. This fire hydrant will be utilized to provide water to the proposed on-site "dry" stand pipe fire suppression system. Applicant shall coordinate with Medford Fire Department for approved location of proposed Fire Department Connection (FDC). Proposed FDC shall be located outside of public right-of-way.
4. Static water pressure is expected to be near 105 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
5. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35>.

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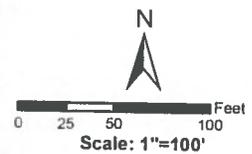
CITY OF MEDFORD
EXHIBIT # Q 1013
FILE # AC-18-147
Page 1 of 2



Continued from Previous Page

COMMENTS

1. Off-site water line installation is required. (See Condition 3 above)
2. On-site water facility construction is not required. The "Dry Standpipe" system is not a Medford Water Commission facility, it is a "Private" fire protection system.
3. MWC metered water service does exist to this property. There is an existing ¾-inch water meter located along the Delta Waters Road frontage, approximately mid-lot on Tax Lot 1200 (1884 Delta Waters Rd) that could be used to serve the proposed. There is also a ¾-inch water meter near the northwest property corner that currently serves the existing building at 1862 Delta Waters Road, which could be utilized for landscape irrigation water meter.
4. Access to MWC water lines is available. There is an existing 6-inch water line in Delta Waters Road.
5. There is also an existing 6-inch water line which is stubbed to the south right-of-way line of Delta Waters Road located near the common property corner of Lots 1200 & 1300. (See Condition 3 above)



Water Facility Map
City of Medford
Planning Application:
AC-18-147/CUP-18-148
(Delta Waters Lenders
Self Storage Facility)
November 7, 2018

Legend

- Air Valve
- ⊙ Sample Station
- Fire Service
- ⊕ Hydrant
- ▲ Reducer
- Blow Off
- ⊕ Plugs-Caps

Water Meters:

- Active Meter
- On Well
- Unknown
- Vacant

Water Valves:

- ⊙ Butterfly Valve
- ⊕ Gate Valve
- Tapping Valve

Water Mains:

- Active Main
- - - Abandoned Main
- Reservoir Drain Pipe
- Pressure Zone Line

Boundaries:

- Urban Growth Boundary
- City Limits
- Tax Lots

MWC Facilities:

- Control Station
- Pump Station
- Reservoir



This map is based on a digital database compiled by Medford Water. Medford Water is not responsible for errors or omissions in this database. The user assumes all responsibility. There are no warrants, express or implied.

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 11" Q"

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City of Medford

City Attorney's Office

Continuous Improvement ~ Customer Service

MEMORANDUM

TO: Liz Conner, Planner II
FROM: Eric Mitton, Deputy City Attorney
RE: AC-18-147 CUP-18-148
DATE: January 10, 2019

Although the Applicant's Questionnaire Findings do not specifically address the issue of the cross-access easement and paving thereof, Applicant has communicated with this office repeatedly on that topic. During a phone call on January 8, 2019, Mr. Huycke clarified that the challenge was being made on two fronts. First, Applicant challenges the establishment of any cross-access easement under the cases *Nollan* and *Dolan*. Second, if such an easement is established, Applicant challenges the paving of any such easement, contending the easement is not a "maneuvering area" under the Medford Municipal Code where paving is required. For the reasons below, our office respectfully disagrees with each of these two legal assertions. Please place this memorandum in the record.

I. Establishment of a cross-access easement between applicant's business and neighboring businesses is not an unconstitutional taking under *Nollan* and *Dolan*.

A cross-access easement is required by Medford Municipal Code 10.550(3), which states in material part:

Any parcel or tract granted driveway access to an Arterial or Collector Street shall grant cross-access easements to all contiguous parcels or tracts that do not abut a street of a lower order than an Arterial or Collector Street. Site design must accommodate future use of such accesses.

Because Applicant's property has driveway access onto Delta Waters Road, this provision applies, requiring cross-access to both the east and west.

