

SITE PLAN & ARCHITECTURAL  
COMMISSION  
AGENDA  
DECEMBER 6, 2019



**MEDFORD**  
OREGON

Commission Members

Jim Quinn, Chair

Bill Chmelir, Vice Chair

Jeff Bender

Jim Catt

David Culbertson

Bob Neathamer

Marcy Pierce

Milo Smith

Rick Whitlock

Regular Commission meetings are held on  
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380

# SITE PLAN & ARCHITECTURAL COMMISSION AGENDA



**MEDFORD**  
OREGON

December 6, 2019

12:00 P.M.

Medford City Hall, Council Chambers  
411 West 8<sup>th</sup> Street, Medford, Oregon

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10. Roll Call

20. Consent Calendar / Written Communications None.

30. Approval or Correction of the Minutes.

30.1 Consideration for approval of minutes from the November 15, 2019, meeting.

40. Oral Requests and Communications from the Audience

COMMENTS WILL BE LIMITED TO 3 MINUTES PER INDIVIDUAL OR 5 MINUTES IF REPRESENTING A GROUP OR ORGANIZATION. **PLEASE SIGN IN.**

50. Public Hearings

COMMENTS ARE LIMITED TO A TOTAL OF 10 MINUTES FOR APPLICANTS AND/OR THEIR REPRESENTATIVES. YOU MAY REQUEST A 5-MINUTE REBUTTAL TIME. ALL OTHERS WILL BE LIMITED TO 3 MINUTES PER INDIVIDUAL OR 5 MINUTES IF REPRESENTING A GROUP OR ORGANIZATION. **PLEASE SIGN IN.**

New Business.

50.1 **AC-19-062** Consideration of the construction of a 37,652 square foot, four-story hotel (My Place Hotel) on 1.9 acres located at the southwest corner of Airport Road and Biddle Road in the C-R (Regional Commercial) zoning district (372W12A1102). Applicant, MP Medford LLC; Agent, Ausland Group; Planner, Dustin Severs.

60. Written Communications None.

70. Unfinished Business None.

80. New Business None.

90. Report from the Planning Department

100. Messages and Papers from the Chair

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110. Propositions and Remarks from the Commission

120. Adjournment

# SITE PLAN & ARCHITECTURAL COMMISSION MINUTES



**MEDFORD**  
OREGON

November 15, 2019

12:00 P.M.

Medford City Hall, Council Chambers

411 West 8<sup>th</sup> Street, Medford, Oregon

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The regular meeting of the Site Plan and Architectural Commission was called to order at noon in the Council Chambers on the above date with the following members and staff in attendance:

**Commissioners Present**

Jeff Bender, Acting Chair  
David Culbertson  
Bob Neathamer  
Marcy Pierce  
Milo Smith  
Rick Whitlock

**Staff Present**

Matt Brinkley, Planning Director  
Kelly Evans, Assistant Planning Director  
Carla Paladino, Principal Planner  
Katie Zerkel, Senior Assistant City Attorney  
Debbie Strigle, Recording Secretary

**Commissioners Absent**

Jim Quinn, Chair, excused  
Bill Chmelir, Vice Chair, excused  
Jim Catt, excused

10. **Roll Call**

20. **Consent Calendar (voice vote)**

20.1 AC-19-061 Final Order for the development of a single parcel totaling 12.5 acres—currently the Crater Lake Plaza, containing Hobby Lobby and the Ashley Home Store—consisting of the construction of two new buildings: A 7,536 square foot building to house an Advanced Auto Parts retail store, and a second 27,036 square foot building to house a 16,000 square foot Planet Fitness health club and a 11,036 square foot Party City retail store. The property is located at 3643 Crater Lake HWY in the C-R (Regional Commercial) zoning district (371W07A TL 5204).

Motion: Adopt the consent calendar.

Moved by: Commissioner Whitlock

Seconded by: Commissioner Culbertson

Voice Vote: Motion passed, 6-0

30. **Approval or Correction of the Minutes (voice vote)**

30.1 There being no additions or corrections, the minutes for the November 1, 2019, meeting were approved as submitted.

40. **Oral Requests and Communications from the Audience.** None.

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50. **Public Hearings**

Katie Zerkel, Senior Assistant City Attorney, read the rules governing the public hearings.

Continuance Request.

50.1 AC-19-056 Consideration of a proposed multi-phase development for Public Works Service Center on a 6.2 acre leased area located on the east side of Corona Avenue and at the terminus of Hilton Road zoned I-L (Light Industrial) (371W18A4200); Applicant, City of Medford; Agent, CSA Planning Ltd.; Planner: Steffen Roennfeldt.

Motion: Continue this item to the February 7, 2020, meeting.

Moved by: Commissioner Whitlock      Seconded by: Commissioner Culbertson

Voice Vote: Motion passed, 5-0-1, with Commissioner Neathamer abstaining as he had a potential conflict of interest. His office has provided professional services on this application.

60. **Written Communications** None.

70. **Unfinished Business** None.

80. **New Business**

80.1 Discussion of Type II review for multi-family housing types. (Type II reviews are administrative review with property owner noticing – no public hearing.)

Ms. Evans wanted to know, in general, if this commission wanted to see projects that meet the clear and objective standards for multi-family development.

After discussion and input from Matt Brinkley, the majority of the commission felt that it seemed appropriate they not be involved in those decisions where the commission does not have any discretion. City Council would be the best fit to make those determinations.

Motion: Recommend to City Council that the Site Plan & Architectural Commission not be involved in multi-family development proposals where there is no SPAC discretion that can be exercised.

Moved by: Commissioner Whitlock      Seconded by: Commissioner Smith

Commissioner Whitlock commented he felt the acreage issue was not that important. He said he would be comfortable doing anything for City Council they would like to have this commission look at. He reiterated that City Council would be the better body to make determinations; however, if this commission did have the final decision on some of the applications and they got appealed to LUBA (Land Use Board of Appeals), it might save City Council some grief where they don't have any discretion either. He added he really would like to see the whole proposal before he voted for or against it on all the various aspects.

Mr. Brinkley stated that after talking with City Council staff will write everything up to reflect City Council's feelings about the appeal issue. They will also add the 300 foot noticing to it and whatever else comes out of the meetings with City Council. Staff will bring it all back to this commission for review and feedback.

Voice Vote: Motion passed, 5-1, with Commissioner Neathamer voting "nay."

Commissioner Neathamer explained he was not in favor of a recommendation because he felt the commission didn't have enough information to make a motion. He said he wished a motion had not been made.

**90. Report from the Planning Department**

90.1 Ms. Evans stated there would be business for the December 6<sup>th</sup> meeting but no business is scheduled for the December 20<sup>th</sup> meeting.

90.2. Ms. Evans reported that on November 7<sup>th</sup> City Council had approved emergency shelters, a street vacation in Cedar Landing, and the Housing Opportunity Fund (HOF) recommendation.

90.3 On November 21<sup>st</sup> City Council will be considering an annexation on Table Rock Road, just south of Airport, and funding priorities for Homeless System Action Plan implementation.

90.4 Ms. Evans reported the number of multi-family units that have run through this commission was over 250 in 2017, 38 in 2018, and 26 so far in 2019.

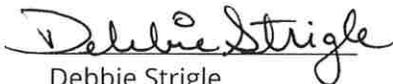
**100. Messages and Papers from the Chair None.**

**110. Propositions and Remarks from the Commission None.**

**120. Adjournment**

120.1 The meeting was adjourned at approximately 12:45 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:



Debbie Strigle  
Recording Secretary

\_\_\_\_\_  
Jim Quinn  
Site Plan and Architectural Commission Chair

Approved: December 6, 2019



## STAFF REPORT

for a Type-III quasi-judicial decision: Site Plan & Architectural Review

**Project** My Place Hotel  
Applicant: MP Medford LLC; Agent: Ausland Group

**File no.** AC-19-062

**To** Site Plan and Architectural Commission *for 12/6/2019 hearing*

**From** Dustin Severs, Planner III

**Reviewer** Kelly Evans, Assistant Planning Director *ke.*

**Date** November 29, 2019

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### BACKGROUND

#### Proposal

Consideration of the construction of a 37,652 square foot, four-story hotel (My Place Hotel) on 1.9 acres located at the southwest corner of Airport Road and Biddle Road in the C-R (Regional Commercial) zoning district (372W12A1102).

#### Vicinity Map



### Subject Site Characteristics

GLUP        CM (Commercial)  
Zoning      C-R  
Overlay     AC (Airport Area of Concern)  
Use         Vacant

### Surrounding Site Characteristics

*North*      Zone:        C-R  
                  Use:        Vacant land & Airport

*South*      Zone:        I-L (Light Industrial)  
                  Use:        Navigators Landing PUD & Candlewood Suites Medford Hotel

*East*        Zone:        C-R & C-C (Community Commercial)  
                  Use:        KRWQ

*West*        Zone:        I-L & I-G (General Industrial)  
                  Use:        Courtyard by Marriot Hotel

### Related Projects

CP-97-215                      GLUP change from General Industrial to CM  
LDP-19-055/ZC-19-003        Two-lot partition and zone change from I-L to C-R  
PA-19-60                        Pre-application to review subject request

### Applicable Criteria

Medford Municipal Code §10. 200(E)(1) – Site Plan & Architectural Review Criteria

*The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:*

- (a) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (b) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.186.*

### Approval Authority

This is a Type III land use decision. The Site Plan & Architectural Commission is the approving authority under MLDC 10.108(1).

### ISSUES AND ANALYSIS

#### Background

##### *History*

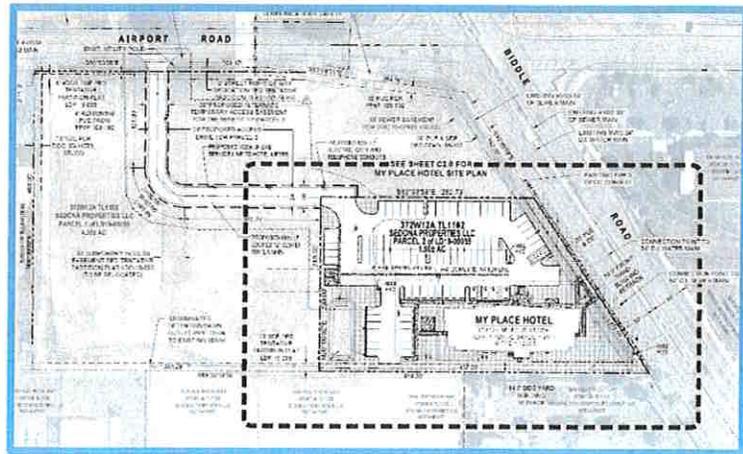
FILE #	DATE	DESCRIPTION
CP-97-215	1997	GLUP change from GI to CM approved by City Council.
ZC-19-003/ LDP-19-055	June 27, 2019	Two-lot partition and zone change from I-L to C-R approved by Planning Commission.
PA-19-60	July 31, 2019	Pre-application to discuss subject project
FP-19-017	Recorded October 2, 2019	Final Plat of two-lot partition

The subject site was part of a two-lot partition, along with a change of zone from I-L to C-R, approved by the Planning Commission on June 27, 2019. The partition split the 6.20-acre parent parcel, creating two lots: a 4.30-acre parcel, and the subject 1.90-acre parcel. A temporary access easement is shown on the plat, providing vehicular access to the subject site, which only had street frontage with Biddle Road—a Major Arterial street. The final plat was filed with the Jackson County Recorder on October 2, 2019.



*Current proposal*

The applicant is now proposing to develop the property with a four-story, 37,652 square foot extended stay hotel (My Place Hotel) with 85 guest rooms. Access to the subject site is shown off of Airport Road—a Minor Collector street under the jurisdiction of Jackson County—via an access easement; however the applicant is proposing to shift the access slightly east to avoid the relocation of a power pole (Exhibit S).



**Development Standards**

Commercial – Industrial Site Development Table (MLDC 10.721)

C-R Zone	Allowed/Required	Proposed
Height	85 feet (35 feet if located within 150 feet of a residential zone)	49'-1"
Lot Coverage	40%	14.4%
Setback (front)	10 feet	30 feet
Setback (side yards)	14.5	20 feet / 140 feet
Setback (rear)	14.5	165 feet

As shown in the Site Development Table above, it can be found that the proposed hotel building identified on the submitted site plan meets the bulk standards for the C-R zoning district as found in Article V of the Medford Land Development Code.

## Parking

**Parking Table (MLDC 10.743-751)**

	Required	Shown
Vehicle Spaces	86 min. / 87 max.	92
Handicap Spaces	4	4
Bicycle Spaces	9	9

The submitted site plan shows a total of 92 parking stalls, along with four handicap spaces and nine bicycle spaces. The 92 proposed spaces, however, exceed the maximum allowed as per MLDC 10.743-751. The applicant has requested relief from the Code in order to exceed the maximum number of parking stalls by five spaces.

Per MLDC 10.743(3)(a), the approving authority may allow exceptions to the number of parking spaces if they find the applicant's findings adequately explain why the characteristics of the proposed use require different off-street parking standard than what is otherwise required.

The applicant's findings (Exhibit L) state the following:

*The proposed development is for an extended stay hotel. According to 2018 data provided by the applicant, 21.7% of visitors will be weekly customers and 22.8% will be monthly customers. The average length of total stay across all transactions is 2.92 days per visit. It is also common at extended stay accommodations to have visitors traveling with multiple vehicles in the same party or group. The extended stay customers staying together in one room may need multiple vehicles during their stay for business or relocation. This use is slightly more parking intensive than a standard hotel, thus the application is seeking a slightly greater amount of off-street parking than otherwise allowed by the maximum parking standards of this section.*

Staff is supportive of the applicant's request to exceed the number of parking spaces allowed per the Code. Neither Biddle Road nor Airport Road permits on-street parking, eliminating a possible outlet for the subject site's overflow parking. It is staff's view that the applicant's findings adequately demonstrate that the number of needed parking spaces is more than the maximum allowed, and that the granting of relief can be made in keeping with the intent and purpose of MLDC 10.743.

## Landscaping

### Frontage Landscaping (MLDC 10.797)

Biddle Road	Required	Shown
Trees	11	11
Shrubs	69	75

### Parking Lot Landscaping (MLDC 10.746[3])

	Required	Shown
Area (square feet)	1,938 sf	2,820 sf
Trees	12	13
Shrubs	23	27

The applicant's submitted landscape plan (Exhibit I) shows landscaping along the sites frontage with Biddle Road, meeting the minimum required number of trees and shrubs as per MLDC 10.797. The landscape plan also shows interior landscaping, meeting the required number of trees, shrubs, and total landscaping within the site's parking lot as per MLDC 10.746(3).

### Pedestrian Access (MLDC 10.772-776)

The site plan shows a pedestrian walkway connecting the proposed building to the public sidewalk running along the site's frontage with Biddle Road; however, the site plan does not show pedestrian connectivity to the abutting parcels to the north, south, and west.

MLDC 10.772-773 reads as follows:

**10.772 Pedestrian Walkway General Provisions.**

These walkway provisions are intended to provide safe, direct and convenient pedestrian access in new office and commercial developments.

Internal pedestrian circulation shall be provided in new office and commercial developments through clustering of buildings, construction of pedestrian ways, skywalks, where appropriate, and similar techniques.

**10.773 Pedestrian Walkway Connections and Routing.**

A pedestrian walkway shall be provided:

- (1) To each street abutting the property, and for every 300 feet of street frontage not including limited access freeways.
- (2) To connect with walkways, sidewalks, bikepaths, and other bicycle or pedestrian connections on adjacent properties.
- (3) To connect building entrances to one another, to existing or planned transit stops and to connect the pedestrian circulation system to other areas of the site such as parking lots, children's play areas, required outdoor areas and any pedestrian amenities such as plazas, resting areas and viewpoints.

The subject site abuts vacant property along its north, south, and west boundaries. In the interest of promoting pedestrian connectivity with the abutting undeveloped lands—consistent with MLDC 10.773(2) cited above—it is staff's view that the site plan should include pedestrian walkways stubbed at all three boundaries, providing the abutting lands the ability to connect their own internal pedestrian circulation to the subject site at the time they are developed in the future.

**Concealments**

*Trash Enclosure (MLDC 10.781)*

The applicant's site plan identifies a trash enclosure on the west end of the lot, and the submitted dumpster enclosure plan (Exhibit K) shows elevations of the proposed trash enclosure, meeting the requirements outlined in MLDC 10.781.

**Architecture**



The applicant's submitted narrative (Exhibit K) describes the building's proposed architecture as the following:

*The nearest surrounding developments are both existing hotels of similar size, function and design. . . The exterior treatments have been expressly designed to break up the 'big box' mass feel.*

### **Other agency comments**

#### *Rogue Valley Sewer Services (RVSS) (Exhibit Q)*

The subject property is within RVSS service area. In their submitted report, RVSS requires that future sewer improvements be designed and constructed in accordance with RVSS standards, that the sewer system must be accepted by RVSS prior to construction, and the applicant must pay sewer system development charges to RVSS.

As a condition of approval, the applicant will be required to comply with the conditions of RVSS.

#### *Jackson County Roads (Exhibit P)*

The segment of Airport Road fronting the subject property is under the jurisdiction of Jackson County. The report received by Jackson County Roads lists nine comments, including a request that the applicant submit a Traffic Impact Study (TIS) that looks at the safety and capacity of the site access off of Airport Road. The staff at Jackson County Roads, in discussions with City staff, have mentioned the possibility of the site's proposed driveway access off of Airport Road necessitating a westbound left-turn lane into the subject site to accommodate the increase traffic volume generated by the subject development. A TIS is being requested so as to determine whether or not said turn lane is necessary.

As a condition of approval, the applicant will be required to comply with all applicable requirements of Jackson County Roads, prior to the issuance of building permits for vertical construction.

#### *Rogue Valley International – Medford Airport (Exhibit R)*

Rogue Valley International Airport requests an Avigation, Noise and Hazard Easement to be required as part of the permit process. In the 2010 LUBA decision on Michelle Barnes vs. City of Hillsboro and the Port of Portland, Nollan/Dolan findings are required to support the request (LUBA No. 2010-011). None were provided; therefore, a condition requiring compliance with the airport's request for an Avigation, Noise and Hazard Easement has not been included.

### **Committee Comments**

No comments were received from committees such as BPAC.

No other issues were identified by staff.

## **FINDINGS OF FACT**

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the applicant's findings and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

## **FINDINGS AND CONCLUSIONS**

Staff has reviewed the applicant's findings and conclusions (Exhibit L) and recommends the Commission adopt the findings as recommended by staff.

## **RECOMMENDED ACTION**

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-19-062 per the staff report dated November 29, 2019, including Exhibits A through S.

## **EXHIBITS**

- A Conditions of Approval, drafted November 29, 2019.
- B Cover Sheet, received November 7, 2019.
- C Site Plan, received November 7, 2019.
- D Grading & Drainage Plan, received October 9, 2019.
- E Conceptual stormwater detention plan, received October 9, 2019.
- F Floor Plans (4 of 4), October 9, 2019.
- G Roof Plan, received October 9, 2019.
- H Elevation Plans (2 of 2), October 9, 2019.
- I Landscape Plan, received October 9, 2019.
- J Picnic Area details, received October 9, 2019.
- K Dumpster enclosure plan, received October 9, 2019.
- L Applicant's Narrative, Questionnaire, and Findings of Fact, received October 9, 2019.

- M Public Works staff report, received November 13, 2019.
- N Medford Water Commission report & map, received November 13, 2019.
- O Medford Fire Department report, received November 13, 2019.
- P Jackson County Roads memo, received November 1, 2019.
- Q Rogue Valley Sewer Services (RVSS), received November 4, 2019.
- R Rogue Valley International–Medford Airport, received by email on November 1, 2019.
- S Applicant’s proposed access realignment map.  
Vicinity map

**SPAC AGENDA**

**DECEMBER 6, 2019**

## EXHIBIT A

My Place Hotel  
AC-19-062  
Conditions of Approval  
November 29, 2019

### DISCRETIONARY CONDITION

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Provide staff with documentation proving legal access off of Airport Road through the abutting property, as illustrated on the submitted site plan.

