

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA APRIL 6, 2018



Commission Members

Jim Quinn, Chair

Bill Chmelir, Vice Chair

Jeff Bender

Jim Catt

David Culbertson

Bob Neathamer

Marcy Pierce

Rick Whitlock

City Council Liaison - Dick Gordon

City Council Liaison Alt. - Tim D'Alessandro

Regular Commission meetings are held on
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380



Site Plan and Architectural Commission

Agenda

Public Hearing

April 6, 2018

12:00 noon

Council Chambers, City Hall, Room 300
411 West Eighth Street, Medford, Oregon

-
10. Roll Call.
20. Consent Calendar.
- 20.1 AC-17-167 Final Order for consideration of a proposal for the development of a single 1.7-acre lot consisting of the construction of a 4,485 square foot restaurant to be located at 1383 Center Drive in the Regional Commercial (C-R) zoning district (371W32B4703); MESK Investment, Applicant; Tormod Hellwig Architecture, Agent; Dustin Severs, Planner).
- 20.2 AC-18-001 Final Order for consideration of plans for the construction of a 4-unit multiple-family complex on one parcel totaling 0.22 acres located southwest of the intersection of E. 10th Street and Portland Avenue within the MFR-20 (15 to 20 dwelling units per gross acre) zoning district. (371W30DB1200) (Mike Pagnini, Applicant; Richard Stevens & Associates, Agent; Steffen Roennfeldt, Planner).
30. Minutes.
- 30.1 Consideration for approval of minutes from the March 16, 2018, meeting.
40. Oral and Written Requests and Communications.
Comments will be limited to 3 minutes per individual or 5 minutes if representing a group or organization. **PLEASE SIGN IN.**
50. Public Hearings.
Comments are limited to a total of 10 minutes for applicants and/or their representatives. You may request a 5-minute rebuttal time. All others will be limited to 3 minutes per individual or 5 minutes if representing a group or organization. **PLEASE SIGN IN.**
- New Business.**
- 50.1 AC-18-011 Consideration of plans for the construction of an approximately 21,000 square foot, two-story, general office building on 1.39 acres in the Navigators Landing Planned Unit Development zoned I-L/PD located west of the Medford International Airport on the corner of O'Hare Parkway and National Drive (372W12D TL11000 & 11100); (Mayers 5 LLC, Applicant; Kistler + Small + White Architects, Agent; Steffen Roennfeldt, Planner).

Meeting locations are generally accessible to persons with disabilities. To request interpreters for hearing impaired or other accommodations for persons with disabilities, please contact the ADA Coordinator at (541) 774-2074 or ada@cityofmedford.org at least three business days prior to the meeting to ensure availability. For TTY, dial 711 or (800) 735-1232.

- 50.2 **AC-18-016** Consideration of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district. (362W23D111); (Gerald Reynolds, Applicant/Agent; Steffen Roennfeldt, Planner).
60. **Written Communications.**
70. **Unfinished Business.** None
80. **New Business.**
90. **Report from the Planning Department.**
100. **Messages and Papers from the Chair.**
110. **Propositions and Remarks from the Commission.**
120. **City Council Comments.**
130. **Adjournment.**

BEFORE THE MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

STATE OF OREGON, CITY OF MEDFORD

IN THE MATTER OF SITE PLAN AND ARCHITECTURAL COMMISSION)
FILE AC-17-167 APPLICATION FOR PROJECT REVIEW SUBMITTED) ORDER
BY MESK INVESTMENT)

AN ORDER granting approval of plans for the development of a single 1.7-acre lot consisting of the construction of a 4,485 square foot restaurant to be located at 1383 Center Drive in the Regional Commercial (C-R) zoning district (371W32B4703).

WHEREAS:

1. The Site Plan and Architectural Commission has duly accepted the application filed in accordance with the Land Development Code, Section 10.285.
2. The Site Plan and Architectural Commission has duly held a public hearing on the matter of an application of plans for the development of a single 1.7-acre lot consisting of the construction of a 4,485 square foot restaurant to be located at 1383 Center Drive in the Regional Commercial (C-R) zoning district (371W32B4703), with a public hearing a matter of record of the Site Plan and Architectural Commission on March 16, 2018.
3. At the public hearing on said application, evidence and recommendations were received and presented by the Planning Department staff; and
4. At the conclusion of said public hearing, after consideration and discussion, the Site Plan and Architectural Commission, upon a motion duly seconded, granted approval and directed staff to prepare a final order with all conditions and findings set forth for the granting of approval.

THEREFORE LET IT BE HEREBY ORDERED that the application of MESK Investment, stands approved subject to compliance with the conditions stated in the Commission Report dated March 16, 2018.

AND LET IT FURTHER BE OF RECORD that the action of the Site Plan and Architectural Commission approving this application is hereafter supported by the following findings:

- (a) That the proposed development, with the conditions of approval, complies with the applicable provisions of all city ordinances as determined by the staff review.

FINAL ORDER AC-17-167

- (b) That the proposed development is compatible with uses and development that exist on adjacent land, based upon information provided in the Applicant's Questionnaire and presented at the public hearing.

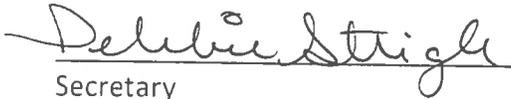
BASED UPON THE ABOVE, it is the finding of the Medford Site Plan and Architectural Commission that the project is in compliance with the criteria of Section 10.290 of the Land Development Code.

Accepted and approved this 6th day of April, 2018.

MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

Site Plan and Architectural Commission Chair

ATTEST:


Secretary



City of Medford

Planning Department

Working with the community to shape a vibrant and exceptional city

COMMISSION REPORT

for a Type-C quasi-judicial decision: **Site Plan and Architectural Review**

PROJECT IHOP Restaurant
Applicant: MESK Investment
Agent: Tormod Hellwig Architecture

FILE NO. AC-17-167

DATE March 16, 2018

BACKGROUND

Proposal

Consideration of a proposal for the development of a single 1.7-acre lot consisting of the construction of a 4,485 square foot restaurant to be located at 1383 Center Drive in the Regional Commercial (C-R) zoning district (371W32B TL 4703).

Vicinity Map



Subject Site Characteristics

Zoning: C-R (Regional Commercial)
GLUP: CM (Commercial)
Overlay(s): None
Use: Vacant

Surrounding Site Characteristics

<i>North</i>	Zone: C-R Use(s): Southern Oregon Ice Rink, commercial plaza
<i>South</i>	Zone: C-R Use(s): Marriot Hotels
<i>East</i>	Zone: C-R Use(s): Future Holiday Inn Express
<i>West</i>	Zone: C-R Use(s): Walmart

Related Projects

None

Applicable Criteria

MLDC Section: 10.290 – Site Plan & Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

ISSUES AND ANALYSIS

Project Summary

The subject site consists of a single, vacant 1.7-acre lot fronted by Center Drive – a major arterial street. The applicant is proposing to develop the property in phases, with the subject proposal being the first of three buildings planned for the site in the future. With the subject

development, the applicant is proposing the construction of a 4,485 square foot IHOP restaurant.

Site Development Standards

SITE DEVELOPMENT TABLE

	Allowed/Required	Proposed
Height	85 feet Max.	26'-5"
Lot Coverage	40% Max.	6.5%
Setback (front)	10 Foot Min.	32 feet
Setback (rear)	None	132 feet
Setback (sides)	None	12 feet / 198 feet

As shown in the Site Development Table above, it can be found that the proposed structure identified on the submitted site plan meets the bulk standards for the C-R zoning district as found in Article V of the Medford Land Development Code.

Parking

PARKING TABLE (10.743-751)

	Required	Provided
Total Spaces	40 min. / 49 max.	47
Accessible Spaces	2	1
Bicycle Spaces	5	6

The submitted site plan identifies a total of 47 proposed parking spaces, meeting the minimum required spaces for the site per MLDC 10.741. Additionally, the site plan includes the requisite number of bicycle parking spaces as outlined in the Code; however, the site plan is deficient one accessible parking space. As a condition of approval, the applicant will be required to submit a revised site plan with their building permit submittals showing one additional accessible parking space, prior to the issuance of a building permit for vertical construction.

Landscaping

LANDSCAPE TABLE - Frontage Landscaping (10.797)

Center Drive	Required	Shown
Trees	5	5
Shrubs	34	34+

LANDSCAPE TABLE – Parking Area Planters (10.746[3])

	Required	Shown
Trees	4	6
Shrubs	8	8+
Coverage area	950 square feet	950+ square feet

It can be found that the submitted Landscape Plan (Exhibit G) meets and/or exceeds the frontage landscaping requirements along the property's frontage of Center Drive, and meets the parking lot planter requirements per MLDC 10.797 and 10.746(3).

Access

Vehicular Access

Access to the subject site is proposed to be provided by a driveway off of the existing private road located to the north of the subject site contained within the flagpole section of the abutting flag lot to the east (tax lot 4701 - future site of Holiday Inn Express). Per the staff report submitted by Public Works (Exhibit I), Public Works recommends - in the interest of providing direct and convenient cross access between the abutting commercial properties – that the proposed north driveway shown on the site plan be directly aligned with the existing east driveway of tax lot 4700 (property to the north of the site) and the stubbed cross access driveway shared with tax lot 4702 to the south, which is stubbed at the southerly lot line of the subject site. As a discretionary condition of approval, staff recommends that the applicant be required to submit a revised site plan with their building permit submittals showing the property's north access driveway directly aligned between the existing east driveway of tax lot 4700, located to the north of the subject site, and the stubbed cross access driveway shared with tax lot 4702 to the south of the subject site, prior to the issuance of a building permit for vertical construction.

At the Land Development meeting held on February 14, 2018, with the applicant's agent, Public Works expressed concern with the accuracy of the submitted site plan in the location shown on the plan of the existing cross access driveway provided by tax lot 4702 to the south of the site and stubbed at the southerly lot line of the subject site. The submitted site plan shows the driveway located 22 feet from the site's easterly lot line shared with tax lot 4701. It was staff's view that the access driveway – based on available data - is significantly closer to the site's easterly lot line than what is shown on the applicant's site plan. The applicant's agent explained that it was the applicant's intent to have a map of survey performed on the site to confirm the location of the driveway. As a condition of approval, the applicant will be required to submit a map of survey prepared by an Oregon licensed surveyor confirming the location of the cross access driveway shared with tax lot 4702 to the south of the subject site, prior to the issuance of a building permit for vertical construction. If the map of survey shows the location of the cross access driveway shown on the site plan to be inaccurate, the applicant will be

required to submit a revised site plan delineating the confirmed location of the access driveway, also prior to the issuance of a building permit for vertical construction.

A minor modification of a site plan review can be approved administratively by the Planning Director provided that the Planning Director determines that the modification does not constitute a major modification based on the standards outlined in MLDC 10.294(B)(1-8). Per MLDC 10.294(A), if a modification to an approved plan is determined to be a major modification, the plan shall be processed in the same manner as a request for a site plan and architectural review in MLDC 10.285.

Pedestrian Access

The submitted site plan shows pedestrian walkways connecting the proposed building's entrance with the existing public sidewalk along Center Drive, and connecting the proposed building to the abutting future phase of the site to the east, consistent with Pedestrian Walkway provisions found in MLDC 10.772-776. However, the submitted site plan does not show a pedestrian walkway connecting the proposed building to the abutting future phase of the site to the north. As a condition of approval, the applicant will be required to submit a revised site plan with their building permit submittals illustrating how pedestrian connectivity will be provided between all three phases of the property with the subject development, prior to the issuance of a building permit for vertical construction.

Concealments

Trash Enclosure

The submitted site plan (Exhibit B) identifies a CMU gated trash enclosure located to the east of the proposed building and includes an elevation drawing showing the trash receptacle concealed behind a 17'-4"x 10'-8" foot concrete masonry wall (5'8" in height) with a metal gate fence, consistent with the requirements found in MLDC 10.781.

Architecture

The applicant's submitted narrative (Exhibit H) describes the building's proposed architecture and how it fits with and complements adjacent buildings, as the following:

The exterior materials of the proposed building utilize the same palette of materials that is found on the adjacent buildings - stucco siding, facing stone as well as accent material and metal roofing. The scale of the building at 4,500 sf is also in keeping with the general size and scale of the adjacent buildings.

Wetlands

There is a designated wetland identified on the Local Wetland Inventory map which shows the subject lot partially impacted by the presence of a wetland. As required by ORS 227.350, staff forwarded the application to the Oregon Department of State lands (DSL) as a reviewing agency. At the time of this writing, staff has not received a response from DSL.

Facility Adequacy

Per the agency comments submitted to staff (Exhibits I-L), it can be found that there are adequate facilities to serve the proposed development.

Other Agency Comments

Rogue Valley Sewer Services (RVSS) (Exhibit L)

The subject property is within RVSS service area, which requires that future sewer improvements be designed and constructed in accordance with RVSS standards. As a condition of approval, the applicant will be required to comply with all conditions of RVSS.

Committee Comments

No comments were received from committees such as BPAC.

DECISION

At the public hearing held on March 16, 2018, the Commission voted 4-2 in support of the motion to approve the request while striking condition #5, which was found to be a clerical error (duplicate of condition #8); and at the request of staff, approving the modification of condition #7, requiring that the applicant provide pedestrian connectivity to the future phases of the site and to the abutting property to the east; and upholding the inclusion of discretionary condition #10 recommended by staff, requiring that the applicant submit a revised site plan directly aligning the property's access with the access points of tax lot 4700, located to the north of the subject site, and the stubbed cross access driveway shared with tax lot 4702 to the south of the subject site

During the public hearing, the following Exhibits, provided during public testimonies, were entered into the record:

Mark McKechnie, agent for the applicant, presented a tentative revised site plan showing the relocation of the cross access driveway shared with tax lot 4702, based on the map of survey performed on the site confirming the inaccuracy of the location of the driveway shown on the submitted site plan (Exhibit B); and proposing pedestrian connectivity points to the future phases of the site and to the abutting property to the east, in order to comply with the modified condition #7. The revised site plan has been added to this Commission Report as Exhibit N. *(Mr. McKechnie's revised site plan was for illustrative purposes only, in order to demonstrate to the Commission how a revised site plan for future building permit submittal would conform with the conditions imposed by staff, and was not presented as an immediate replacement for the official site plan (Exhibit B) submitted for the Commission's formal review.)*

Brian Westerhout, speaking on behalf of Dan Nash (property owner of tax lot 3600), supported staff's recommendation that the site's access points be aligned (discretionary condition #10), presenting a plan illustrating that said alignment would allow for the future extension of the left turn lane, thereby preventing the future stacking of vehicles seeking to a

make a right out, and averting future traffic congestion. Mr. Westerhout's plan has been added to this Commission Report as Exhibit O.

Note: By eliminating condition #5, which was found to be a clerical error (duplicate of condition #8), the associated condition numbers cited above have been shifted/altered accordingly in the attached revised conditions of approval (Exhibit A-1).

FINDINGS OF FACT

MLDC 10.290

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the Applicant's Questionnaire and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit H) and recommends the Commission adopt the findings as recommended by staff.

ACTION TAKEN

Adopted the findings as recommended by staff and directed staff to prepare the Final Order for approval of AC-17-167 per the Commission Report dated March 16, 2018, including Exhibits A-1 through O.

EXHIBITS

- A-1** Revised conditions of approval, drafted March 16, 2018.
- B** Site Plan, received January 12, 2018.
- C** Drainage Plan, received January 12, 2018.
- D** Floor/Roof Plan, received December 11, 2017.
- E** Elevation Plan (south/west), received January 12, 2018.
- F** Elevation Plan (north/east), received January 12, 2018.
- G** Landscape Plan, received December 11, 2017.

- H Applicant's Narrative, Questionnaire, and Findings of Fact, received December 11, 2017
- I Public Works staff report, received February 14, 2018.
- J Medford Water Commission report & map, received February 14, 2018.
- K Medford Fire Department report, received February 14, 2018.
- L Rogue Valley Sewer Services (RVSS) report, received February 2, 2018.
- M SPAC Application, received December 11, 2017.
- N Tentative revised site plan presented at hearing by Mark McKechnie, March 16, 2018.
- O Letter from Dan Nash and associated Site plan showing extension of left-turn lane presented at hearing by Brian Westerhout (speaking on behalf of Dan Nash), March 16, 2018.
Vicinity map

SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA

March 16, 2018

Jim Quinn, Chair

EXHIBIT A-1

IHOP Restaurant
AC-17-167
Conditions of Approval - Revised
March 16, 2018

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit I).
2. Comply with all conditions stipulated by the Medford Water Commission (Exhibit J).
3. Comply with all requirements of the Medford Fire Department (Exhibit K).
4. Comply with all requirements of the Rogue Valley Sewer Services (RVSS) (Exhibit L).
5. Submit a revised site plan showing one additional accessible parking space.
6. **Submit a revised site plan showing a pedestrian walkway connecting the site to the sidewalk running along the westerly boundary of the adjacent property to the east, and illustrating how pedestrian connectivity will be provided between all three phases of the property, consistent with the requirements of MLDC 10.772-775.**
7. Comply with all requirements of the Department of State Lands (DSL) to insure the protection of any wetlands identified on the site.
8. Submit a map of survey prepared by an Oregon licensed surveyor confirming the location of the cross access driveway shared with tax lot 4702 to the south of the subject site, and if the map of survey shows the location of the cross access driveway shown on the submitted site plan to be inaccurate, the applicant shall submit a revised site plan delineating the confirmed location of the access driveway. The approval of a modified site plan submittal shall be based on the standards outlined in MLDC 10.294, titled *Modification of a Site Plan and Architectural Review*.

DISCRETIONARY CONDITIONS

9. Submit a revised site plan showing the property's north access driveway directly aligned between the existing east driveway of tax lot 4700, located to the north of the subject site, and the stubbed cross access driveway shared with tax lot 4702 to the south of the subject site.

Members of the Site Plan Architectural Review Committee

My Name is Dan Nash and I am the Managing Member for Nash Holdings LLC. Our Family owns Tax Lot 3600, the 3.5 acres at the end of the drive East of the subject site. We have owned the property since 1965.

First, I would like to state for the record, that we support the applicants planned use for this site. However after careful review of the application we are concerned that the transportation elements of access, circulation, and connectivity have not been properly addressed or provided for by the applicant. Medford's land use code requires that, in order to insure an efficient movement of cars, all property owners in a given area work to create a transportation plan that facilitates and maximizes the movement of cars. Especially in this area, which is in close proximity to Interstate 5 and Hwy 99. This plan does none of that. In fact, if this plan were approved as presented, it would create both immediate and down the road traffic problems.

The applicant, despite having adequate property to do so, does not provide for additional left turn stacking. Currently there is minimal storage for left turn movement at the intersection (about 2 cars). The applicant provides for a connection to the existing Hotels, and we support that since the traffic from the existing Hotels cannot make a left turn from their sites. We concur with Public Works and Planning Staff that access to the Interior Access Road needs to be as far east as possible to minimize conflicts with vehicles entering the Interior Access Road from Center Drive. In addition, the 2 car stacking for the left turn has already proven to be inadequate and this problem will only increase with the addition of the hotels and restaurant to the Interior Access Road. This needs to be addressed now. We have taken the liberty of showing a possible solution that benefits all properties and provides for the orderly movement of cars.

We believe that our proposed solution provides for the best and safest vehicular and pedestrian circulation for all parties and best meets the standards of MLDC 10.426 regarding Street Circulation Design and Connectivity, and Interior Access Roads.

However, we do not agree that the project should be approved without moving the access point to the interior drive aisle as far east as possible (aligning with the current drive/access point to the north) AND without adding to the left turn capacity of the private access drive. We feel that constructing the project without these allowances for vehicle access and circulation does not provide adequate mitigation of the proposed projects impacts.

We have previously outlined our concerns and proposed a possible solution to the owners and to the SPAC with regard to the hotel to the east (AC 16-150). In

the Staff Report to SPAC prepared by the Medford Planning Department, staff concurred with our analysis that increasing the left turn capacity as suggested by Mr Nash was 'a necessary and appropriate condition of approval'.

We concur with Staffs recommendation in the current SPAC Staff Report that the access point to the interior access drive be located as far east as possible and aligned with the drive aisle. In addition, we suggest that the left hand turn lane capacity be increased. Our sketch of a suggested solution is the same as we presented before, and is attached.

Now that this restaurant is being developed, the situation is only exacerbated and the need for the left turn lane extension is even greater.

We look forward to your consideration of these concerns.

Respectfully Submitted,

Dan Nash
Nash Holdings LLC

BEFORE THE MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

STATE OF OREGON, CITY OF MEDFORD

IN THE MATTER OF SITE PLAN AND ARCHITECTURAL COMMISSION)
FILE AC-18-001 APPLICATION FOR PROJECT REVIEW SUBMITTED) **ORDER**
BY MIKE PAGNINI)

AN ORDER granting approval of plans for the construction of a 4-unit multiple-family complex on one parcel totaling 0.22 acres located southwest of the intersection of E. 10th Street and Portland Avenue within the MFR-20 (15 to 20 dwelling units per gross acre) zoning district.

WHEREAS:

1. The Site Plan and Architectural Commission has duly accepted the application filed in accordance with the Land Development Code, Section 10.285.
2. The Site Plan and Architectural Commission has duly held a public hearing on the matter of an application of plans for the construction of a 4-unit multiple-family complex on one parcel totaling 0.22 acres located southwest of the intersection of E. 10th Street and Portland Avenue within the MFR-20 (15 to 20 dwelling units per gross acre) zoning district, with a public hearing a matter of record of the Site Plan and Architectural Commission on March 16, 2018.
3. At the public hearings on said application, evidence and recommendations were received and presented by the Planning Department staff; and
4. At the conclusion of said public hearings, after consideration and discussion, the Site Plan and Architectural Commission, upon a motion duly seconded, granted approval and directed staff to prepare a final order with all conditions and findings set forth for the granting of approval.