Under *Nollan v. California Coastal Commission*, 483 US 825 (1987), in order to avoid the requirement constituting a taking, the City must demonstrate an essential nexus between the condition and a legitimate state interest. Here, that standard is met. The cross-access easement, currently required of all businesses on higher order streets, provides connectivity between Applicant's business and neighboring businesses. Providing access for Applicant's customers to access to neighboring businesses without having to enter and then immediately exit Delta Waters Road improves both safety and traffic flow.

411 West 8th Street, Medford, OR 97501

Tel. 541.774.2020 • email: cityattorney@cityofmedford.org • Fax 541.774.2567

www

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org

CITY OF MEDFORD

EXHIBIT # R of 4

FILE # AC-18-147

It is our understanding that Applicant will argue that there is no essential nexus because Applicant contends that its customers will never use the cross-access easement. Without agreeing that this factual assertion alone would make the cross-access easement a taking under *Nollan*, Staff respectfully disagrees with this factual assumption. There are at least three practical scenarios where Applicant's customers would utilize this cross-access for egress from Applicant's property. First, it provides a far more direct route onto Crater Lake Highway northbound than any other option (please note that the driveway to Applicant's business will be right-in-right-out, so turning left onto Delta Waters is not an option). Second, it allows access to the car wash business at the intersection of Crater Lake Highway and Delta Waters Road, which may be used to vacuum out a vehicle after transporting objects for storage. Finally, it allows access to the gas station immediately adjacent to that car wash business.

Under *Dolan v. City of Tigard*, 512 US 374 (1994), in order to avoid the requirement constituting a taking, the City must demonstrate rough proportionality between the exactions and the impact of the development on legitimate and substantial governmental interests. Here, such proportionality exists. The exaction is minimal: Applicant's proposed site plan already has space for the easement without disrupting or requiring the relocation of any buildings or structures.¹ On the other hand, the impact of eliminating this existing *de facto* access point² onto Applicant's parcel is substantial, since it would direct increased traffic onto a higher-order street near a busy intersection. Cross-access easements like these, that reduce the impacts of driveways onto arterial and collector streets, are currently required for all properties where driveway access is to an arterial or collector street and the contiguous parcels do not abut a lower order street. For all of these reasons, this cross-access easement is proportional in both nature and extent to the impact of the proposed development.

Even setting aside the already-existing *de facto* access point to the County's triangular parcel, and instead looking solely at Applicant's parcel and the stove store due west of it, no *Nollan/Dolan* concern exists with the cross-access easement. If an applicant could avoid establishing a cross-access easement just because its neighbors had not yet established their own cross-access easements, nobody would ever establish cross-access easements. In practicality, though, somebody has to take the first step. When the stove store redevelops, it will be subject to the same code provisions requiring it grant a cross-access, and then the two parcels will have mutual cross-access easements at that time. Cross-access easements can survive a *Nollan/Dolan* inquiry even if a neighboring

¹ Although this easement would not disrupt any buildings or structures, Applicant does currently plan to place trees on top of where the cross-access easement would be located. However, in part because the business at issue is a mini-storage business where the exact layout of landscaping has only so much impact on the business model, relocating these trees to avoid obstructing the easement would not constitute a significant exaction.

² Aerial photographs show tire tracks leading to Applicant's parcel from the driveway to the carwash (technically a separate parcel owned by Jackson County) at the northwest corner of Applicant's parcel.

parcel has not yet granted its own cross-access easement. The nexus still exists because the easement will serve Applicant's customers once the neighbor redevelops and grants its own cross-access easement. The requirement is roughly proportional because the traffic safety and traffic flow benefits in reducing the number of customers who drive in and out of adjacent driveways on a higher-order streets still outweighs the minimal impact of requiring a site plan that contemplates cross-access between neighboring parcels in these situations.

These comments are meant to supplement, and not replace, any comments made by Planning and/or Public Works staff in their respective reports on these issues.