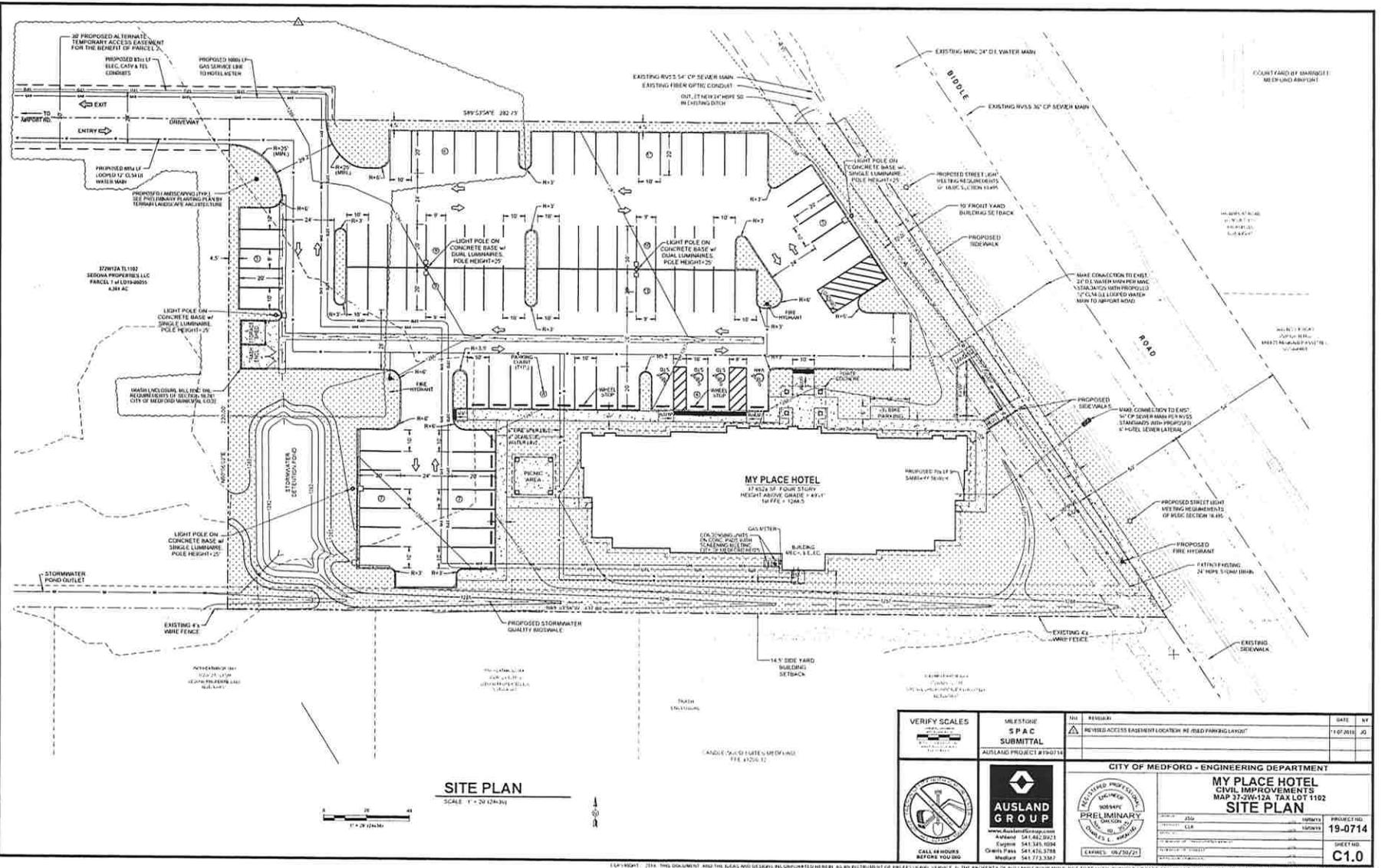
### CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

2. Comply with all conditions stipulated by the Public Works Department (Exhibit M).
3. Comply with all conditions stipulated by the Medford Water Commission (Exhibit N).
4. Comply with all requirements of the Medford Fire Department (Exhibit O).
5. Comply with all applicable requirements of Jackson County Roads (Exhibit P).
6. Comply with requirements of Rogue Valley Sewer Services (Exhibit Q).
7. Submit a revised site plan showing a single pedestrian walkway stubbed at each boundary of the site—north, south, and east boundaries—connecting the site's internal pedestrian circulation to the abutting undeveloped lands.

CITY OF MEDFORD  
EXHIBIT # A  
FILE # AC-19-062

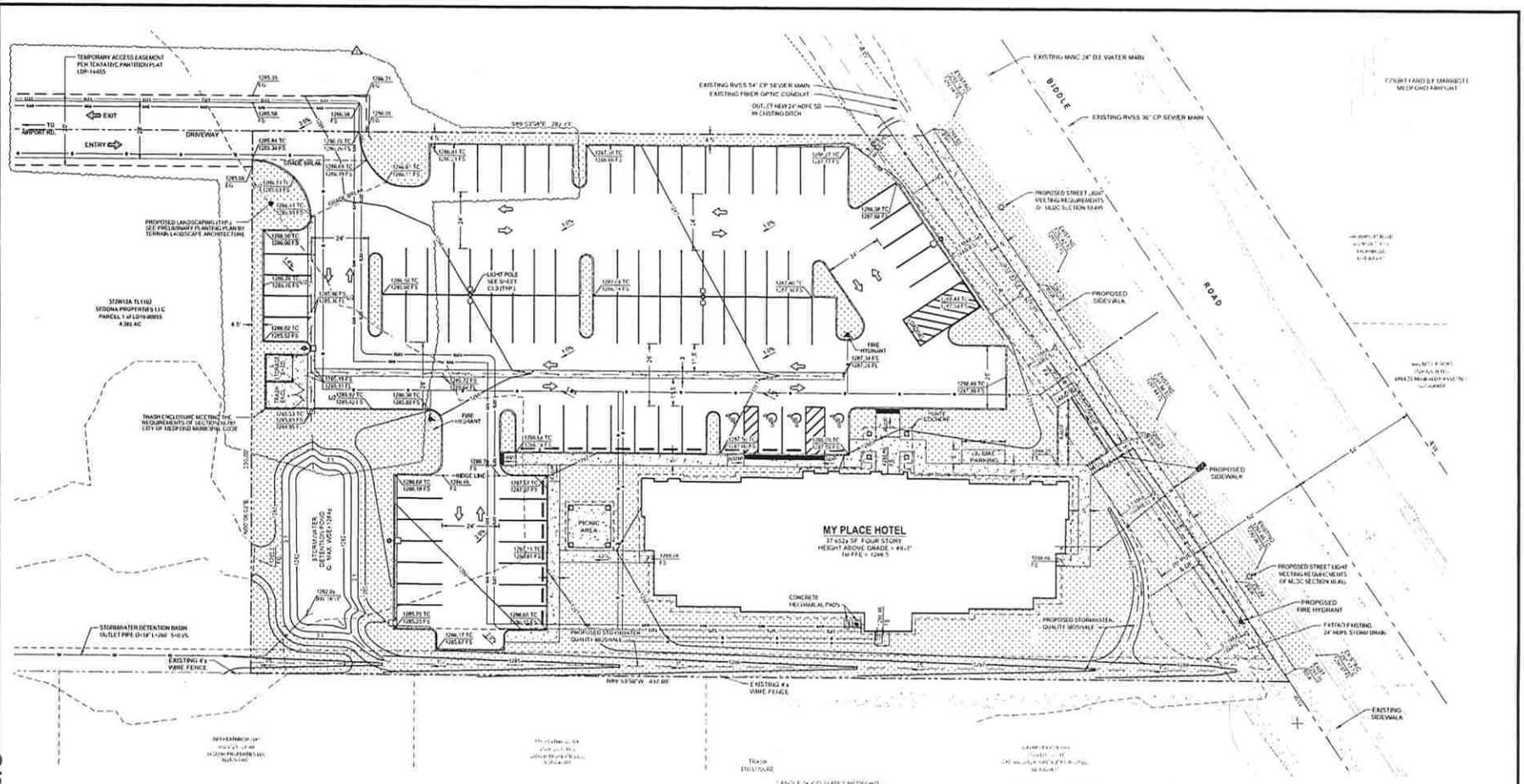




**SITE PLAN**  
 SCALE: 1" = 20' (AS SHOWN)

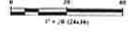
<b>VERIFY SCALES</b> 	SHEET NO. <b>S.P.A.C. SUBMITTAL</b> AUSLAND PROJECT #190114	THE DESIGNER'S REVIEW AND ACCESS EASEMENT LOCATION AS SHOWN ON PARKING LAYOUT?	DATE: 11/07/2019 BY: JG
	CITY OF MEDFORD - ENGINEERING DEPARTMENT		
	www.auslandgroup.com Portland, OR 97208 Eugene, OR 97401 Medford, OR 97504	<b>MY PLACE HOTEL CIVIL IMPROVEMENTS</b> MAP 37-2W-12A TAX LOT 1102 <b>SITE PLAN</b>	
		PREPARED BY: JG CHECKED BY: JG DATE: 11/07/2019 SHEET NO. <b>C1.0</b>	PROJECT NO. <b>19-0714</b> SHEET NO. <b>C1.0</b>

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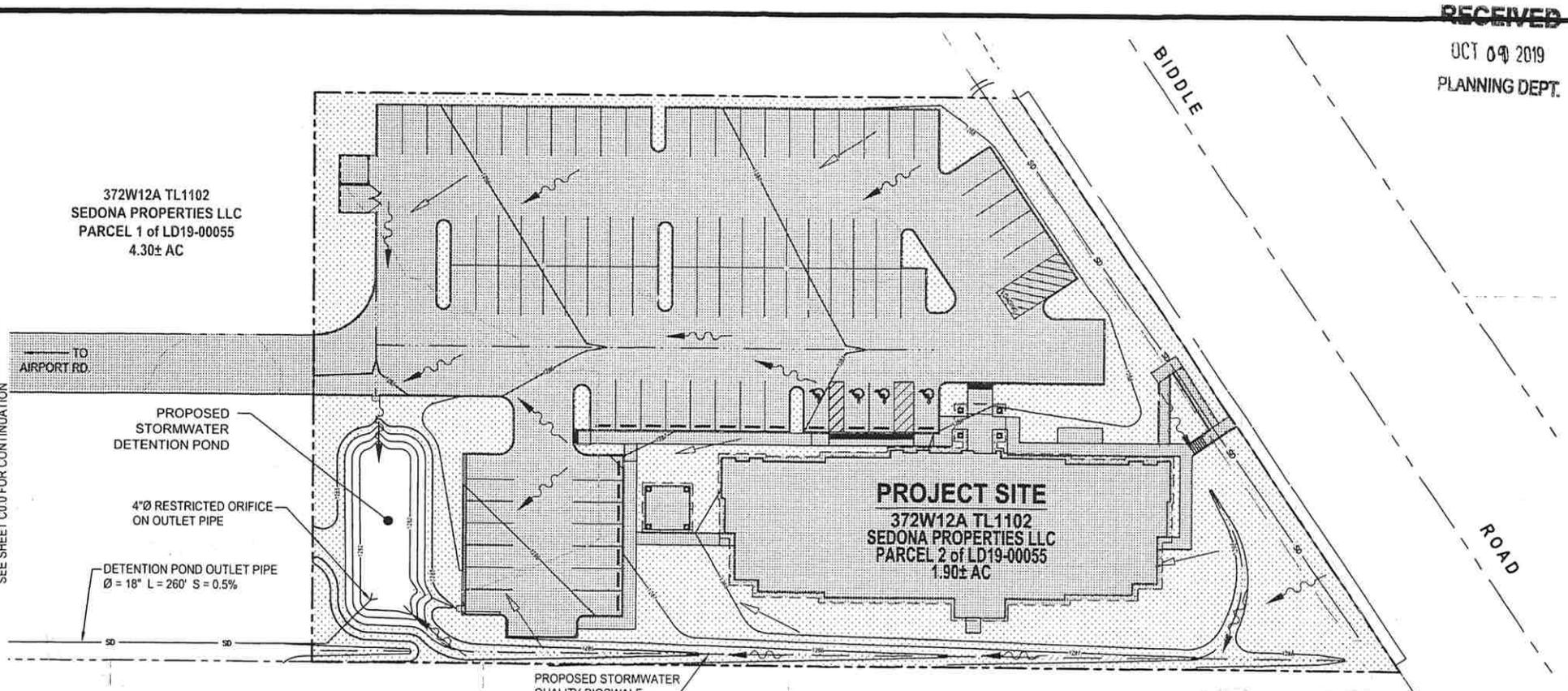
**GRADING & DRAINAGE PLAN**

SCALE: 1" = 20' (20/40)



<b>VERIFY SCALER</b>  CHARLES L. SPIVEY P.E.	<b>MILESTONE</b> <b>SPAC</b> <b>SUBMITTAL</b> AUSLAND PROJECT # 19-0714	<b>REVISION</b> 1. REISED ACCESS EASEMENT LOCATION, RE-USED PARKING LAYOUT	<b>DATE</b> 11/07/2019	<b>BY</b> JC
	<b>CITY OF MEDFORD - ENGINEERING DEPARTMENT</b> <b>MY PLACE HOTEL</b> <b>CIVIL IMPROVEMENTS</b> <b>MAP 21-2W-12A TAX LOT 1102</b> <b>GRADING &amp; DRAINAGE PLAN</b>			
 <b>AUSLAND GROUP</b> www.AuslandGroup.com Ashland 541.482.8233 Eugene 541.385.1094 Gresham 541.476.3788 Medford 541.773.3367	 <b>REGISTERED PROFESSIONAL ENGINEER</b> CIVIL PRELIMINARY CHARLES L. SPIVEY LICENSE NO. 10000	PROJECT NO. <b>19-0714</b> SHEET NO. <b>C2.0</b>		

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Page 21  
 SEE SHEET C0.0 FOR CONTINUATION  
 FILE # AC-19-062  
 CITY OF MEDFORD

**AREA CALCULATIONS**

	EXISTING	PROPOSED
PERVIOUS SURFACE AREA:	1.90 ACRES	0.45 ACRES
SCS CURVE NUMBER:	CN 80	CN 61
IMPERVIOUS SURFACE AREA:	0.00 ACRES	1.45 ACRES <sup>1</sup>
SCS CURVE NUMBER:	CN 98	CN 98

<sup>1</sup> INCLUDES 24' WIDE PAVED ACCESS TO AIRPORT ROAD

**STORMWATER RUNOFF CALCULATIONS**

SCS 24-HR PRECIPITATION:	$Q_{10} = 3.00"$	$Q_{25} = 3.25"$
IDF CURVE FOR MEDFORD: $T_c = 16.4$ MINUTES	$Q_{10} = 2.30"/hr$	$Q_{25} = 3.10"/hr$
SBUH PEAK FLOW RUNOFF:		
EXISTING:	$Q_{10} = 0.42$ cfs	$Q_{25} = 0.50$ cfs
PROPOSED <sup>2</sup> :	$Q_{10} = 0.92$ cfs	$Q_{25} = 1.04$ cfs

<sup>2</sup> STORMWATER RUNOFF FROM THE PROPOSED IMPERVIOUS SURFACE TO BE ROUTED TO THE DETENTION POND.

**STORMWATER DETENTION POND**

SIDE SLOPES:	3:1 (MAX.)	
ELEVATIONS:	1282± BOTTOM	1285± TOP
MAX. WSE:	$Q_{10} = 1283.44$	$Q_{25} = 1283.60$
POND OUTLET:	18"Ø SD PIPE w/ 4"Ø RESTRICTED ORIFICE	
POND OUTFLOW <sup>3</sup> :	$Q_{10} = 0.41$ cfs	$Q_{25} = 0.44$ cfs

<sup>3</sup> POST DEVELOPMENT RUNOFF FROM THE PROJECT SITE SHALL NOT EXCEED THE PRE-DEVELOPMENT RATE

**LEGEND**

EXISTING:

SURFACE FLOW DIRECTION: PROPOSED:

IMPERVIOUS SURFACE:

PERVIOUS SURFACE:

0 40 80  
1" = 40' (11x17)

**NOTES**

THE PROPOSED STORMWATER QUALITY SWALES AND DETENTION POND SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF MEDFORD REQUIREMENTS AND THE RVSS STORMWATER QUALITY DESIGN MANUAL.



**MY PLACE HOTEL  
CONCEPTUAL STORMWATER DETENTION PLAN**

590 AIRPORT RD. MEDFORD, OR 97504  
MAP 37-2W-12A TAX LOT 1102

CITY OF MEDFORD  
EXHIBIT # 1  
FILE # AC-19-062

DRAWN BY: JSG  
DESIGNED BY: CLK  
PROJECT: 19-0714  
ISSUE DATE: 10/08/19  
SHEET NUMBER

**GENERAL NOTES**

- A. SEE A110 SERIES SHEETS FOR WALL TYPES INDICATION PLANS
- B. SEE A400 SERIES SHEETS FOR ENLARGED GUESTROOM PLANS
- C. SEE SHEET A100 FOR DOOR AND WINDOW TYPES
- D. 1/2" MINOR DIMENSIONS ARE TO FACE OF SHIMMING UNLESS NOTED OTHERWISE. INTERIOR DIMENSIONS ARE TO FACE OF STUD UNLESS NOTED OTHERWISE

**KEYNOTES**

- 1. REFERENCE TO SH SPECIFICATIONS
- 2. FREEZER. SEE SPECIFICATIONS
- 3. AIRWORK GROUP #10. MOUNT AT 17" TO 1" OF AIRWORK. SEE ALPHA SPECIFICATIONS
- 4. RECESSED TEC CABINET. SEE DETAIL A1400
- 5. PROVIDE CLOSED CELL SPRAY FOAM INSULATION TO PROTECT PLUMBING NEEDS. SEE PLUMBING DRAWINGS FOR EXACT LOCATIONS



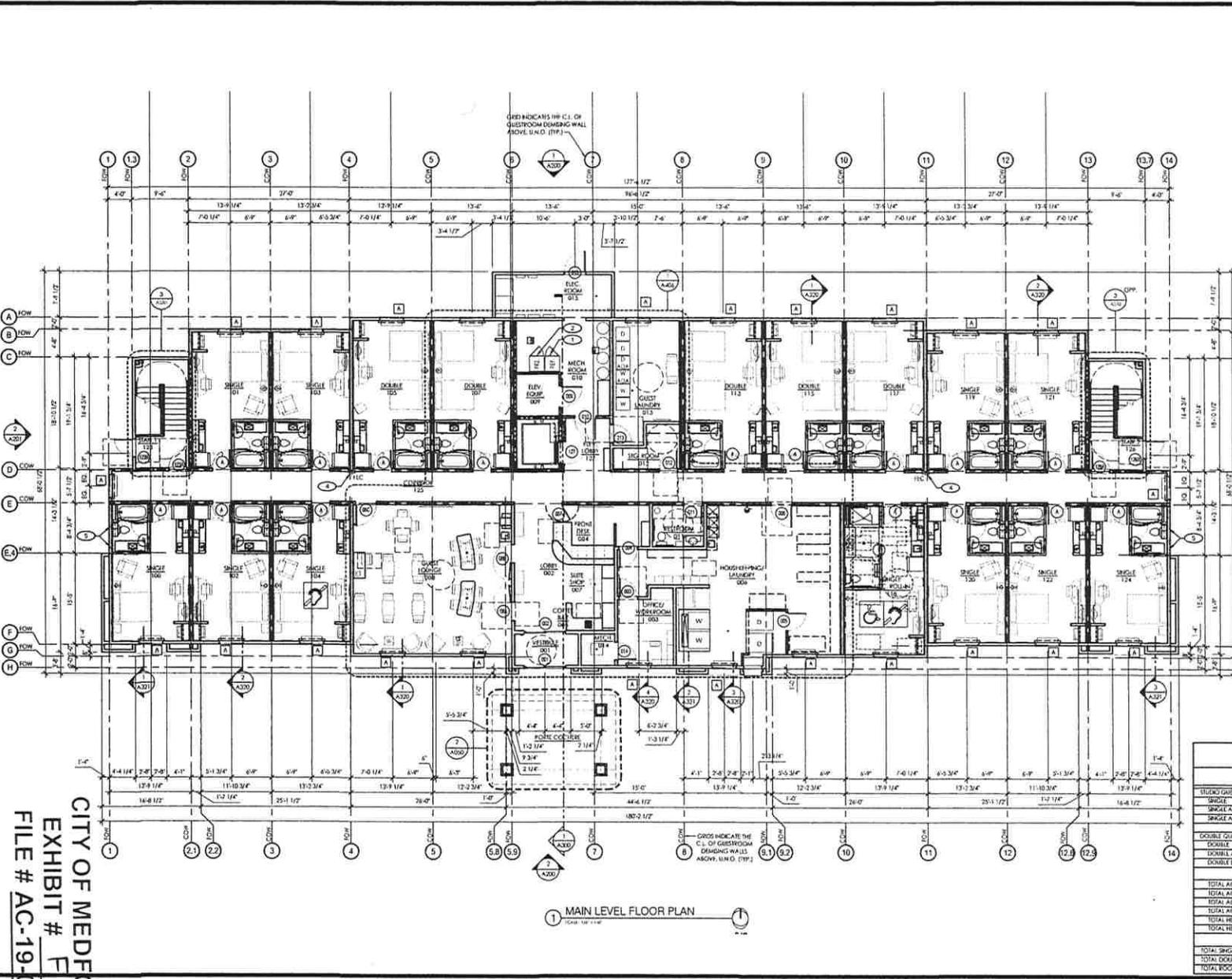
**THE RICHARDSON DESIGN PARTNERSHIP, LLC.**  
 310 South Hill Road  
 5th Floor, Suite 500  
 P.O. Box 4880  
 P.O. Box 4880  
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**MY PLACE HOTELS**  
 4101 13th Line  
 PROJECT

DATE: 03/01/22  
 DESCRIPTION: 00000000

PROJECT # 16-217

**MAIN LEVEL FLOOR PLAN**



**ROOM DISTRIBUTION MATRIX**

GUEST ROOMS	ROOM TYPES				TOTALS
	1	2	3	4	
STUDIO SUITE					
SINGLE	10	10	10	10	40
SINGLE ACCESSIBLE WALK-IN	0	0	0	0	0
SINGLE ACCESSIBLE	0	0	0	0	0
DOUBLE QUEEN					
DOUBLE	4	9	10	10	33
DOUBLE ACCESSIBLE	0	1	1	1	3
DOUBLE ENTICED	0	2	3	2	7
TOTAL ACCESSIBLE REQUIRED (FMR)	0	0	0	0	0
TOTAL ACCESSIBLE PROVIDED (FMR)	0	2	1	1	4
TOTAL ACCESSIBLE REQUIRED (FMR)	0	0	0	0	0
TOTAL ACCESSIBLE PROVIDED (FMR)	0	0	0	0	0
TOTAL HEARING IMPAIRED REQUIRED	0	0	0	0	0
TOTAL HEARING IMPAIRED PROVIDED	0	0	0	0	0
TOTAL SINGLE	11	11	10	10	42 (100%)
TOTAL DOUBLE	4	12	13	13	42 (100%)
TOTAL ROOMS	15	23	23	23	84