THEREFORE LET IT BE HEREBY ORDERED that the application of Mike Pagnini, stands approved subject to compliance with the conditions stated in the Staff Report dated March 7, 2018.

AND LET IT FURTHER BE OF RECORD that the action of the Site Plan and Architectural Commission approving this application is hereafter supported by the following findings:

- (a) That the proposed development, with the conditions of approval, complies with the applicable provisions of all city ordinances as determined by the staff review.

FINAL ORDER AC-18-001

- (b) That compliance with the criterion determining that the proposed development is compatible with uses and development that exist on adjacent land is not applicable as the application is regulated by ORS 197.307(4).

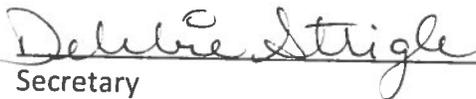
BASED UPON THE ABOVE, it is the finding of the Medford Site Plan and Architectural Commission that the project is in compliance with the criteria of Section 10.290 of the Land Development Code.

Accepted and approved this 6th day of April, 2018.

MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

Site Plan and Architectural Commission Chair

ATTEST:


Secretary



Site Plan and Architectural Commission

Minutes

From Public Hearing on March 16, 2018

The regular meeting of the Site Plan and Architectural Commission was called to order at noon in the Council Chambers on the above date with the following members and staff in attendance:

Commissioners Present

Bill Chmelir, Acting Chair
Jim Catt
Dave Culbertson
Bob Neathamer
Marcy Pierce
Rick Whitlock
Dick Gordon, City Council Liaison

Staff Present

Kelly Akin, Assistant Planning Director
Madison Simmons, Senior Assistant City Attorney
Doug Burroughs, Public Works/Eng Development Services Mgr.
Dustin Severs, Planner III
Steffen Roennfeldt, Planner III
Debbie Strigle, Recording Secretary

Commissioners Absent

Jim Quinn, Chair, excused
Jeff Bender, excused

10. Roll Call.

20. Consent Calendar/Written Communications. None.

30. Minutes.

30.1 The minutes for the February 16, 2018, meeting, were approved as submitted.

40. Oral and Written Requests and Communications. None.

50. Public Hearings.

Madison Simmons, Senior Assistant City Attorney, read the rules governing the public hearings.

New Business.

50.1 **AC-17-167** Consideration of a proposal for the development of a single 1.7-acre lot consisting of the construction of a 4,485 square foot restaurant to be located at 1383 Center Drive in the Regional Commercial (C-R) zoning district (371W32B4703); MESK Investment, Applicant; Tormod Hellwig Architecture, Agent; Dustin Severs, Planner).

Acting Chair Chmelir asked for any potential conflicts of interest, ex-parte communications, or site visits. There were none.

Dustin Severs, Planner III, gave a PowerPoint presentation of the March 9, 2018, Staff Report. Staff recommended approval.

Commissioner Whitlock wanted to know at what point the City would require a traffic analysis. Mr. Severs answered a traffic analysis is always difficult when dealing with private roads. He deferred the question to the Public Works Department.

Doug Burroughs, Public Works Department, explained that traffic impact analyses are looked at the time of a zone change. That analysis would look at the impact to the public system not necessarily the internal ramifications of other developments connecting on the private side. Mr. Burroughs added that Public Works staff doesn't typically comment on private circulation but in this case the Traffic Manager did comment because the northern driveway was seen as a potential problem in the future and that is why there was a recommendation. In regards to the driveway to the south, Mr. Burroughs stated it needed to line up to the other one.

Commissioner Whitlock asked if staff had an opinion on the extended left-hand turn lane from the private driveway onto Center Drive. Mr. Burroughs answered there had not been much discussion about that but looking at it it's probably a good idea to ask at what point would it be warranted to do a traffic study.

Commissioner Whitlock wanted to know if the property to the far east of the current developments would require a zone change. Mr. Burroughs answered that Mr. Severs indicated the property would require a zone change.

The public hearing was opened and the following testimony was given:

a) Mark McKechnie, agent for the applicant, had some concerns regarding the Conditions of Approval. He suggested that #6 is a building code requirement and this Commission didn't need to deal with it and they could just skip that one; condition #2 required a comprehensive stormwater management plan which he didn't understand since this is a phased development. He wanted that condition to clarify it to mean the IHOP portion of the site only. Mr. McKechnie displayed and spoke to a proposed plan for the different access points.

Mr. McKechnie said he believed that when the South Gateway Center was planned and developed a traffic study had been done as if the whole entire center was built out. He felt a traffic study at this point would be inappropriate for development that's already been approved.

Mr. McKechnie talked more about access, cross-access, parking spaces, and the overall site plan.

Acting Chair Chmelir asked if the parking spaces the other property owner will be building at the same time is part of the 47 parking spaces for this site. Mr. McKechnie said he did not believe so.

Commissioner Culbertson wanted to know if any conversations had taken place between all the neighboring property owners regarding vehicular circulation and traffic impacts. Mr. McKechnie stated he knew that the owners of the hotel and this parcel have had conversations in the past regarding cross-access. Other than that, Mr. McKechnie did not have any other additional information.

Commissioner Whitlock inquired if there was a cross-access agreement in place between the hotel property to the south and this subject property. Mr. McKechnie replied he was not aware of any formal agreement other than what would be required by the City.

Mr. McKechnie reserved time for rebuttal.

b) Jerry Shean, applicant's representative, asked that the Commission remove Discretionary Condition #10, and leave the realignment up to the developer.

c) Brian Westerhout, speaking on behalf of Dan Nash, suggested that the left-hand turn lane be extended at this time. He added they feel the driveway should align with the driveway between the strip mall building and ice skating rink.

d) Frank Pulver, partner in the project and agent for the overall property, stated he didn't think there had ever been any discussions about modifying the improvements to the access way to the Nash

property. He said his group would be open to discussions with the other parties on what fix should be done and how it would be financially participated in if it should be required. Mr. Pulver added that it's all speculation and would be premature to put any requirements in place at this point.

Commissioner Whitlock inquired as to the reasoning behind staff wanting to move the driveway to the easterly edge of the property. He also wanted to know if staff had any concerns regarding Dolan issues with respect to future development of the property.

Mr. Burroughs confirmed that Public Works doesn't normally comment on internal private circulation plans but they had concerns about this particular project. He said their biggest concern with public streets and off-set driveways are opposing left turns and multi-directional traffic patterns. Mr. Burroughs stated he believes that aligning the driveways is important to do now because once buildings are built there it just won't happen.

Commissioner Culbertson said knowing that Nash's property still has yet to have a zone change; if they come forward and request one and it triggers a traffic analysis that dictates that the signalized intersection there is failing or would fail immediately upon development of that property, what would the City's recourse be to tell the property owners that somebody needs to improve that intersection.

Mr. Burroughs answered that whoever had to pay for that would probably be included in the cross-access agreement but if the signal fails then the traffic impact analysis should recommend mitigation to those failures. Mitigation could take the form of many things.

Commissioner Whitlock wanted to know if the signalized access onto Center Drive is considered a signalized driveway from Public Works' perspective. Mr. Burroughs answered it is and Public Works maintains the signal.

Commissioner Whitlock inquired if the City would intercede if the intersection were to fail when it's considered a private driveway. Mr. Burroughs replied his understanding is that if the failure was that more cars were backed up onto the private driveway, Public Works might not consider that a failure; if cars backed up on Center Drive they would consider that a failure and would then probably intercede.

Commissioner asked that staff address his question regarding concerns with the Dolan issue.

Kelly Akin, Assistant Planning Director, spoke briefly to the Dolan issue. She said that in this case it's not a public improvement it's a private driveway. Ms. Akin also spoke to the Commission's authority. She stated the Commission has authority in MLDC Section 10.291 to apply conditions of approval that ensure compliance with the standards of the code and otherwise protect the health, safety, and general welfare of the surrounding area and the community as a whole. Ms. Akin also quoted MLDC Section 10.291(7). She stated this Commission has quite a bit of latitude as far as the alignment of the driveways, and taking into consideration the recommendations of the City's Traffic Engineer.

Mr. McKechnie said it seemed to him that it was perhaps with good intention on the part of the City Traffic Engineer to offer some advice to a private developer that maybe went a bit too far and ended up unintentionally trying to redesign the site. He said putting in a driveway that seems to suit one purpose may unintentionally hinder another purpose. Mr. McKechnie said his feeling is as long as the parking spaces and drive aisles are adequate, people will find their way around without too much trouble and thinks the same is true with the rest of the center. He felt the access that was proposed on this project is probably the most practical and reasonable one.

The public hearing was closed.

Motion: Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-17-167 per the staff report dated March 9, 2018, including all exhibits, and adding the following conditions:

- Discretionary Condition #10 recommending that the applicant submit a revised site plan directly aligning the access points
- Modification to condition #7 requiring the applicant to submit a revised site plan showing a pedestrian connectivity to the adjacent property to the east
- Strike condition #5 in its entirety (repetitive)
- Finding in support of moving the access to the easterly edge of the property as proposed by staff. The finding would be that this is a unique situation as evidenced by the information provided by Mr. Burroughs relative to this application. There are conflicting driveway movements that were talked about. Moving the driveway to that location would protect the health, safety, and general welfare of the public justifying moving a point of ingress and egress to the property further to the east as proposed. The Commission can find that the circulation as proposed by the applicant, where traffic movements from the property to the south would flow through the middle of this development, would definitely create conflicts between vehicle and pedestrian traffic and moving this to the easterly boundary as proposed by staff would significantly lessen those impacts.

Moved by: Commissioner Whitlock

Seconded by: Commissioner Neathamer

Commissioner Culbertson disagreed with the inclusion of Discretionary Condition #10 because it is a private access and anything that happens within those boundaries is the responsibility of the applicant and developer. If that traffic is going to fail and the parties to the private access can't get together and resolve it, it would be their problem, not the City's responsibility to dictate how flow is going to happen between different stages or sections of development on private land, particularly in this type of development. Commissioner Culbertson stated he would oppose the motion with the inclusion of Discretionary Condition #10.

Acting Chair Chmelir concurred with Commissioner Culbertson by possibly making traffic flow more difficult on their site but that's their discretion. He added that with the intersection the negative impact would be into the property and not onto the public road.

Commissioner Whitlock said he agreed with Acting Chair Chmelir's last statement and felt staff had articulated well that their concern was not with congestion on the private driveway and how that might impact traffic flow to those properties. He added he was thinking that the property owners would be financially incentivized to make changes if the public stopped going to those businesses because they feel it's a little constrained. Commissioner Whitlock stated he felt strongly about staff's recommendation for the easterly access because he thought it would be better for traffic flow and does minimize the pedestrian/vehicular conflicts as opposed to re-routing the traffic through the center of the development. The three opposing driveways as identified by staff do raise a unique problem.

Commissioner Whitlock asked Acting Chair Chmelir and Commissioner Culbertson if they were comfortable letting the developers of this project decide where the access should be. Acting Chair Chmelir indicated he was and Commissioner Culbertson answered he felt that it would ultimately be up to the builder/developers.

Commissioner Pierce said she appreciated what Acting Chair Chmelir and Commissioner Culbertson were saying but at the same time this is an opportunity for the Commission to put something in place

that eliminates the builder/developer having to go down that road in the future where there may be accidents or disagreements happening. If the Commission decides the driveways need to be aligned today it may prevent problems down the road.

Commissioner Whitlock suggested that there appears to be some identifying conflicts, and given that this Commission has the responsibility to deal with circulation issues, if the Commission doesn't deal with it in this case, then when? He reiterated he felt staff made a good factual showing as to why it was important from their perspective to modify the circulation pattern and more directly the access to avoid potential conflicts.

Roll Call Vote: Motion passed, 4-2-0, with Acting Chair Chmelir and Commissioner Culbertson voting "no".

50.2 **AC-18-001** Consideration of plans for the construction of a 4-unit multiple-family complex on one parcel totaling 0.22 acres located southwest of the intersection of E. 10th Street and Portland Avenue within the MFR-20 (15 to 20 dwelling units per gross acre) zoning district. (371W30DB1200) (Mike Pagnini, Applicant; Richard Stevens & Associates, Agent; Steffen Roennfeldt, Planner).

Acting Chair Chmelir asked for any potential conflicts of interest, ex-parte communications, or site visits.

Commissioner Neathamer declared he has had a past professional relationship with Mr. Pagnini on other projects but is not involved in this one. He felt he could make a reasonable decision on this application.

Commissioner Whitlock disclosed he had made a site visit.

Steffen Roennfeldt, Planner III, gave a PowerPoint presentation of the March 7, 2018, Staff Report. Staff recommended approval.

Commissioner Whitlock referenced page 2 of the Public Works Staff Report regarding the improvements to E. 10th Street and an option. He asked Mr. Burroughs to explain what the option is about.

Mr. Burroughs explained the option is that the developer can do the improvements to the standard or if they want to verify what's there then they can perhaps not have to do as much of the street improvements.

The public hearing was opened and the following testimony was given:

a) Joe Slaughter, agent for the applicant, gave a brief PowerPoint presentation and summary of the project. He asked that the Commission approve the project as proposed and the conditions as recommended by staff.

Mr. Slaughter reserved time for rebuttal.

Commissioner Catt wanted to know if the bicycle parking could be located inside the garage. Mr. Roennfeldt answered yes.

The public hearing was closed.

Motion: Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-18-001 per the staff report dated March 7, 2018, including Exhibits A through P.

Moved by: Commissioner Pierce

Seconded by: Commissioner Neathamer

Roll Call Vote: Motion passed, 6-0

60. Written Communications. None.

70. Unfinished Business. None.

80. New Business. None.

90. Report from the Planning Department.

90.1 Ms. Akin announced there is business scheduled for the next three meetings on April 6th, April 20th, and May 4th.

90.2 Ms. Akin said that City Council had approved the proposed round-about at Springbrook and Cedar Links and a partial street vacation on Evergreen, between 3rd and 4th Streets. Upcoming for City Council is a General Land Use Plan (GLUP) map amendment on Airport Road, between Table Rock and Biddle Roads. It will go from an Industrial to a Commercial designation.

90.3 Urban Growth Boundary (UGB) Update:

- 1) Ms. Akin stated staff had sent the application to the Department of Land Conservation & Development.
- 2) Staff is continuing to work on the Transportation System Plan (TSP). City Council has had study sessions and have another one scheduled for March 22nd; the Planning Commission will consider prioritizing the projects at their meeting on March 26th; and there's a joint study session between the Planning Commission and City Council scheduled for March 29th.
- 3) Staff is working on housing related code amendments addressing regulatory barriers, such as density and parking, and design standards for residential developments.

Councilmember Gordon pointed out that the street vacation concerns property between the railroad tracks and the Pallet Wine Company. He stated that application has to go to a second reading. There's still a question mark as to ownership of the land. He added his personal opinion was that the vote could change depending on the ownership of that land.

In regards to the round-about at Springbrook and Cedar Links Drive, Councilmember Gordon stated that something needed to be done there because it's a very dangerous intersection. He said there were a lot of concerns but it had been approved with only one Councilmember voting "no".

Councilmember Gordon spoke briefly to the bicycle issue saying he has a problem at times with bicycle projects because they overlook sidewalk projects and the city needs sidewalks as a priority. He added that in most areas, bicyclists can use sidewalks.

100. Messages and Papers from the Chair. None.

110. Propositions and Remarks from the Commission. None.

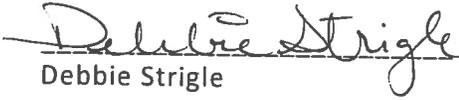
120. City Council Comments.

120.1 Councilmember Gordon said regarding the UGB, there had been three objections filed.

130. Adjournment

130.1 The meeting was adjourned at approximately 1:45 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:



Debbie Strigle
Recording Secretary

Jim Quinn

Site Plan and Architectural Commission Chair

Approved: April 6, 2018



STAFF REPORT

for a Type-C quasi-judicial decision: **Site Plan and Architectural Review**

Project Navigator's Landing Office Building
Applicant: Mayers 5 LLC; Agent: Kistler Small White Architects

File no. AC-18-011

To Site Plan and Architectural Review Commission *for 04/06/2018 hearing*

From Steffen Roennfeldt, Planner III

Reviewer Kelly Akin, Assistant Planning Director *ka.*

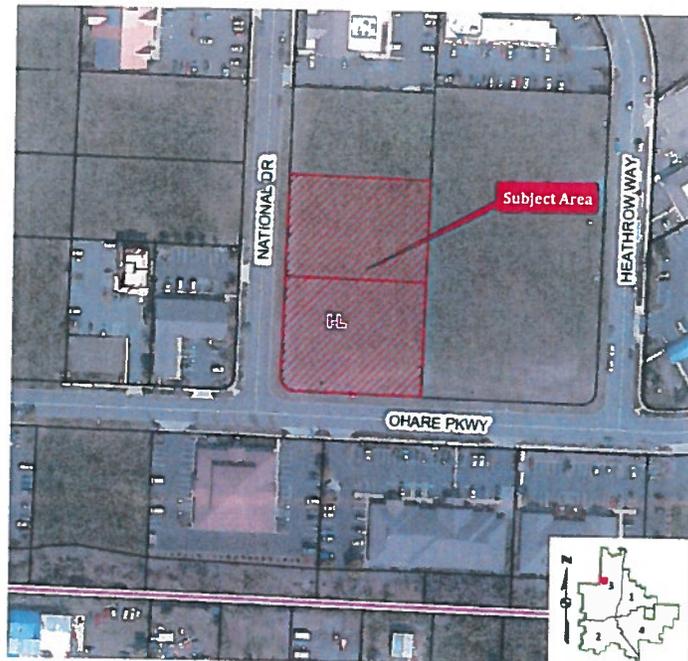
Date March 28, 2018

BACKGROUND

Proposal

Developer proposes to build an approximately 21,000 square foot, two-story general office building on two parcels totaling 1.39 acres located northeast of the intersection of O'Hare Parkway and National Drive in the Navigator's Landing Planned Unit Development zoned I-L/AC/PD (Light Industrial/Airport Area of Concern/Planned Unit Development).

Vicinity Map



Subject Site Characteristics

Zoning	I-L	Light Industrial
Overlays	AC/PD	Airport Area of Concern/Planned Unit Development
GLUP	G-I	General Industrial
Use	Vacant	

Surrounding Site Characteristics

North	Zone:	I-L/PD
	Use:	Vacant
South	Zone:	I-L/PD
	Use:	Ticor Title Company
East	Zone:	I-L/PD
	Use:	Vacant
West	Zone:	I-L/PD
	Use:	OBEC Consulting Engineers

Related Projects

PUD-01-155	Navigator's Landing Planned Unit Development
PA-18-004	Pre-Application for construction of office building

Applicable Criteria

Medford Municipal Code §10.290 – Site Plan & Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

Corporate Names

Douglas Schmor is the Registered Agent for Mayers 5 LLC according to the Oregon Secretary of State Business Registry. Robb Mayers is listed as a Member.

Jerome White is the Registered Agent for Kistler Small & White Architects LLC according to the Oregon Secretary of State Business Registry. Raymond Kistler, Matthey Small and Jerome White are listed as Managers.

ISSUES AND ANALYSIS

Background

The proposed development is within the Navigator's Landing PUD (PUD-01-155), which was approved by Planning Commission on March 14, 2002. The Preliminary PUD Plan approval established that Site Plan and Architectural Commission review of building, site, and landscape plans would occur for each lot within the project.

Uses

The approval of the mixed-use PUD also established that a maximum of 9.98 acres (20 percent) of the site could contain uses not allowed in the underlying I-L zone and the mix of uses would generate a maximum of 12,730 Average Daily Trips. The vehicle trip generation limitation was removed at the time Phase 1 was approved. However, accounting for the acreage will be ongoing as each site develops. The proposed use is for Business Offices which is a permitted use in this zoning district and therefore does not count towards the 20 percent maximum. The current acreage allocation for Navigator's Landing PUD is summarized below:

Phase (Lot #)	Non-Permitted Use (Acres) ¹	Permitted Use	SPAC File #
1 (Lot 34)	0	Bank, etc.	AC-02-09
2 (Lot 26)	0	Telemarketing	AC-02-80
3 (Lot 33)	0	Office, etc.	AC-02-74
4 (Lots 13 & 14)	0	Warehouse/office	AC-03-65
5 (Lot 7)	0	Advertising office	AC-04-09
6 (Lot 19)	0	Insurance office	AC-04-28
7 (Lot 10)	0	Computer services	AC-04-11
8 (Lot 35)	0	Medical laboratory	AC-04-213
9 (Lots 38, 39 & 40)	1.67*	Hotel	AC-05-161
10 (Lots 11, 12 & 13)	0	Stone Products	AC-05-149
11 (Lot 9)	0	Employment Agency	AC-05-254
12 (Lot 28)	0.57*	Dentist/Therapist	AC-05-317
13 (Lots 5 & 6)	0	Title Insurance	AC-05-262
14 (Lot 25)	0	Commercial office	AC-06-198
15 (Lot 22)	0	Engineering office	AC-07-287
16 (Lot 15)	0	Administrative office	AC-08-075
17 (Lot 37)	1.24*	Medical Office/Healthcare	AC-08-122
18 (Lot 4)	0	Trust Company / Shell	AC-08-138
19 (Lot 21)	0	Business Office	AC-09-014
20 (Lot 36)	0	Business Office	AC-15-070
21 (Lot 29)	0	Office (Life Coach)	AC-16-119
22 (Lot 30 & 31)	0	Office/Parking	AC-18-011
Acres non-permitted:	3.48	Percent of Total	6.99%

Site Plan (Exhibit B)

Site Development Table

	Allowed/Required	Proposed
Height	Max.: 85 feet	29 feet
Lot Coverage	Max.: 50%	32.5%
Setback (front)	Min.: 10 feet	17.9 feet
Setback (street side)	Min.: 10	20 feet
Setback (side)	Min.: 4.5 feet	12 feet
Setback (rear)	Min.: 4.5 feet	12 feet

Proposed is a two-story office building with a footprint of 10,328 square feet. The proposed building will be facing O'Hare Parkway and be located behind the 15 foot Public Utility Easement. The parking lot will be located behind the building to the north. Vehicle access will be available from National Drive via two driveway approaches.