II. The cross-access easement requested here constitutes a "maneuvering area."

With the exception of two situations not applicable here, Medford Municipal Code 10.746 requires paving of "all parking, loading, driveway, and vehicle maneuvering areas." We understand that Applicant will argue that this cross-access easement does not constitute and will not constitute a maneuvering area, because Applicant anticipates that its customers will never use it. However, because of the existing *de facto* access that is taking place onto Applicant's property, as shown in aerial photos, and because of the connectivity to Crater Lake Highway northbound, the car wash, and the gas station, Staff respectfully disagrees with this assumption.

We understand Applicant also will argue that this easement is not a "driveway" or "maneuvering area" because the property directly west of the parcel, a stove store, does not currently provide cross-access, and has, at times, placed a series of rocks along Applicant's western property line. However, that stove store directly west of Applicant is not the only property in that general direction. The triangular parcel to the northwest (owned by Jackson County, and in practical terms the driveway leading to the carwash) is not similarly blocked off. This cross-access easement would connect to that parcel, allowing cars to drive from Applicant's property to the triangular property to the car wash, ultimately reaching those businesses or Crater Lake Highway northbound, without ever crossing the stove property. The cross-access easement would continue to provide this connectivity if the cross-access easement is required. As such, it is a vehicle maneuvering area, and paving is required by Medford Municipal Code 10.746.

Even setting aside the already-existing *de facto* access point to the County's triangular parcel, and instead looking solely at Applicant's parcel and the stove store due west of it, the cross-access easement still constitutes a "maneuvering area" where paving should be required. Even if the stove store's current lack of cross-access easement will prevent any use of the easement on Applicant's property—an assertion with which we do not agree—the easement's purpose is still vehicular maneuvering, and thus it is still a "maneuvering area" as described in Medford Municipal Code 10.746. Vehicles will start to cross that easement as soon as the stove store redevelops, even if they do not do so sooner. And there are very practical implementation and enforcement concerns in asking

Applicant to pave the easement at some unknown future date, when Applicant itself is not seeking any additional development permit.

For these reasons, this Office is of the opinion that requiring a cross-access easement, and paving thereof, are consistent with the Medford Municipal Code and applicable law, including *Nollan* and *Dolan*.

"R"
4 of 4



City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Legal Description
File no. AC-18-147/CUP-18-148
To Jon Proud, Engineering
From Liz Conner, Planning Department
Date November 7, 2018

Please verify the attached legal description covering the below subject at your earliest convenience. See attached map.