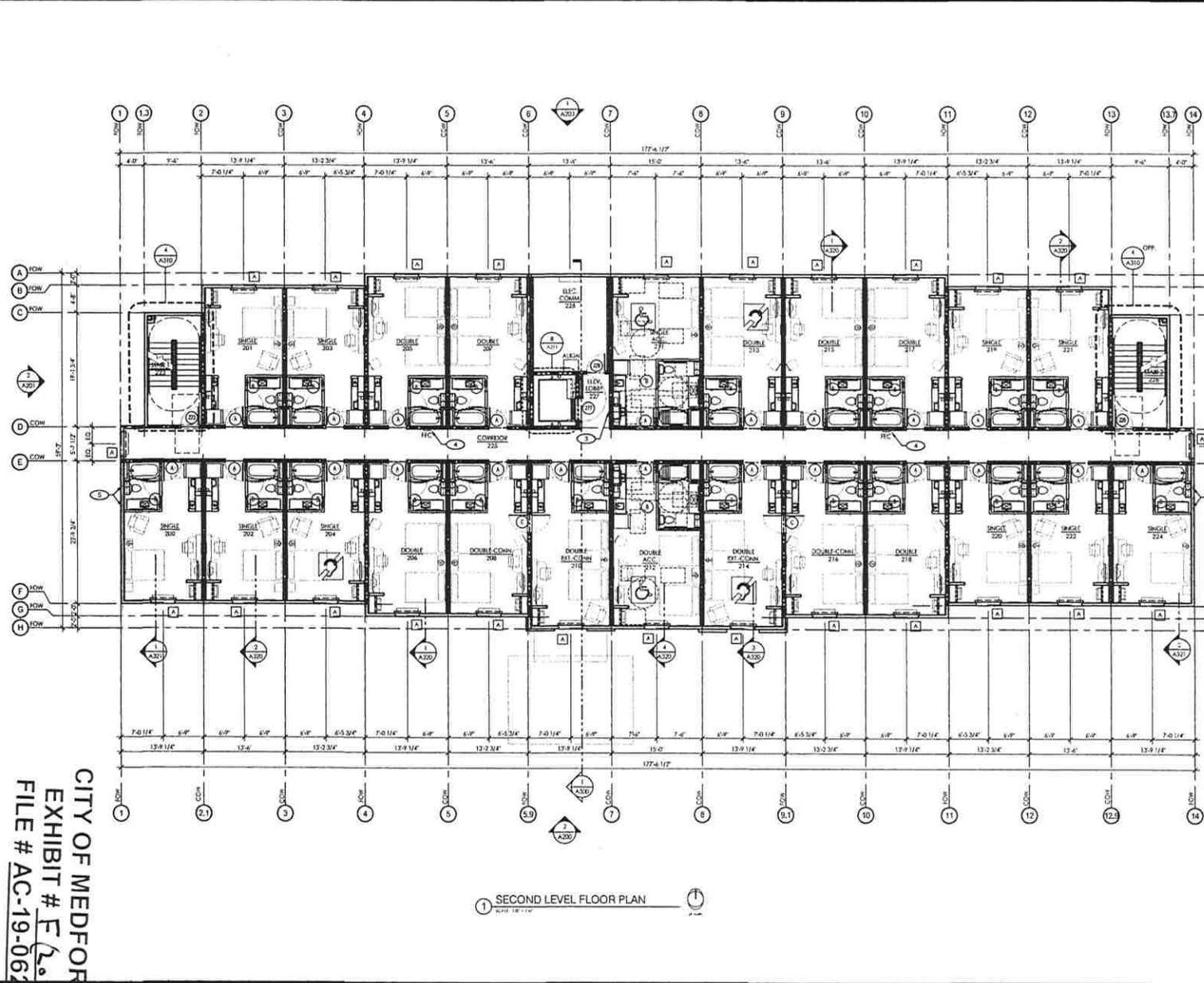
CITY OF MEDFORD  
 EXHIBIT # F (1 of 4)  
 FILE # AC-19-062

- GENERAL NOTES**
- SEE A10 SERIES SHEETS FOR WALL TYPE INDICATION PLANS.
  - SEE A06 SERIES SHEETS FOR ENLARGED GUESTROOM PLANS.
  - SEE SHEET A200 FOR DOOR AND WINDOW TYPES.
  - FINISH WORK DIMENSIONS ARE TO FACE OF FINISHING UNLESS NOTED OTHERWISE. FINISH WORK DIMENSIONS ARE TO FACE OF FINISH UNLESS NOTED OTHERWISE.
- KEYNOTES**
- REFRIGERATOR - SEE SPECIFICATIONS
  - FREEZER - SEE SPECIFICATIONS
  - ARTWORK GROUP #10, LOCATED AT A11.7 TO 11.9 OF ARTWORK - SEE ARCHITECT SPECIFICATIONS
  - RECESSED REC. CABINET - SEE DETAIL A1000
  - PROVIDE CLOSED-CELL SPRAY FOAM INSULATION TO PROTECT PLUMBING RISERS. SEE PLUMBING DRAWINGS FOR EXACT LOCATIONS

**THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.**  
 515 South 485 East  
 508 West City, Utah 84102  
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 F: 801.258.4840

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1 SECOND LEVEL FLOOR PLAN  
 DATE 10-1-17

**ROOM DISTRIBUTION MATRIX**

GUEST ROOMS	ROOM LEVEL				TOTALS
	1	2	3	4	
<b>SINGLE GREEN</b>					
SINGLE	10	10	10	10	30
SINGLE ACCESSIBLE	1	0	0	0	1
SINGLE ACCESSIBLE	0	1	0	0	1
<b>DOUBLE GREEN</b>					
DOUBLE	4	2	10	10	26
DOUBLE ACCESSIBLE	0	1	1	1	3
DOUBLE EXTENDED	0	2	2	2	6
<b>TOTAL ACCESSIBLE REQUIRED (RUM)</b>					4
TOTAL ACCESSIBLE PROVIDED (RUM)	0	2	1	1	4
<b>TOTAL ACCESSIBLE REQUIRED (RUM)</b>					1
TOTAL ACCESSIBLE PROVIDED (RUM)	1	0	0	0	1
<b>TOTAL HEARING IMPAIRED REQUIRED</b>					0
TOTAL HEARING IMPAIRED PROVIDED	2	3	2	2	9
<b>TOTAL SINGLE</b>	11	11	10	10	42 (50%)
<b>TOTAL DOUBLE</b>	4	12	13	13	42 (50%)
<b>TOTAL ROOMS</b>	15	23	23	23	64

**MY PLACE HOTELS**  
 PROJECT # 1000000000

**SECOND LEVEL FLOORPLAN**

**A102**

PROJECT # 10217

- GENERAL NOTES**
- A. SEE A10 SERIES SHEETS FOR WALL TYPES INDICATION PLANS
  - B. SEE A400 SERIES SHEETS FOR ENLARGED GUESTROOM PLANS
  - C. SH SHEET A100 FOR DOOR AND WINDOW TYPES
  - D. EXTERIOR DIMENSIONS ARE TO FACE OF CHANGING UNLESS NOTED OTHERWISE. INTERIOR DIMENSIONS ARE TO FACE OF STUD UNLESS NOTED OTHERWISE.
- KEYNOTES**
- 1 REFRIGERATOR. SEE SPECIFICATIONS
  - 2 FREEZER. SEE SPECIFICATIONS
  - 3 ARTWORK GROUP W/O. ABOUT 6" X 18" X 4" OF ARTWORK. SEE ALPHA SPECIFICATIONS
  - 4 RECESSED REC CABINET. SEE DETAIL A1000
  - 5 PROVIDE CLOSED-CELL SPRAY FOAM INSULATION TO PROTECT PLUMBING RISERS. SEE PLUMBING DRAWINGS FOR EXACT LOCATIONS.



**THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.**  
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 SALT LAKE CITY, UTAH 84102  
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 F: 801.338.8880

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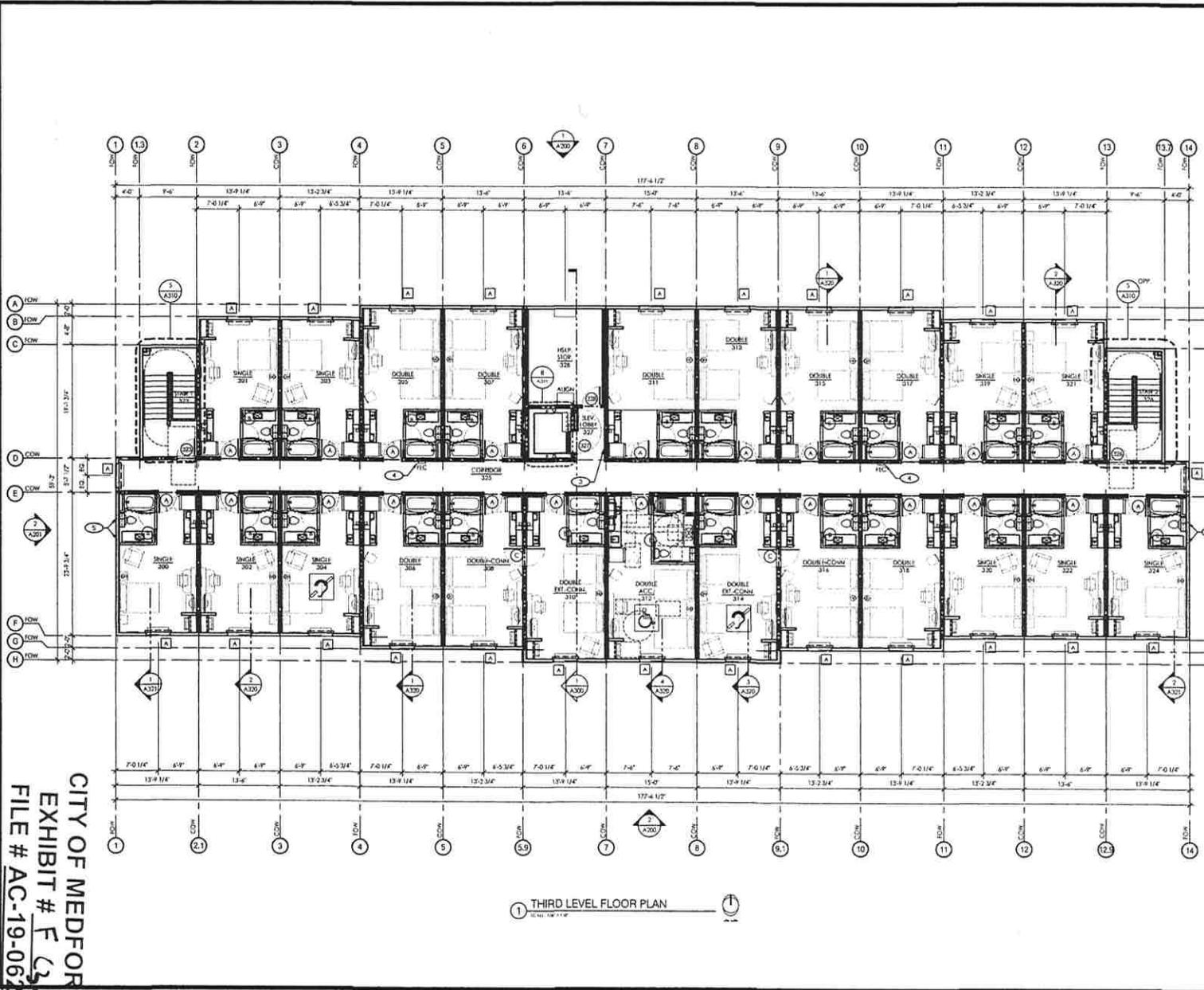
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**MY PLACE HOTELS**  
 PROJECT

DATE: DESCRIPTION:  
 10/10/2018 1000000000

PROJECT #  
 100121

**THIRD LEVEL FLOOR PLAN**  
**A103**



**1 THIRD LEVEL FLOOR PLAN**  
SCALE: 1/8" = 1'-0"

**ROOM DISTRIBUTION MATRIX**

GUEST ROOMS	ROOM LEVEL				TOTALS
	1	2	3	4	
<b>STUDIO SUITES</b>					
SINGLE	10	10	10	10	40
SINGLE ACCESSIBLE ROOMS	1	0	0	0	1
SINGLE EXTENDED	0	0	0	0	0
<b>DOUBLE SUITES</b>					
DOUBLE	5	9	10	10	34
DOUBLE ACCESSIBLE	0	1	1	1	3
DOUBLE EXTENDED	0	3	2	2	7
<b>TOTAL ACCESSIBLE REQUIRED (TAR)</b>					4
<b>TOTAL ACCESSIBLE PROVIDED (TAP)</b>					4
<b>TOTAL ACCESSIBLE PROVIDED (TAP) - (TAR)</b>					0
<b>TOTAL ACCESSIBLE PROVIDED (TAP) - (TAR) - (TAP)</b>					0
<b>TOTAL ACCESSIBLE PROVIDED (TAP) - (TAR) - (TAP) - (TAP)</b>					0
<b>TOTAL HEARING IMPAIRED ROOMS</b>					0
<b>TOTAL HEARING IMPAIRED PROVIDED</b>					0
<b>TOTAL SINGLE</b>	11	11	10	10	42 (100%)
<b>TOTAL DOUBLE</b>	5	13	13	13	44 (100%)
<b>TOTAL ROOMS</b>	16	24	23	23	66

- GENERAL NOTES**
- A. SEE A10 SERIES SHEETS FOR WALL TYPES INDICATION PLANS
  - B. SEE A400 SERIES SHEETS FOR ENLARGED GUESTROOM PLANS
  - C. SEE SHEET A100 FOR DOOR AND WINDOW TYPES
  - D. EXTERIOR FINISHES ARE TO FACE OF CONCRETE UNLESS NOTED OTHERWISE. INTERIOR FINISHES ARE TO FACE OF GIRD UNLESS NOTED OTHERWISE.
- KEYNOTES**
- 1 REFRIGERATOR. SEE SPECIFICATIONS
  - 2 FREEZER. SEE SPECIFICATIONS
  - 3 ARTWORK GROUP #10. MOUNT AT A/F TO A OF ARTWORK. SEE ARCHIT. SPECIFICATIONS
  - 4 RECESSED TV CABINET. SEE DETAIL 60A00
  - 5 PROVIDE CLOSED-CELL SPRAY FOAM INSULATION TO PROTECT PLUMBING RISERS. SEE PLUMBING DRAWINGS FOR EXACT LOCATIONS



**THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.**  
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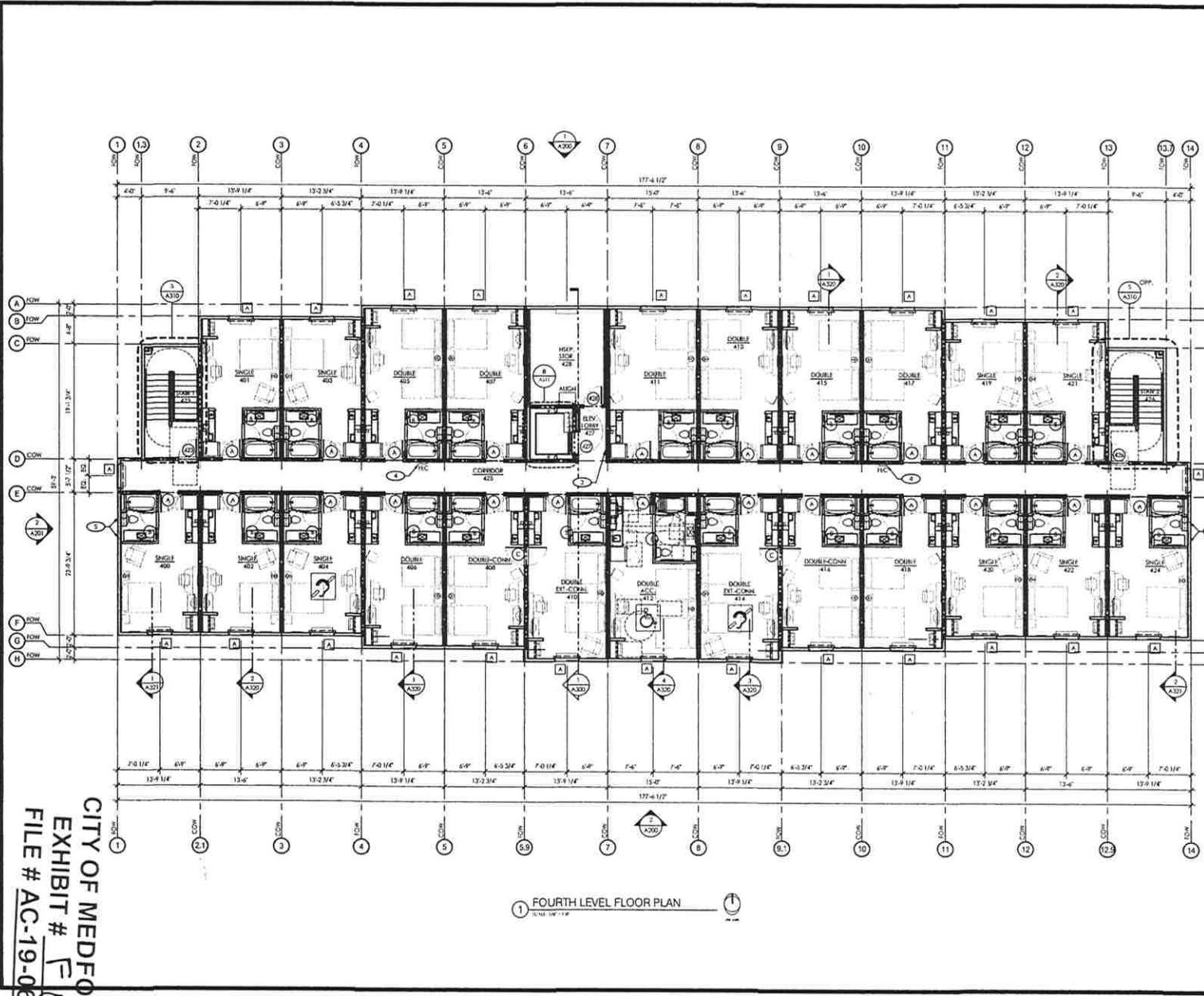
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**MY PLACE HOTELS**  
 PROJECT

DATE: DISCIPLINE:  
 XXXXXXXX XXXXXXXX

FOURTH LEVEL FLOOR PLAN  
 PROJECT # 19-037

**A104**

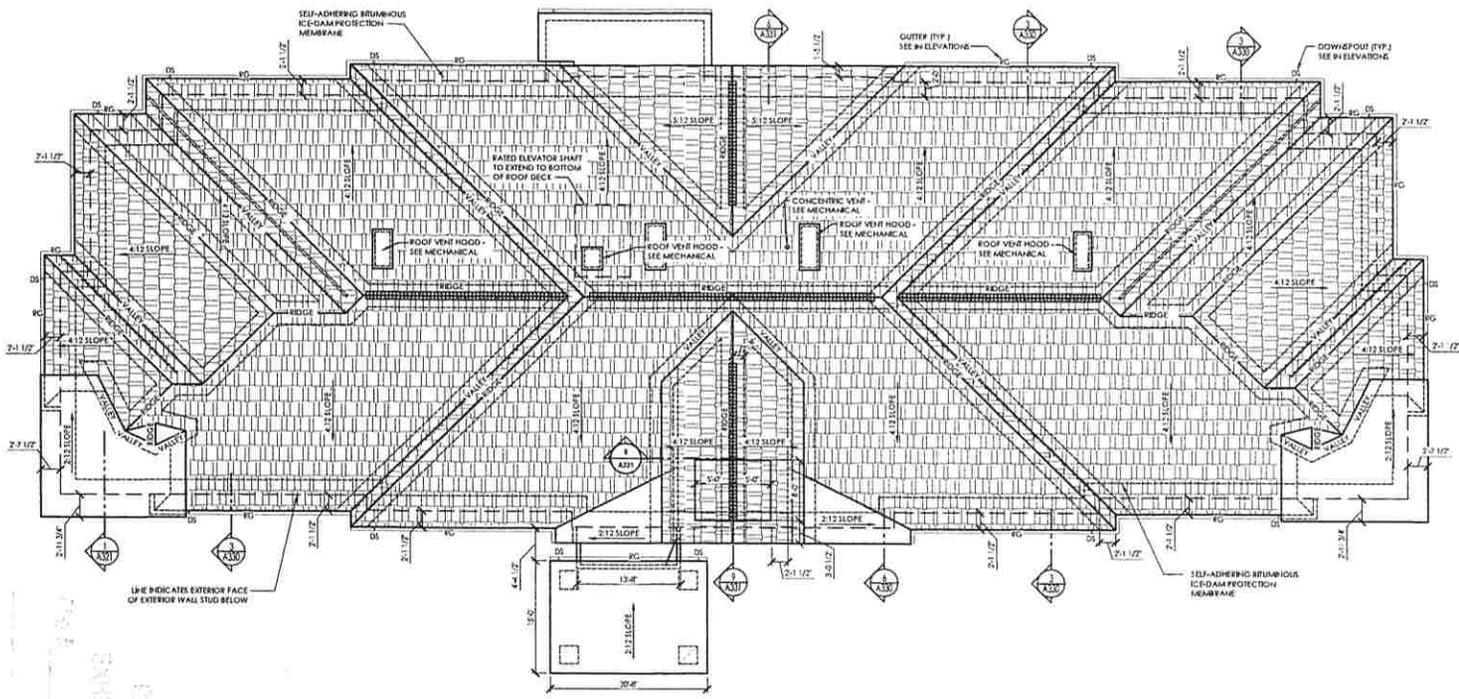


1 FOURTH LEVEL FLOOR PLAN  
 SCALE: 1/8" = 1'-0"

**ROOM DISTRIBUTION MATRIX**

GUEST ROOMS	ROOM LEVEL				TOTALS
	1	2	3	4	
<b>SINGLE QUEEN</b>					
SINGLE	10	10	10	10	40
SINGLE ACCESSIBLE (PROV)	1	0	0	0	1
SINGLE ACCESSIBLE	0	1	0	0	1
<b>DOUBLE QUEEN</b>					
DOUBLE	5	9	10	10	34
DOUBLE ACCESSIBLE	0	1	1	1	3
DOUBLE EXTENDED	0	2	2	2	6
<b>TOTAL ACCESSIBLE PROVIDED (RUB)</b>					4
<b>TOTAL ACCESSIBLE PROVIDED (RUB)</b>					4
<b>TOTAL ACCESSIBLE PROVIDED (RUB)</b>					1
<b>TOTAL ACCESSIBLE PROVIDED (RUB)</b>					1
<b>TOTAL HEARING IMPAIRED PROVIDED</b>					0
<b>TOTAL HEARING IMPAIRED PROVIDED</b>					0
<b>TOTAL SINGLE</b>	11	11	10	10	42 (44%)
<b>TOTAL DOUBLE</b>	5	12	13	13	43 (50%)
<b>TOTAL ROOMS</b>	16	23	23	23	65

CITY OF MEDFORD  
 EXHIBIT # F-104  
 FILE # AC-19-062



1 ROOF PLAN  
SCALE: 1/8" = 1'-0"

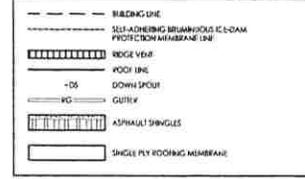
**ROOF PLAN GENERAL NOTES:**

**ATC VENTILATION REQUIRED:**  
 1000 PPH ATC VENTILATION  
 PITCHED ROOF: 1000 PPH = 250 SQ. FT. ROOF AREA = 9.589 T.S.  
 9.589 DIVIDED BY 300 S.F. = 31.88 T.S.  
 31.88 MULTIPLIED BY 144 SQ. IN. = 4593 SQ. IN.  
 PITCHED ROOF AREA TOTAL SQ. IN. VENTILATION REQUIRED = 4593 SQ. IN.  
**ROOF VENTILATION PROVIDED AT ROOF HIGH POINT:**  
 127 IN. FEET OF RIDGE VENT (PER SPECIFICATIONS) WITH 15.8 SQ. IN. N.Y.A. PER  
 FOOT (300" x 48" = 2,916 SQ. IN.)  
**ATC VENTILATION PROVIDED:**  
 OVERALL TOTAL VENTILATION PROVIDED: 4,973 SQ. IN. MEETS VENTS AS SHOWN  
 PROVIDED IN RIDGE - 2,204 SQ. IN. = 2,307 SQ. IN. TO BE PROVIDED @ SOFFIT.  
 CONTRACTOR TO VERIFY.