The site plan features two entry plazas, one on both the north and south sides of the development. Pedestrian access will be provided from a new five-foot sidewalks along National Drive and O'Hare Parkway. There will also be pedestrian walkways connecting the sidewalks and parking lot to the proposed structure.

Architecture

Per the applicant, the proposed building is industrial/modern with brick veneer, storefront windows, steel and cement fiber (wood look) accents. Window panels are punched into the brick wall with cement fiber siding accents adjacent to the windows. There is a steel channel cornice at the top of the parapets, and steel trellis entrance structures. Many of the exterior materials proposed are present in other buildings with the development creating compatibility. The building façade is broken up by large recessed entries into two main building masses, these recessed entries include second floor balconies and steel trellis, and storefront windows with fiber cement siding accents that open up the mass of the brick wall continue to break up the façade into smaller wall segments. Being able to see inside the building at ground level is important in creating an inviting atmosphere and proposes to those passing by that the occupants are open to interaction with them, which helps to reinforce the pedestrian accessibility.

Landscaping

Landscape Table – Parking Area Planters (10.746[3])

	Required	Proposed
Trees	9	13
Shrubs	17	172
Coverage Area	1,333 square feet	More than 1,333 square feet

The project will use a variety of deciduous trees throughout the interior of the site including the parking lot. The landscape strip along National Drive will be planted with nine street trees consisting of *Shantung Maple* and *Japanese Snowbell*. There will also be four *Golden Rain* street trees planted along O'Hare Parkway. Proposed street trees within the landscape strip along O'Hare Parkway and National Drive are consistent with the Navigator's Landing PUD street tree plan. All proposed street trees will be located in the planter strips along both streets which is consistent with the master street tree plan and matches existing development. The provided Landscape Plan meets the requirements of the Land Development Code.

Vehicular and Bicycle Parking

Parking Table (10.743-751)

	Required	Proposed
Total Spaces	Min.: 64 – Max.: 104	98 spaces
Accessible Spaces	4	4
Van/Carpool	10	10
Bicycle Parking	10	10

Vehicular access to the parking lot will be provided via two driveways from National Drive serving all 98 parking stalls. Bicycle parking will be available adjacent to the north entrance of the proposed structure. As required per the Code, 10 bicycle parking spaces (50% of them covered) will be provided.

Pedestrian Walkways

Proposed is a seven foot pedestrian pathway that extends from the sidewalk along National Drive to the north and east entrance of the structure. Per the applicant, *there*

will also be walkways connecting parking lot areas allowing people to walk towards the building without requiring them to walk around the parking lot.

MLDC 10.773 requires pedestrian walkways to connect to adjacent properties. Whereas the sidewalk along National Drive provides a reasonable pedestrian route to the adjacent property to the north, a connection to the adjacent property to the east is not shown. . As a condition of approval, the applicant will be required to submit a revised site plan with their building permit submittals illustrating how pedestrian connectivity will be provided between the subject property and the property to the east.

Floodplain

Parts of the property are currently within a mapped A zone without Base Flood Elevations or a designated floodway. Per the National Flood Insurance Program Regulations, any vertical structures and utilities shall comply with 44 CFR 60.3(a),(b) and (c) and MMC Section 9.7006 (D). The Medford Floodplain regulations are found in Sections 9.701-9.707 of the Municipal Code. It will be a condition of approval for the applicant to comply with all applicable floodplain requirements.

Concealments

The mechanical equipment associated with the building is to be roof mounted and located within a screened rooftop mechanical well, thus entirely concealed from public view.

The submitted site plan identifies a brick trash enclosure located on the east side of the parking lot. No elevation or detail drawings showing the trash receptacle were submitted. It will be a condition for the applicant to comply with the requirements of MLDC 10.781.

Facility Adequacy

Per the agency comments submitted to staff (Exhibits N to W), it can be found that there are adequate facilities to serve the proposed development.

Other Agency Comments

Rogue Valley Sewer Services (RVSS) (Exhibits S)

The subject property is within RVSS service area, which requires that future sewer improvements be designed and constructed in accordance with RVSS standards. As a condition of approval, the applicant will be required to comply with all conditions of RVSS.

Rogue Valley International Airport (Exhibit T)

States that an Avigation, Noise and Hazard Easement is already on file.

Seattle Airports District Office (Exhibit U) and Oregon Department of Aviation (Exhibit V): Requests that a FAA form 7460-1 be filed with the Department in order to determine if there is a hazard to air navigation.

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS OF FACT

MLDC 10.290

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the Applicant's Questionnaire and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit L) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-18-011 per the staff report dated March 28, 2018, including Exhibits A through Y.

EXHIBITS

- A Conditions of Approval, dated March 28, 2018
- B Site Plan AR0.1, received January 29, 2018
- C Floor Plans AR3.1 & AR3.2, received January 29, 2018
- D Roof Plan AR3.3, received January 29, 2018
- E Exterior Elevations AR6.1 & 6.2, received January 29, 2018
- F Exterior Perspective AR6.5 & 6.6, received January 29, 2018
- G Exterior Materials AR6.7, received January 29, 2018

- H Exterior Precedent AR6.8, received January 29, 2018
- I Interior Precedents AR10.0, received January 29, 2018
- J Conceptual Stormwater, Drainage and Detention Plan, received January 29, 2018
- K Landscape Plan, received January 29, 2018
- L Applicants findings and conclusions, received January 29, 2018
- M Applicants Code Compliance Questionnaire, received January 29, 2018
- N Public Works Staff Report, received March 7, 2018
- O Medford Water Commission Memo, received March 7, 2018
- P Medford Fire Department Memo, received March 7, 2018
- Q Floodplain Coordinator Memo, received March 6, 2018
- R Building Department Memo, received March 7, 2018
- S Rogue Valley Sewer Services Memo, received February 22, 2018
- T E-Mail from Rogue Valley International Airport, received February 23, 2018
- U E-Mail from Seattle Airports District Office, received February 26, 2018
- V E-Mail from Oregon Department of Aviation, received February 28, 2018
- W E-Mail from City Surveyor, received February 26, 2018
- X Additional written consent of owner, received February 27, 2018
- Y Excerpt from Navigators Landing PUD re: landscaping, dated January 2002
Vicinity map

SITE PLAN AND ARCHITECTURAL REVIEW COMMISSION AGENDA:

APRIL 6, 2018

EXHIBIT A

Navigator's Landing Office Building
AC-18-011
Conditions of Approval
March 28, 2018

CODE REQUIRED CONDITIONS

Prior to the issuance of the first building permit for vertical construction, the applicant shall:

1. Provide detailed plans for proposed trash receptacle in compliance with MLDC 10.781.
2. Provide revised site plan showing pedestrian connectivity between the subject parcel and the property to the east in compliance with MLDC 10.773.
3. Comply with all conditions stipulated by Medford Public Works Department (Exhibit N).
4. Comply with all conditions stipulated by the Medford Water Commission (Exhibit O).
5. Comply with all conditions stipulated by the Floodplain Coordinator (Exhibit Q).
6. Comply with all conditions stipulated by Rogue Valley Sewer Services (Exhibit S)
7. Comply with all conditions stipulated by Seattle Airports District Office (Exhibit U) and Oregon Department of Aviation (Exhibit V).
8. Submit all necessary documents for final PUD approval to the Planning Department per the Final Order for Navigator's Landing PUD on March 1, 2002.

PROJECT INFORMATION
 PROPOSED OFFICE BUILDING
 NAVIGATORS LANDING
 3400 O'HARE PARKWAY
 MEDFORD, OREGON

AGENT INFORMATION
 MEDFORD WATER COMMISSION
 200 SOUTH WY STABLET, ROOM 177
 MEDFORD, OREGON 97501

ZONING INFORMATION
 ASSESSOR'S MAP: 27 2W 12D, TL: 11000, 11100
 ZONING MAP DESCRIPTION: LUPO - LIGHT INDUSTRIAL/PLANNED DEVELOPMENT
 GENERAL LAND USE PLAN MAP DESIGNATION: G1

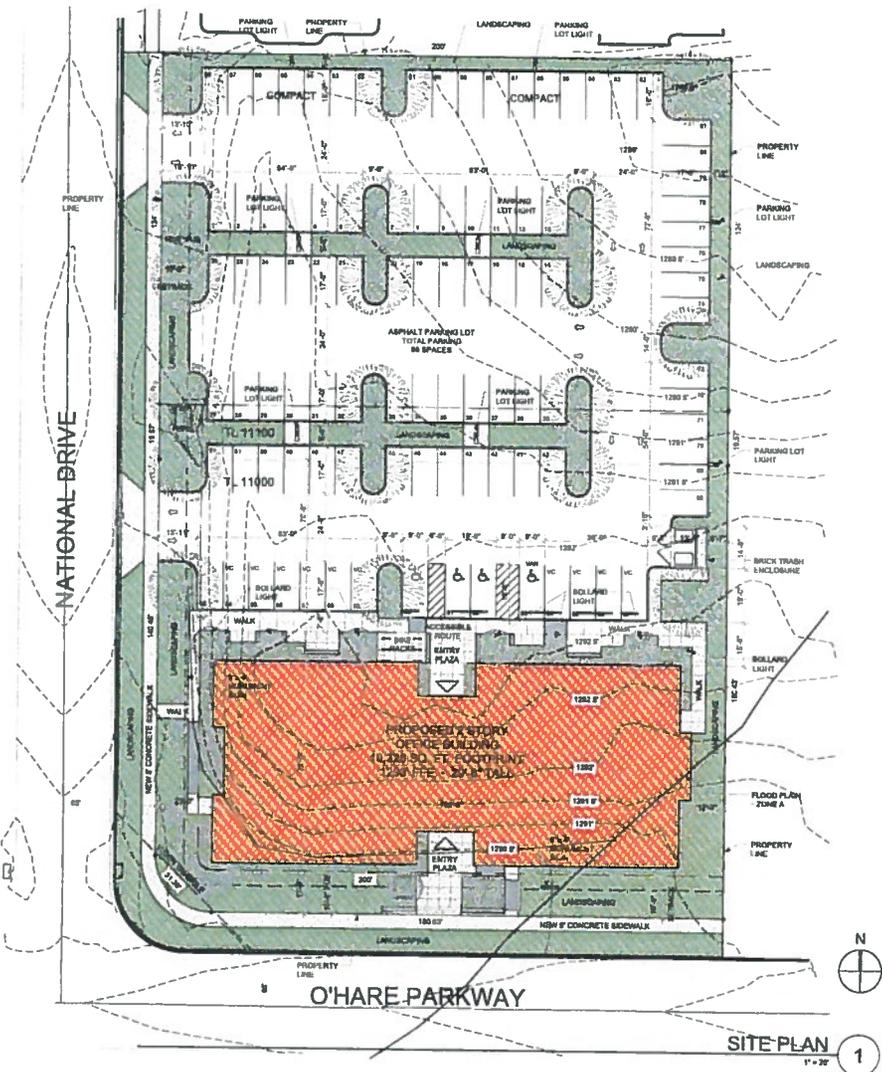
BUILDING SUMMARY

OFFICE BUILDING	PROPOSED	OCC. CLASS
1st FLOOR	18,328 sf	B
2nd FLOOR	13,833 sf	B
TOTAL	32,161 sf	

LOT COVERAGE:

LOT AREA	80,714 sf
LANDSCAPE AREA	18,157 sf
BUILDING AREA	13,228 sf
PARKING LOT AREA	28,837 sf
% COVERED BY STRUCTURES	17.2%
% IMPERVIOUS AREA (PARKING, WALKS)	43.1%
% PERVIOUS AREA	29.8%

PARKING REQUIREMENTS
 MINIMUM OF 88 PARKING SPACES REQUIRED 39,880 / 1,000 sq ft = 82
 MAXIMUM OF 100 PARKING SPACES ALLOWED 29,866 / 1,000 sq ft = 100
 STANDARD PARKING SPACE DIMENSION IS 8' X 12'
 PARKING SPACES PROVIDED ARE 8' x 12' WITH 24' ABLE
 EACH PITCHER ALLOWS 2' OVERHANG AT 7' WALK ON LANDSCAPING
 25% CAN BE COMPACT SPACES: 88 x 25% = 22
 COMPACT SPACE SIZE: 8' x 11'
 COMPACT SPACES PROVIDED: 17
 RECD: VAN/POOL/CARPOOL SPACES: 88 x 15% = 13
 VAN/POOL/CARPOOL SPACE SIZE: 8' x 11'
 VAN/POOL/CARPOOL SPACES PROVIDED: 10 (DESIGNATED WITH VC)
 RECD: ACCESSIBLE SPACES: 4
 ACCESSIBLE SPACES ARE 8' x 18' WITH 4' ABLE
 RECD: VAN ACCESSIBLE SPACES: 1
 VAN ACCESSIBLE SPACES ARE 8' x 18' WITH 4' ABLE
 PLANTS PER 24 SPACES: 3
 TREES PER 24 SPACES: 4
 LANDSCAPING PER 24 SPACES: 329 sq ft
 RECD: BICYCLE PARKING SPACES: OFFICE - 15%, 88 x 15% = 13
 BICYCLE PARKING SPACES PROVIDED: 13
 BICYCLE SPACE DIMENSIONS: 7' 6" WITH 4' ABLE
 15 OR MORE SPACES REQUIRE SIGN TO BE COVERED 10 x 20% = 6
 COVERED SPACES PROVIDED: 6
 RECD: LOADING SPACES: OFFICE: 8 - 24,388 SQ. FT. = 8
 23,000 - 43,000 SQ. FT. = 1 TYPE B
 43,001 - 100,000 SQ. FT. = 2 TYPE B
 LOADING SPACES PROVIDED: 8
 PARKING LOT LIGHTING: LITHONIA D-SERIES LED W/ EXTREME CUTOFF OPTICS ON 18' TALL POLE



File # A-18-011
 EXHIBIT # B
 CITY OF MEDFORD
 NAVIGATORS LANDING
 SCHEMATIC DESIGN
 01/24/2018

RECEIVED
 JAN 29 2018
 PLANNING DEPARTMENT
 SITE PLAN AR0.1

RECEIVED

JAN 29 2018

PLANNING DEPT.



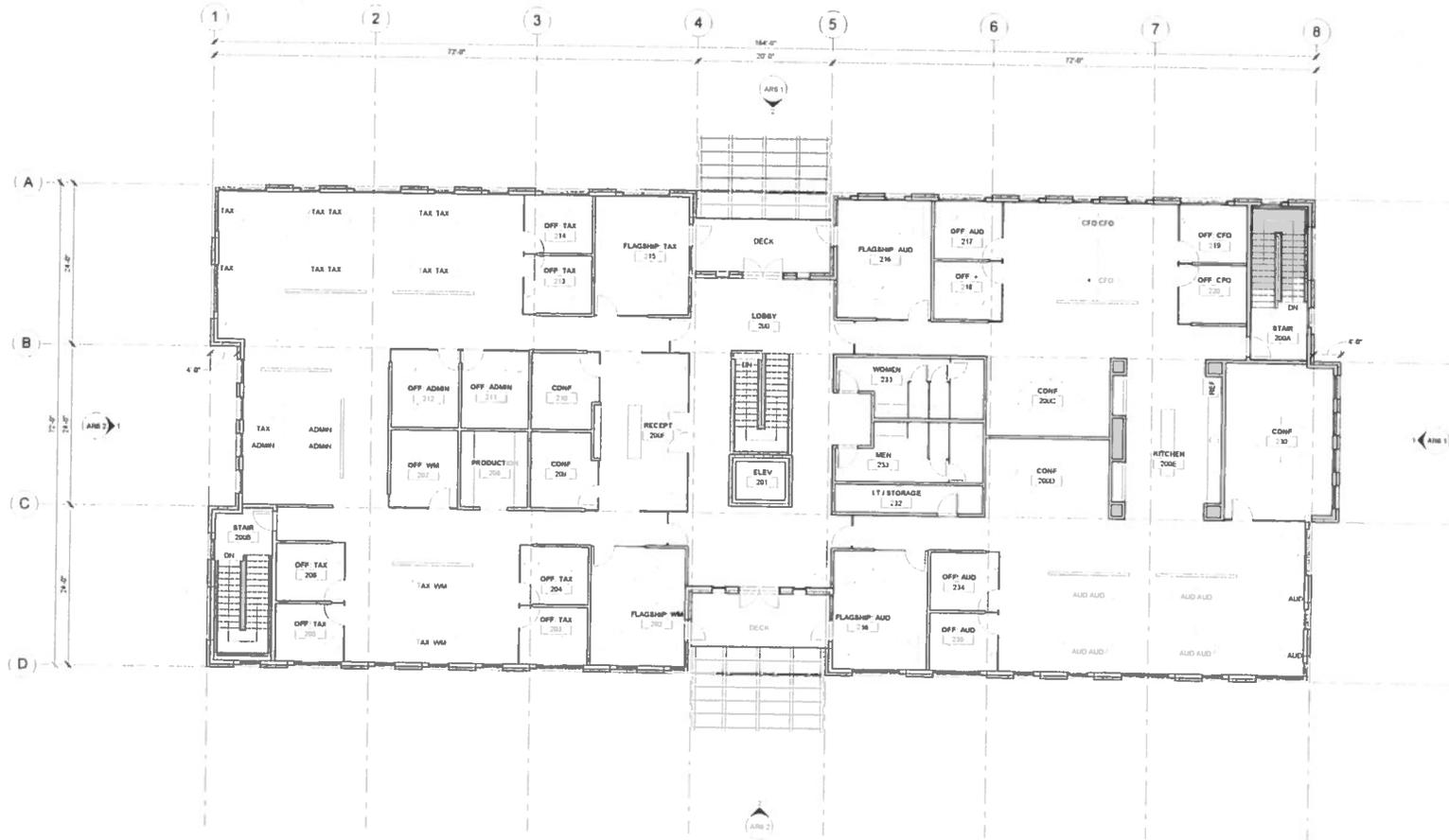
FLOOR PLAN - LEVEL 1 1/8" = 1'-0"

FLOOR PLAN - LEVEL 1 AR3.1

NAVIGATORS LANDING
SCHEMATIC DESIGN
01 24 2018

CITY OF MA
WIND

File # AC-18-011



FLOOR PLAN - LEVEL 2
 1/8" = 1'-0" 1 ⊕

NAVIGATORS LANDING
 SCHEMATIC DESIGN
 01/24/2018

RECEIVED
 JAN 29 2018
 PLANNING DEPT.

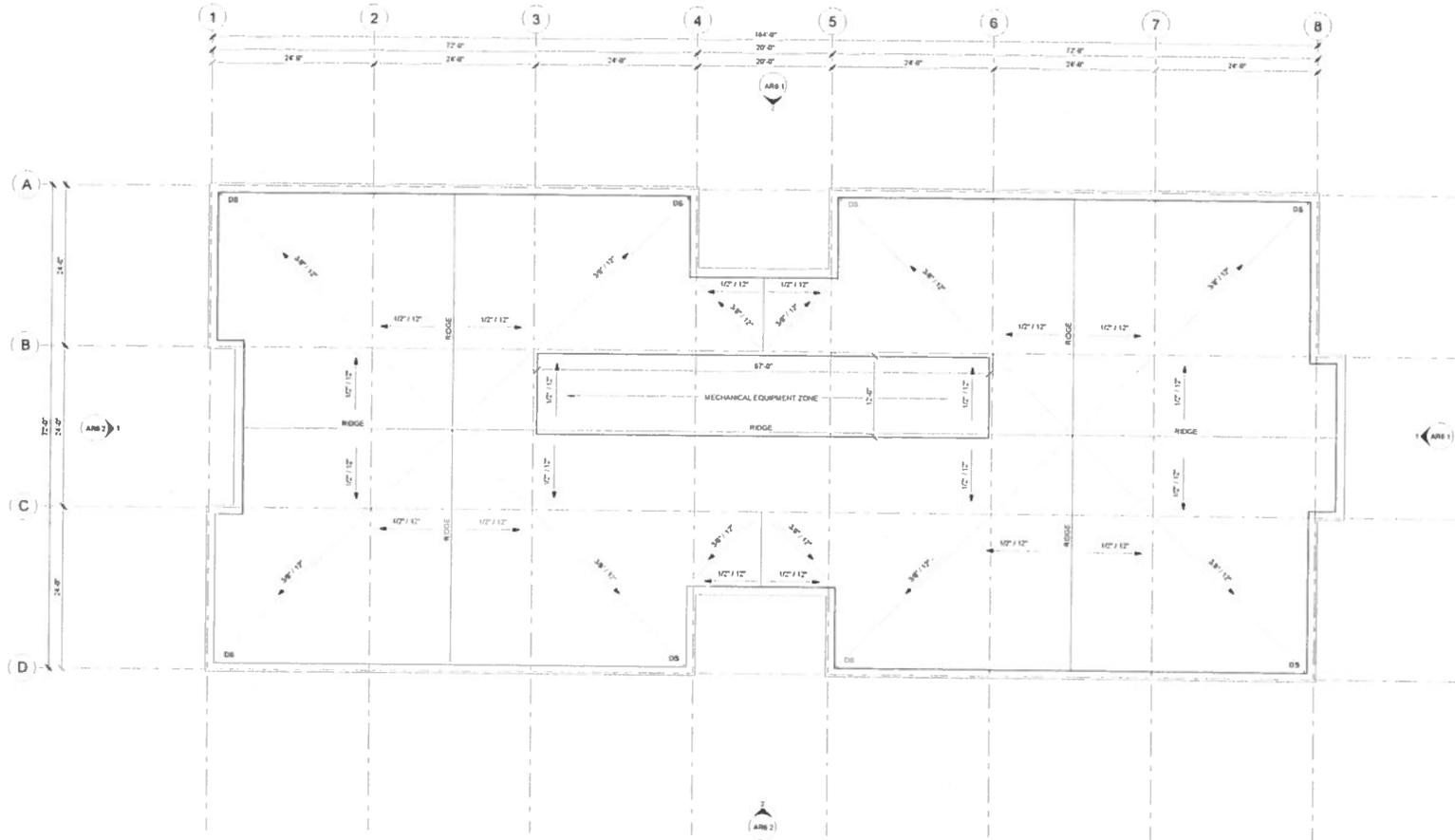
FLOOR PLAN - LEVEL 2 AR3.2

RECEIVED

JAN 29 2018

PLANNING DEPT.