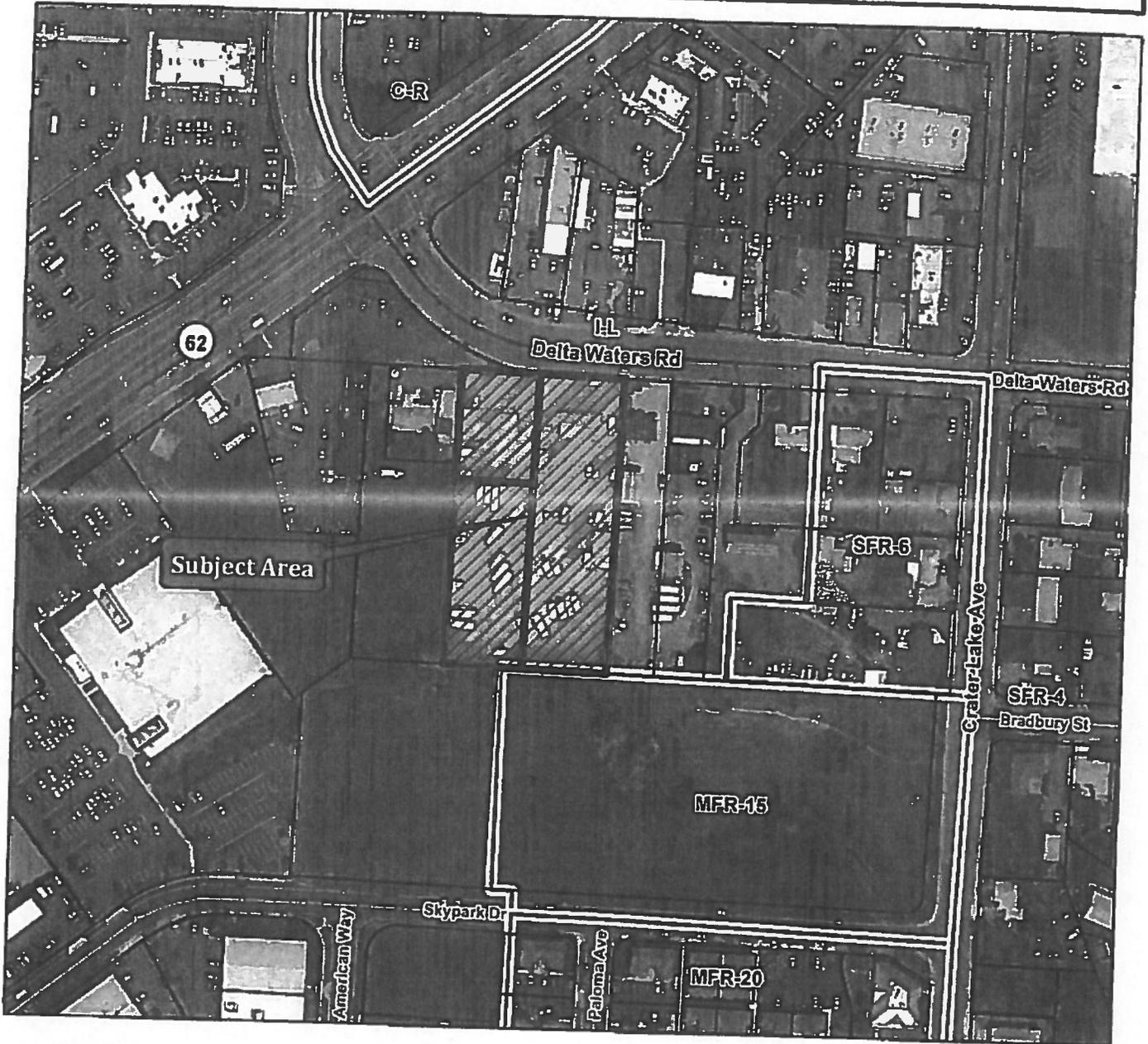
1. AC-18-147/CUP-18-148
Applicant: Delta Waters Lenders, LLC
Agent: Bill Philp

Liz, the descriptions describe the "subject area" shown on the attached vicinity map. It should be noted per a cursory review of JC front counter that the parcels might of been created by deed in the 90's without benefit of a partition map. I would suggest that the applicant provide evidence that the properties were configured as described per land division law.
Thanks, Jon 11-15-18

cp

Attachments:

Vicinity Map, Legal description



Project Name:

**Delta Waters Lenders, LLC
 Self Storage**

Map/Taxlot:

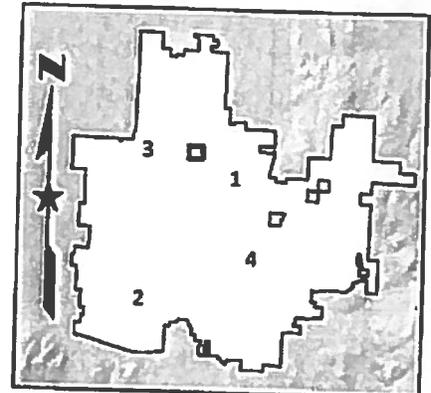
371W18AA TL 1200-1400



10/10/2018

Legend

-  Subject Area
-  Zoning Districts
-  Tax Lots



11/5/11
 2013
 RD

PARCEL 1:

TRACT A: Beginning at the Southeast corner of Lot 1, Block 2, ROGUE VALLEY HEIGHTS in the City of Medford, Jackson County, Oregon; thence North 89°52' West, along the Southerly line of said lot, 115.0 feet; thence North 0°26'22" East, 428.10 feet, to intersect the Northerly line of said lot; thence South 89°54'50" East, along said Northerly line, 115.0 feet, to the Northeast corner of said lot; thence South 0°26'22" West, 428.22 feet, to the point of beginning.