- GENERAL NOTES:**
- SEE ELEVATIONS FOR ADDITIONAL GUTTER AND DOWNSPOUT INFORMATION.
  - ALL VALLEY CONDITIONS TO RECEIVE METAL FLASHING.
  - TYPICAL ROOF FINISH TO BE ASPHALT SHINGLES UNLESS NOTED OTHERWISE (S.M.C.).
  - ALL VENTS THROUGH THE ROOF ARE TO BE SET IN ATIC AND ONLY PENETRATE ROOF ON ELECTRICAL ROOM SIDE OF RIDGE (IN O.C.).
  - VERIFY ALL MECHANICAL EQUIPMENT LOCATIONS WITH MECHANICAL DRAWINGS.
  - DRIFT STOPS LOCATED WITH ASSUMED SPINERULE ATIC, VERIFY WITH LOCAL CODE OF JURISDICTION REQUIREMENTS.
  - DO NOT SMASH FINISH AREA OF RIDGES BETWEEN MAIN ROOF & DOWNPOUT. DOWNPOUTS MUST BE VENTILATED.
  - FRAMER TO BUILD CAT WALKS IN ATIC. PROVIDE 2" x 4" OSB FLOOR SYSTEM 4" UP OVER 2" x 4" FRAMING FROM ATIC ACCESS TO MECHANICAL EXHAUST FANS AS INDICATED ON MECHANICAL DRAWINGS. MAINTAIN MIN. 6" HIGH CLEAR. IN MAIN WALKWAY.
  - PROVIDE LIGHTING EVERY 30' WITH OUTLET PLUG ON THE PORCELAIN INTERIOR. SWITCH SHOULD BE INSTALLED AT ATIC ACCESS AREA. SEE ELECTRICAL.
  - FRAMING FOR ATIC ACCESS DOORS TO BE 2" x 4" PLYWOOD BOX BETWEEN RIDGES 7' HIGH AND 3' LONG.
  - PROVIDE RIPA 1/2" X 3/4" SPINERULE SYSTEM IN COMBUSTIBLE CONCEALED ATIC SPACES.

- GENERAL NOTES:**
- SEE MECHANICAL DRAWINGS FOR THEIR WALL LOCATIONS.
  - REFER TO MECHANICAL & PLUMBING DRAWINGS FOR LOCATIONS & SIZES OF ROOF PENETRATIONS.

**LEGEND**



**THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.**  
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 508 Linn City, Utah 84102  
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**WYAT PLACE HOTELS**  
 1500 WYAT PLACE  
 PROJECT # 16-217

DATE	DESCRIPTION
06/05/2024	0000000000

**ROOF PLAN**  
**A105**

DATE: 06/05/24  
 DRAWN BY: G  
 PROJECT: AC-14-062

**COLOR LEGEND**

- (A) MAKE: JAMES HARVE  
COLOR: ELEGANT RED
- (B) MAKE: JAMES HARVE  
COLOR: MISTRAL BLUE
- (C) MAKE: JAMES HARVE  
COLOR: SWIRLY BEIGE

**MATERIAL LEGEND**

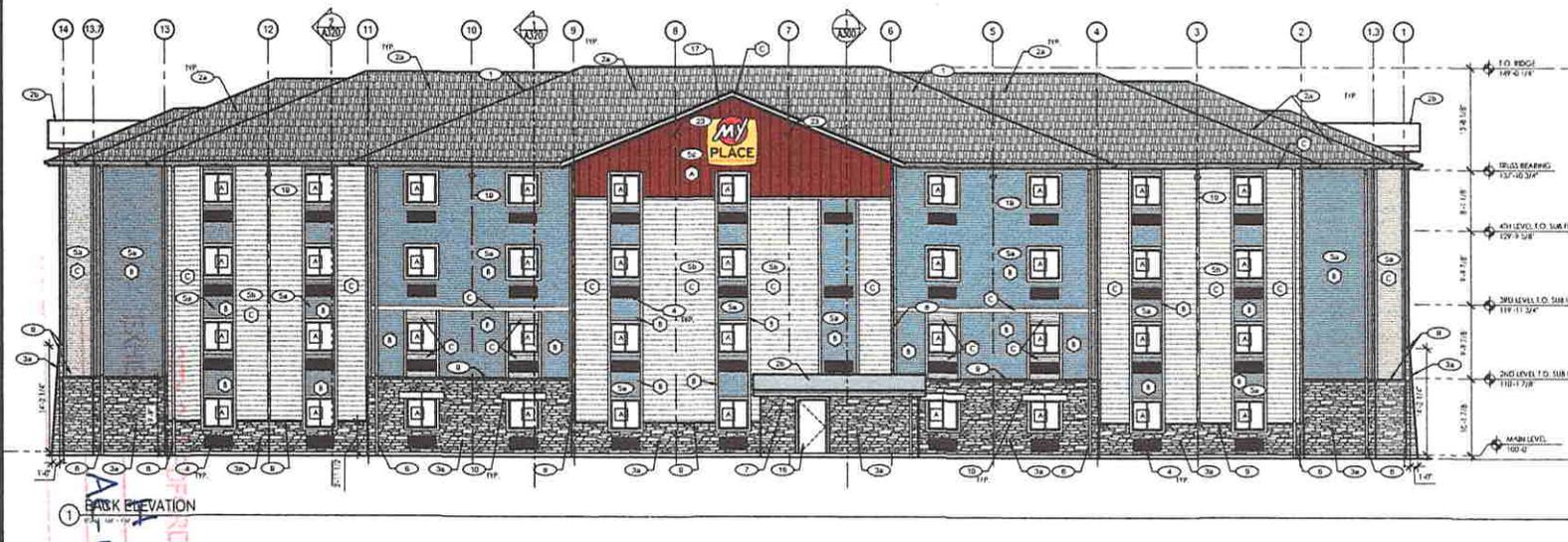
- PROVIDE THE FOLLOWING MAKE OF DESIGN FINISHES OR APPROVED EQUAL PRODUCTS
- (1) ASPHALT SHINGLES  
COLOR: MATCH CHIMNEY
  - (2) SINGLE-PLY MEMBRANE ROOF  
COLOR: OWNER TO SELECT
  - (3) ADHERED STONE VENEER 'A'  
MAKE: HARRISTONE  
TYPE: A COLOR: OWNER TO SELECT
  - (4) CHIMNEY BOARD SIDING 'A' - LAP SIDING  
EXPOSURE: 4"  
COLOR: SEE ELEVATION
  - (5) CHIMNEY BOARD SIDING 'B' - LAP SIDING  
EXPOSURE: 8"  
COLOR: SEE ELEVATION
  - (6) CHIMNEY BOARD SIDING 'C' - ROMED AND BATTEN  
EXPOSURE: 14" VERTICAL (14" PAWNS WIDTH)  
COLOR: SEE ELEVATION
  - (7) DISCREPANT STONE LINES  
MAKE: HARRISTONE  
COLOR: OWNER TO SELECT
  - (8) SOFFIT PANEL  
EXPOSURE: 12"  
COLOR: COLOR TO MATCH 'SWIRLY BEIGE'
- COLOR OF ALL FACIAL TRIM & GUTTERS TO MATCH SOFFIT PANEL

**KEYNOTES**

- (1) RIDGE VENT
- (2) ASPHALT SHINGLES  
SEE MATERIAL LEGEND
- (3) SINGLE-PLY ROOF MEMBRANE  
SEE MATERIAL LEGEND
- (4) ADHERED STONE VENEER 'A'  
SEE MATERIAL LEGEND
- (5) ADHERED STONE VENEER 'B'  
SEE MATERIAL LEGEND
- (6) HEADING/CORNING ENDS: VENT ROUGH  
OPENING W/ MANUFACTURER
- (7) CHIMNEY BOARD SIDING W/ 4" EXPOSURE  
SEE MATERIAL LEGEND
- (8) CHIMNEY BOARD SIDING W/ 8" EXPOSURE  
SEE MATERIAL LEGEND
- (9) CHIMNEY BOARD SIDING W/ 14" EXPOSURE  
SEE MATERIAL LEGEND
- (10) GUTTER AND DOWNSPOUTS: SIZE, AND LOCATION  
TO BE DETERMINED BY RAINFALL CALCULATIONS  
AND SHOP DRAWINGS. VERIFY DOWNSPOUT  
DISCHARGE W/ G.C. TO PREVENT WATER  
FLOWING ACROSS (LANDSCAPING OR COVER  
RECOMMENDED WHEN REQUIRED). ROUTE  
DOWNSPOUTS TO STORM SEWER. SEE CIVIL  
DRAWINGS.
- (11) EXTERIOR LIGHTING: SEE DETAIL 11A(33)
- (12) SIDING BAND AT MATERIAL CHANGE AND  
WINDOW TRIM COLOR TO MATCH 'SWIRLY BEIGE'
- (13) STONE CAP: COLOR TO MATCH STONE LINE  
PROVIDE METAL FLASHING: PAINT TO MATCH  
STONE UNTIL TO RUN BEHIND SIDING AND OVER  
STONE CAP.
- (14) STONE DECORATIVE ENDS (10/17/2)
- (15) ROUGH SAWY TIMBER BEAMS: CLEAR COAT  
FINISH
- (16) 8"X 11" PLACE BUILDING SIGNAGE UNDER  
SEPARATE PLUMB
- (17) SOFFIT PANEL: INSTALL VENT PANELS AS INDICATED  
ON ROOF PLAN. SEE MATERIAL LEGEND
- (18) 3"X 11" PLACE BUILDING SIGNAGE UNDER  
SEPARATE PLUMB
- (19) WOOD DECORATIVE BRACKET: SEE DETAIL 11A(33)  
FINISH TO MATCH TIMBER BEAMS
- (20) DOOR: AS SCHEDULED
- (21) 7"X 11" PLACE BUILDING SIGNAGE UNDER  
SEPARATE PLUMB
- (22) ROOF PARAPET SCUPPER DRAIN
- (23) BUILDING MOUNTED FLOOD LIGHTING: MOUNT AT  
37'-0" AFF. SEE DETAIL 11A(33)
- (24) BUILDING ADDRESS & BUILDING ID NUMBERS PER  
MICHIGAN CITY CODE: TITLE 10 CH. 4 SECTION  
505.1.1
- (25) BUILDING MOUNTED FLOOD LIGHTING: MOUNT AT  
37'-0" AFF. SEE DETAIL 11A(33)
- (26) BUILDING MOUNTED FLOOD LIGHTING: MOUNT AT  
42'-0" AFF. SEE DETAIL 11A(33)
- (27) BUILDING MOUNTED FLOOD LIGHTING: MOUNT AT  
42'-0" AFF. SEE DETAIL 11A(33)



2 FRONT ELEVATION



1 BACK ELEVATION



THE  
RICHARDSON  
DESIGN  
PARTNERSHIP,  
LLC.

810 South 100 East  
Salt Lake City, Utah 84102  
P: 801.335.4848  
F: 801.335.4880

NOT FOR  
CONSTRUCTION

PROVIDOR ENGINEER/ARCHITECT OF  
DESIGN CENTER AND/OR FOR  
CONSTRUCTION. THE OWNER  
PROVIDES CONTRACT DOCUMENTS  
AND CONDITIONS SHALL BE SOLELY  
RESPONSIBLE FOR INTERPRETING  
CONTRACT DOCUMENTS AND  
BE AWARE OF ANY AMENDMENTS,  
REVISIONS AND/OR SUPPLEMENTAL  
REQUIREMENTS OF ADMINISTRATIVE  
AGREEMENTS AND SPECIFICATIONS.

PROJECT  
DATE: DIS-CRIP-100  
NOV/06/06 XXXXXXXX

MY PLACE HOTELS  
AT 1500 E 1000 N

EXTERIOR  
ELEVATIONS - URBAN  
PROJECT #  
A200  
16.2017

10/17/2)  
14-062



ELEGANT RED  
MANUF. JAMES HARBOR  
COLOR: ELEGANT RED



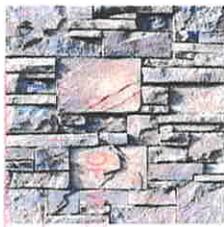
NEPTUNE BLUE  
MANUF. HARBOR  
COLOR: NEPTUNE BLUE



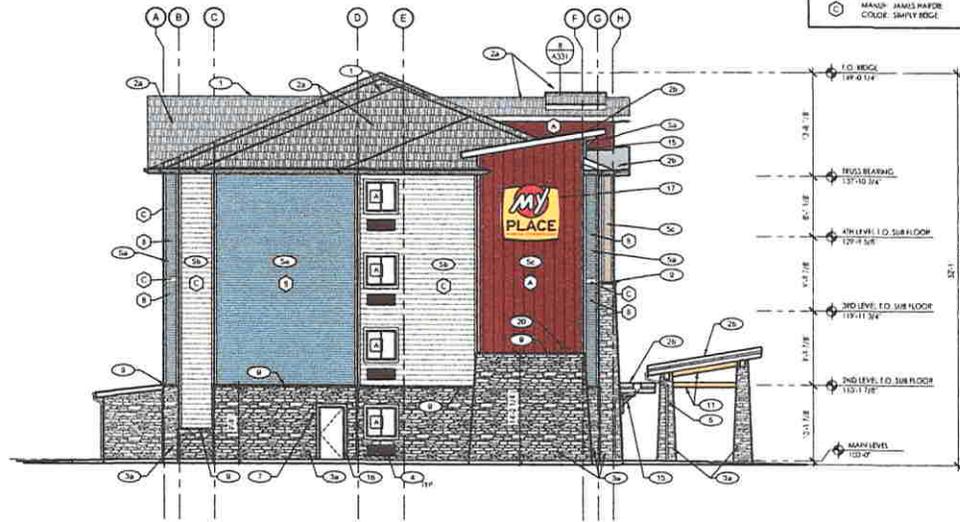
SIMPLY BEIGE  
MANUF. HARBOR  
COLOR: SIMPLY BEIGE



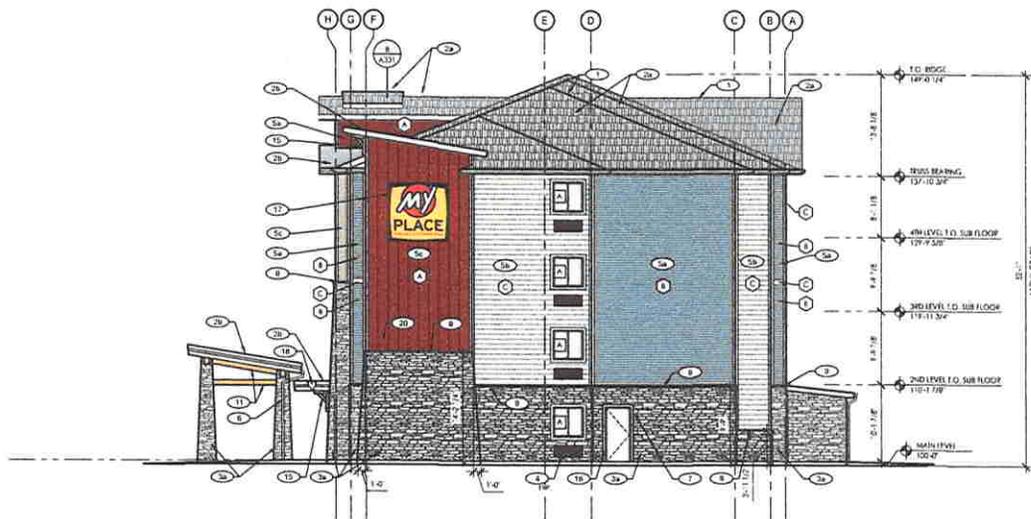
EXTATE GRAY  
MANUF. HARBOR  
COLOR: EXTATE GRAY



CAPE COD  
MANUF. HARBOR  
COLOR: CAPE COD



2 SIDE ELEVATION  
SCALE: 1/8\"/>



1 SIDE ELEVATION  
SCALE: 1/8\"/>

COLOR LEGEND	
1	MANUF. JAMES HARBOR COLOR: ELEGANT RED
2	MANUF. JAMES HARBOR COLOR: NEPTUNE BLUE
3	MANUF. JAMES HARBOR COLOR: SIMPLY BEIGE

MATERIAL LEGEND	
1	PROVIDE THE FOLLOWING BASIS OF DESIGN FINISHES OR APPROVED EQUAL PRODUCTS: ASPHALT SHINGLES COLOR: BASS-CROWNS CORNER: ESTE GRAY SINGLE PLY MEMBRANE ROOF COLOR: OWNER TO SELECT
2	ADHERED STONE VENEER "A" MANUF. HARBOR SITE & COLOR: OWNER TO SELECT
3	ADHERED STONE VENEER "B" MANUF. HARBOR SITE & COLOR: OWNER TO SELECT
4	CEMENT BOARD SINGING "A" - LAP SINGING EXPOSURE: 4" COLOR: SEE ELEVATION
5	CEMENT BOARD SINGING "B" - LAP SINGING EXPOSURE: 4" COLOR: SEE ELEVATION
6	CEMENT BOARD SINGING "C" - BOARD AND BATTEN EXPOSURE: 1/4" VERTICAL (1/2" PANEL WIDTH) COLOR: SEE ELEVATION
7	DISCREETIVE STONE (INH) MANUF. HARBOR COLOR: OWNER TO SELECT
8	SOFFIT PANEL EXPOSURE: 1/4" COLOR: COLOR TO MATCH "SIMPLY BEIGE" COLOR OF ALL FASCIA, TRIM & GUTTERS TO MATCH SOFFIT PANEL

- KEYNOTES**
- ROOF VENT
  - ASPHALT SHINGLES  
SEE MATERIAL LEGEND.
  - SINGLE PLY ROOF MEMBRANE  
SEE MATERIAL LEGEND.
  - ADHERED STONE VENEER "A"  
SEE MATERIAL LEGEND.
  - ADHERED STONE VENEER "B"  
SEE MATERIAL LEGEND.
  - HANGING COILING UNIT - VERIFY ROUGH  
FRAMING W/ MANUFACTURER.
  - CEMENT BOARD SINGING W/ 4" EXPOSURE  
SEE MATERIAL LEGEND.
  - CEMENT BOARD SINGING W/ 8" EXPOSURE  
SEE MATERIAL LEGEND.
  - CUTTER AND DOWNSPOUTS, SIZE AND LOCATION  
TO BE DETERMINED BY PUNCH CALCULATIONS  
AND SHOP DRAWINGS. VERIFY DOWNSPOUT  
DISCHARGE W/ G.C. TO PREVENT WATER  
FLOWING ACROSS LANDSCAPING OR COVER  
DOWNWALLS WHEN REQUIRED. ROOTS  
DOWNSPOUTS TO STORM SEWER. SEE CIVIL  
DRAWINGS.
  - EXTERIOR LIGHTING. SEE DETAIL 11(A33).
  - SEWER BASED AT MATERIAL CHANGE AND  
WINDOW TRIM COLOR TO MATCH "SIMPLY BEIGE".
  - STONE CAP. COLOR TO MATCH STONE UNIT.  
PROVIDE METAL FLASHING. PAINT TO MATCH  
STONE UNIT. TO MATCH BEING SINGING AND OVER  
STONE CAP.
  - STONE DISCREETIVE (INH) (11(A37)).
  - ROUGH SAWN TRIMMER BEAMS. CIVIL COAT  
FINISH.
  - 8" O" MY PLACE BUILDING SEWAGE, UNDER  
SEPARATE PERMIT.
  - 3" O" MY PLACE BUILDING SEWAGE, UNDER  
SEPARATE PERMIT.
  - WOOD DISCREETIVE BRACKET. SEE DETAIL 11(A33).  
FINISH TO MATCH TRIMMER BEAMS.
  - DOOR AS SCHEDULED.
  - 7" O" MY PLACE BUILDING SEWAGE, UNDER  
SEPARATE PERMIT.
  - ROOF PARAPET SCUPPER DRAIN.
  - BUILDING MOUNTED FLOOD LIGHTING. MOUNT AT  
38'-0" AFF. SEE DETAIL 11(A33).
  - BILL BOARD ADDRESS. 6" READING LETTERS PER  
MICHIGAN CITY CODE - TITLE 10 CH. 4 SECTION  
505.1.1
  - BUILDING MOUNTED FLOOD LIGHTING. MOUNT AT  
42'-0" AFF. SEE DETAIL 11(A33).
  - BUILDING MOUNTED FLOOD LIGHTING. MOUNT AT  
42'-0" AFF. SEE DETAIL 11(A33).
  - BUILDING MOUNTED FLOOD LIGHTING. MOUNT AT  
42'-0" AFF. SEE DETAIL 11(A33).