ROOF PLAN AR3.3



ROOF PLAN 1 1/8" = 1'-0"

NAVIGATORS LANDING
SCHEMATIC DESIGN
04 24 2018

CITY OF MEDFORD

EXHIBIT # D

File # AC-18-011



ELEVATION - EAST 1



ELEVATION - NORTH 2

EXTERIOR ELEVATIONS

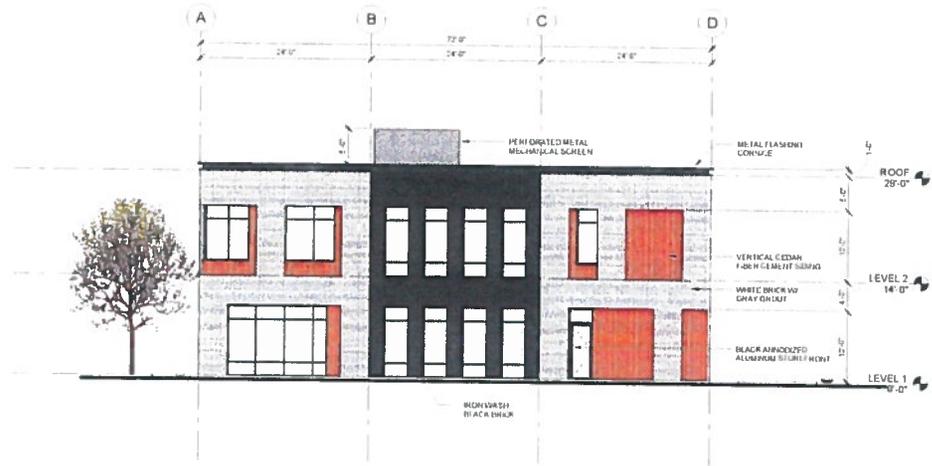
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JAN 29 2018
AR6.1

PLANNING DEPT

NAVIGATORS LANDING
SCHEMATIC DESIGN
01.24.2018



CITY OF MEDFORD
EXHIBIT # E
File # AC-18-011



ELEVATION - WEST 1
1/8" = 1'-0"



ELEVATION - SOUTH 1
1/8" = 1'-0"

NAVIGATORS LANDING
 SCHEMATIC DESIGN
 01.24.2018

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 EXTERIOR ELEVATIONS AND



File # AC-18-011

EXHIBIT # F

CITY OF MEDFORD



NAVIGATORS LANDING

SCHEMATIC DESIGN
01.24.2018

PLANNING DEPT

JAN 29 2018

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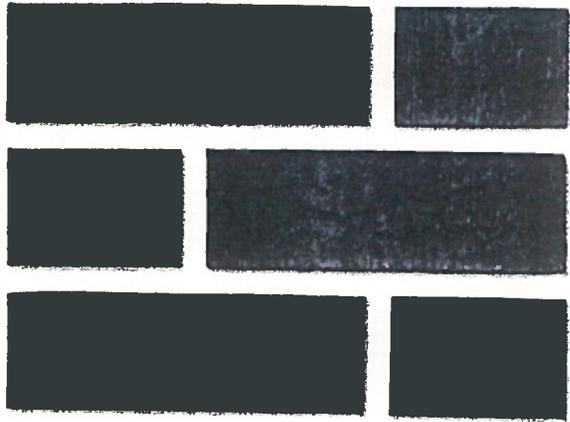
EXTERIOR PERSPECTIVE 1

AR6.5

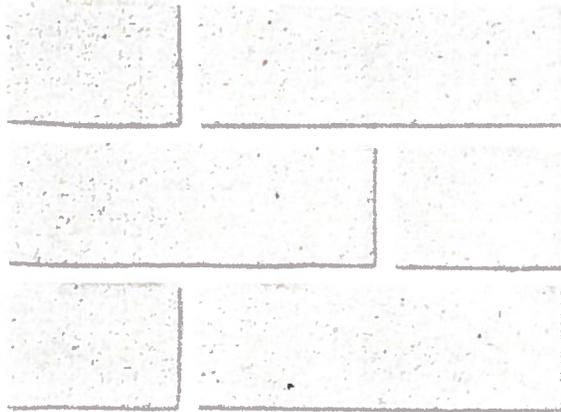


NAVIGATORS LANDING
SCHEMATIC DESIGN
01.24.2018

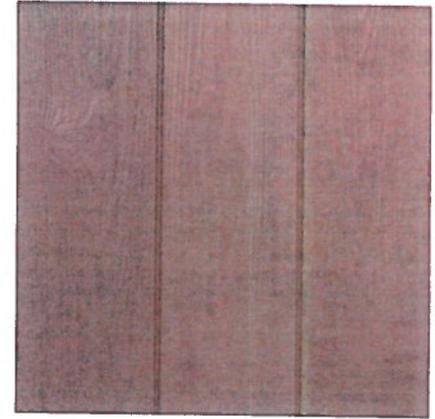
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JAN 29 2018
PLANNING DEPT.
EXTERIOR PERSPECTIVE # 2 AR6.6



IRON WASH BLACK BRICK



WHITE BRICK



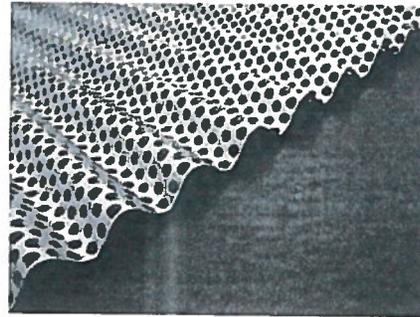
VERTICAL CEDAR
FIBER CEMENT SIDING



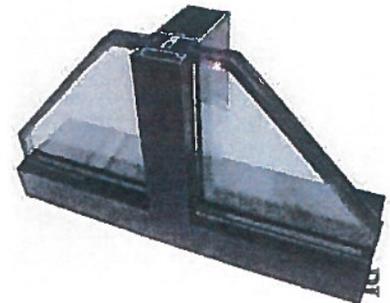
EXPOSED STEEL
CANOPY



CABLE GUARDRAIL



PERFORATED CORRUGATED METAL
MECHANICAL SCREEN



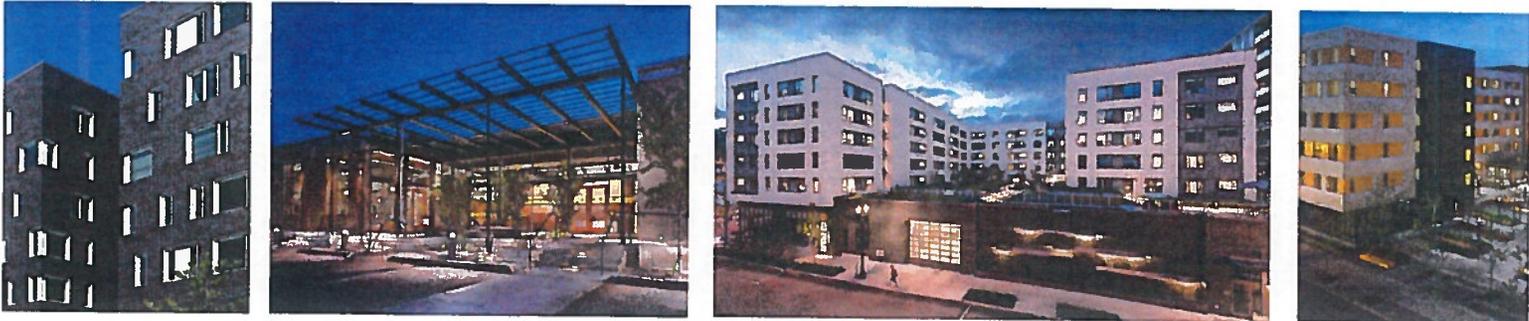
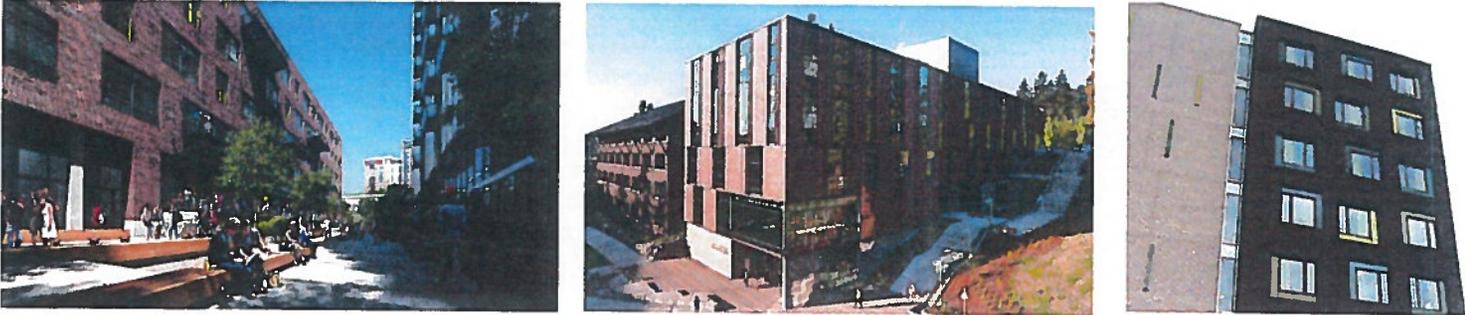
ALUMINUM STOREFRONT

EXTERIOR MATERIALS



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JAN 29 2018
AR6.7

PLANNING DEPT.



NAVIGATORS LANDING

SCHEMATIC DESIGN
01.24.2018

Mistler +
Sutcliffe
+ White

CITY OF MEDFORD

EXHIBIT # *H*

File # *A-18-011*

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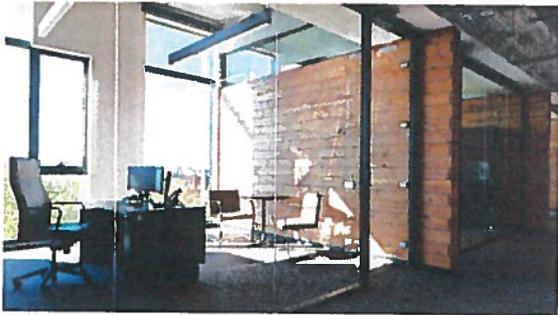
JAN 29 2018

PLANNING DEPT.

EXTERIOR PRECEDENT

AR6.8

OFFICE



NAVIGATORS LANDING
SCHEMATIC DESIGN
01.24.2018

Kistler -
STRELL
+ WENIG

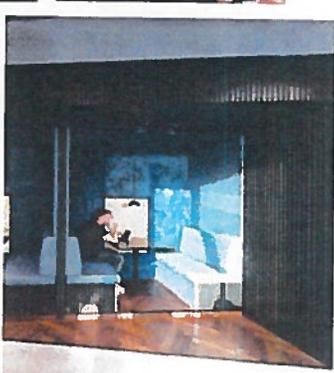
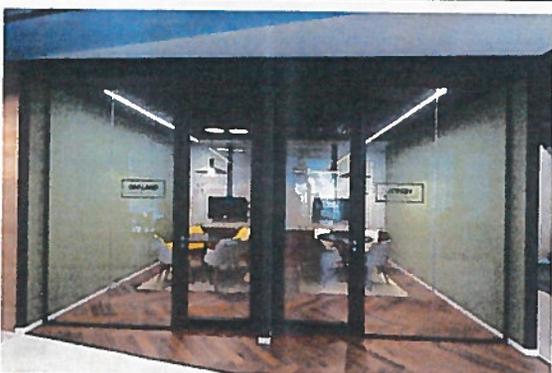
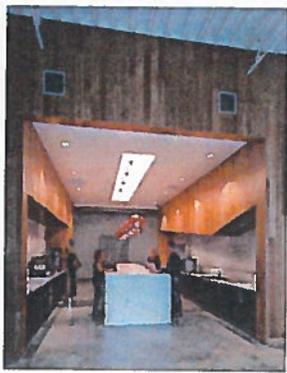
PLANNING DEPT

JAN 29 2018

RECEIVED

INTERIOR PRECEDENT AR10.0

CONFERENCE/BREAK OUT



Page 48

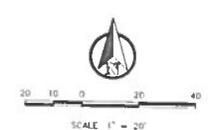
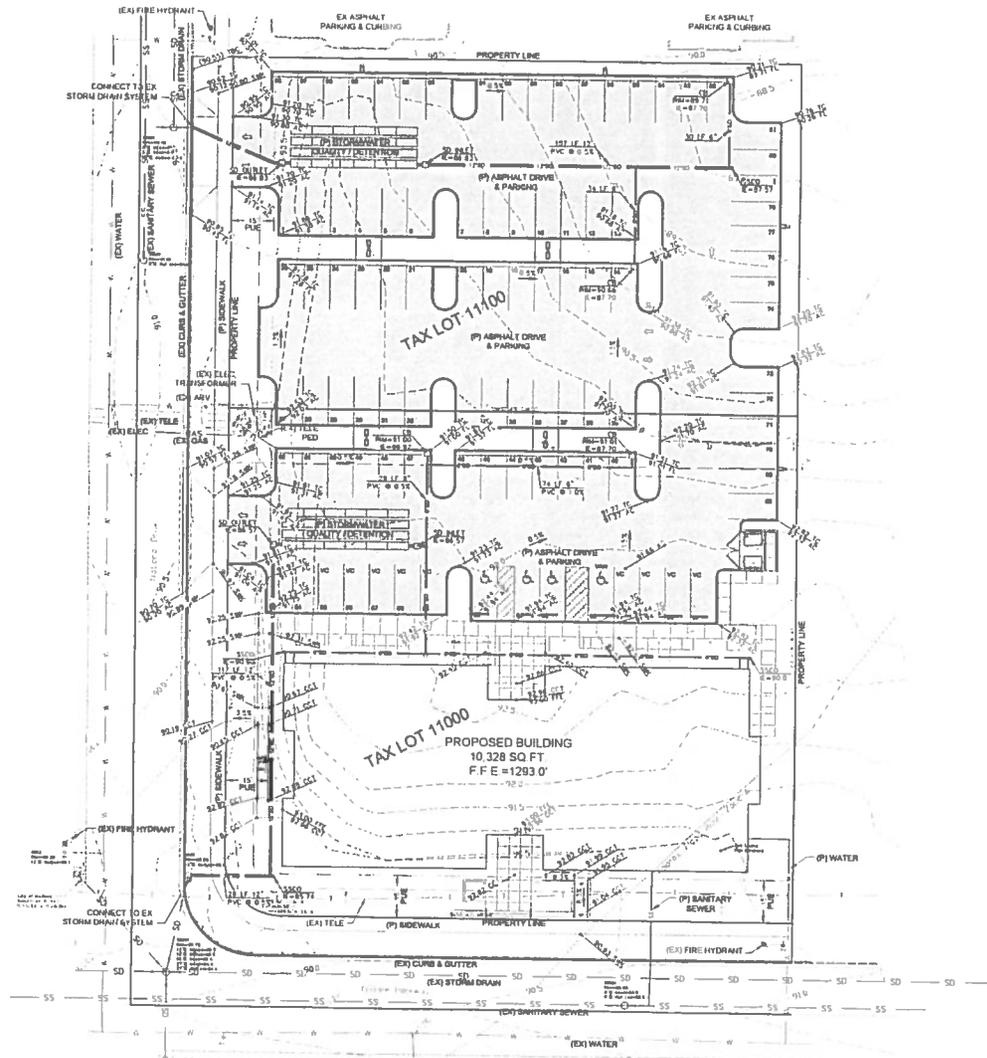
kiester -
SMITH
+ white
NAVIGATORS LANDING
SCHEMATIC DESIGN
01.14.2018

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JAN 29 2018
PLANNING DEPT.
INTERIOR PRECEPT ARCHITECTURE

CIRCULATION



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JAN 29 2018
PLANNING DEPT.
INTERIOR PRECEDENT ARCHITECTURE



LEGEND - NEW CONSTRUCTION

AC	ASPHALT	SSMH	SANITARY SEWER MANHOLE
CB	CATCH BASIN	SS	SANITARY SEWER
EX	EXISTING	CO	CLEAN OUT
TOG	TOP OF GRATE	WM	WATER METER
FS	FINISH SURFACE	FL	FLOW LINE
E	INVERT ELEVATION	FFE	FRESH FLOOR ELEVATION
CTT	CONCRETE SURFACE	EX	EXISTING FEATURE
IP	PROPOSED FEATURE	(000)	EXISTING GRADE

CALL BEFORE YOU DIG
 1-800-332-2344
 48 HOURS BEFORE BEGINNING EXCAVATION
 (SEE LOCAL ORDINANCES FOR ANY ADDITIONAL REQUIREMENTS)
 ADAPTED BY THE ONE CALL CENTER FROM THE
 800-332-2344. TRUCKING AND BULKY ITEMS SHOULD
 BE DELIVERED THROUGH A TRUCKING COMPANY. YOU MAY
 OBTAIN COPIES OF THE BLUE BOOK BY CALLING THE CENTER
 AT 863.232.1867



kistler+small+white
 ARCHITECTS
 88 WATER STREET
 SUITE 101
 ARLAND, OH
 43028
 TEL: 614.488.6200

PRELIMINARY
 THESE DRAWINGS SHALL NOT BE USED FOR CONSTRUCTION, BIDDING, RECORDATION, CORRECTION, OR ANY OTHER PURPOSE WITHOUT THE ISSUANCE OF A PERMIT.
 10% SD

NAVIGATOR'S LANDING
 PROPOSED OFFICE BUILDING
 OWNER: PROJECT ADDRESS

REVISIONS

RECEIVED
 CONCEPTUAL DRAINAGE AND RETENTION PLAN
 JAN 29 2018

PLANNING DEPT.

CITY OF MEDFORD

T37 S, R 02 W, SEC. 12D, TAX LOT 11000, 11100

EXHIBIT # 7
 AC-18-01

FILE #

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JAN 29 2010

PLANNING DEPT

SECTION I - ARCHITECTURAL DESIGN NARRATIVE

The project consists of a new 21,000 square foot 2 story office building, a paved parking lot, pedestrian plaza areas, and landscaping. The building design is modern industrial with 2 colors of brick veneer (light and dark), storefront, wood look cement fiber siding in and steel accents. The building is sited facing O'Hare Parkway just behind the PUE creating a pedestrian friendly frontage, the parking lot is located to the rear of the property with another pedestrian plaza space between the parking and the building. The building entries are defined by pedestrian plaza spaces, recessed areas within the wall line and cantilevered steel trellis structures. There will be a central lobby with elevator and public services for employees and customers, egress stairs are located at each end of the building with direct access to the exterior. Mechanical units will be roof mounted and screened from view with perforated metal.

SECTION II - COMPATIBILITY

- A. The use of adjacent buildings are office occupancies. The style of adjacent buildings varies, buildings mostly have sloped roofs however there are flat roofs or buildings with parapets within the development, materials consist of stucco, cultured stone, brick, cement tile roofing, metal roofing, metal wall panels, aluminum storefront windows. Colors tend to be in the earth tones except for a few instances. Parking tends to be in front of most buildings with limited pedestrian access. The condition of all adjacent building appears to be good and built between 2002 and 2010. See attached photos.
- B. The proposed building is industrial/modern with brick veneer, storefront windows, steel and cement fiber (wood look) accents. Window panels are punched into the brick wall with cement fiber siding accents adjacent to the windows. There is a steel channel cornice at the top of the parapets, and steel trellis entrance structures. Many of the exterior materials proposed are present in other buildings within the development creating compatibility.
- C. The building façade is broken up by large recessed entries into two main building masses, these recessed entries include second floor balconies and steel trellis, storefront windows with fiber cement siding accents that open up the mass of the brick wall continue to break up the façade into smaller wall segments. Being able to see inside the building at ground level is important in creating an inviting atmosphere and proposes to those passing by that the occupants are open to interaction with them, which helps to reinforce pedestrian accessibility.
- D. The proposed building is sited facing O'Hare Parkway just behind the 15 foot PUE, an entry plaza space with seat walls and landscaping creates a comfortable pedestrian friendly space and a transition between the building and the street. Instead of ignoring pedestrians as is so common in vehicular oriented developments, those passing by will find the space refreshing and inviting. The parking lot is located to the rear of the property with another pedestrian plaza space between the parking and the building.