EXCEPTING THEREFROM the following: Beginning at the Southeast corner of Lot 1 in Block 2 of ROGUE VALLEY HEIGHTS in Jackson County, Oregon, according to the Official Plat thereof, now of record; thence North 89°52' West along the Southerly line of said lot, a distance of 115.0 feet; thence North 0°26'22" East 268.10 feet; thence South 89°54'50" East, 115.0 feet to the East line of said Lot One; thence South 0°26'22" West 268.22 feet to the point of beginning.

TRACT B: Beginning at the Southeast corner of Lot 1 in Block 2 of ROGUE VALLEY HEIGHTS in Jackson County, Oregon, according to the Official Plat thereof, now of record; thence North 89°52' West along the Southerly line of said lot, a distance of 115.0 feet; thence North 0°26'22" East 268.10 feet; thence South 89°54'50" East, 115.0 feet to the East line of said Lot One; thence South 0°26'22" West 268.22 feet to the point of beginning.

PARCEL 2:

Beginning at the Southwest corner of Lot 27, Block 2, ROGUE VALLEY HEIGHTS in the City of Medford, Jackson County, Oregon; thence North 0°26'22" East, along the West line of said Lot 428.22 feet, to the Northwest corner thereof; thence South 89°54'50" East, along the North line of said lot, 125.0 feet; thence South 0°26'22" West, 428.25 feet, to intersect the South line of said lot; thence North 89°52' West, 125.0 feet, to the point of beginning.

"S"
30f3

1



JACKSON COUNTY

Roads

Roads
Engineering

Chuck DeJanvier
Construction Engineer

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
dejanvca@jacksoncounty.org

www.jacksoncounty.org

November 16, 2018

Attention: Elizabeth Conner
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Consideration of a 43,170 square foot storage facility and a Conditional Use Permit for storm water on a 2.37 acre parcel on Delta Waters Road - a City maintained road. And near Highway 62 - an ODOT maintained road
Planning File: AC-18-147 / CUP-18-147

Dear Elizabeth:

Thank you for the opportunity to comment on consideration to construct a 43,170 square foot storage facility and a Conditional Use Permit for storm water facilities within the Riparian Corridor of Lone Pine Creek on a 2.37 acre parcel zoned I-L (light Industrial), located approximately 300 feet from the intersection of Delta Waters and Highway 62

1. Please contact the Oregon Department of Transportation for comments.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,

Chuck DeJanvier
Construction Engineer

CITY OF MEDFORD
EXHIBIT # 7
FILE # AC-18-147

Liz A. Conner

From: Amber Judd <JuddAJ@jacksoncounty.org>
Sent: Tuesday, November 13, 2018 12:28 PM
To: Liz A. Conner
Subject: File No. AC-18-147/CUP-18-148 Project Name: Delta Waters Lenders-Self Storage Facility

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Elizabeth,

The Airport requests an Avigation, Noise and Hazard Easement be a requirement of this project. In addition, due to the proximity to the Airport, the applicant needs to contact the FAA regarding filing a 7460-1 Notice of Proposed Construction or Alteration. The FAA contact is: Paul Holmquist, phone (206) 231-2990.

I have inserted some information below from the FAA's website:

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference CFR Title 14 Part 77.9.