THE RICHARDSON  
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MY PLACE HOTELS  
PROJECT

DATE	DESCRIPTION
XX/XX/XX	XXXXXXX

EXTERIOR ELEVATIONS - URBAN  
PROJECT #  
A201

Handwritten notes: H (2 of 2), AC-19-062



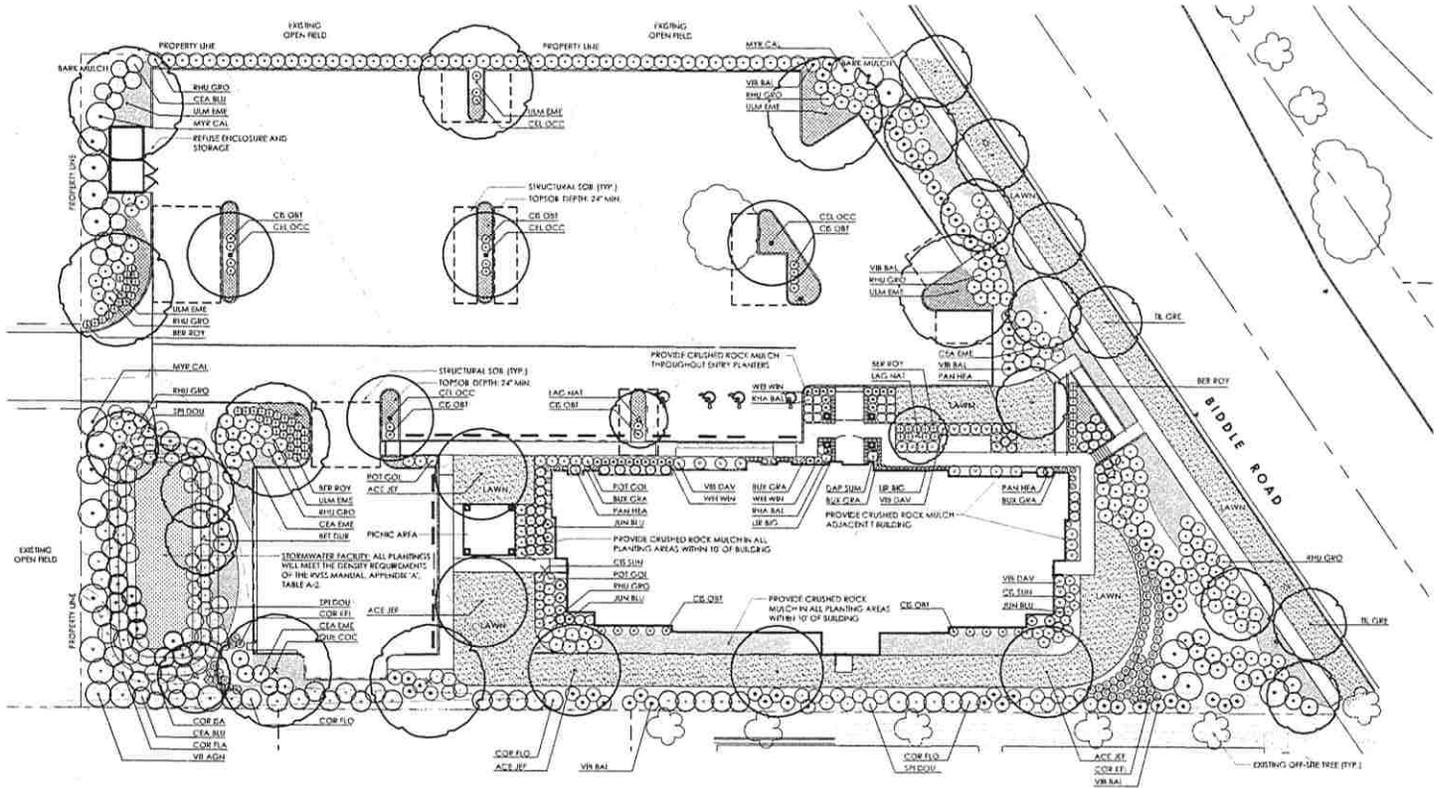
MY PLACE HOTEL  
590 AIRPORT ROAD  
Medford, Oregon 97504



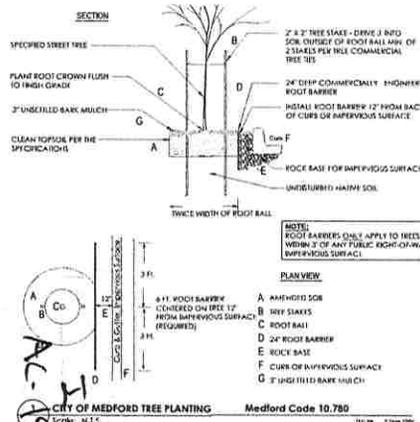
REVISIONS  
# DATE DESCRIPTION

SPAC SUBMITTAL  
PRELIMINARY PLANTING PLAN

L1.0  
PROJECT NO. 1938  
10.04.19  
TEAM: JCL



Page 29



PRELIMINARY PLANT LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	ALTERNATE
<b>TREES</b>				
ACE JEF	<i>Acer x freemanii</i> 'Jefferson'	Ashburn Black Maple	1.75' cal	<i>Acer rubrum</i> 'October Glory'
ACE JEF	<i>Acer x freemanii</i> 'Jefferson'	Ashburn Black Maple	10' T.L.	<i>Betula nigra</i>
ACE JEF	<i>Acer x freemanii</i> 'Jefferson'	Ashburn Black Maple	1.75' cal	<i>Prunus x vanvolkenburghii</i> 'Royal Burgundy'
LAG NAT	<i>Lagotis arvensis</i> 'hatchler'	Hatchler White Crane Myrtle	1.75' cal	<i>Lagotis arvensis</i> 'Glenmore White'
QUE COC	<i>Quercus coccinea</i>	Scarlet Oak	7' cal	<i>Quercus laevis</i>
TR GAT	<i>Tilia cordata</i> 'Greenstar'	Greenstar Linden	7' cal	<i>Tilia cordata</i> 'Robey'
ULM EME	<i>Ulmus parviflorus</i> 'Emerald Green'	Emerald Green	1.75' cal	<i>Ulmus parviflorus</i>
<b>SHRUBS</b>				
ABR ROS	<i>Abelia rosea</i>	Rose Creek Abelia	5 gal	<i>Abelia x grandis</i>
BER ROY	<i>Berberis x royal burgundy</i>	Royal Burgundy Berberis	2 gal	<i>Berberis monticola</i> 'Common Pyram'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	3 gal	<i>Saxifraga oppositifolia</i> 'Green Spire'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	5 gal	<i>Buxus x green velvet</i>
CTA FWE	<i>Ceanothus x emoryi</i> 'Frost-free'	Emory's Blueberry	2 gal	<i>Ceanothus gl. Pauli Meyer</i>
CEA BU	<i>Ceanothus x blue jean</i>	Blue Jean Ceanothus	2 gal	<i>Ceanothus l. 'Dark'</i>
CE OST	<i>Celastrus occidentalis</i>	White Rockrose	5 gal	<i>Celastrus x 'Sensation'</i>
CE SUN	<i>Celastrus x sunset</i>	Sunset Rockrose	2 gal	<i>Wegelia laevis</i> 'White and Rose'
DM SUM	<i>Daphne x summer joy</i>	Summer Joy Daphne	3 gal	<i>Daphne genkylifolia</i> 'Eternal Fragrance'
IRM SU	<i>Irigoienia scopulorum</i> 'Blue Arrow'	Blue Arrow Juniper	3 gal	<i>Juniperus x 'Spartan'</i>
UK BG	<i>Ulmus parviflorus</i> 'Big Blue'	Big Blue Ulm	1 gal	<i>Ulmus parviflorus</i> 'Silver Sunburst'
MYR CAI	<i>Myrica carolinensis</i>	Towhee Wax Myrtle	5 gal	<i>Viburnum coccineum</i>
PAH HEA	<i>Panicum virgatum</i> 'Heavy Metal'	Heavy Metal Switchgrass	1 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
POI GOL	<i>Potentilla fruticosa</i> 'Goldfinger'	Goldfinger Potentilla	2 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BHA BAL	<i>Baccharis halimifolia</i> 'Indica'	Indica Baccharis	3 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	1 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	2 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	3 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	4 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	5 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	6 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	7 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	8 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	9 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	10 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	11 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	12 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	13 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	14 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	15 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	16 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	17 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	18 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	19 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	20 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	21 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	22 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	23 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	24 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	25 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	26 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	27 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	28 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	29 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	30 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	31 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	32 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	33 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	34 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	35 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	36 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	37 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	38 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	39 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	40 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	41 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	42 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	43 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	44 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	45 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	46 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	47 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	48 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	49 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'
BRX GRA	<i>Buxus x green mountain</i>	Green Mountain Boxwood	50 gal	<i>Chamaecrista nivalis</i> 'Fall Frenzy'

LANDSCAPE SITE DATA

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	ALTERNATE
<b>PERENNIALS &amp; TURF</b>				
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	1 gal	<i>Arctostaphylos uva-ursi</i> 'Wood's Compact'
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	42' x 6'	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	1 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	2 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	3 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	4 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	5 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	6 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	7 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	8 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	9 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	10 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	11 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	12 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	13 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	14 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	15 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	16 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	17 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	18 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	19 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	20 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	21 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	22 gal	<i>Ceanothus x 'Sensation'</i>
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ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	25 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	26 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	27 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	28 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	29 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	30 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	31 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	32 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	33 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	34 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	35 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	36 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	37 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	38 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	39 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	40 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	41 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	42 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	43 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	44 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	45 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	46 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	47 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	48 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	49 gal	<i>Ceanothus x 'Sensation'</i>
ACR CLO	<i>Arctostaphylos uva-ursi</i> 'Emerald Carpet'	Emerald Carpet Huckleberry	50 gal	<i>Ceanothus x 'Sensation'</i>

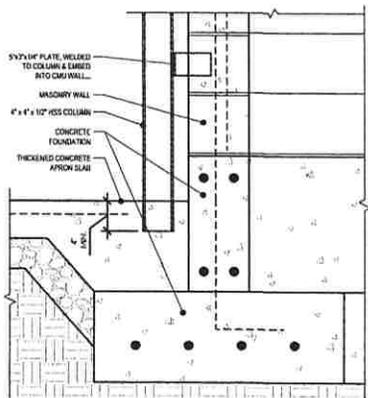
PRELIMINARY LANDSCAPE NOTES

- All landscape planting areas, other than parking islands, shall receive clean, sandy loam topsoil to a minimum depth of 12" or as noted on the plan. Additionally, all planting areas shall be prepared per the procedures outlined in the City of Medford Development Code, Section 10.780(1)(9)(b). All parking island trees shall be planted in a min. 24" depth of topsoil.
- All planting areas within 10' of the proposed building shall receive 2" of (1-1-1/2) Sanborn Gold crushed rock mulch. All planting areas outside of that zone shall receive 3" of unmulched organic mulch.
- All planting areas shall be served by an existing automatic water meter and a City of Medford approved backflow prevention device.
- The irrigation system will provide a fully automatic irrigation controller that will meet the requirements as noted in the City of Medford Development Code.
- The proposed irrigation system will consist of low volume distribution.

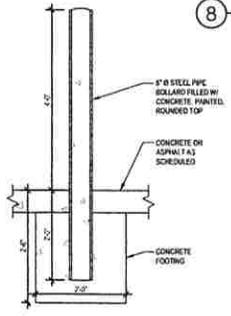
NOTE: MATRIEX COMPOST SHALL BE ADDED TO THE TOPSOIL OF LANDSCAPING AREAS AT A RATE OF THREE CUBIC YARDS OF COMPOST PER 1,000 SQUARE FEET OF LANDSCAPING AREA TO BE PLANTED.



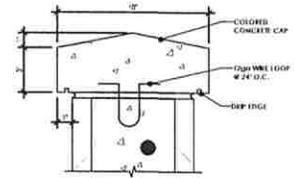




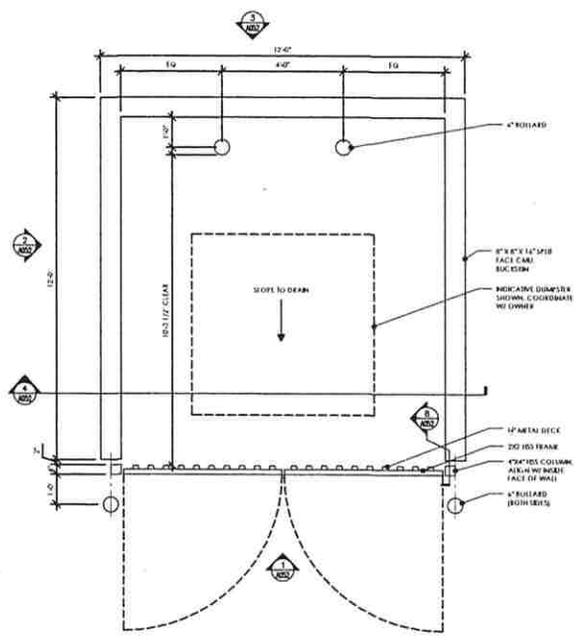
8 COLUMN BASE DETAIL  
SCALE: 1/2" = 1'-0"



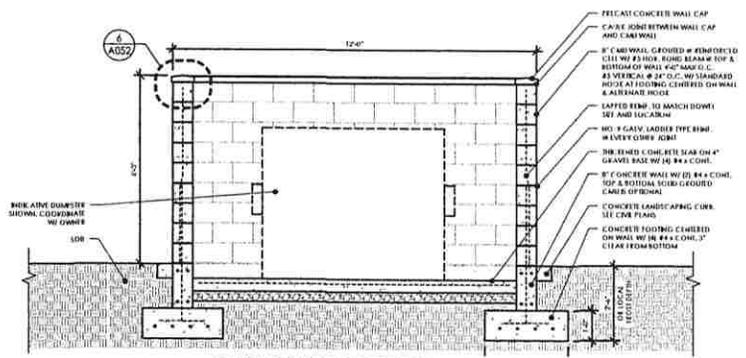
7 BOLLARD DETAIL  
SCALE: 3/4" = 1'-0"



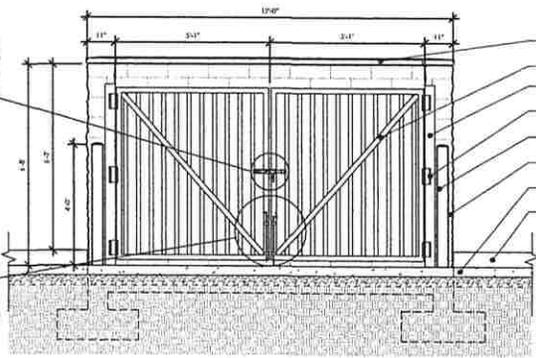
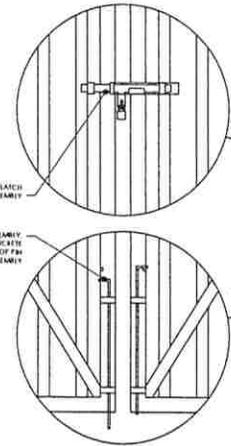
6 CONCRETE CAP DETAIL  
SCALE: 3" = 1'-0"



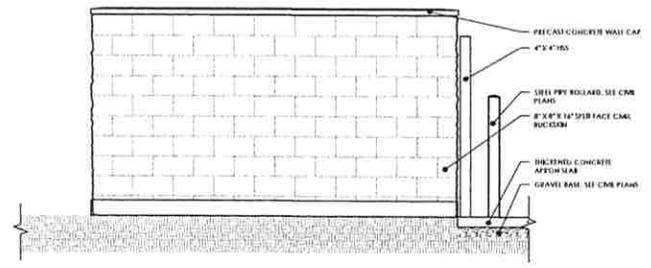
5 FLOOR PLAN  
SCALE: 1/2" = 1'-0"



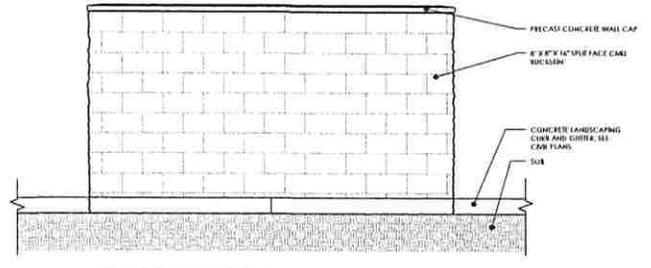
4 ENCLOSURE SECTION  
SCALE: 1/2" = 1'-0"



1 FRONT ELEVATION  
SCALE: 1/2" = 1'-0"



2 SIDE ELEVATIONS  
SCALE: 1/2" = 1'-0"



3 REAR ELEVATIONS  
SCALE: 1/2" = 1'-0"

GENERAL NOTES  
FOR DUMPSTER ENCLOSURE SEE  
CMB SHEET



THE  
RICHARDSON  
DESIGN  
PARTNERSHIP,  
LLC.

310 South 400 East  
Salt Lake City, Utah 84102  
P: 801-355-4844  
F: 801-355-4880

NOT FOR  
CONSTRUCTION

THE ENGINEER'S RESPONSIBILITY IS TO DESIGN AND PREPARE THE DRAWINGS FOR CONSTRUCTION. THE OWNER'S RESPONSIBILITY IS TO OBTAIN ALL NECESSARY PERMITS AND TO BE RESPONSIBLE FOR OBTAINING ALL NECESSARY APPROVALS FROM THE LOCAL GOVERNMENT. THE ENGINEER'S LIABILITY IS LIMITED TO THE DESIGN AND PREPARATION OF THE DRAWINGS. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR ANY OTHER MATTERS.

THE DRAWING, WHETHER IN ORIGINAL OR REPRODUCED FORM, IS THE PROPERTY OF THE ENGINEER. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.

PROJECT  
MAY PLACE HOTELS  
1500 S 1000 E  
STREET 1000 E

DATE: 05/20/2014  
KX/XX/XX XXXXXXXX

PROJECT #  
16-317

DUMPSTER  
ENCLOSURE  
A052

AL-19-062  
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# SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

Applicant: MP Medford LLC

*My Place Hotel, Medford*



## Applicant's Questionnaire & Findings of Fact

**Prepared By:**  
Ausland Group  
3935 Highland Avenue  
Grants Pass, OR 97526  
Contact: Charles L. Kroning, PE

October 8, 2019

CITY OF MEDFORD  
EXHIBIT #       
FILE # AC-19-062

**Applicant's Questionnaire & Findings of Fact  
My Place Hotel, Medford**

**Prepared by:**  
Ausland Group  
(Applicant's Representative)

**Submittal Date:**  
October 8, 2019

**Procedure Type:** Site Plan and Architectural Review Application - Type III

**Project Type:** Extended Stay Hotel (PA-19-060)

**Owner:** MP Medford LLC  
Attn: Aaron Lawson & Courtney Stephens  
P.O. Box 680453  
Marietta, GA 30068

**Applicant:** MP Medford LLC  
Attn: Aaron Lawson & Courtney Stephens  
P.O. Box 680453  
Marietta, GA 30068

**Representative:** Ausland Group

**Property Location:** 590 Airport Road - Corner of Airport Road and Biddle Road

**Map Lot and Tax Lot:** 372W12A, Tax Lot 1102 (Parcel 2)

**Zoning District:** C-R (Regional Commercial)

## APPLICANT'S QUESTIONNAIRE

### I. NARRATIVE:

The applicant is seeking to build a 37,625 square foot extended stay hotel on a 1.9 acre parcel zoned C-R (Regional Commercial). This lot was recently partitioned from the parent parcel located at 590 Airport Road (372W12A1102) (pursuant to **LDP-19-055 / ZC-19-003**). No Traffic Impact Analysis is required for this application, but ADTs are limited to the trips generated by the prior zoning (IL) specifically, 819 ADTs. The proposal will generate 710.6 ADTs, as shown on Sheet C0.0

The project includes related parking and pedestrian amenities normally associated with a hotel project. Access to the hotel will be taken from Airport Road as shown on the provided materials.

### II. COMPATIBILITY: CRITERION NO. 1

- A. List existing uses and development adjacent to your project site. Along with this list, describe the architecture (materials, colors, etc.), age, and condition of the adjacent buildings (you may use photographs to supplement this information).*

#### **Findings: The Applicant adjacent uses and developments as follows:**

##### **North**

The land directly north of the subject property is the parent parcel from which the subject property was recently partitioned. It is vacant and zoned I-L.

At the northeast corner of the intersection of Airport Road and Biddle Road is the Rogue Valley International Airport. This is a series of tan, stucco buildings in good condition. The lot is zoned I-L.

The lot at the northwest corner of the intersection of Airport Road and Biddle Road is vacant and zoned C-R.

##### **South**

The development directly south of the subject project and adjacent to Biddle Road is the Candlewood Suites Hotel. This is a four story, 60-unit, 39,492 square foot white and tan stucco building built in 2007 in good condition. Properties to the south are zoned I-L with light industrial and commercial uses and includes development within Navigators Landing PUD.

## East

The lot to the east of the subject property, at the intersection of Airport Road and Biddle Road, is zoned C-R and its current use is a Courtyard by Marriott Hotel. This is a three story, 60-unit, 56,962 square foot tan stucco building built in 2005 in good condition.

The parcel south of the Marriott property is zoned C-C and is vacant.

## West

The land directly west of the subject property is the parent parcel from which the subject property was recently partitioned. It is vacant and zoned I-L.

- B.** *Describe the building architecture and exterior treatments in your proposal, and how they fit with and complement adjacent buildings and development.*

**Findings:** The nearest surrounding developments are both existing hotels of similar size, function and design. The attached architectural materials submitted by The Richards Design Partnership LLC, (Jason Van Noord) (See Sheets A200 & A201) show a development of similar scale and color tones as the surrounding hotel uses.

- C.** *Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass. The Site Plan and Architectural Commission Design Guidelines are a helpful reference, and can be found on the City's website, and at the Planning Department.*

**Findings:** The exterior treatments have been expressly designed to break up the large facades of the building to break up the 'big box' mass feel. The Applicant has provided specific details of the building treatments and elements that accomplish this. See, letter dated September 26, 2019 from The Richardson Design Partnership LLC to Ausland Group.