- E. Within the entry plaza spaces there are several benches in various sizes and seat walls with adjacent landscaping creating comfortable pedestrian friendly break-out spaces that will encourage interaction with other people. These spaces are also intended to give its users a place outdoors to decompress which is important to wellness of employees and visitors alike.
- F. Vehicle access will be from National Drive, there will be two drive approaches creating a flow through parking lot design. Most access will be from vehicles, however bicycle parking for 10 bicycles will be provided at the North entry, half of which will be covered. Pedestrian access is available from sidewalks on O'Hare Parkway as well as National Drive, both providing access for people with disabilities. There will be walkways connecting parking lot areas allowing people to walk towards the building without requiring them to walk around the parking lot.
- G. There are no existing trees or vegetation on the site.
- H. Stormwater quality detention will be accomplished through the use of underground storage pipes in two locations within the parking lot. Rain water collected on the roof and in the parking lot will be piped to the detention pipes prior to going to the storm drain system.
- I. The existing landscape currently offers little with regards to either aesthetic beauty or simple utilitarian considerations. Further, after the initial site preparation and grading the land will be left essentially barren. A beautiful building, carefully planned parking spaces, corresponding walkways, artificial lighting, entrances/exits to O'Hare Parkway and National Drive and all the varied and intricate nuances of the new building process shall them be constructed. And yet, the beauty of nature itself shall remain all but obliterated until the remaining open grounds are landscaped.

The landscaping will serve to bring shade to buildings, parking areas, walkways, street, benches and open areas as well as reducing temperatures and offering protection from the wind, cold and the elements. It will further provide habitat for many of nature's creatures not the least of which being the human being. The landscape will hold the ground, the soil itself and serve to prevent erosion. The newly installed landscape will soften the entire area and provide a beautiful, living, aesthetically pleasing natural 'art' while possessing low and moderate water consuming shrubbery.

Without this final touch this sculpting of the land that from the building process itself suffers so immensely, no construction is truly complete. This landscape shall be constructed following and in keeping with the codes, covenants and restrictions presented and required in the City of Medford's Development Codes, thereby positively influencing the entire environment within and immediately surrounding the area.

- J. Exterior lighting will be provided by LED cut-off parking lot pole lights in both single and double configurations evenly spaced throughout the parking area providing safe access to vehicles and the building. Pedestrian plaza areas will be lighted with a

combination of bollard lights and building mounted lights. Additional recessed downlights at the entries will help reinforce those entry points. Exterior building and landscaping lighting will be directed towards the building facade and landscaping to highlight features and will not reduce the view of the night sky or create glare on adjacent properties per Section 10.764.

- K. It is anticipated that a monument sign will be located in the south yard of the site, facing O'Hare Parkway and also on the west yard of the site, near the entrance into the parking area. The signs will be short, solid monument signs without poles and will be accented with appropriate landscaping. The signs will serve to identify the name of the building which is yet to be decided.
- L. There will not be any fencing on the project.
- M. There will not be any noise generated on the site.
- N. All compatibility issues have been previously described.
- O. No exceptions are requested.
- P. No petitions for relief of landscaping is requested.

Photos of Nearby Buildings:



3531 Avion Drive – East Elevation



800 O'Hare Parkway – North Elevation



801 O'Hare Parkway – South Elevation



810 O'Hare Parkway – North Elevation



821 O'Hare Parkway – South Elevation



830 O'Hare Parkway – North Elevation



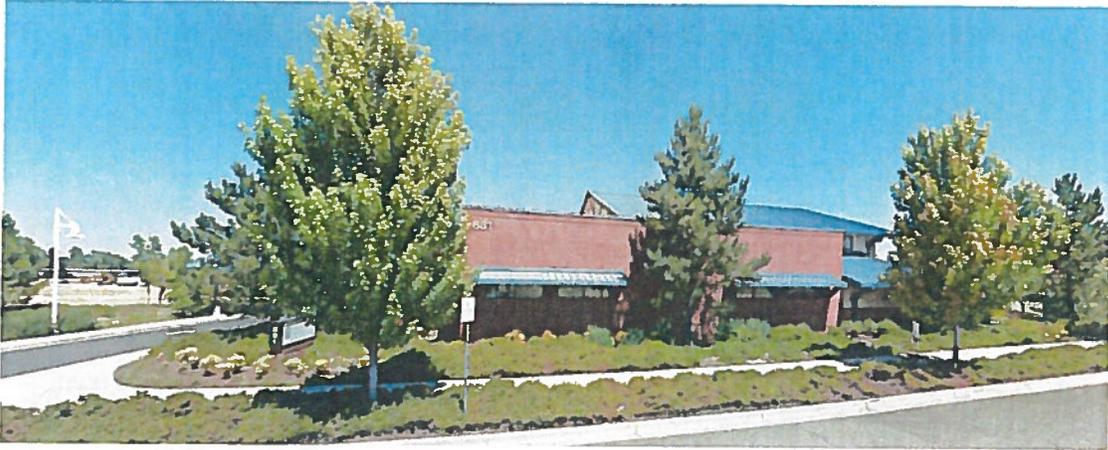
831 O'Hare Parkway – East Elevation



831 O'Hare Parkway – South Elevation



850 O'Hare Parkway – North Elevation



881 O'Hare Parkway – South Elevation



3559 National Drive – East Elevation



3560 National Drive – West Elevation



3569 National Drive – East Elevation



3539 Heathrow Way – North Elevation

Section II – Code Compliance: Criterion No. 2

PROJECT SITE		
	PROPOSED	REQUIRED
• Zoning District	I-L/PD	
• Overlay District(s)	NA	
• Proposed Use	Office	
• Project Site Acreage	1.39	
• Site Acreage (+ right-of-way)	1.79	
• Proposed Density (10.708)	17	
• # Dwelling Units	NA	
• # Employees	210	

STRUCTURES		
	EXISTING	PROPOSED
• # Structures	0	1
• Structure Square Footage (10.710-10.721)	0	20,656

SITE DESIGN STANDARDS		
	PROPOSED	REQUIRED
• Front Yard Setback (10.710-721)	18	10
• Side Yard Setback (10.710-721)	20	10
• Side Yard Setback (10.710-721)	12	5
• Rear Yard Setback (10.710-721)	213	0
• Lot Coverage (10.710-721)	10,328	30,357

PARKING		
	PROPOSED	REQUIRED
• Regular Vehicular Spaces (10.743)	84	63 Min, 105 Max
• Disable Person Vehicular Spaces (10.746[8])	4	4
• Carpool/Vanpool Spaces (10.809)	10	10
• Total Spaces (10.743)	98	77 Min, 119 Max
• Bicycle Spaces (10.748)	10	10
• Loading Berths (10.742)	NA	NA

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

LANDSCAPING

	PROPOSED	REQUIRED
• Total Landscape Area (square feet)	18,880	
• Total Landscape Area in High Water Use Landscaping (square feet)	0	
• Total Landscape Area in High Water Use Landscaping (percentage)	0%	
• Total % Landscape Coverage		
• Required Organic Content (cu.yd.)	56.4	56.4
• Frontage Landscaping (10.797)		
• Street:	O Hare Parkway	O Hare Parkway
• Feet:	214	214
• # Trees:	9	9
• # Shrubs:	62	52
• Street:	National Drive	National Drive
• Feet:	270	270
• # Trees:	11	11
• # Shrubs:	80	68
• Bufferyard Landscaping (10.790)		
• Type:		
• Distance (ft):		
• # Canopy Trees:		
• # Shrubs:	172	
• Fence/Wall:		
• Parking Area Planter Bays (10.746)		
• Type:		
• # Bays:	98	
• Area:	8,235	
• # Trees:	13	12
• # Shrubs:	172	24

STRUCTURE

PROPOSED

- Materials _____
- Colors _____

Please remember that the information you provide in response to the questionnaire must be included with your SPAR application submittal. Remember to sign and date your written response.



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 3/7/2018
File Number: AC-18-011
Reference: PUD-01-155/PA-18-004

PUBLIC WORKS DEPARTMENT STAFF REPORT **National Drive at O'Hare Parkway** **Navigator's Landing – TLs 11000 & 11100**

- Project:** Consideration of plans for the construction of an approximately 21,000 square foot, two-story, general office building on 1.39 acres in the Navigator's Landing Planned Unit Development zoned I-L/PD.
- Location:** Located west of the Medford International Airport on the corner of O'Hare Parkway and National Drive (372W12D TL11000 & 11100).
- Applicant:** Applicant: Mayers 5 LLC, Agent: Kistler + Small + White Architects, Planner: Steffen Roennfeldt.
- Applicability:** The Medford Public Works Department's conditions of Preliminary Plan Approval for Navigator's Landing PUD were adopted by Order of the Medford Planning Commission on March 14th, 2002 (PUD-01-155).

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention, if applicable.
- Completion of all public improvements, if required. The Applicant may provide security for 120% of the improvements prior to issuance of vertical building permits. Construction plans for the improvements shall be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Certification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan, if applicable.
- Completion of all public improvements, if applicable.

A. STREETS

1. Dedications

National Drive and O'Hare Parkway are classified as Commercial streets within the Medford Land Development Code (MLDC) 10.429. Right-of-way has already been dedicated with the final plat for Navigator's Landing (Survey #17310). **No additional right-of-way is required.**

There is currently a 15-foot PUE along the frontage of National Drive and O'Hare Parkway that was originally dedicated with the final plat for Navigator's Landing (Survey #17310).

2. Public Improvements

a. Public Streets

Standard street section improvements have been completed on **National Drive and O'Hare Parkway**, including pavement, curb and gutter, street lights and partial sidewalk as part of The Landing Subdivision (P1295D) improvements. **However, a 5-foot wide sidewalk with an 8-foot planter strip will be required along this developments frontage, including construction of the curb ramp at the corner of O'Hare Parkway and National Drive to current standards.**

b. Street Lights and Signing

No additional street lights are required.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer's contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this frontage to National Drive or O'Hare Parkway.

d. Access to Public Street System

Driveway access to the proposed development site shall comply with MLDC 10.550.

3. Section 10.668 Analysis

To support a condition of development that an applicant dedicate land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and storm drains to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining “rough proportionality” have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

National Drive and O’Hare Parkway:

The additional improvements will provide a planter strip and sidewalk on National Drive and O’Hare Parkway. The planter strip moves pedestrians a safe distance from the edge of the roadway. National Drive and O’Hare Parkway will be the primary route for pedestrians traveling to and from this development. The development shall construct approximately 510 linear feet of

sidewalk along the frontage of the property. All developments in Medford are required to construct their frontage sidewalk and therefore this is roughly proportional.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the project's impacted site with sufficient information to determine the direction of runoff to the existing or proposed drainage system, and also showing elevations of the proposed drainage system (if applicable), shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

A Site/Utility Plan shall be submitted with the building permit application to show the location of existing or proposed stormdrain lateral/s for the site.

All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

4. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. GENERAL CONDITIONS

1. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans.

Contractors proposing to do work on public streets, sewers, or storm drains shall 'prequalify' with the Engineering Division prior to starting work. Contractors shall work off a set of public improvement drawings that have been approved by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County. Any work falling within another agencies jurisdiction shall require a separately issued permit or approval from the respective agency.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and storm drain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the Developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

2. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

3. System Development Charges (SDCs)

Buildings in this development are subject to street, sanitary sewer treatment and storm drain SDCs. All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

National Drive at O'Hare Parkway
Navigator's Landing – TLs 11000 & 11100
AC-18-011

A. Streets

1. Street Dedications to the Public:

- National Drive and O'Hare Parkway – No additional right-of-way required.
- 15-foot Public Utility Easements have been dedicated.

2. Improvements:

Public Streets

- National Drive and O'Hare Parkway – No improvements are required aside from 5-foot wide sidewalk and 8-foot planter strip.

Lighting and Signing

- No additional street lights are required.

Access to Public Street System

- Driveway access shall comply with MLDC 10.550.

Other

- There is no pavement moratorium currently in effect on National Drive or O'Hare Parkway.

B. Sanitary Sewer:

- The site is situated within the RVSS area.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers certification of stormwater facility construction.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



BOARD OF WATER COMMISSIONERS

Staff Memo

TO: Planning Department, City of Medford

FROM: Rodney Grehn P.E., Water Commission Staff Engineer

SUBJECT: AC-18-011

PARCEL ID: 372W12D TL's 11000 & 1110

PROJECT: A pre-application request to construct a 21,000 square foot, two story general office building on 1.39 acres in the Navigators Landing Planned Unit Development zoned I-L/PD located west of the Medford International Airport on the corner of O'Hare Parkway and National Drive (372W12D TL11000 & 11100) Applicant: Mayers 5 LLC, Agent: Kistler + Small + White Architects, Planner: Steffen Roennfeldt.

DATE: March 7, 2018

I have reviewed the above plan authorization application as requested. Conditions for approval and comments are as follows:

CONDITIONS:

1. The water facility planning/design/construction process will be done in accordance with the Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities/Fire Protection Systems/Backflow Prevention Devices."
2. All parcels/lots of proposed property divisions will be required to have metered water service prior to recordation of final map, unless otherwise arranged with MWC.
3. Installation of metered water service is required. The proposed water meter to the building shall be located along the O'Hare Parkway right-of-way. (See Comment 4 and 5 below)
4. Static water pressure is expected to be near 90 psi. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves".
5. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link <http://www.medfordwater.org/Page.asp?NavID=35> .
6. Dedication of a 10 foot wide (minimum) access and maintenance easement to MWC over all water facilities located outside of public right-of-way is required. Easement shall be submitted to MWC for review and recordation prior to construction.

Continued to Next Page

CITY OF MEDFORD
EXHIBIT 0
File # AC-18-011



Continued from Previous Page

COMMENTS:

1. Off-site water line installation is not required.
2. On-site water facility construction may be required, depending on Medford Fire Dept requirements.
3. Static water pressure is around 90 psi. (See Condition 4 above)
4. MWC-metered water service does not exist to these properties.
5. Access to MWC water lines is available. There is an existing 12-inch water in both O'Hare Parkway, and also in National Drive. There is also an 8-inch water line stub at the southeast property corner.



Page 72



0 25 50 100 Feet
Scale: 1"=100'

**Water Facility Map
for
AC-18-011**

March 14, 2018

Legend

- ★ Air Valve
- Sample Station
- Fire Service
- ◆ Hydrant
- ▲ Reducer
- Blow Off
- + Plugs-Caps

Water Meters:

- ⊕ Active Meter
- On Well
- Unknown
- Vacant

Water Valves:

- ⊕ Butterfly Valve
- ⊕ Gate Valve
- ⊕ Tapping Valve

Water Mains:

- Active Main
- - Abandoned Main
- Reservoir Drain Pipe
- Pressure Zone Line

Boundaries:

- ▭ Urban Growth Boundary
- ▭ City Limits
- ▭ Tax Lots

MWC Facilities:

- C** Control Station
- P** Pump Station
- R** Reservoir



This map is based on a digital data set in shapefile (.shp) files. Medford Water Commission does not warrant the accuracy of the data. It is the user's responsibility to verify the accuracy of the data for their intended use. Medford Water Commission is not responsible for any errors or omissions in this map.



Medford Fire-Rescue Land Development Report

Review Info

Reviewed By: Kleinberg, Greg

Review Date: 3/1/2018
Meeting Date: 3/7/2018

LD #: AC18011

Planner: Steffen Roennfeldt

Applicant: Applicant: Mayers 5 LLC, Agent: Kistler + Small + White Architects

Project Location: West of the Medford International Airport on the corner of O'Hare Parkway and National Drive

ProjectDescription: Consideration of plans for the construction of an approximately 21,000 square foot, two-story, general office building on 1.39 acres in the Navigators Landing Planned Unit Development zoned I-L/PD located west of the Medford International Airport on the corner of O'Hare Parkway and National Drive (372W12D TL11000 & 11100) Applicant: Mayers 5 LLC, Agent: Kistler + Small + White Architects, Planner: Steffen Roennfeldt.

Site Name: Navigators Landing Planned Unit Development

Specific Requirements for Access & Water Supply

Conditions

Reference	Description
Approved	Approved as submitted

General Information/Requirements

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only. Specific fire protection systems may be required in accordance with the Oregon Fire Code.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300

CITY OF MEDFORD
EXPERT # 8
File # AC-18-011



MEMORANDUM

Subject Mayer 5 LLC
File no. AC-18-011
To Steffen Roennfeldt, Planner III
From *W*Liz Conner, Planner II, Floodplain Coordinator
Date March 6, 2018

SITE CHARACTERISTICS

- Light Industrial (I-L) zoning district, Planned Unit Development
- Southeast corner of property within 100 year floodplain
- FIRM panel 41029C 1957F effective May 3, 2011

PROJECT DESCRIPTION

Consideration of plans for the construction of an approximately 21,000 square foot, two-story, general office building on 1.39 acres in the Navigators Landing Planned Unit Development zoned I-L/PD located west of the Medford International Airport on the corner of O'Hare Parkway and National Drive.

FLOODPLAIN REGULATIONS

The property is currently within a mapped A zone without Base Flood Elevations or a designated floodway. Per the National Flood Insurance Program Regulations, any vertical structures and utilities shall comply with 44 CFR 60.3(a),(b) and (c) and MMC Section 9.706 (D).

The Medford Floodplain regulations are found in Sections 9.701-9.707 of the Municipal Code. The sections pertaining to areas of special flood hazard without Base Flood Elevations (BFE) need to be reviewed and adhered to specifically, along with other relevant sections.

A Floodplain Development Permit is required prior to development in the Special Flood Hazard Areas. Development is broadly defined and includes, but is not limited to, grading, filling, paving, and construction of buildings.

CITY OF MEDFORD

EXHIBIT # *2*

File # *Ac-18-011*

Future tentative plans shall identify the special flood hazard areas. Structures shall be constructed a minimum of one-foot above the BFE.

Existing and proposed grades shall be provided and the effect of this earth movement on the floodplain shall be described in a narrative.

Floodplain Permit

Submit a floodplain development application and fee (\$150) along with submittal requirements identified in Section 9.705 (C). An Elevation Certificate (EC) is required with the submittal of building permits for new commercial structures located in the special flood hazard area (one at the time of building permit submittal, one during construction, and one prior to certificate of occupancy).

Submit copies of all necessary permits from other governmental agencies from which approval is required prior to start of construction.

Construction shall be in compliance with applicable building and fire codes and floodplain regulations.

Expiration of Floodplain Permit

A floodplain Development Permit shall become invalid unless work is started within 180 days after its issuance. Extensions for periods of not more than 180 days each shall be requested in writing.



Memo

To: Steffen Roennfeldt, Planning Department
From: Chad Wiltrout, Building Department (541) 774-2363
CC: Applicant: Mayers 5 LLC, Agent: Kistler + Small + White Architects
Date: March 6, 2018
Re: March 6, 2018 LDC Meeting: AC-18-011

Please Note:

This is not a plan review. Unless noted specifically as Conditions of Approval, general comments are provided below based on the general information provided; these comments are based on the 2014 Oregon Structural Specialty Code (OSSC) unless noted otherwise. Plans need to be submitted and will be reviewed by a commercial plans examiner, and there may be additional comments.

Fees are based on valuation. Please contact Building Department front counter for estimated fees at (541) 774-2350 or building@cityofmedford.org.

For questions related to the Conditions or Comments, please contact me, Chad Wiltrout, directly at (541) 774-2363 or chad.wiltrout@cityofmedford.org.

General Comments:

1. For list of applicable Building Codes, please visit the City of Medford website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Design Criteria" on left side of screen and select the appropriate design criteria.
2. All plans are to be submitted electronically. Information on the website: www.ci.medford.or.us Click on "City Departments" at top of screen; click on "Building"; click on "Electronic Plan Review (ePlans)" for information.
3. A site excavation and grading permit may be required, see attached handout.
4. A separate demolition permit will be required for demolition of any structures not shown on the plot plan.

Comments:

5. ADA parking spaces and accessible routes shall be required in accordance with code section 1104 and 1106 of the Oregon Structural Specialty Code.
6. The building and building appurtenances shall be designed by an Oregon licensed design professional in accordance with 107.1 and 107.3.4 OSSC. The project narrative you provided has a contractor listed as the designer for plumbing and mechanical work. This work is required to be provided by an Oregon licensed design professional.
7. A code analysis providing occupant load, means of egress plan, type of construction, occupancy classification, mixed use provisions, fire protection systems, etc... shall be provided at time of application.

CITY OF MEDFORD
EXHIBIT # R
File # AC-18-011

8. The city provided special inspection agreement form required by chapter 17 Oregon Structural Specialty Code shall be completed along with a special inspection statement addressing the required special inspections in accordance with chapter 17 OSSC.
9. Com-check forms are required for lighting, mechanical equipment and exterior envelope to show energy efficiency compliance with the 2014 Oregon Energy Efficiency Specialty Code.
10. A geotechnical engineer report shall be provided to address the requirements of 1803 OSSC for seismic considerations of the subgrade and any other soil conditions such as expansive soils.
11. It appears that a corner of the proposed building will be placed in the flood hazard zone. Buildings located in a flood hazard zone are required to comply with the requirements of code section 1612 OSSC, ASCE 7-10, and ASCE 24.
12. Design connection of masonry veneer shall comply with chapter 13 of ASCE 7-10.



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

February 22, 2018

City of Medford Planning Department
200 S. Ivy Street
Medford, Oregon 97501

Re: AC-18-011, Navigator's Landing (372W12D – 11000 & 11100)
Ref: PA-18-004

ATTN: Steffen,

The subject property is within the RVSS service area. There are 8 inch sewer mains along National Drive and O'Hare Parkway with 4 inch services stubbed to both tax lots. Sewer service for the proposed development can be had by connecting to the service stubbed to tax lot 11000.

The sewer connection permit will be issued by the City of Medford. However, there will be system development charges owed to Rogue Valley Sewer Services.

Rogue Valley Sewer Services requests that approval of this application be subject to the following conditions:

1. The applicant must provide RVSS with a plumbing fixture plan for the determination of system development charges.
2. The applicant must pay sewer system development charges to Rogue Valley Sewer Service prior to issuance of a building permit.

Please feel free contact me with any questions.