You must file with the FAA at least 45 days prior to construction if:

- Your structure will exceed 200 ft above ground level
- Your structure will be in proximity to an airport and will exceed the slope ratio
- Your structure involves construction of a traverseway (i.e. highway, railroad, waterway, etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- Your structure will emit frequencies, and does not meet the conditions of the FAA Co-location Policy
- Your structure will be in an instrument approach area and might exceed part 77 Subpart C
- Your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- Your structure will be on an airport or heliport
- Filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the Air Traffic Areas of Responsibility map for Off Airport construction, or contact the FAA Airport Region/District Office for On Airport construction.

Results

You exceed the following Notice Criteria:

Your proposed structure exceeds an instrument approach area by approximately 32 feet and aeronautical study is needed to determine if it will exceed a standard of subpart C of 14CFR Part 77. The FAA, in accordance with 77.9, requests that you file.

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(b) by 17 ft. The nearest airport is MFR, and the nearest runway is 14LF/32RF.

The FAA requests that you file.

Thank you,

Amber Judd

Deputy Director-Administration
Rogue Valley International-Medford Airport (MFR)



1000 Terminal Loop Parkway, Suite 201
Medford, Oregon 97504
541-776-7222

"U"
20+2

2019 Image from Google Earth



CITY OF MEDFORD
EXHIBIT # V 1 of 3
FILE # AC-18-147

June 28, 2018 image from Google Earth



"V"
2 of 3

August 22, 2012 Image from Google Earth

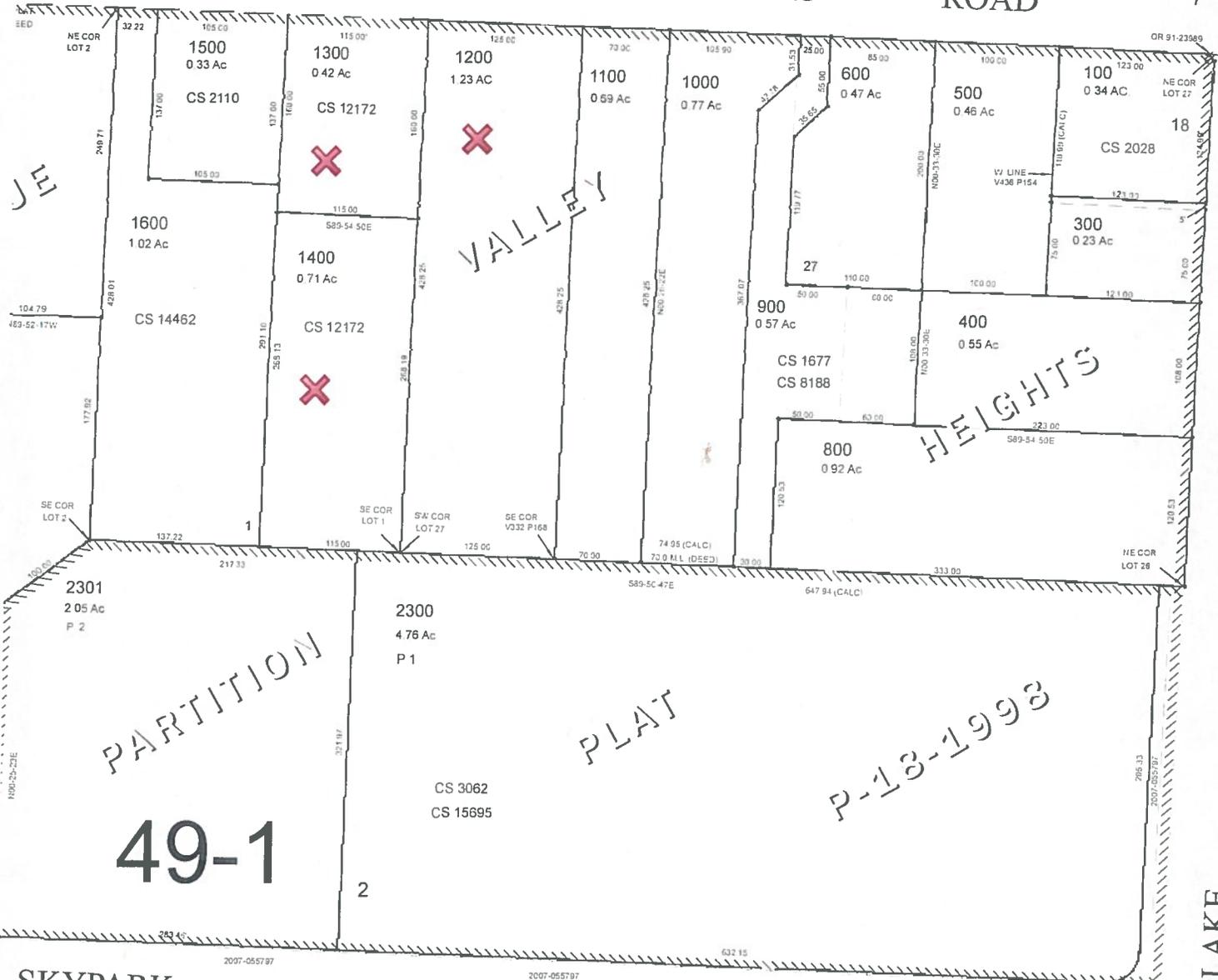


"V"
3 of 3

N.E.1/4, N.E.1/4, SEC.18, T.37S., R.1W., W.M.
JACKSON COUNTY
1" = 100'

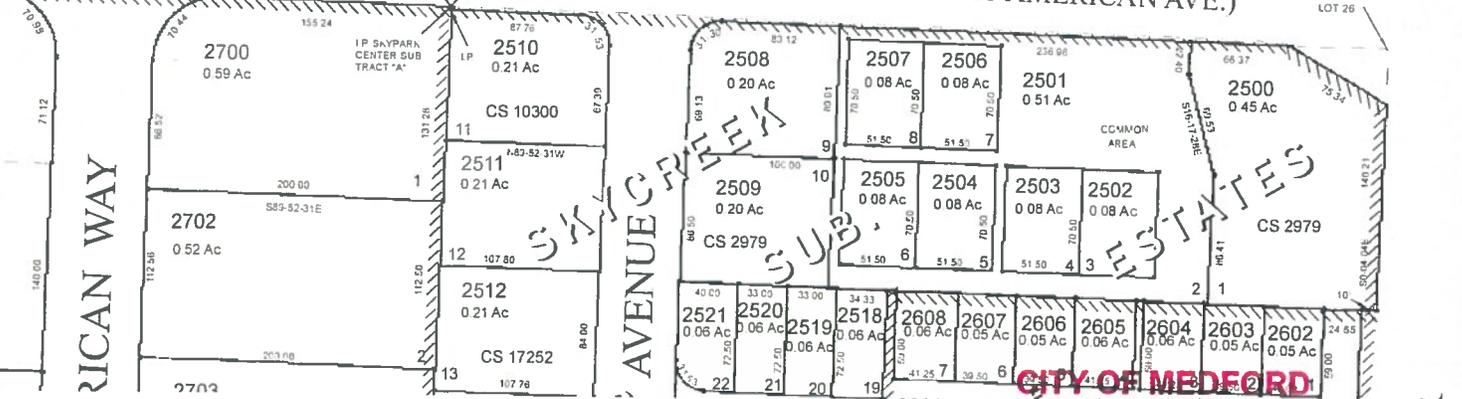
SEE MAP 37 1W 7DD

DELTA WATERS ROAD



SKYPARK
OR 93-66649 OP 1998-35

DRIVE (FORMERLY AMERICAN AVE.)



CITY OF MEDFORD
EXHIBIT # W
FILE # AC-18-147



Subject Area

Project Name:

**Delta Waters Lenders, LLC
 Self Storage**

Map/Taxlot:

371W18AA TL 1200-1400

Legend

-  Subject Area
-  Zoning Districts
-  Tax Lots



10/10/2018