- D.** *Describe how the placement and orientation of the proposed building(s) relate(s) to the street facilities, and how this orientation promotes a more pedestrian-friendly site design.*

**Findings:** The main entrance of the building, as shown on the Site Plan, is oriented toward the intersection of Biddle Road and Airport Road. The building is located in the southeast corner of the subject property allowing the shortest and easiest pedestrian access and connectivity to the facilities along Biddle Road possible.

- E.** *Describe the pedestrian facilities and amenities on your site (useable outdoor space, benches, etc.), and how they will function for pedestrians.*

**Findings:** The proposal includes full pedestrian connectivity around the development with connection to the existing sidewalk system along Biddle

**Road. Within the property, the development will have a Porte Cochere for pedestrian shelter at the main entrance and a shelter for recreation and picnicking at the west end of the development.**

- F. Describe vehicle and pedestrian access to the site, and how it relates internally on the site, and to adjacent sites.*

**Findings: The subject property is served by an access easement across tax lot 1102 (See, LDP-19-055 / ZC-19-003) and was created on the final plat thereby allowing access to Airport Road. This access point is in conflict with an existing utility pole. The Applicant is requesting the option of relocating this access easement slightly to the east, as shown on the Site Plan.**

- G. Describe if and how the proposed plan is sensitive to retaining any existing trees or significant native vegetation on the site. Should existing trees be preserved, a Tree Protection Plan shall also be included in this application.*

**Findings: The subject property is vacant and does not have any significant native vegetation. A landscape plan addressing trees and other landscaping to be installed and maintained is included in this application (Sheet L1.0).**

- H. Describe stormwater detention facilities on the site (underground storage, surface pond, etc.). If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.*

**Findings: Stormwater detention on site is located at the southwest corner of the property, as shown on the Applicant's Drainage Plan (Sheet C2.0). The proposal includes existing and new drainage easements as shown on the Site Plan. The detention pond has extensive landscaping which is integrated into the entirety of the project landscaping as shown on the Landscape Plan (Sheet L1.0).**

- I. Describe how your proposed landscaping design will enhance the building and other functions on the site.*

**Findings: The Landscape Plan (Sheet L1.0) serves many critical functions. The parking facilities will benefit from the shading and breaks in the hard surfaces provided by the landscaping. The landscaping provides a buffer with adjacent uses and future uses. The use of lawn indicates pedestrian use areas to the visitors, invites users to enter the property (as shown by the design along Biddle Road), and creates a recreational area (around the Picnic Shelter). The landscaping will also enhance the storm water detention area turning this element of the project into something aesthetically beneficial to the project.**

- J. Describe how your exterior lighting illuminates the site and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of Section 10.764.*

**Findings: Exterior lighting on the building and the site is generally downward casting and in compliance with the City Lighting standards, as shown on the**

**attached elevations. The Applicant will submit a full lighting plan as required by a condition of this approval.**

- K. Describe any proposed signage, and how it will identify the location of the occupant and serve as an attractive complement to the site.*

**Findings: No signage is proposed as part of this application.**

- L. Explain any proposed fencing, including its purpose, and how you have incorporated it as a functional, attractive component of your development. (See Sections 10.731-10.733).*

**Findings: No new fencing is proposed by this application. The property adjacent to the south (Candlewood Suites property) has existing fencing separating the two lots.**

- M. Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of Sections 10.752-10.761.*

**Findings: Noise from the proposed use is limited to ingress and egress of visitors mainly. It is not anticipated that new ambient noise will reach the thresholds set forth in 10.752 (2)(a).**

- N. Explain anything else about your project that adds to the compatibility of the project with adjacent development and uses.*

**Findings: The nearest surrounding developments are both existing hotels of similar size, function and design.**

- O. List and explain any exceptions or modifications requested and provide reasons for such.*

**Findings: The Applicant is seeking relief from the maximum number of off-street parking spaces allowed as set forth below. Based on the use of visitors to an extended stay hotel, the Applicant seeks approval of 92 off-street parking spaces rather than the 87 allowed by Code.**

- P. Section 10.780(C)(2) - List any petition for relief of landscaping standards (i.e., request an increase in turf area at a facility for active recreation; eliminate requirement for root barriers when trees are planted in structural soils). Provide rationale for requested deviation from standard.*

**Findings: No exceptions or relief from the landscape standards are requested.**

**II. CODE COMPLIANCE: CRITERION NO. 2**

The Applicant has submitted the necessary line information to the City as part of this application.

**END APPLICANTS QUESTIONNAIRE**

## FINDINGS OF FACT

### 10.200(E) Site Plan and Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and development that exist on adjacent land.
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in Section 10.186.

**Finding: Satisfied. Please reference Applicant's response to the criteria above.**

### 10.425 Street Access and Dedication Requirements

Prior to the issuance of a development permit, land shown on any development proposal and intended for vehicular use shall have access to a paved street. Land intended for vehicular and accessway use by the general public shall be offered for dedication.

**Finding: Satisfied. The Site Plan shows access to Airport Road through an access easement (created by final plat). The Applicant proposes, in the alternative, to shift Airport Road access slightly east to avoid the relocation of a power pole and the creation of a new access easement, which shall be a condition of this approval. No additional dedication is required as part of this approval.**

### 10.721 Commercial and Industrial Site Development Standards

The following standards apply to commercial and industrial development.

- A. Maximum lot coverage – 40%
- B. Minimum front and street side yard building setback – 10 feet; EXCEPT 20 feet for vehicular entrances to garages or carports
- C. Minimum side and rear yard building setback – None; EXCEPT 1/2 foot for each foot in building height over 20 feet
- D. Height - 85 feet; EXCEPT 35 feet if structure is within 150 feet of a residential zoning district boundary or Special Area Plan designation.

	Allowed/Required	Proposed
<b>Height</b>	<b>85 Feet</b>	<b>49 Feet</b>
<b>Lot Coverage</b>	<b>40%</b>	<b>11.4%</b>
<b>Front Yard Setback</b>	<b>10 Feet</b>	<b>10 Feet</b>
<b>Setback (side)</b>	<b>14.5 Feet / 0</b>	<b>14.5 Feet</b>
<b>Setback (side)</b>	<b>14.5 Feet / 0</b>	<b>0</b>
<b>Setback (rear)</b>	<b>14.5 Feet / 0</b>	<b>0</b>

**Finding: Satisfied.** Applicant has proposed a front yard setback of 10 feet along Biddle Road. Set back from the rear and side of the building is calculated according to code to be 14.5' for the main hotel building. The building is set back at least the required set back from all side and rear yard boundaries.

#### **10.729 Stormwater Quality and Detention Facilities, Private Property**

**A. Purpose.** *It is the City's policy to maintain the natural hydrology and preserve water quality by mitigating the direct impacts of new development and preserving the environmental benefits of natural water bodies.*

**B. Applicability.** *Stormwater quality and detention facilities shall be required for development and building permits.*

**Finding: Satisfied. See Grading & Drainage Plan Sheet C2.0.**

**C. Location.** *Stormwater detention facilities for development that does not include public rights-of-way, as per Section 10.486, shall be constructed on private property.*

**Finding: Satisfied. See Grading & Drainage Plan Sheet C2.0.**

#### **D. Construction and Maintenance Standards.**

(1) *Stormwater detention facilities shall be constructed in conformance with Section 10.481, Improvement Standards Adopted.*

(2) *Stormwater detention facilities for development that does not include public rights-of-way shall be privately maintained and shall have an Operation and Maintenance Plan approved by the City prior to construction of the facility. An approved form of the Operation and Maintenance Plan is located in Appendix of the current adopted version of the Rogue Valley Stormwater Quality Design Manual.*

(3) *When the property owner will not be responsible for maintenance of the private stormwater detention facility or when there will be multiple responsible parties, an Operation and Maintenance Agreement, in a form acceptable to the Public Works Department and the City Attorney, shall be required, in addition to the Operation and Maintenance Plan.*

**Finding: Satisfied.**

**This development is proposed on soils classified as belonging to the type B hydrologic soil group as mapped by Soil Survey of Jackson County and on a slope of 5% or less, thus, will implement Low Impact Development techniques as listed in the Rogue Valley Stormwater Quality Design Manual. To account for increased runoff from the new development, the applicant proposes a detention pond in the southwest corner of the property. The stormwater pond outlet**

structure will ensure that stormwater runoff is the same or lower than the predevelopment amount. The applicant has dedicated a 10-foot storm drain easement per tentative partition plat LDP-19-055 for the stormwater detention basin outlet pipe. The pipe will tie into the existing storm drain manhole. See Grading & Drainage Plan Sheet C2.0.

The stormwater detention facility (south-west corner of the property) shall be constructed in conformance with section 10.481 of Medford Land Development Code. The facility shall be privately maintained and will have an Operation and Maintenance Plan approved by the City. Required stormwater easements are identified on C2.0, including along Biddle Road, where stormwater is proposed to be piped, as it has been along adjacent properties to the south.

The proposed development will tap into the 54 inch sewer main to the east within RVSS service area. A "Looped" 12-inch water line is required across the property extending from the existing 24-inch ductile iron water line in the middle of Biddle Road across the property in a paved travel lane with connection to the 24-inch ductile iron in Airport Road.

#### 10.742 Off-Street Loading Standards

(1) *Types of Loading Berths; Design.* Required off-street loading space shall be provided in berths which conform to the following minimum specifications.

(b) *Type "B" berths shall be at least 30 feet long by 12 feet wide by 14 feet 6 inches high, inside dimensions with 30-foot maneuvering apron.*

(2) *Number of Loading Spaces Required:* The following numbers and types of berths shall be provided for the specified uses. The uses specified below shall include all structures designed, intended or arranged for such use.

Use	Gross Floor Area (Sq. ft.)	Berths Required	Type
Hotels or office buildings	25,000-40,000	1	B

(5) *Location of required loading facilities.* The off-street loading facilities required for the uses mentioned in this code shall be in all cases on the same lot or parcel of land as the structure they are intended to serve. In no case shall the required off-street loading space be part of the area used to satisfy the off-street parking requirements.

(6) *Manner of using loading areas.* No space for loading shall be so located that a vehicle using such loading space projects into any public right-of-way. Loading space shall be provided with access to an alley, or if no alley adjoins the lot, with access to the street. Any required front, side or rear yard may not be used for loading.

**Finding: Satisfied.** The applicant will provide one loading berth to meet “type B” requirements (12’ wide x 30’ length x 14.5’ high). The off-street loading facility is on the proposed lot and is not part of the area used to satisfy the off-street parking requirements. The off street loading facility is positioned as not to project into public right-of-way nor conflict with fire access. See Site Plan Sheet C1.0.

**10.743 Off-Street Parking Standards**

(1) *Vehicle Parking – Minimum and Maximum Standards by Use.* The number of required off-street vehicle parking spaces shall be determined in accordance with the standards in Table 10.743-1.

*Parking spaces that count toward the minimum requirement are parking spaces meeting minimum dimensional and access standards in garages, carports, parking lots, bays along driveways, and shared parking areas.*

<b>MLDC 10.743</b>	
<b>Minimum # of Required Parking Spaces</b>	<b>Maximum Permitted Parking Spaces</b>
1.0 space per room, plus 1.0 space per every 3.3 employees on the largest shift, plus 1.0 space per 3.3 persons at the maximum occupancy of each public meeting and/or banquet room, plus 45% of the spaces otherwise required for accessory uses (e.g., restaurants and bars)	1.0 space per room, plus 1.0 space per every 2.6 employees on the largest work shift, plus 1.0 space per 2.6 persons at the maximum occupancy of each public meeting and/or banquet room, plus 55% of the spaces otherwise required for accessory uses (e.g., restaurants and bars)

(2) *Number of Required Parking Spaces.* Off-street vehicle parking spaces shall be provided as follows:

(a) *Parking Space Calculation.* Parking space ratios are based on spaces per 1,000 square feet of gross floor area, unless otherwise noted.

(b) *Parking Categories.*

A. *Minimum Number of Required Parking Spaces.* For each listed land use, the City shall not require more than the minimum number of parking spaces

calculated for each use.

*B. Maximum Number of Permitted Parking Spaces. The number of parking spaces provided shall not exceed the maximum number of parking spaces allowed for each listed land use.*

**Finding: According to applicable section of table 10.743-1 for hotels or motels, the applicant must have a minimum of 86 spaces or a maximum of 87 spaces. Parking space ratios are based on spaces per hotel guest room, and number of employees on largest shift. The hotel does not include public meeting spaces, banquet rooms, or other uses. Parking spaces meet minimum dimensional and access standards. See parking analysis on sheet C0.0 of the Site Plan.**

**The Applicant is seeking to exceed the maximum allowed parking from 87 to the 92 spaces shown on the attached site plan and as allowed in Section 10.746(3).**

*(3) Exceptions to Required Off-Street Parking for Non-Residential Uses. The approving authority may allow exceptions to the number of parking spaces in Table 10.743-1 for specific uses without complying with Section 10.186 if they find that the applicant's detailed description of the proposed use demonstrates that the number of needed parking spaces is less than the minimum required or more than the maximum allowable based upon one or both of the following (effective Dec. 1, 2013):*

*(a) An explanation why the characteristics of the proposed use require a different off-street parking standard than what is otherwise required.*

*(b) An analysis providing parking data for the same business or a similar use within the city that demonstrates a need for a different off-street parking standard than what is otherwise required.*

**Finding: The proposed development is for an extended stay hotel. According to 2018 data provided by the Applicant, 21.7% of visitors will be weekly customers and 22.8% will be monthly customers. The average length of total stay across all transactions is 2.92 days per visit. It is also common at extended stay accommodations to have visitors traveling with multiple vehicles in the same party or group. The extended stay customers staying together in one room may need multiple vehicles during their stay for business or relocation. This use is slightly more parking intensive than a standard hotel, thus the Application is seeking a slightly greater amount of off-street parking than otherwise allowed by the maximum parking standards of this Section.**

**The Finding is made that the characteristics of this specific proposed use, within the broader use category of hotel, requires a different amount of off-street parking than otherwise set forth in 10.746.**

### **10.746 General Design Requirements for Parking**

(2) *Curb Cuts.* Access points with the street shall be the minimum necessary to provide access while not inhibiting the safe circulation and carrying capacity of the street. Curb cuts shall be located not closer than 5 feet to a side lot line, except that a common access way to two adjacent properties (width not exceeding 45 feet) may be provided at the common lot line. Common access ways shall be encouraged in order to reduce the number of access points to streets. Access grades shall not exceed 15% and shall be graded to allow clearance to pass a standard American automobile 15 feet in length.

**Finding: Satisfied. A 30 foot temporary access easement was proposed per tentative partition plat LDP-19-055. As proposed, an existing utility pole blocks access onto Airport Road. As an alternative, the applicant proposes to have a portion of the temporary access easement relocated and moved to the east to avoid the existing utility pole. Access grade will not exceed 15%. See Site Plan Cover Sheet C0.0.**

(3) *Parking Area Planters.*

*It is the purpose of this subsection to create shade and visual relief for large expanses of parking.*

a. *Parking areas exceeding 24 parking spaces shall contain areas of interior landscaping, such as planter islands or planter projections into the parking area, which comply with the planting schedule provided in Subsection 10.746 (3)f. and Section 10.780, Landscape and Irrigation Requirements, and as approved by the approving authority.*

b. *Planters shall be dispersed throughout the parking area and contain, at minimum, the landscaping area square footage specified in the Planting schedule of Subsection 10.746 (3)f. Square footage of each parking area planter may vary; however, each parking area planter shall meet the soil volume requirements of Subsection 10.780 G(10)a.*

c. *Prior to installing plant materials in parking area planters, the developer shall remove detrimental construction materials and prepare the soil within the planters in accordance with Subsection 10.780 G(9). If structural soils are necessary, areas under planned impervious surfaces surrounding planters, shall be prepared in accordance with Subsection 10.780 G(10)(a).*

d. *So as to not obstruct driver vision, nothing shall be erected, placed, planted or allowed to grow in such a manner as to impede vision between a height of three (3) feet and eight (8) feet above the top of the curb. The property owner shall maintain shrub and tree growth in planter areas to ensure shrubs are kept lower than three (3) feet and tree canopies are maintained above eight (8) feet.*

e. *Trees planted in parking area planters shall have a moderate to broad spreading canopy.*

f. *The minimum landscaped area within parking area planters and number of required plants per 24 spaces is as follows:*

**Parking Area Planters  
Planting Schedule**

<b>Zoning District</b>	<b>Plants/ per 24 Spaces trees/shrubs</b>	<b>Sq. Ft./ per 24 Spaces</b>
Industrial Zones	2/4	325
SFR (Nonresidential uses only) MFR zones Commercial Zones	3/6	500
*Public Zones	3/6	500

\* Shrub and ground cover within the parking lot planter bays in parks may be substituted with non-living ground cover if allowed by the approving authority (artificial turf not allowed).  
[Amd. Sec. 4, Ord. No. 2013-84, June 6, 2013 (effective Dec. 1, 2013); Amd. Sec. 16, Ord. No. 2018-52 June 8, 2017, (effective July 9, 2018).]

**Finding: Satisfied.** See Landscape Plan, Sheet L1.0. The Applicant’s Landscape Plan demonstrates compliance with the standards of this Section 10.746(3), further described in the chart entitled, “Landscape Site Data” identifying 13 trees and 27 shrubs within the Parking Area Planter Bays.

(8) Accessible Parking Space Requirements. The following rates and design regulations are derived from ORS 447.233. The number of accessible parking spaces shall be provided at these rates:

<b>Total Parking in Lot</b>	<b>Required Minimum number of accessible spaces</b>	<b>Required Minimum number of Van-accessible spaces</b>
<b>76-100</b>	<b>4</b>	<b>1</b>

**Finding: Satisfied.** 4 accessible spaces and 1 van accessible space has been provided. See Site Plan Sheet C0.0.

(b) In addition, one in every eight accessible spaces, but not fewer than one, shall be van accessible. Where five or more parking spaces are designated accessible, any space that is designated as van accessible shall be reserved for wheelchair users. A van-accessible parking space shall be at least nine feet wide and shall have an adjacent access aisle that is at least eight feet wide.

**Finding: Satisfied.** One in every four of accessible spaces is van accessible. Van accessible parking is 10 feet wide and has an adjacent access aisle that is 8 feet wide. See Site Plan Sheet C1.0.

(c) Accessible parking spaces shall be at least nine feet wide and shall have an adjacent access aisle that is at least six feet wide.

**Finding: Satisfied. Accessible parking spaces are 10 feet wide, with an adjacent access aisle that is 6 feet wide. See Site Plan Sheet C1.0.**

(d) The access aisle shall be located on the passenger side of the parking space except that two adjacent accessible parking spaces may share a common access aisle.

**Finding: Satisfied. Access aisle is located on the passenger side of the parking space except that two adjacent accessible parking spaces may share a common access aisle. See Site Plan Sheet C1.0.**

(e) A sign shall be posted for each accessible parking space. The sign shall be clearly visible to a person parking in the space, shall be marked with the International Symbol of Access and shall indicate that the spaces are reserved for persons with disabled person parking permits. A van-accessible parking space shall have an additional sign marked "Van Accessible" mounted below the sign. A van-accessible parking space reserved for wheelchair users shall have a sign that includes the words "Wheelchair User Only."

**Finding: Satisfied. The appropriate signage will be installed as a condition of approval and done prior to Certificate of Occupancy.**

(f) Accessible parking spaces and signs shall be designed in compliance with the standards set forth by the Oregon Transportation Commission in consultation with the Oregon Disabilities Commission.

**Finding: Satisfied. Accessible parking spaces and signs will be designed in compliance with Oregon Transportation Commission Standards in consultation with Oregon Disabilities Commission.**

(g) No ramp or obstacle may extend into the parking space or the aisle, and curb cuts and ramps may not be situated in such a way that they could be blocked by a legally parked vehicle.

**Finding: Satisfied. No ramp or obstacle will extend into the parking space or the aisle, and curb cuts and ramps will not be situated in such a way that they could be blocked by a legally parked vehicle. See Site Plan Sheet C1.0.**

(h) Accessible parking spaces shall be located on the shortest practical accessible route to an accessible building entry. In facilities with multiple accessible building entries with adjacent parking, accessible parking spaces shall be dispersed and located near accessible entries.