Sincerely,

Nicholas R. Bakke

Nicholas R. Bakke, P.E.
District Engineer

K:\DATA\AGENCIES\MEDFORD\PLANNING\ARCH COMM\2018\AC-18-011_NAVIGATOR'S LANDING OFFICE BLDG.DOC

CITY OF MEDFORD
EXHIBIT # S
File # AC-18-011

Steffen K. Roennfeldt

From: Marcy Black <BlackMA@jacksoncounty.org>
Sent: Friday, February 23, 2018 2:10 PM
To: Steffen K. Roennfeldt
Subject: File No. AC-18-011 Project Name: Navigator's Landing Office Building

Steffen:

Due to the proximity to the Airport, the applicant needs to contact the FAA regarding filing a 7460-1 Notice of Proposed Construction or Alteration. The FAA contact is: Paul Holmquist, phone (425) 227-2625.

An Avigation, Noise & Hazard easement is already on file.

Thank you,

Marcy Black
Deputy Director-Administration

CITY OF MEDFORD
EXHIBIT # T
File # AC-18-011

Steffen K. Roennfeldt

From: valerie.thorsen@faa.gov
Sent: Monday, February 26, 2018 2:13 PM
To: Steffen K. Roennfeldt
Cc: matt@kistlersmallwhite.com
Subject: RE: AC-18-011: Navigator's Landing Office Building

Steffan,

Thank you for the quick response. Given the proximity to MFR, an FAA Form 7460- Notice of Proposed Construction or Alteration should be filled. This form can be filled through our Obstruction Evaluation/Airport Airspace Analysis (OE/AAA) website. Please let me know if you have any questions on this process.

Best regards,
Valerie Thorsen
Airport Planner (OR)
425-227-2655

Note:

- We will be moving March 23, 2018.
- My new phone number will be 206-231-4139.
- Our new address will be:
Seattle Airports District Office
2200 S 216th Street
Des Moines, WA 98198

From: Steffen K. Roennfeldt [mailto:Steffen.Roennfeldt@cityofmedford.org]
Sent: Monday, February 26, 2018 1:52 PM
To: Thorsen, Valerie (FAA) <valerie.thorsen@faa.gov>
Subject: RE: AC-18-011: Navigator's Landing Office Building

Hi Valerie,

Here are the coordinates:

42°21'58.5"N 122°52'49.0"W

42.366249, -122.880288

Hope this helps,
Steffen

From: valerie.thorsen@faa.gov [mailto:valerie.thorsen@faa.gov]
Sent: Monday, February 26, 2018 1:01 PM
To: Steffen K. Roennfeldt <Steffen.Roennfeldt@cityofmedford.org>
Cc: matt@kistlersmallwhite.com
Subject: AC-18-011: Navigator's Landing Office Building

Good afternoon,

I'm writing in response to the City of Medford's Planning Department proposal for construction of Navigator's Landing Office building on the corner of O'Hare Parkway and National Drive. Could you please provide me with the coordinates of the proposal?

Thank you,
Valerie Thorsen
Airport Planner (OR)
Seattle Airports District Office
425-227-2655

Note:

- We will be moving March 23, 2018.***
- My new phone number will be 206-231-4139.***
- Our new address will be:***
Seattle Airports District Office
2200 S 216th Street
Des Moines, WA 98198

Steffen K. Roennfeldt

From: CAINES Jeff <Jeff.CAINES@aviation.state.or.us>
Sent: Wednesday, February 28, 2018 3:04 PM
To: Steffen K. Roennfeldt
Subject: AC-18-011 - ODA Comments

Steffen:

Thank you for allowing ODA to comment on the proposed 2-story general office building located I the Navigators Landing PUD. ODA has reviewed the proposed development and have the following comments:

The site is approximately .6 miles from the Rouge Valley International Airport. There is an approximately 24 foot elevation difference between the elevation of the site and the runway. Therefore, ODA finds that due to the elevation difference, distance to the runway and existing developments between the project site and the airport no FAA form 7460-1 will be required.

Thank you again for allowing ODA to comment.

Please feel free to contact me if you or the applicant have any questions.

Jeff

JEFF CAINES, AICP
OREGON DEPARTMENT OF AVIATION
AVIATION PLANNER / SCIP COORDINATOR

OFFICE 503-378-2529
CELL/TEXT 503-507-6965

EMAIL jeff.caines@aviation.state.or.us
WEBSITE www.oregon.gov/aviation



3040 25th Street SE, Salem, OR 97302

*****CONFIDENTIALITY NOTICE*****

This e-mail may contain information that is privileged, confidential, or otherwise exempt from disclosure under applicable law. If you are not the addressee or it appears from the context or otherwise that you have received this e-mail in error, please advise me immediately by reply e-mail, keep the contents confidential, and immediately delete the message and any attachments from your system.

Steffen K. Roennfeldt

From: Jon M. Proud
Sent: Monday, February 26, 2018 10:12 AM
To: Steffen K. Roennfeldt
Subject: RE: YELLOW PAPER 18-011.pdf

Yes! Those are the deeds that reconfigured the units of land into the current configuration. Jon

-----Original Message-----

From: Steffen K. Roennfeldt
Sent: Monday, February 26, 2018 9:55 AM
To: Jon M. Proud
Subject: RE: YELLOW PAPER 18-011.pdf

Jon, are these the correct ones?

<http://web.jacksoncounty.org/Document/Service/2005/39777> and
<http://web.jacksoncounty.org/Document/Service/2005/39776>

Thanks!

-----Original Message-----

From: Jon M. Proud
Sent: Monday, February 26, 2018 8:38 AM
To: Steffen K. Roennfeldt <Steffen.Roennfeldt@cityofmedford.org>
Subject: YELLOW PAPER 18-011.pdf

CITY OF MEDFORD
EXHIBIT # W
File # AC-18-24

771308-EP

NAVIGATORS LANDING LLC
 1175 E. MAIN SUITE 2A
 MEDFORD, OR 97504
 Grantor's Name and Address

NAVIGATORS LANDING LLC
 1175 E. MAIN SUITE 2A
 MEDFORD, OR 97504
 Grantee's Name and Address

After recording return to:
 AMERITITLE
 1501 E. MCANDREWS
 MEDFORD, OR 97504

THIS SPACE RESERVED

Jackson County Official Records 2005-039776
 R-LLA
 Cnt=1 Stn=9 WALKERC 07/05/2005 09:00:00 AM
 \$10.00 \$5.00 \$11.00 Total:\$26.00



I, Kathleen S. Beckett, County Clerk for Jackson County, Oregon, certify that the instrument identified herein was recorded in the Clerk records
 Kathleen S. Beckett - County Clerk
 PLANNING DEPT.

Until a change is requested all tax statements shall be sent to
 The following address:
 NO CHANGE

Escrow No. 771308

LOT LINE ADJUSTMENT DEED

KNOW ALL MEN BY THESE PRESENTS, That NAVIGATORS LANDING LLC, AN OREGON LIMITED LIABILITY COMPANY, hereinafter called grantor, for the consideration hereinafter stated, does hereby grant, bargain, sell and convey unto NAVIGATORS LANDING LLC, AN OREGON LIMITED LIABILITY COMPANY, hereinafter called grantee, and unto grantee's heirs, successors and assigns all of that certain real property with the tenements, hereditaments and appurtenances thereunto belonging or in anyway appertaining, situated in the County of JACKSON, State of Oregon, described as follows, to wit: SEE ATTACHED EXHIBIT 'A' HERETO AND MADE A PART HEREOF

THIS INSTRUMENT WILL NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USES AND TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES AS DEFINED IN ORS 30.930.

To Have and to Hold the same unto grantee and grantee's heirs, successors and assigns forever.
 The true and actual consideration paid for this transfer, stated in terms of dollars, is \$LOT LINE ADJUSTMENT.
 However, the actual consideration consists of or includes other property or value given or promised which is the whole / part of the consideration.

In construing this deed, where the context so requires, the singular includes the plural and all grammatical changes shall be made so that this deed shall apply equally to corporations and to individuals.
 In Witness Whereof, the grantor has executed this instrument this 23 day of June 2005; if a corporate grantor, it has caused its name to be signed and its seal if any, affixed by an officer or other person duly authorized to do so by order of its board of directors.

NAVIGATORS LANDING, LLC
 BY L. JOHN PIERCE, MANAGING MEMBER

State of Oregon
 County of JACKSON

This instrument was acknowledged before me on June 23, 2005 by L. JOHN PIERCE AS MANAGING MEMBER OF NAVIGATORS LANDING LLC



Cindi Poling
 (Notary Public)
 My commission expires 12/17/2008



APPROVED
 City Of Medford
 Planning Dept
 S. Sauer 6/30/05
 File #

EXHIBIT "A"

**DESCRIPTION SHEET
TRACT 16**

All that certain real property located in the Southeast One-quarter of Section 12, Township 37 South, Range 2 West of the Willamette Meridian, Jackson County, Oregon, more particularly described as follows:

Lot 31 of NAVIGATORS LANDING a Subdivision, filed for record May 2, 2002, and recorded in Volume 28 of Plats at Page 24 of the Records of Jackson County, Oregon.

EXCEPTING THEREFROM: the northerly 19.57 feet of said Lot 31, being parallel to and contiguous with the northerly line of said Lot 31.

Resultant tract containing 32,000 square feet, more or less.

Basis Bearings for this description is Navigator's Landing as recorded in Volume 28 of Plats at Page 24 of Records of Jackson County, Oregon, and filed as Survey Number 17310 in the office of the Jackson County Surveyor.

Prepared By: Neathamer Surveying, Inc.
3126 State Street, Suite 200
P.O. Box 1584
Medford, Oregon 97501
Phone: (541) 732-2889
FAX: (541) 732-1382

Date: June 17, 2005



Robert V. Neathamer



RENEWAL: DEC. 31, 2006

2

771308-CP
NAVIGATORS LANDING LLC
1175 E. MAIN SUITE 2A
MEDFORD, OR 97504
Grantor's Name and Address
NAVIGATORS LANDING LLC
1175 E. MAIN SUITE 2A
MEDFORD, OR 97504
Grantee's Name and Address

THIS SPACE RESERVED F
12

Jackson County Official Records 2005-039777
R-LLA
Cnt=1 Stn=9 WALKERC 07/05/2005 09:00:00 AM
\$10 00 \$5 00 \$11 00 Total:\$26.00



Kathleen S. Beckett, County Clerk for Jackson County Oregon
certify that the instrument identified herein was recorded in the Clerk
records
Kathleen S. Beckett - County Clerk

After recording return to:
AMERITITLE
1501 E. MCANDREWS
MEDFORD, OR 97504

Until a change is requested all
tax statements shall be sent to
The following address:
NO CHANGE

Escrow No. 771308

LOT LINE ADJUSTMENT DEED

KNOW ALL MEN BY THESE PRESENTS, That NAVIGATORS LANDING LLC, AN OREGON LIMITED LIABILITY COMPANY, hereinafter called grantor, for the consideration hereinafter stated, does hereby grant, bargain, sell and convey unto NAVIGATORS LANDING LLC, AN OREGON LIMITED LIABILITY COMPANY, hereinafter called grantee, and unto grantee's heirs, successors and assigns all of that certain real property with the tenements, hereditaments and appurtenances thereunto belonging or in anyway appertaining, situated in the County of JACKSON, State of Oregon, described as follows, to wit: SEE ATTACHED EXHIBIT 'A' HERETO AND MADE A PART HEREOF

THIS INSTRUMENT WILL NOT ALLOW USE OF THE PROPERTY DESCRIBED IN THIS INSTRUMENT IN VIOLATION OF APPLICABLE LAND USE LAWS AND REGULATIONS. BEFORE SIGNING OR ACCEPTING THIS INSTRUMENT, THE PERSON ACQUIRING FEE TITLE TO THE PROPERTY SHOULD CHECK WITH THE APPROPRIATE CITY OR COUNTY PLANNING DEPARTMENT TO VERIFY APPROVED USES AND TO DETERMINE ANY LIMITS ON LAWSUITS AGAINST FARMING OR FOREST PRACTICES AS DEFINED IN ORS 30.930.

To Have and to Hold the same unto grantee and grantee's heirs, successors and assigns forever.
The true and actual consideration paid for this transfer, stated in terms of dollars, is \$LOT LINE ADJUSTMENT.
However, the actual consideration consists of or includes other property or value given or promised which is the whole / part of the consideration.

In construing this deed, where the context so requires, the singular includes the plural and all grammatical changes shall be made so that this deed shall apply equally to corporations and to individuals.

In Witness Whereof, the grantor has executed this instrument this 23rd day of June, 2005; if a corporate grantor, it has caused its name to be signed and its seal if any, affixed by an officer or other person duly authorized to do so by order of its board of directors.

NAVIGATORS LANDING, LLC
BY L. JOHN PIERCE, MANAGING MEMBER

State of Oregon
County of JACKSON

This instrument was acknowledged before me on June 23 2005 by L. JOHN PIERCE AS MANAGING MEMBER OF NAVIGATORS LANDING LLC

(Notary Public)

Commission expires 12/17/2005



APPROVED
City Of Medford
Planning Dept.
S.Sousa 4/30/05

EXHIBIT "A"

**DESCRIPTION SHEET
TRACT 17**

All that certain real property located in the Southeast One-quarter of Section 12, Township 37 South, Range 2 West of the Willamette Meridian, Jackson County, Oregon, more particularly described as follows:

Lot 30 of NAVIGATORS LANDING a Subdivision, filed for record May 2, 2002, and recorded in Volume 28 of Plats at Page 24 of the Records of Jackson County, Oregon.

TOGETHER WITH: the northerly 19.57 feet of Lot 31 said NAVIGATORS LANDING, being parallel to and contiguous with the northerly line of said Lot 31.

Resultant tract containing 28,714 square feet, more or less.

Basis Bearings for this description is Navigator's Landing as recorded in Volume 28 of Plats at Page 24 of Records of Jackson County, Oregon, and filed as Survey Number 17310 in the office of the Jackson County Surveyor.

Prepared By: Neathamer Surveying, Inc.
3126 State Street, Suite 200
P.O. Box 1584
Medford, Oregon 97501
Phone: (541) 732-2889
FAX: (541) 732-1382

Date: June 17, 2005



Robert V. Neathamer



RENEWAL: DEC. 31, 2006

2

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

RECEIVED

FEB 27 2018

PLANNING DEPT.

WRITTEN CONSENT OF OWNER

We, Navigators Landing LLC, the property owner(s) of Tax Lot(s) 11100 on Jackson County Assessor Map 372 W12 D, hereby consent to the filing of an application for New Construction on said property, and will allow Robb Mayers to represent me before the City of Medford Approving Authority.

Signed: [Signature] Signed: _____

Print Name: Tom Becker, Member Print Name: _____

Date: 2/27/18 Date: _____

Signed: _____ Signed: _____

Print Name: _____ Print Name: _____

Date: _____ Date: _____

CITY OF MEDFORD
EXHIBIT # X
File # AC-18-011

and fire suppression, storm drainage, electrical and natural gas service, CATV and telephone service.

- The construction of commercial and industrial buildings, including site landscaping and the installation of paved off-street parking facilities.
- The construction of two egress and one ingress travel lanes to serve the subject property.
- As described above, applicants plan to undertake landscaping enhancements and pedestrian improvements within the Lone Pine Creek riparian corridor. If the PUD is approved, applicants intend to submit an application for a conditional use permit which will be for the purpose of permitting the riparian corridor landscaping enhancement and placement of a footpath within the corridor area. Applicants have also agreed to stipulate in Section II, to prepare and coordinate a detailed riparian planting plan with the Oregon Department of Fish and Wildlife (ODF&W) and have offered the same as a stipulation following approval of the PUD. *See*, Section II.¹

3. Landscaping

There are several forms of landscaping to be provided as part of the PUD. Some of the landscaping will be installed by applicants and maintained as common elements (as explained in Section I(E)), while others will be installed and maintained by the future owners of individual lots within the PUD. The landscaped elements are:

- The landscaped frontage along Biddle Road, consisting of irrigated trees, shrubs and groundcover.
(Maintained by Association of Owners)
- The riparian corridor adjoining Lone Pine Creek along the south boundary of the property. Along the riparian corridor, applicants propose the use of *Populus tremuloides* (Quaking Aspen) in a 1-inch caliper (B&B). The selected plant materials are native and will require no artificial irrigation or maintenance.
(Maintained by Association of Owners)
- The irrigated trees, shrubs and groundcover within the landscaped “planterstrips” which adjoin the curbs of the internal streets serving the PUD. Applicants have proposed the following tree species as part of their street tree (and riparian planting) plan (*see*, Exhibit 2.6):

¹ The planned pedestrian footpath (within the riparian corridor) is a conditional use that requires approval by the city of a conditional use permit. The landscaping enhancements are a permitted use which are subject to a requirement that the same be coordinated with the Oregon Department of Fish and Wildlife (ODF&W). Applicant does not believe that the planned landscaping enhancements will require coordination with other state or federal agencies.



City of Medford

Planning Department

Vicinity
Map

File Number:

AC-18-011



Project Name:

Mayers 5 LLC

Map/Taxlot:

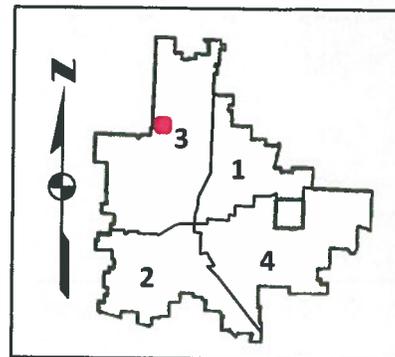
372W12D TL 11000-11100



02/06/2018

Legend

-  Subject Area
-  Medford Zoning
-  Tax Lots



BEFORE THE MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

STATE OF OREGON, CITY OF MEDFORD

IN THE MATTER OF SITE PLAN AND ARCHITECTURAL COMMISSION)
FILE AC-18-016 APPLICATION FOR PROJECT REVIEW SUBMITTED) **ORDER**
BY GERALD REYNOLDS)

AN ORDER granting approval of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district.

WHEREAS:

1. The Site Plan and Architectural Commission has duly accepted the application filed in accordance with the Land Development Code, Section 10.285.
2. The Site Plan and Architectural Commission has duly held a public hearing on the matter of an application of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district, with a public hearing a matter of record of the Site Plan and Architectural Commission on April 6, 2018.
3. At the public hearings on said application, evidence and recommendations were received and presented by the Planning Department staff; and
4. At the conclusion of said public hearings, after consideration and discussion, the Site Plan and Architectural Commission, upon a motion duly seconded, granted approval and directed staff to prepare a final order with all conditions and findings set forth for the granting of approval.

THEREFORE LET IT BE HEREBY ORDERED that the application of Gerald Reynolds, stands approved subject to compliance with the conditions stated in the Staff Report dated March 28, 2018.

AND LET IT FURTHER BE OF RECORD that the action of the Site Plan and Architectural Commission approving this application is hereafter supported by the following findings:

- (a) That the proposed development, with the conditions of approval, complies with the applicable provisions of all city ordinances as determined by the staff review.

FINAL ORDER AC-18-016

(b) That compliance with the criterion determining that the proposed development is compatible with uses and development that exist on adjacent land is not applicable as the application is regulated by ORS 197.307(4).

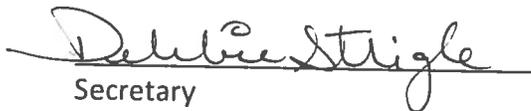
BASED UPON THE ABOVE, it is the finding of the Medford Site Plan and Architectural Commission that the project is in compliance with the criteria of Section 10.290 of the Land Development Code.

Accepted and approved this 6th day of April, 2018.

MEDFORD SITE PLAN AND ARCHITECTURAL COMMISSION

Site Plan and Architectural Commission Chair

ATTEST:


Secretary



STAFF REPORT

for a Type-C quasi-judicial decision: **Site Plan and Architectural Review**

Project Old Dominion Freight Line
 Applicant/Agent: Gerald Reynolds

File no. AC-18-016

To Site Plan and Architectural Review Commission *for 4/6/2018 hearing*

From Steffen Roennfeldt, Planner III

Reviewer Kelly Akin, Assistant Planning Director *h*

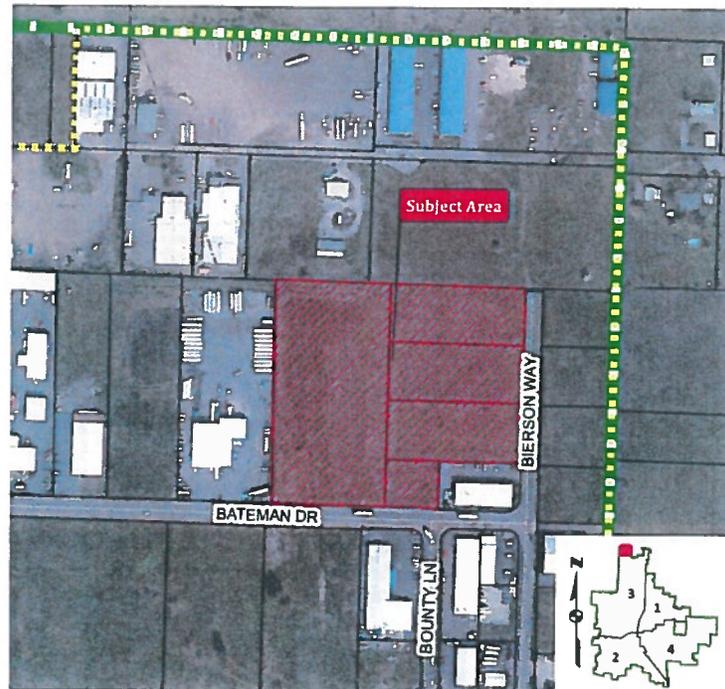
Date March 28, 2018

BACKGROUND

Proposal

Consideration of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located southwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district.

Vicinity Map



Subject Site Characteristics

Zoning	I-L	Light Industrial
Overlays	AA/AC/I-00	Airport Approach District/Airport Area of Concern/Limited Industrial
GLUP Use	GI	General Industrial Vacant

Surrounding Site Characteristics

<i>North</i>	Zone:	I-L
	Use:	Light Industrial & Vacant
<i>South</i>	Zone:	I-L & I-G (General Industrial)
	Use:	Furniture Wholesaler & Vacant
<i>East</i>	Zone:	I-L
	Use:	ATV Dealer & Vacant
<i>West</i>	Zone:	I-L
	Use:	Trucking Company

Related Projects

ZC-08-089	Zone Change from County Zoning to City Zoning
AC-05-100	Climate Control Mini Storage
PLA-17-172	Property Line Adjustment/Lot Consolidation

Applicable Criteria

Medford Municipal Code §10.290 – Site Plan & Architectural Review Criteria

The Site Plan and Architectural Commission shall approve a site plan and architectural review application if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (1) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (2) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

Corporate Names

C T Corporation System is the Registered Agent for Old Dominion Freight Line, Inc. according to the Oregon Secretary of State Business Registry. David Congdon is listed as the President. Ross Parr is listed as the Secretary.

ISSUES AND ANALYSIS

Background

After annexation to the City, the subject parcel(s) received City zoning in 2008. At the same time, SPAC approved plans for the construction of a Climate Control Mini Storage development. As construction never materialized, all five subject parcels are still vacant at the present time.

The developer applied for a Property Line Adjustment (PLA-17-172) application to consolidate all five parcels. The application was deemed complete in early January of 2018 and a preliminary acceptance letter was sent to the applicant on January 30, 2018. At the time this staff report was written, the applicant had not returned the plat and, therefore, the lot consolidation has not been finalized. It will be a condition of approval for the applicant to complete PLA-17-172 prior to the issuance of any building permits for vertical construction.

The remainder of this staff report assumes that all five parcels will be consolidated.

Project Summary

The applicant is requesting approval to develop one lot totaling 8.6 acres with a freight transfer terminal. The terminal will be 20,423 square feet in size and will include 42 dock doors. To maneuver trucks and park trailers, approximately 266,000 square feet of paving is proposed in addition to the employee parking lot.

Site Plan

Structure

The proposed freight transfer terminal will take access from Bierson Way via two 36-foot driveways. The proposed structure will be setback approximately 108 feet from Bierson Way and be situated in the middle between the two proposed driveways. Approximately 1/5 of the structure will be used for offices, breakrooms, restrooms, etc. with the rest of the building housing the actual freight terminal.

Vehicle and Bicycle Parking

The parking will be located between the proposed structure and Bierson Way. It will include 56 total spaces; three of those will be handicap accessible and three will be labeled as "carpool/vanpool" spaces. In addition, six bicycle parking spaces will be provided in three separate locations: On the southeast side just outside the building and indoors adjacent to the breakroom and restroom.

Driveway Width

The applicant initially intended for both driveways to be 40 feet in width. However, per Medford Land Development Code (MLDC) 10.550 Access Standards, the maximum allowable driveway throat width for industrial parcels is 36 feet. After discussion with the Public Works Department staff, the applicant now agrees for the driveways to be 36 feet wide at a maximum as described in the applicant's revised findings (Exhibit L). It will be a condition of approval for the applicant to revise the site plan showing the updated driveway width prior to issuance of building permits for vertical construction.

Landscaping

The proposed landscaping satisfies the required amount of trees and shrubs along the street frontages of Bierson Way and Bateman Drive per MLDC 10.797. However, the locational criteria listed in MLDC 10.797 is not satisfied: street frontage landscaping elements along Bateman Drive are shown to be located in the public right-of-way instead of the required yard area adjacent to the street unless otherwise approved by the approving authority. It will be a condition of approval for the required landscaping to be moved outside the public right-of-way onto private property along Bateman Drive.

Architecture/Elevations

Per the applicant, the façade of the building is broken up by horizontal reveals and vertical panel joints. The logo is cast into the panel with a canopy underneath it. This accentuates the entry. Behind the front façade of the building are the docks which are recessed behind the façade, blocking the view of the actual dock. The overhang provides an architectural treatment differentiating the docks from the office.

The building entrance and public parking are located as close to the public road as possible and we have several connections through the site for pedestrian traffic. We take very seriously the truck traffic and pedestrian safety. We make sure that we only cross these lanes when absolutely necessary.

Other Agency Comments

Rogue Valley International Airport (Exhibit T) Requests an Avigation, Noise and Hazard Easement to be required as part of the permit process. In the 2010 LUBA decision on Michelle Barnes vs. City of Hillsboro and the Port of Portland, Nollan/Dolan findings are required to support the request (LUBA No. 2010-011). None were provided; therefore, a condition requiring compliance with the airport's request for an Avigation, Noise and Hazard Easement has not been included.

Final Order

The applicant requested to adopt the final order at the public hearing on April 6, 2018 (Exhibit L).

Committee Comments

No comments were received from a committee, such as BPAC.

No other issues were identified by staff.

FINDINGS OF FACT

MLDC 10.290

1. The proposed development is compatible with uses and development that exist on adjacent land;

The Commission can find that there is sufficient evidence contained in the Applicant's Questionnaire and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the conditions of approval contained in Exhibit A. This criterion is satisfied.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit K & L) and recommends the Commission adopt the findings as presented.

RECOMMENDED ACTION

Adopt the findings as recommended by staff and adopt the final order for approval of AC-18-016 per the staff report dated March 28, 2018, including Exhibits A through Y.

EXHIBITS

- A Conditions of Approval, dated March 28, 2018
- B Site Plans A101, A102 & A103, received February 9, 2018
- C Pavement Plan A104, received February 9, 2018
- D Site Details A105 & A106, received February 9, 2018
- E Floor Plan A201 to A207, received February 9, 2018
- F Elevations A301 to A304, received February 9, 2018
- G Roof Plan A401 to A405, received February 9, 2018

- H Existing Conditions C-1, Grading Plan C-2, Utility Plan C-3, Detail C-4, Underground Detention Details C-5, Stormwater Pollution Prevention Plan C-6 & SWPPP Details C-7, received February 9, 2018
- I Electrical Plans E0.00 to 1.50, received February 9, 2018
- J Landscape Plan L1, received February 9, 2018
- K Applicant's Findings, received February 9, 2018
- L Applicant's Revised Findings, received March 20, 2018
- M Applicant's Code Compliance: Criterion No. 2, received February 9, 2018
- N Public Works Staff Report, received March 14, 2018
- O Fire Department Report, received March 14, 2018
- P Medford Water Commission Report, received March 14, 2018
- Q Rogue Valley Sewer Services Report, received March 2, 2018
- R Jackson County Roads Report, received March 5, 2018
- S Oregon Department of Aviation Report, received March 13, 2018
- T Rogue Valley International Airport E-Mail, received March 9, 2018
- U Legal Description, received February 9, 2018
- V City Surveyor Report, received February 26, 2018
- W Property Line Adjustment PLA-17-172 preliminary letter of approval & tentative plat, received December 29, 2017
- X Drainage Report (Cover & Summary), received February 9, 2018
- Y Geotechnical Engineering Report (Cover), received February 9, 2018
Vicinity map

SITE PLAN AND ARCHITECTURAL REVIEW COMMISSION AGENDA:

APRIL 6, 2018

EXHIBIT A

Old Dominion Freight Line
AC-18-016
Conditions of Approval
March 28, 2018

DISCRETIONARY CONDITIONS

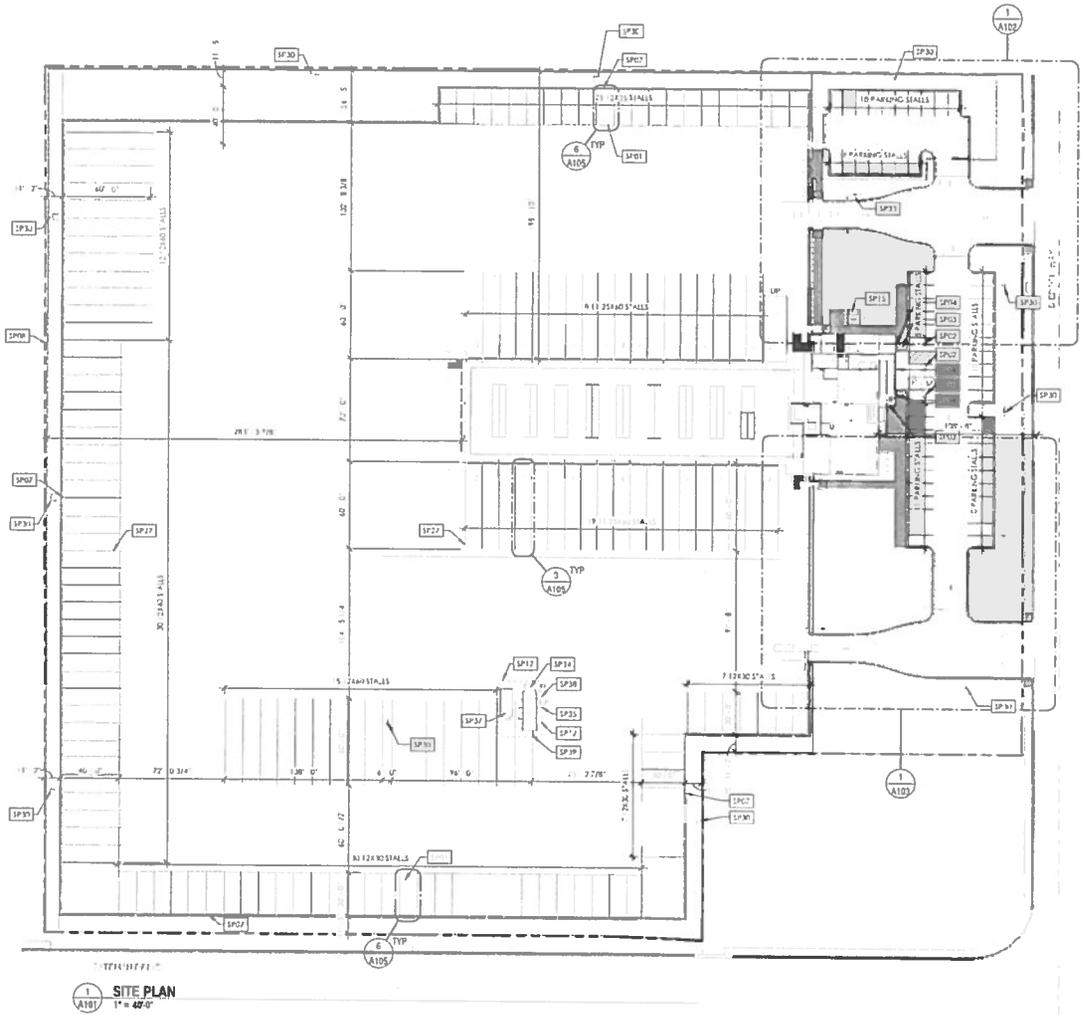
Prior to issuance of a building permit for vertical construction, the applicant shall:

1. Finalize lot consolidation for Property Line Adjustment PLA-17-172.

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

2. Comply with all requirements per MLDC 10.797 Street Frontage Landscaping.
3. Comply with all conditions stipulated by the Public Works Department (Exhibit N).
4. Comply with all conditions stipulated by the Fire Department (Exhibit O).
5. Comply with all conditions stipulated by the Medford Water Commission (Exhibit P).
6. Comply with all conditions stipulated by Rogue Valley Sewer Services (Exhibit Q).
7. Comply with all comments provided by Jackson County Roads (Exhibit R).
8. Comply with all requirements submitted by the Oregon Department of Aviation (Exhibit S).



SITE PLAN
1" = 40' 0"

LETMOIL LEGEND

Tag Number	Explanation
SP01	STANDARD 'V' C' PARKING STALL - SEE DETAIL SHEETS
SP02	VAH ALL ESSE PARKING STALL
SP03	ACCESSIBLE CURB RAMP (A1A-C12-A105)
SP04	ACCESSIBLE DRIVE
SP05	C CONC. CURB AND GUTTER
SP06	B CHAIRBACK RAIL W/ 1 1/2" X 2" BARRED WIRE SEE DETAIL SHEETS
SP07	SCHEMATIC CONC. RCH. ART. SEE DETAIL SHEETS
SP08	BRIDGE PAVT. P.A.S.
SP09	PAVEMENT FINISH TO BE PROVIDED WITH ASPHALT CONCRETE (BRAND) C. MAKE THE FINISH PREVIOUS SUBMITTAL FOR APPROVAL.
SP10	SMOOTH FINISH SEE DETAIL SHEETS
SP11	SEE DETAIL SHEETS
SP12	PAVEMENT FINISH TO BE PROVIDED WITH ASPHALT CONCRETE (BRAND) C. MAKE THE FINISH PREVIOUS SUBMITTAL FOR APPROVAL.
SP13	SMOOTH FINISH SEE DETAIL SHEETS
SP14	SEE DETAIL SHEETS
SP15	PAVEMENT FINISH TO BE PROVIDED WITH ASPHALT CONCRETE (BRAND) C. MAKE THE FINISH PREVIOUS SUBMITTAL FOR APPROVAL.
SP16	SEE DETAIL SHEETS
SP17	SMOOTH FINISH SEE DETAIL SHEETS
SP18	SEE DETAIL SHEETS
SP19	SMOOTH FINISH SEE DETAIL SHEETS
SP20	SEE DETAIL SHEETS
SP21	SMOOTH FINISH SEE DETAIL SHEETS
SP22	SEE DETAIL SHEETS
SP23	SMOOTH FINISH SEE DETAIL SHEETS
SP24	SEE DETAIL SHEETS
SP25	SMOOTH FINISH SEE DETAIL SHEETS
SP26	SEE DETAIL SHEETS
SP27	SMOOTH FINISH SEE DETAIL SHEETS
SP28	SEE DETAIL SHEETS
SP29	SMOOTH FINISH SEE DETAIL SHEETS
SP30	SEE DETAIL SHEETS

PARE AND REQUIREMENTS

CH C	4.501	SGH	18	(1/2)50
WAREHOUSING	16.372	SGH	25	(1/2)50 - PARKING FOR COMPANY VEHICLES
TOTAL REQUIRED	46			
TOTAL PROVIDED	56	(SLOTS OF UNUSUAL FINISH AREA) (INCLUDING A1A)		
	3	ADA		

NOTES:
1. ALL AREA CALCULATIONS ARE APPROXIMATE AND CAN CHANGE DUE TO CONSTRUCTION DIFFERENCES.
2. SEE CIVIL DRAWINGS FOR LOTS 1 THROUGH 4 1/200927 4 1/200928 0 5447 BIERSON WAY, MEDFORD, OR

SITE AREAS

Item	Area
TO 3000	2378 SF
BUILDING FOOTPRINT	76040 SF
CORNER REF. AREA	18729 SF
CORNER REF. AREA	12281 SF
GRAVEL	4896 SF
LANDSCAPE	14055 SF
CONCRETE	32248 SF

TOTAL AREA		
Number	Area	Notes
TOT	324739 SF	Net



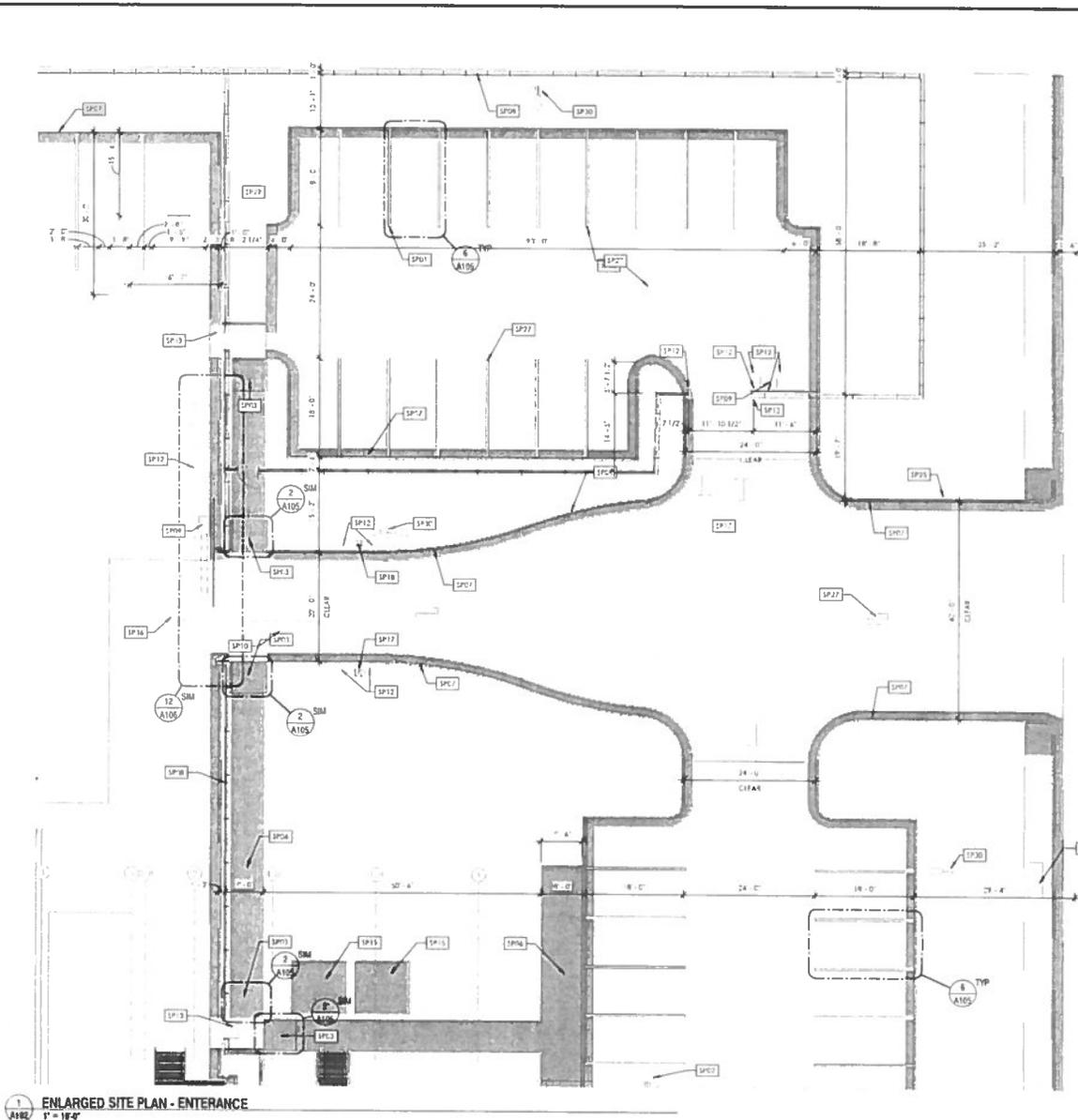
aeurbia
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SALT LAKE CITY, UTAH 84119
PHONE: 801.746.0456 FAX: 801.575.6456
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STATE OF OREGON
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OLD DOMINION Medford
5449 BIERSON WAY, MEDFORD, OREGON

PROJECT NO. 18-016
DATE: 02/02/2018

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ENLARGED SITE PLAN - ENTRANCE
1" = 1/8"

Item Number	Description
SP1	STANDARD 5'0" PARKING STALL, SEE DETAIL 02A-105
SP2	VAH ACCESSIBLE PARKING STALL
SP3	VAH ACCESSIBLE PARKING STALL
SP4	6' LONG, 3' WIDE, 2' HIGH, 2' DEEP
SP5	6' LONG, 3' WIDE, 2' HIGH, 2' DEEP
SP6	6' LONG, 3' WIDE, 2' HIGH, 2' DEEP
SP7	6' LONG, 3' WIDE, 2' HIGH, 2' DEEP
SP8	3" CURB AND GUTTER
SP9	3" CURB AND GUTTER
SP10	3" CURB AND GUTTER
SP11	3" CURB AND GUTTER
SP12	3" CURB AND GUTTER
SP13	3" CURB AND GUTTER
SP14	3" CURB AND GUTTER
SP15	3" CURB AND GUTTER
SP16	3" CURB AND GUTTER
SP17	3" CURB AND GUTTER
SP18	3" CURB AND GUTTER
SP19	3" CURB AND GUTTER
SP20	3" CURB AND GUTTER
SP21	3" CURB AND GUTTER
SP22	3" CURB AND GUTTER
SP23	3" CURB AND GUTTER
SP24	3" CURB AND GUTTER

Item	Area
BIERSON'S CURB CUT	2775 SF
CONCRETE DRIVEWAY	2485 SF
ASPHALT DRIVEWAY	1472 SF
ASPHALT DRIVEWAY	1220 SF
ASPHALT DRIVEWAY	1472 SF
LANDSCAPE	1403 SF
TOTAL	12467 SF