**Finding: Satisfied: Accessible parking spaces are located on the shortest practical accessible route to an accessible building entry. See Site Plan Sheet C1.0.**

*(i) Parking spaces required by this section shall be maintained so as to meet the requirements of this section at all times and to meet the standards established by the state building code.*

**Finding: Satisfied. Parking spaces required by section 10.746.8.i will be maintained.**

*(9) Screening. Where parking, vehicle maneuvering, or loading areas abut a public street, there shall be provided a minimum ten (10) foot wide landscaping buffer. Within public parks, shrub and groundcover within this area may be substituted with non-living ground cover if approved by the approving authority (artificial turf not allowed).*

**Finding: Satisfied. There is a minimum 10 foot landscaping buffer where parking, vehicle maneuvering or loading areas abut Biddle Road. See Site Plan Sheet C1.0.**

*(12) Aisles. Except for single or two family dwellings, groups of more than three parking spaces shall be provided with adequate aisles or turnaround areas so that all vehicles may enter the street in a forward manner.*

**Finding: Satisfied. All vehicles may enter the street in a forward manner. See Site Plan Sheet C1.0.**

*(14) Drainage. Adequate drainage shall be provided to dispose of the run-off generated by the impervious surface area of the parking area. Provisions shall be made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way, and abutting private property.*

**Finding: Satisfied. As shown in the Grading and Drainage Plan Sheet C2.0, a vegetated swale is proposed along the south property line to collect runoff from the roof drains and hardscape surfaces. The swale discharges to the proposed stormwater detention pond. Stormwater runoff from the paved parking area is directed to curb openings and discharges directly to the stormwater detention pond. The treatment is provided consistent with the standards set forth by Medford Land Development Code Section 10.481 and 10.729.**

*(16) Parking Space Minimum Dimension Standards.*

**Finding: Parking spaces meet Minimum Dimension Standards of 9' wide x 19' length x 24' aisle width. See Site Plan Sheet C1.0.**

## **10.748 Bicycle Parking Standards**

*Bicycle parking spaces shall be provided in accordance with the following:  
Commercial: 10% of the number of spaces provided for automobiles, to be calculated by rounding up to the nearest whole number.*

**Finding: Satisfied. 9 bicycle parking spaces have been provided to meet the bike parking required for the maximum number of vehicle spaces allowed per the parking standards (Table 10.742-1).**

#### **10.749 Location of Bicycle Parking Facilities**

*Required bicycle parking facilities shall be located on-site in well lighted, secure locations within 50 feet of well-used entrances or inside a building in a suitable, secure, and accessible location. Bicycle parking shall have direct access to both the public right-of-way and to a main entrance of the principal use. Bicycle parking for multiple uses (such as in a commercial center) may be clustered in one or several locations.*

**Finding: Satisfied. Bicycle facility is well lit as shown on attached Exterior Elevations. See, Site Plan Sheet C1.0 for location and access.**

#### **10.750 General Design Requirements for Bicycle Parking**

*All bicycle parking and maneuvering areas shall be constructed to the following minimum design standards:*

*(1) Surfacing: Outdoor bicycle parking facilities shall be surfaced in the same manner as a motor vehicle parking area or with a minimum of a three-inch thickness of hard surfacing (i.e., asphalt, concrete, pavers or similar material). This surface will be maintained in a smooth, durable and well-drained condition.*

*(2) Parking Space Dimension Standard: Bicycle parking spaces shall be at least 6 feet long and 2 feet wide with minimum overhead clearance of 7 feet.*

*(3) Lighting: Lighting shall be provided in a bicycle parking area so that all facilities are thoroughly illuminated and visible from adjacent sidewalks or motor vehicle parking lots during all hours of use.*

*(4) Aisles: A 5-foot aisle for bicycle maneuvering shall be provided and maintained beside or between each row of bicycle parking.*

*(5) Signs: Where bicycle parking facilities are not directly visible from the public rights-of-way, entry and directional signs shall be provided to direct bicycles from the public rights-of-way to the bicycle parking facility.*

*(6) Rack Type: Bicycle parking shall consist of racks that provide two points of contact with the frame at least 6 inches apart horizontally and have a minimum height of 32 inches. The approving authority may authorize other means of bicycle parking that provides protection, such as bike lockers or secured bicycle group enclosures. The*

*wave rack style shall not be permitted.*

**Finding: Satisfied. Outdoor bicycle parking will be surfaced with concrete that is of sufficient thickness to satisfy 10.750.1. Spaces will be 2 feet wide by 6 feet long. Lighting will be provided so that facilities are illuminated from adjacent sidewalk on Biddle Road and the parking lot. See exterior elevations. Rack type shall be compliant with 10.750.6 as a condition of approval.**

### **10.772 Pedestrian Walkway General Provisions**

*These walkway provisions are intended to provide safe, direct and convenient pedestrian access in new office and commercial developments.*

*Internal pedestrian circulation shall be provided in new office and commercial developments through clustering of buildings, construction of pedestrian ways, skywalks, where appropriate, and similar techniques.*

**Finding: Satisfied.**

### **10.773 Pedestrian Walkway Connections and Routing**

*A pedestrian walkway shall be provided:*

- (1) To each street abutting the property, and for every 300 feet of street frontage not including limited access freeways.*
- (2) To connect with walkways, sidewalks, bike paths, and other bicycle or pedestrian connections on adjacent properties.*
- (3) To connect building entrances to one another, to existing or planned transit stops and to connect the pedestrian circulation system to other areas of the site such as parking lots, children's play areas, required outdoor areas and any pedestrian amenities such as plazas, resting areas and viewpoints.*

**Finding: Satisfied. The development will include a sidewalk along Biddle Road to connect with the existing sidewalk of the adjacent property to the south (Candlewood Suites). Sidewalks are provided to allow for internal pedestrian circulation between the parking lot, hotel and picnic area. See Site Plan Sheet C1.0.**

### **10.775 Pedestrian Walkway Design Standards**

*Pedestrian walkways shall be:*

- (1) At least five feet in paved unobstructed width, except walkways bordering parking spaces which shall be at least seven feet wide unless concrete bumpers, bollards or curbing and landscaping or other similar improvements are provided which prevent parked vehicles from obstructing the walkway. Vehicle encroachment is permitted into*

*the walkway when a minimum of seven feet is maintained.*

*(2) Separated from parking area by grade, different paving materials, speed bumps or landscaping.*

*(3) Provided with stairs and ramps where necessary to provide a direct route.*

*(4) Provided with parking lot or pedestrian scale lighting.*

*(5) As direct as possible and avoid unnecessary meandering.*

*(6) Designed to minimize driveway crossings.*

*(7) Located so that the pedestrian has a minimum distance to walk between a transit stop or a street with a transit stop and the entrance to a building.*

**Finding: Satisfied. Sidewalks are 5 feet in paved unobstructed width. Where walkways are bordering parking spaces, concrete bumpers will be provided. Walkways are separated by a 6 inch high curb. Curb ramps are provided where necessary to access the parking lot, and stairs and an adjacent concrete ramp are provided to access Biddle Road. Walkways are designed to be as direct as possible, to minimize driveway crossings and to provide pedestrians a minimum distance to walk from the entrance of the building to Biddle Road, to access a transit stop. Lighting will be pedestrian in scale and approved prior to Certificate of Occupancy. See Site Plan Sheet C1.0.**

#### **10.776 Exceptions to Pedestrian Walkway Standards**

*A required walkway or walkway connection need not be provided where an alternate route is reasonably direct. The alternate route is reasonably direct if the total walking distance does not increase by more than 50% and not more than 100 feet over the other required route.*

*Walkways are not required between buildings or portions of a site which are not intended for or likely to be used by pedestrians. Such building features include: truck loading docks, warehouses, not including office/warehouse combinations, automobile sales lots, temporary uses, outdoor storage areas, etc.*

**Finding: Satisfied. As an exception to Walkway Design Standards 10.775, no walkway is provided to the trash enclosure and recycle / storage shed as allowed by 10.776. This facility is not designed or intended to be accessible to the public but will be used by vehicular transport from this storage area either off-site or to the hotel itself.**

#### **10.780 Landscape and Irrigation Requirements**

##### **B. General Provisions.**

*(1) Landscaping shall provide a variety of plant sizes, shapes, texture and color while being horticulturally compatible and minimizing irrigation reliance, thus conserving the public water supply.*

##### **E. Landscape Plan and Irrigation Plan Review.**

*All development and building permit applications that include landscaping areas, as defined herein, with the exception of development types provided within Section 10.780 C.(1)a., shall*

include a Landscape Plan and Irrigation Plan in accordance with the provisions of this section. Landscaping and irrigation improvements shall be installed in accordance with approved plans.

**Finding: Satisfied. The Applicant has provided a Landscape and Irrigation Plan (See Sheet L1.0) created by licensed landscape architectural firm, Terrain Landscape Architecture. The submitted Landscape Plan details the types and sizes of required plantings. The Landscape and Irrigation Plan conform to the specific requirements of 10.780(G) and (H).**

#### **10.781 Concealment of Trash Receptacles**

*Trash receptacles shall be kept in enclosures made of solid wood, metal, brick, or masonry block which conceal them from view. Access doors shall be made of solid wood or metal. When possible, enclosures should be incorporated into the building (i.e., wing walls, alcoves). If free standing, the enclosures shall be constructed of materials which are compatible with other structures on the site. Chain link fencing, with or without slats, for this purpose is prohibited.*

**Finding: Satisfied. Trash receptacle is freestanding and made of split face CMU block, buckskin color and concrete. Access doors are metal. Materials are compatible with those used in the construction of the hotel, as shown on Applicants elevations.**

#### **10.782 Concealment of Heating, Ventilation, Air Conditioning (HVAC) Equipment and Roof-Mounted Wireless Communication Facilities**

*All HVAC equipment and roof-mounted wireless communication facilities shall be concealed from view. Where possible, such concealment should be accomplished using the architectural elements of the buildings (i.e., roof forms, parapets, wing walls, alcoves, etc.). Free standing walls or fences may also serve as sight-obscuring concealment devices. Chain link fencing, with or without slats, for this purpose is prohibited.*

**Finding: Satisfied. As set forth on the architectural elevations, no roof mounted HVAC equipment is proposed but all roof-mounted equipment (if any) will be screened from view.**

#### **10.797 Street Frontage Landscaping Requirements**

*A. This section establishes the minimum landscaping requirements along all street frontages outside of the public right-of-way. Plans submitted to comply with this section shall be approved by the approving authority.*

*(1) For land divisions with houses that do not face an arterial street, an arterial street frontage landscape plan shall be submitted showing a vertical separation feature that is a minimum of eight (8) feet in height. The separation feature shall create a solid visual screen. A fence or wall shall be engineered to stand straight. The separation feature shall*

be reduced in height where otherwise required in a front or side yard or clear vision triangle. The Planning Commission may allow adjustments to the above requirement in response to topography.

(2) For all other street frontages, the number of plants required for distances above or below one hundred (100) feet shall be prorated with the resulting numbers of plants rounded so that one-half or more shall be deemed to require a full plant. All required planting shall be located in the required yard area adjacent to the street, or within the planter strip, or a combination of both.

(3) Existing plant materials which meet the requirements of this code may be counted as contributing to the total landscaping required by this section. Interstate 5 and other highway frontages shall be treated the same as city street frontage.

(4) The following table specifies the type and number of plants required by this section.

**Table 797-1. Frontage Landscaping - Materials and Quantities**

<u>Zoning District</u>	<u>Number of plants required per 100 feet of street frontage less driveways</u>	
	<u>Trees</u>	<u>Shrubs</u>
<i>SFR (Nonresidential uses), MFR, C-N, C-S/P, C-C, C-R, C-H</i>	4	25
<i>I-L, I-G, I-H</i>	3	15

Frontage landscaping shall not be required for areas in public parks that do not have buildings within 20 feet from the adjacent right-of-way unless the approving authority determines it is necessary to buffer specific park activities.

**Finding: Satisfied.** The street frontage along Biddle Road is landscaped in compliance with this Section. See, Landscape Plan Sheet L1.0.

**END FINDINGS OF FACT**

## **CONCLUSION**

The Applicant has demonstrated compliance with the standards for this application. The design will create a vibrant new extended-stay hotel with close proximity to the Medford Airport and is compatible with adjacent developments. This project is a primary focus of the development team and construction will be 'fast-tracked' once necessary permits are obtained. This project will be the anchor for commercial development of the surrounding vacant land and will play a vital role in continuing the growth of Medford's hospitality sector.



## **PUBLIC WORKS DEPARTMENT STAFF REPORT**

### **My Place Hotel**

**590 Airport Road (TL 1102)**

**Project:** Consideration of the construction of a 37,652 square foot, four-story hotel (My Place Hotel) on 1.9 acres.

**Location:** Located at the southwest corner of Airport Road and Biddle Road in the C-R (Regional Commercial) zoning district (372W12A1102).

**Applicant:** Applicant, MP Medford LLC; Agent, Ausland Group; Planner, Dustin Severs.

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**NOTE:** The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention, if applicable.
- Completion of all public improvements, if required. The Applicant may provide security for 120% of the public improvements prior to issuance of building permits. Construction plans for the public improvements shall be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A - D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Verification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan, if applicable.
- Completion of all public improvements, if applicable.

## A. STREETS

### 1. Dedications

**Biddle Road** is classified as a Major Arterial street within the Medford Land Development Code (MLDC) Section 10.428. **No additional right-of-way is required.**

**Airport Road** is classified as a Minor Collector street, and in accordance with MLDC Section 10.428, it requires a total right-of-way width of 74-feet. Right-of-way has been dedicated with Partition Plat #P-37-2019 (survey #22921). However, per conditions of approval for planning action LDP-19-055, only 72-feet of right-of-way was dedicated. **No additional right-of-way is required.**

There is currently a 10-foot Public Utility Easement (PUE) along Airport Road that was dedicated with Survey #22921, and a 20-foot PUE along Biddle Road dedicated with Survey #13185.

### 2. Public Improvements

#### a. Public Streets

**Biddle Road** – All street section improvements, with the exception of a planter strip and sidewalk, have been completed in close conformance with current standards, including pavement, curb and gutter. A 5-foot wide sidewalk with a 10-foot planter strip is required along this developments frontage in accordance with MLDC 10.428.

**Airport Road** shall be improved to Minor Collector street standards in accordance with the MLDC, Section 10.428. The Developer shall improve the south half plus 12-feet north of the centerline, or to the far edge of the existing pavement, whichever is greater, along the frontage. This shall include an 8-foot planter strip on the south side as there is a half street width of 36-feet that was dedicated with Partition Plat #FP19-00017. **Improvements will begin from the west side of the proposed access road/driveway then east to tie into improvements along Biddle Road. Applicant shall with work the City of Medford on design of improvement at the corner of the intersection.**

As an option, the Developer may elect to provide evidence of the existing structural section to Public Works for consideration in order to determine if the extent of construction may be reduced. Depending on the results, the Developer still may be responsible for the improvements noted above or at minimum improve the remainder of the north half of Airport Road from a point 1-foot inside the existing edge of pavement.

#### b. Street Lights and Signing

The Developer shall provide and install in compliance with Section 10.495 of the Medford

Municipal Code (MMC). Based on the preliminary plan submitted, the following number of street lights and signage will be required:

Street Lighting & Signage – Developer Provided & Installed:

- A. 3 – Type A-400 LED (Biddle Road)
- B. 2 – Type C-250 LED (Airport Road)
- C. 1 – BMC-2\*

Traffic Signs and Devices – City Installed, paid by the Developer:

- A. N/A

*\*NOTE: Could use existing BMC to the east along Airport Road (600-feet away) if voltage drop calculations show that it is not an issue.*

Numbers are subject to change if changes are made to the plans. All street lights shall be installed per City standards and be shown on the public improvement plans. Public Works will provide preliminary street light locations upon request. All street lights shall be operating and turned on at the time of the final “walk through” inspection by the Public Works Department.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer’s contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

**c. Pavement Moratoriums**

There is no pavement cutting moratorium currently in effect along this developments respective frontages.

**d. Access and Circulation**

The access easement shall be permanent, not temporary as shown on the submitted site plan, in accordance with the approval conditions of LDP-19-055.

If the proposed alternate access location is used, evidence of an easement encompassing the new drive aisle shall be submitted prior to issuance of permits for vertical construction.

Driveway access to the site shall be in accordance with MLDC 10.550, including width and radius standards. No driveway access shall be allowed to Biddle Road, all access shall be from Airport Road.

The proposed 85 room Hotel is expected to generate 711 average daily trips (ADT) which is less than the zone change requirement of a maximum of 819 ADT from ZC-19-055.

### 3. Section 10.668 Analysis

To support a condition of development that an Applicant dedicate land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

#### 10.668 Limitation of Exactions

*Notwithstanding any other provisions of this Chapter 10, an Applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:*

*(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the Developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or*

*(2) a mechanism exists and funds are available to fairly compensate the Applicant for the excess burden of the exaction to the extent that it would be a taking.*

#### 1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and stormdrain to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

#### 2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining "rough proportionality" have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

## Airport Road and Biddle Road:

Airport Road is a 40 mile per hour facility, which currently carries approximately 3,100 vehicles per day.

The development shall construct approximately 275 linear feet of sidewalk along the Biddle Road frontage of the property. All developments in Medford are required to construct frontage sidewalk. And therefore, this exaction is roughly proportional to all other development within Medford.

The development shall also construct approximately 400 linear feet of roadway along Airport Road. All developments in Medford are required to construct frontage/onsite as well as off-site improvements necessary to serve the development. Airport Road will be a primary route for pedestrians and the only route for vehicles traveling to and from this development. No vehicular access shall be granted to Biddle Road.

The additional street lighting on Airport Road and Biddle Road will provide the needed illumination to meet current MLDC requirements.

The City assesses System Development Charges (SDCs) to help pay for acquisition of right-of-way and construction of additional Arterial & Collector Street capacity required as a result of new development. Because a mechanism exists in the form of SDC credit for right-of-way dedication and street improvements in accordance with Medford Municipal Code (MMC) 3.815 and other applicable parts of the Code, to fairly compensate the applicant, the conditions of MLDC, Section 10.668 are satisfied.

## **B. SANITARY SEWERS**

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

## **C. STORM DRAINAGE**

### **1. Drainage Plan**

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

A Site/Utility Plan shall be submitted with the building permit application to show the location of existing or proposed stormdrain lateral/s for the site.

All private stormdrain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

## 2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

## 3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

It appears that this development is on soils classified as belonging to the Type B hydrologic soil group as mapped by the Soil Survey of Jackson County, and on a slope of 5% or less. As such, the project will need to implement **Low Impact Development** techniques as listed in the Rogue Valley Stormwater Quality Design Manual. The Applicant may elect to test the soil to determine classification, and if so, testing must be conducted by a licensed Geotechnical Engineer in the state of Oregon.

## 4. Verification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design Engineer shall verify that the construction of the stormwater quality and detention system was constructed per plan. Verification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

## 5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

## D. GENERAL CONDITIONS

### 1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

### 2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional Engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Construction drawings for public improvements shall be submitted only for the improvements to be constructed with each phase. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, stormdrains, and street lights as required by the governing Commission's Final Order, together with all pertinent details and calculations. A checklist for public improvement plan submittal can be found on the City of Medford, Public Works web site (<http://www.ci.medford.or.us/Page.asp?NavID=3103>). The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

In order to properly maintain an updated infrastructure data base, the Surveyor of Record shall submit an as-built survey prior to the Final Inspection and, the Engineer of Record shall submit mylar "as-constructed" drawings to the Engineering Division within sixty (60) calendar days of the Final Inspection (walk through). Also, the Engineer shall coordinate with the utility companies, and show all final utility locations on the "as built" drawings.

### 3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans.

Contractors proposing to do work on public streets (including street lights), sewers, or stormdrain shall 'prequalify' with the Engineering Division prior to starting work.

Contractors shall work off a set of public improvement drawings that have been approved

by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and stormdrain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the Developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

#### 4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

#### 5. System Development Charges (SDC)

Buildings in this development are subject to system development charges (SDCs). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Jodi K Cope

Reviewed by: Doug Burroughs

# SUMMARY CONDITIONS OF APPROVAL

## Extended Stay Hotel

590 Airport Road (TL 1102)

AC-19-062

### A. Streets

#### 1. Street Dedications to the Public:

- Biddle Road – No additional right-of-way required.
- Airport Road – No additional right-of-way required.
- Public Utility Easements have already been dedicated along both frontages.

#### 2. Improvements:

##### Public Streets

- Biddle Road – No improvements are required at this time except for sidewalk with planter.
- Improve Airport Road to Minor Collector street standards, ½ plus 12-feet.

##### Lighting and Signing

- Developer supplies and installs all street lights at own expense.
- City installs traffic signs and devices at Developer's expense.

##### Access and Circulation

- The access easement shall be permanent, not temporary as shown on the submitted site plan, in accordance with the approval conditions of LDP-19-055.
- If the proposed alternate access location is used, evidence of an easement encompassing the new drive aisle shall be submitted prior to issuance of permits for vertical construction.
- Driveway access to the site shall be in accordance with MLDC 10.550, including width and radius standards. No driveway access shall be allowed to Biddle Road, all access shall be from Airport Road.
- The proposed 85 room Hotel is expected to generate 711 average daily trips (ADT) which is less than the zone change requirement of a maximum of 819 ADT from ZC-19-055.

##### Other

- No pavement moratorium currently in effect along this frontage to Biddle Road or Airport Road.

### B. Sanitary Sewer:

- Ensure or construct separate individual sanitary sewer connection.
  - Cap remaining unused laterals at the main.

### C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers verification of stormwater facility construction.
- Implement **Low Impact Development** techniques or conduct testing.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



BOARD OF WATER COMMISSIONERS

**Staff Memo**

**TO:** Planning Department, City of Medford

**FROM:** Rodney Grehn P.E., Water Commission Staff Engineer

**SUBJECT:** AC-19-062

**PARCEL ID:** 372W12A TL 1102

**PROJECT:** Consideration of the construction of a 37,652 square foot, four-story hotel (My Place Hotel) on 1.9 acres located at the southwest corner of Airport Road and Biddle Road in the C-R (Regional Commercial) zoning district (372W12A1102). Applicant, MP Medford LLC; Agent, Ausland Group; Planner, Dustin Severs.

**DATE:** November 13, 2019

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

**CONDITIONS**

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. The installation of a "on-site" 12-inch water line is required per MWC Standards. Applicants' Civil Engineer shall coordinate with MWC engineering department for "on-site" water facility layout. Water lines are required to be installed in paved travel lanes. They shall not be installed through landscaping islands, parking islands, and shall not extend through parking stalls.
4. The proposed 4-inch domestic water meter shall be located in a water meter vault per MWC Standards. Vault shall be located in a proposed landscape island, or other open space. Vaults with accessible metal lids shall not be located in roadways, or sidewalk area.
5. The proposed 6-inch fire service shall be located in a fire service vault per MWC Standards. Vault shall be located in a proposed landscape island, or other open space. Vaults with accessible metal lids shall not be located in roadways, or sidewalk area.
6. Dedication of a 10-foot wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement shall be submitted to MWC for review and recordation prior to construction.

*Continued to Next Page*



## Staff Memo

*Continued from Previous Page*

7. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35> .
8. Static water pressure is expected to be approximately 94 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
9. If a well is located on this parcel it will be required to be abandoned per State of Oregon water Resources Department regulations per Oregon Water Resources Department Chapter 690 Division 220.
10. Applicant and/or their Civil Engineer shall coordinate with MWC engineering staff for available pressure, and fire hydrant flow testing for design use in the proposed fire sprinkler system. A "Summer" flow test is required, between the months of late June through September.

### COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required. (See Condition 3 & 4 above)
3. Static water pressure is approximately 94 psi. (See Condition 5 above)
4. MWC-metered water service does not exist to this property.
5. Access to MWC water lines is available. There is an existing 24" DI water line on the south side of Airport Road, and also in Biddle Road



BUILDING SAFETY DEPARTMENT  
ROOM 277

CITY OF MEDFORD  
LAUSMANN ANNEX  
200 SOUTH IVY STREET  
MEDFORD, OREGON 97501

TELEPHONE (541) 774-2350  
FAX (541) 774-2575  
E-MAIL:  
bldmed@ci.medford.or.us

## Policy on Installation of Pressure Reducing Valves

August 5, 2014

Section 608 of the 2011 Edition of the Oregon Plumbing Specialty Code requires a pressure regulator (commonly called a Pressure Reducing Valve or PRV) where the static pressure in the water supply piping exceeds 80 psi. Although this section gives limited guidance as to installation, it does require the device to be

*“...accessibly located above ground or in a vault equipped with adequate means to provide drainage and shall be protected from freezing, and shall have the strainer readily accessible for cleaning without removing the regulator or strainer body or disconnecting the supply piping.”*

“Accessible” and “readily accessible” are defined in chapter 2.

To assure uniform and appropriate installation of these devices within Medford, the following standards have been agreed to by the City of Medford Building Safety Department and the Medford Water Commission:

1. The need for these devices will be based on pressure information provided by the Medford Water Commission, and can be verified on-site with a pressure gage. While factory settings of these devices may be adjusted, MWC recommends that the regulated pressure be set no higher than 65 psi.
2. PRVs shall NOT be installed when static pressure is less than 50 psi, except for limited specific equipment-based needs.
3. The PRV shall be installed outside the street right of way as close as practical to the water meter.
4. No expansion tank is necessary.
5. No fixture, device or system is permitted between the meter and the PRV.
6. The PRV must NOT be direct buried nor installed in a crawl space.
7. PRVs shall be installed within a readily accessible valve box / vault following the same standard as used for double check backflow assemblies, as follows:

*“On new installations, at least 12-inches clearance will be required as per section 603.3.4. When replacing an existing assembly, the 12-inch clearance requirement can be waived as long as there is at least 3-inches clearance between the bottom of the assembly and the ground, and the device is tested and serviced from the top.”*

***Sam Barnum***

Building Safety Director



**Medford Fire-Rescue Land Development Report**

**Review/Project Information**

**Reviewed By:** Kleinberg, Greg

**Review Date:** 10/31/2019  
**Meeting Date:** 11/13/2019

**LD File #:** AC19062

**Planner:** Dustin Severs

**Applicant:** MP Medford LLC; Agent Ausland Group

**Site Name:** My Place Hotel

**Project Location:** Southwest corner of Airport Road and Biddle Road

**Project Description:** Consideration of the construction of a 37,652 square foot, four story hotel (My Place Hotel) on 1.9 acres

**Specific Development Requirements for Access & Water Supply**

Conditions

Reference	Comments	Description
505.1	Address numbers must be visible from the Airport Road driveway entrance.	New and existing buildings shall have approved address numbers, building numbers or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. Where required by the fire code official, address numbers shall be provided in additional approved locations to facilitate emergency response. Address numbers shall be Arabic numbers or alphabetical letters. Numbers shall be a minimum of 4 inches (101.6 mm) high with a minimum stroke width of 0.5 inch (12.7 mm). Where access is by means of a private road and the building cannot be viewed from the public way, a monument, pole or other sign or means shall be used to identify the structure. Address numbers shall be maintained.
OFC 508.5	The three (3) new fire hydrant locations are approved as submitted on the plans.	Fire hydrants with reflectors will be required for this project.  The approved water supply for fire protection (hydrants) is required to be installed prior to construction when combustible material arrives at the site.  Plans and specifications for fire hydrant system shall be submitted to Medford Fire-Rescue for review and approval prior to construction. Submittal shall include a copy of this review (OFC 501.3).
OFC 503.2.1	Fire apparatus road design requirements. Note: The 135 degree turn on the east side of the parking lot does not meet the minimum 25' inside turning radius requirement. The tight radius on the parking island will have to be redesigned to meet requirements.	Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 60,000 pounds. (See also OFC 503.4; D102.1)  The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).

O  
AC-19-062

OFC  
503.4

Curbs shall be painted and stenciled to prohibit parking along the fire lanes.

Fire apparatus access roads 20-26' wide shall be posted on both sides as a fire lane. Fire apparatus access roads more than 26' to 32' wide shall be posted on one side as a fire lane (OFC D103.6.1).

Where parking is prohibited for fire department vehicle access purposes, NO PARKING-FIRE LANE signs shall be spaced at minimum 50' intervals along the fire lane (minimum 75' intervals in 1 & 2 family residential areas) and at fire department designated turn-around's. The signs shall have red letters on a white background stating "NO PARKING-FIRE LANE" (See handout).

For privately owned properties, posting/marking of fire lanes may be accomplished by any of the following alternatives to the above requirement (consult with the Fire Department for the best option):

Alternative #1:

Curbs shall be painted red along the entire distance of the fire department access. Minimum 4" white letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals.

Alternative #2:

Asphalt shall be striped yellow or red along the entire distance of the fire department access. The stripes shall be at least 6" wide, be a minimum 24" apart, be placed at a minimum 30-60 degree angle to the perimeter stripes, and run parallel to each other. Letters stating "NO PARKING-FIRE LANE" shall be stenciled on the asphalt at 25-foot intervals.

Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12).

This restriction shall be recorded on the property deed as a requirement for future construction.

A brochure is available on our website at:

<http://www.ci.medford.or.us/Files/Fire%20Lane%20Brochure.pdf>

SECTION D105-AERIAL FIRE APPARATUS ACCESS ROADS

OFC D105 The aerial apparatus access is approved as shown on the plans.

D105.1 Where required. Where the vertical distance between the grade plane and the highest roof surface exceeds 30 feet (9144 mm), approved aerial fire apparatus access roads shall be provided. For purposes of this section, the highest roof surface shall be determined by measurement to the eave of a pitched roof, the intersection of the roof to the exterior wall, or the top of parapet walls, whichever is greater.

D105.2 Width. Aerial fire apparatus access roads shall have a minimum unobstructed width of 26 feet (7925 mm), exclusive of shoulders, in the immediate vicinity of the building or portion thereof.

D105.3 Proximity to building. At least one of the required access routes meeting this condition shall be located within a minimum of 15 feet (4572 mm) and a maximum of 30 feet (9144 mm) from the building, and shall be positioned parallel to one entire side of the building. The side of the building on which the aerial fire apparatus access road is positioned shall be approved by the fire code official.

D105.4 Obstructions. Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and the building. Other obstructions shall be permitted to be placed with the approval of the fire code official.

OFC 903	<p>A fire sprinkler system is required by code for this occupancy.</p> <p>The backflow device for the fire sprinkler system is required to be placed in a vault to prevent freezing.</p> <p>The fire department connection shall be located at an approved location away from the building and within 75' of a fire hydrant. The best location would be near the west fire hydrant.</p>	<p>Fire sprinkler system requirement information.</p> <p>Where a fire sprinkler system is required, it shall meet the requirements of the Oregon Fire Code and the applicable National Fire Protection Association (NFPA) Standard.</p> <p>Consult the Medford Water Commission for proper water meter sizing for fire sprinkler systems.</p>
OFC 907	<p>A fire alarm system is required by code for this occupancy.</p>	<p>Fire alarm system requirement information.</p> <p>Where a fire alarm system is required, it shall meet the requirements of the Oregon Fire Code and the National Fire Protection Association (NFPA) 72 Standard.</p>
OFC 905	<p>A standpipe system is required by code for this occupancy.</p>	<p>Standpipe system requirement information.</p> <p>Where a fire sprinkler system is required, it shall meet the requirements of the Oregon Fire Code and the National Fire Protection Association (NFPA) 14 Standard.</p>

**Construction General Information/Requirements**

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code. This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

**Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300**

[www.medfordfirerescue.org](http://www.medfordfirerescue.org)



# JACKSON COUNTY

## Roads

Roads  
Engineering

Chuck DeJanvier  
Construction Engineer

200 Antelope Road  
White City, OR 97503  
Phone: (541) 774-6255  
Fax: (541) 774-6295  
dejanvca@jacksoncounty.org

www.jacksoncounty.org

November 1, 2019

Attention: Dustin Severs  
Planning Department  
City of Medford  
200 South Ivy Street, Lausmann Annex, Room 240  
Medford, OR 97501

RE: Site plan review for an extended stay hotel on  
Airport Road – a county maintained road  
and Biddle Road – a city maintained road  
Planning File: AC-19-062

Dear Dustin:

Thank you for the opportunity to comment on this site plan review of a 37,652 square foot, four story hotel (My Place Hotel, an Extended Stay Hotel) on a 6.20 acre parcel, zoned Regional Commercial (C-R) and Light Industrial (I-L) located at the corner of Airport Road and Biddle Road (590 Airport Road) (37-2W-12A tax lot 1102). Jackson County Roads has the following comments:

1. Jackson County Roads has requested a TIS that looks at the safety and capacity of the site access on Airport Road. If mitigations are recommended, they shall be required.
2. Jackson County's General Administration Policy #1-45 sets forth the County's position as it relates to the management of County roads located within existing or proposed city limits or Urban Growth Boundaries (UGB). The County has no current plans for improvements to Airport Road. Jackson County Roads recommends that the city request jurisdiction of this road.
3. Jackson County Roads would like to review and comment on the hydraulic report including the calculations and drainage plan. Capacity improvements or on site detention, if necessary, shall be installed at the expense of the applicant. Upon completion of the project, the developer's engineer shall certify that construction of the drainage system was constructed per plan and a copy of the certification shall be sent to Jackson County Roads.
4. The applicant shall submit construction plans to Jackson County Roads, so we may determine if county permits will be required.

5. Storm water should meet City of Medford requirements that also include water quality.
6. Jackson County would encourage frontage improvements to Airport road. If frontage improvements are required on Airport Road, they shall be permitted and inspected by the City of Medford.
7. Any new or improved road approaches off Airport Road shall be permitted and inspected by the City of Medford.
8. We would like to be notified of future development proposals, as county permits may be required.
9. Airport Road is a County Minor Collector and is county-maintained. The Average Daily Traffic Count was 3,100 on the Medford 2016 Traffic Volumes map.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Chuck DeJanvier  
Construction Engineer



# ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005  
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

November 4, 2019

City of Medford Planning Department  
200 S. Ivy Street  
Medford, Oregon 97501

**Re: My Place Hotel (Map 372W12A, Tax Lot 1102)**  
**Ref: LDP-19-055 & ZC-19-003**

ATTN: Dustin,

The subject property is within the RVSS service area. There is an 8 inch sewer main stubbed to the northwest corner of the property and a 54 inch sewer located on the northeast corner of the property as shown on the submitted site layout.

Sewer Service for proposed hotel can be had by a new sewer service tap on the 54 inch sewer main to the east. Tap permits are issued by RVSS and sewer connection permit will be issued by the City of Medford. Sewer System Development Charges will be owed to RVSS.

Rogue Valley Sewer Services requests that approval of this application be subject to the following conditions:

1. All development must be designed and constructed per RVSS standards.
2. The applicant must submit sewer construction drawings to RVSS for review and approval prior to construction.
3. The applicant must submit architectural drawings to RVSS for the calculation of System Development Charges.
4. Applicant must pay related System Development Charges to RVSS prior to issuance of building permits.

Please feel free contact me with any questions.

Sincerely,

Nicholas R. Bakke, P.E.  
District Engineer

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Q  
AC-19-062

## Dustin J. Severs

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**From:** Amber Judd <JuddAJ@jacksoncounty.org>  
**Sent:** Friday, November 1, 2019 1:41 PM  
**To:** Dustin J. Severs  
**Subject:** File No. AC-19-062 Project Name: My Place Hotel

<EXTERNAL EMAIL \*\*Be cautious with links and attachments\*\*>

Dustin,

The Airport requests an Avigation, Noise and Hazard Easement be a requirement of this project. In addition, due to the proximity to the Airport, the applicant needs to contact the FAA regarding filing a 7460-1 Notice of Proposed Construction or Alteration. The FAA contact is: Paul Holmquist, phone (206) 231-2990.

I have inserted some information below from the FAA's website:

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference CFR Title 14 Part 77.9.

You must file with the FAA at least 45 days prior to construction if:

- Your structure will exceed 200 ft above ground level
- Your structure will be in proximity to an airport and will exceed the slope ratio
- Your structure involves construction of a traverseway (i.e. highway, railroad, waterway, etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- Your structure will emit frequencies, and does not meet the conditions of the FAA Co-location Policy
- Your structure will be in an instrument approach area and might exceed part 77 Subpart C
- Your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- Your structure will be on an airport or heliport
- Filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the Air Traffic Areas of Responsibility map for Off Airport construction, or contact the FAA Airport Region/District Office for On Airport construction.

### Results

You exceed the following Notice Criteria:

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(b) by 26 ft. The nearest airport is MFR, and the nearest runway is 14LF/32RF.

The FAA requests that you file.

Thank you,

*Amber Judd*

Deputy Director-Administration  
Rogue Valley International-Medford Airport (MFR)



1000 Terminal Loop Parkway, Suite 201  
Medford, Oregon 97504  
541-776-7222

22921

**SURVEY FOR:**  
SEDONA PROPERTIES, LLC  
1175 E MAIN ST, #2B  
MEDFORD, OR 97504

**SURVEY BY:**  
L.J. FRIAR & ASSOCIATES, P.C.  
CONSULTING LAND SURVEYORS  
P.O. BOX 1947  
PHOENIX, OREGON 97535  
PHONE: (541) 772-2782  
lfriarandassociates@charter.net

**DATE:**  
AUGUST 29, 2019

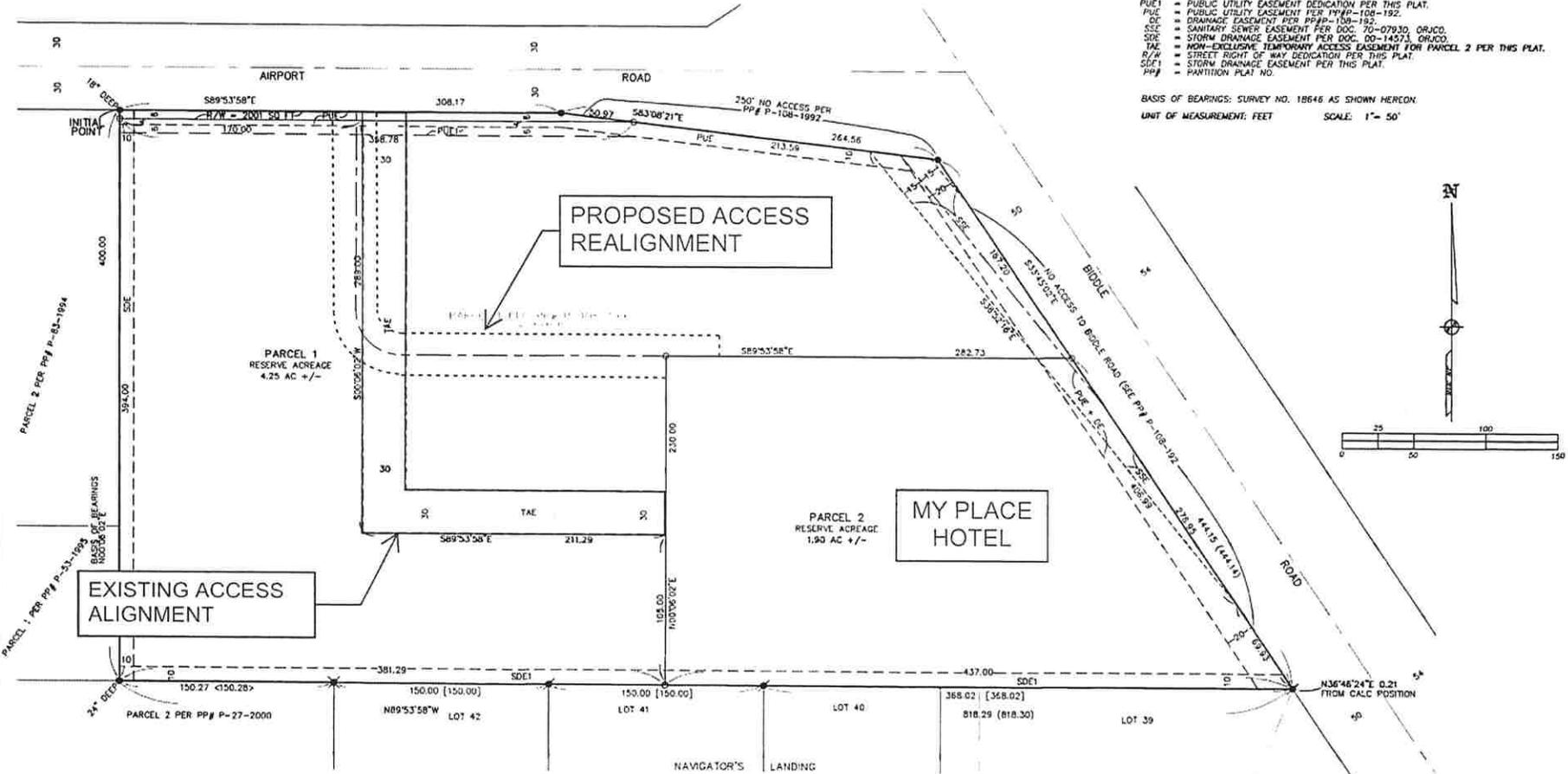
# PARTITION PLAT NO. P-37-2019

Located in the N.E. 1/4 of Sec. 12, T.37S, R.2W, W.M.  
in the City of Medford, Jackson County, Oregon  
(File No. LDP-19-055)

**LEGEND:**

- = FD. 5/8" IRON PIN & PLASTIC CAP MKD. MARQUESS & ASSOC. PER FS13185.
- ⊕ = FD. 5/8" IRON PIN & PLASTIC CAP MKD. D. MCMAHAN LS1913 PER NL.
- ⊗ = FD. 5/8" IRON PIN & PLASTIC CAP MKD. G. NEATHAMER LSS6545 PER FS18846.
- = SET 5/8" X 24" IRON PIN & PLASTIC CAP MKD. L.J. FRIAR & ASSOC.
- = RECORD DATA PER PP#P-108-192 (FS13185).
- = RECORD DATA PER NL.
- = RECORD DATA PER FS18506.
- = NAVIGATOR'S LANDING (FS17310).
- ORJCO = OFFICIAL RECORDS OF JACKSON COUNTY, OREGON.
- JCOJ = JACKSON COUNTY DEED RECORDS.
- FS = FILED SURVEY #.
- = FENCE.
- PUE1 = PUBLIC UTILITY EASEMENT DEDICATION PER THIS PLAT.
- PUE2 = PUBLIC UTILITY EASEMENT PER PP#P-108-192.
- DC = DRAINAGE EASEMENT PER PP#P-108-192.
- SSE = SANITARY SEWER EASEMENT PER DOC. 70-07930, ORJCO.
- SDE = STORM DRAINAGE EASEMENT PER DOC. 00-14573, ORJCO.
- TAE = NON-EXCLUSIVE TEMPORARY ACCESS EASEMENT FOR PARCEL 2 PER THIS PLAT.
- R/W = STREET RIGHT OF WAY DEDICATION PER THIS PLAT.
- SDE1 = STORM DRAINAGE EASEMENT PER THIS PLAT.
- PP# = PARTITION PLAT NO.

BASIS OF BEARINGS: SURVEY NO. 18646 AS SHOWN HEREON.  
UNIT OF MEASUREMENT: FEET      SCALE: 1" = 50'



**EASEMENTS PER PUBLIC RECORDS REPORT**

1. ELECTRICAL EASEMENT PER DOC. NO. 05-08146, ORJCO. NO SPECIFIC LOCATION GIVEN.
2. EASEMENT FOR IRRIGATION SIPHON PER DOC. 70-07928, ORJCO. DOES NOT APPEAR TO BE LOCATED ON SUBJECT TRACT.
3. SANITARY SEWER EASEMENT PER DOC. 70-07930, ORJCO. SHOWN.
4. UTILITY EASEMENTS AS DELINEATED ON MINOR LAND PARTITION P-108-1992, SHOWN.
5. DRAINAGE EASEMENT AS DELINEATED ON MINOR LAND PARTITION P-108-1992, SHOWN.
6. NOTATION ON THE RECORDED PLAT THAT PARCEL NO. 1 HAS NO ACCESS ONTO AIRPORT ROAD FROM BIDDLE ROAD TO 250 FEET WEST OF BIDDLE ROAD, SHOWN.
7. STORM DRAIN EASEMENT PER DOC. 00-14573, ORJCO. SHOWN.

I HEREBY DECLARE THAT THIS IS AN EXACT COPY OF THE ORIGINAL PLAT.  
SURVEYOR: *J. Friar*

REGISTERED PROFESSIONAL LAND SURVEYOR  
J. Friar  
OREGON  
JULY 17, 1986  
JAMES E. HIBBS  
2224  
RENEWAL DATE 6-30-21

FILED  
Date 10/2/19 By J.P.D.  
This Survey Consists Of:  
2 sheet(s) Map  
0 page(s) Narrative  
JACKSON COUNTY SURVEYOR

SHEET 2 OF 2

372W12A TL1102

THIS PLAT WAS PREPARED USING AN EPSON SC-15200 WITH EPSON 16925 INK ON CONTINENTAL JPC4V2 POLYESTER FILM.

22921

Page 75

AC-19-062



Project Name:

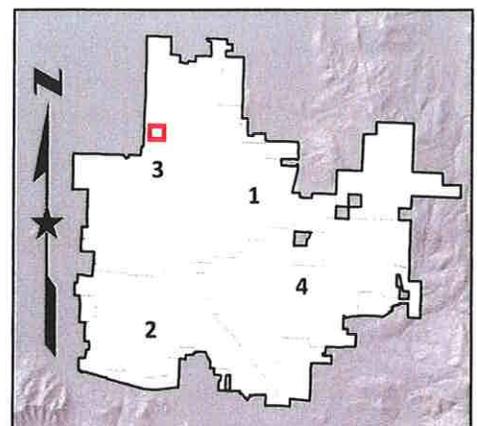
**My Place Hotel**  
**590 Airport Rd**

Map/Taxlot:

**372W12A TL 1102**

**Legend**

-  Subject Area
-  Tax Lots



0 155 310  
Feet

11/19/2019