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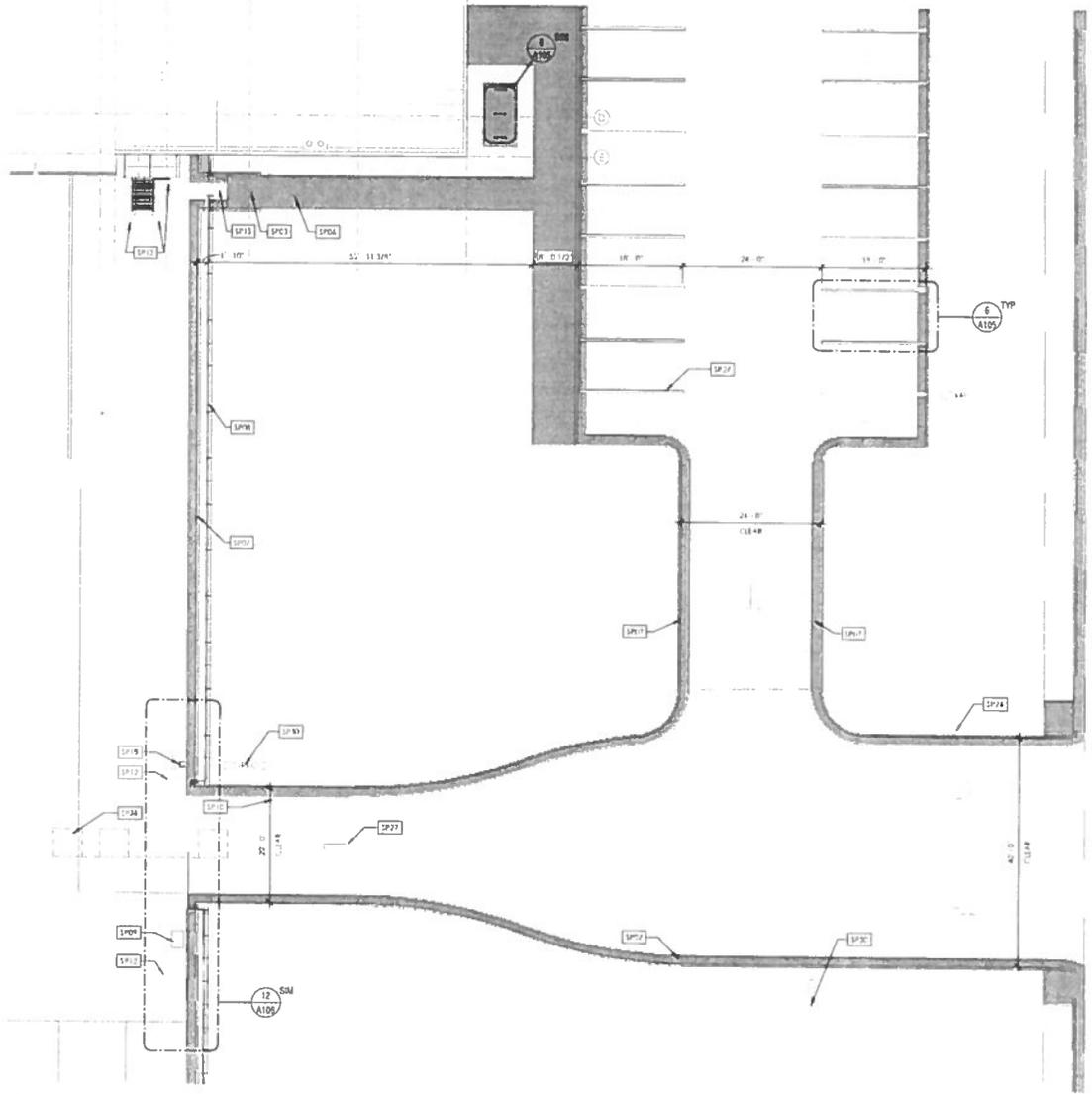
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1 ENLARGED SITE PLAN - EXIT
A103 1" = 10'-0"



Item No.	Description	Remarks
SP1	ACCEPTIBLE CLEARANCE (SEE SPECIFICATIONS)	
SP2	6.00' NO. WALLWAY (MINIMUM 4' EFFICIENCY)	
SP3	CONCRETE CURB AND GUTTER	
SP4	12" CHANGING FENCE WITH 3" X 4" X 8" RAIL AND 1/2" WIRE. SEE SLAB DETAILS.	
SP5	GRADE MOTOR LOCATIONS PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP6	AT ALL TURNAROUNDS, SHEET PILE WALLS SHALL PROVIDE (10) 12" DIAMETER CONCRETE COLUMNS WITH 4" X 4" REINFORCING BARS AT 4' ON CENTER. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP7	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP8	PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP9	PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP10	PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP11	PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP12	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP13	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP14	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
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SP24	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	
SP25	CONCRETE WALLS SHALL BE 12" THICK AND 8' HIGH. PROVIDE SIGNAGE AND PPE REQUIREMENTS SEE DETAILS FOR SIGNAGE.	

SITE AREAS	
Area	Area
BUILDING FOOTPRINT	27,741 SQ FT
CONCRETE DRIVEWAY	2,800 SQ FT
CLEARANCE DRIVEWAY	4,779 SQ FT
CLEARANCE DRIVEWAY	2,294 SQ FT
CLEARANCE DRIVEWAY	4,455 SQ FT
CLEARANCE DRIVEWAY	2,455 SQ FT
TOTAL	74,554 SQ FT

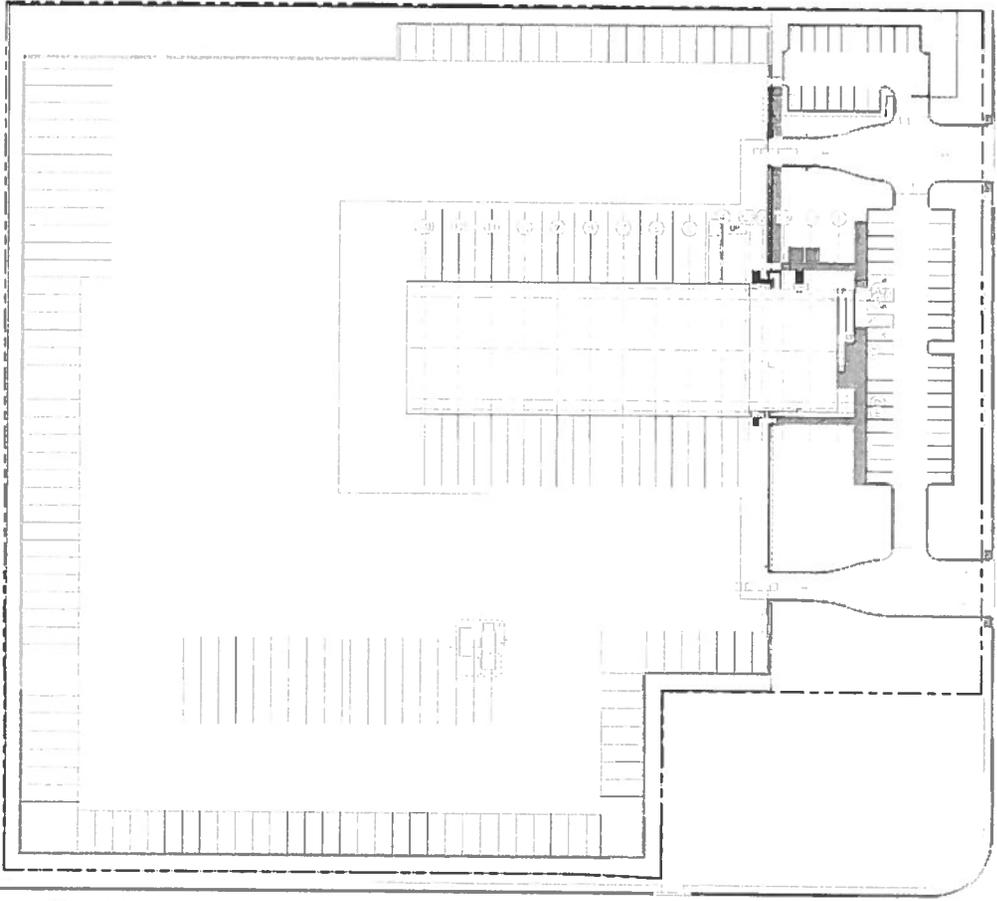
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5449 BIERSON WAY, MEDFORD, OREGON



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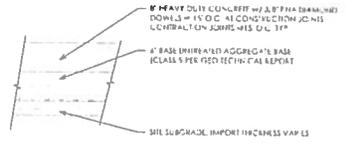


1 PAVING PLAN
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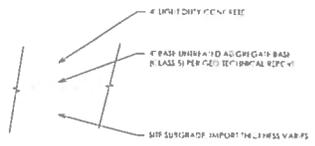
NOTE:
1. ALL AREA CALCULATIONS ARE APPROXIMATE AND CAN CHANGE DUE TO CHANGES IN THE PLAN.

PAVEMENT AREAS	
CONCRETE DRIVEWAYS	28,645 sq. ft.
CONCRETE SIDEWALKS	12,229 sq. ft.
CONCRETE DRIVE	12,281 sq. ft.
TOTAL	53,155 sq. ft.

HEAVY DUTY CONCRETE



LIGHT DUTY CONCRETE



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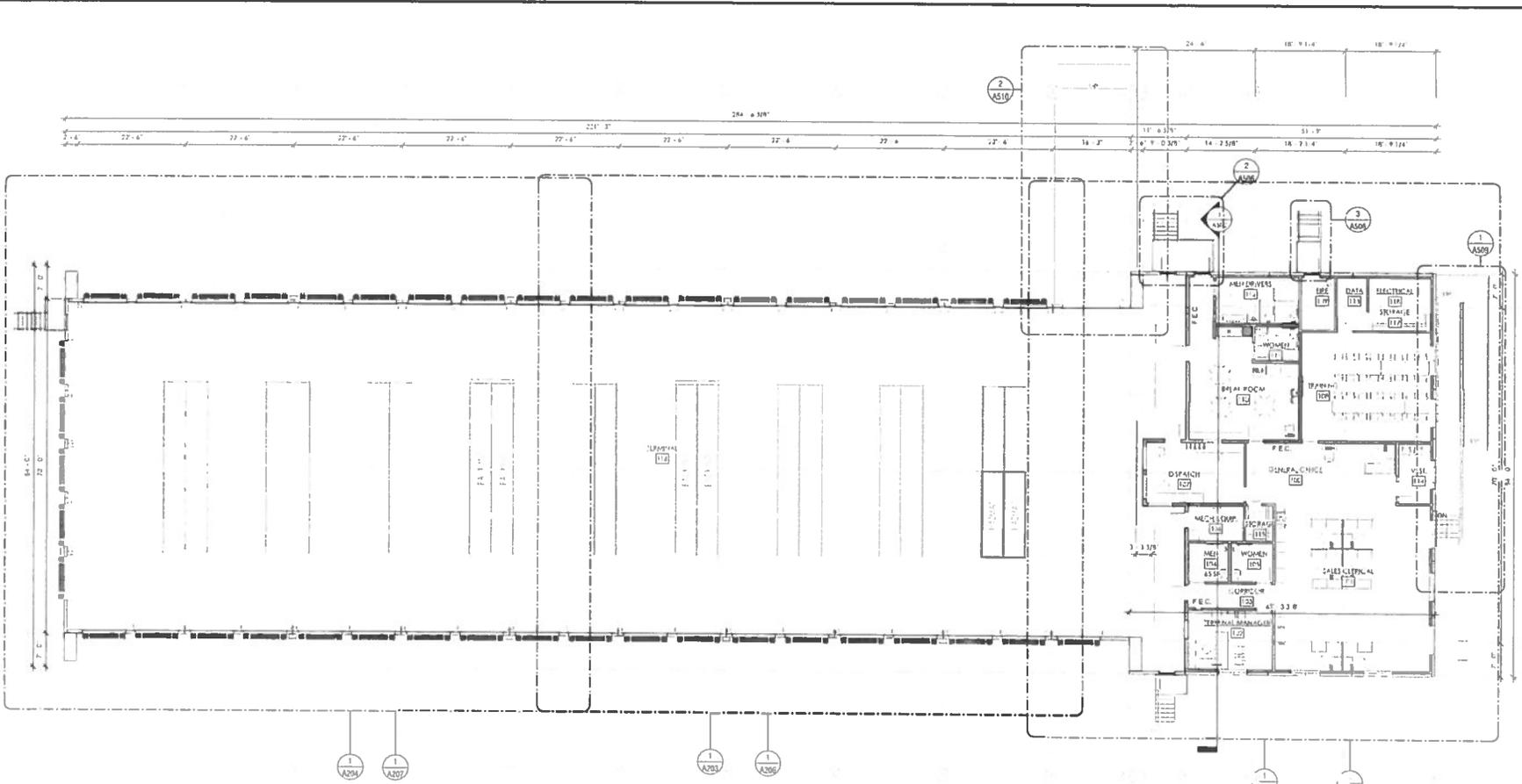
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File # AC-18-016



1 FINISH FLOOR
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architects and engineers
2875 south doctor lake drive, suite 275
astoria city, oregon 97103
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STATE OF OREGON
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OLD DOMINION Medford
5449 BIERSON WAY, MEDFORD, OREGON

Project: Old Dominion
SHEET: E-01

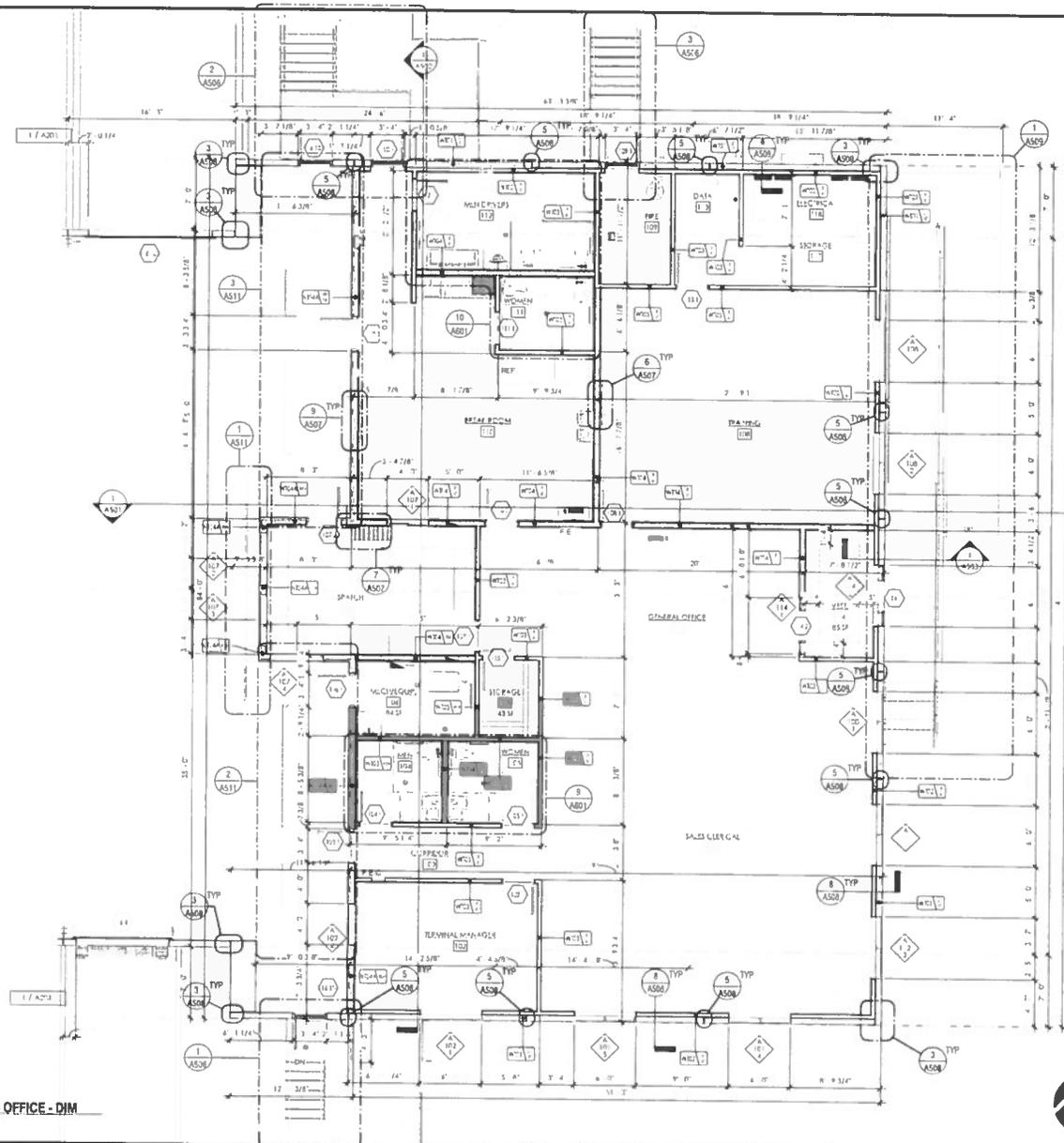
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FLOOR PLAN -
OVERALL
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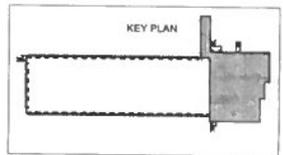
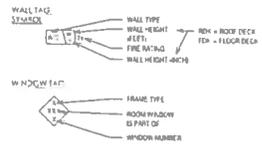
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EXHIBIT #

FLOOR PLAN - ENLARGED - OFFICE - DIM



Symbol	Description
WALL TAG	WALL TYPE
WALL HEIGHT	WALL HEIGHT
WALL FINISH	WALL FINISH
WALL HEIGHT - 60"	WALL HEIGHT - 60"
WINDOW TAG	FRAME TYPE
ROOM WINDOW IS PART OF	ROOM WINDOW IS PART OF
WINDOW NUMBER	WINDOW NUMBER



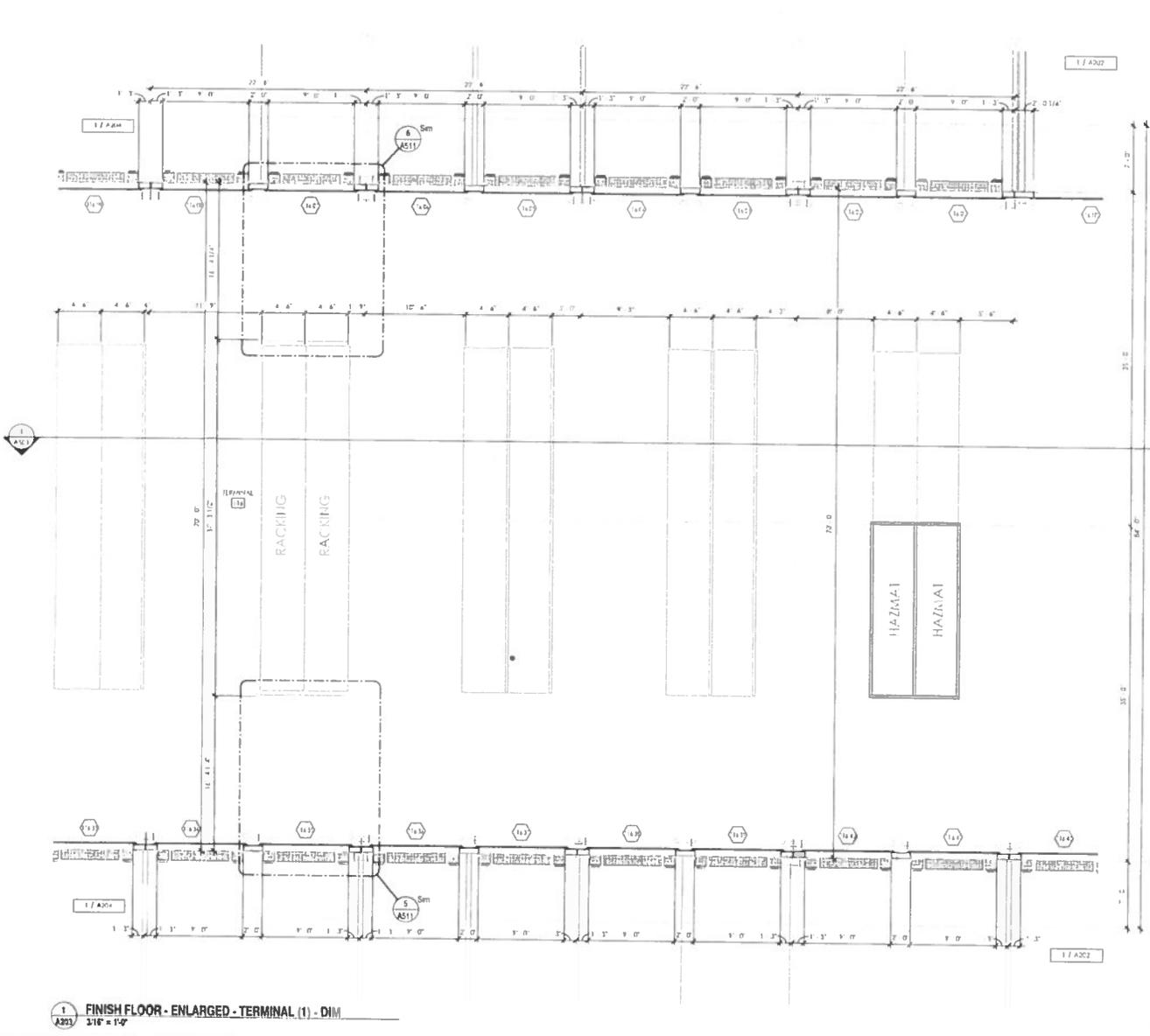
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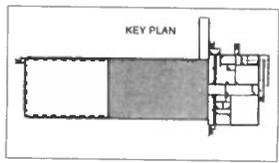
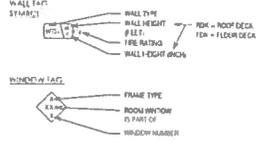
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Wall Schedule	
Type Area	Description
W01	7 1/2" CONCRETE F.L.P.
W02	REFLECTOR H.L.P. OVER WALL 1.5" METAL SB. 2" W/ SUP. STYP OVER SIDE
W03	REFLECTOR 3" SUP. METAL SB. 2" W/ SUP. STYP OVER BOTH SIDES
W04	REFLECTOR 4" METAL STYP PARTITION WALL W/ 6" SUP. STYP OVER SIDES
W05A	6" METAL STYP WITH ARCHIT. COR. METAL PANEL SYSTEM



1 FINISH FLOOR - ENLARGED - TERMINAL (1) - DIM
3/16" = 1'-0"

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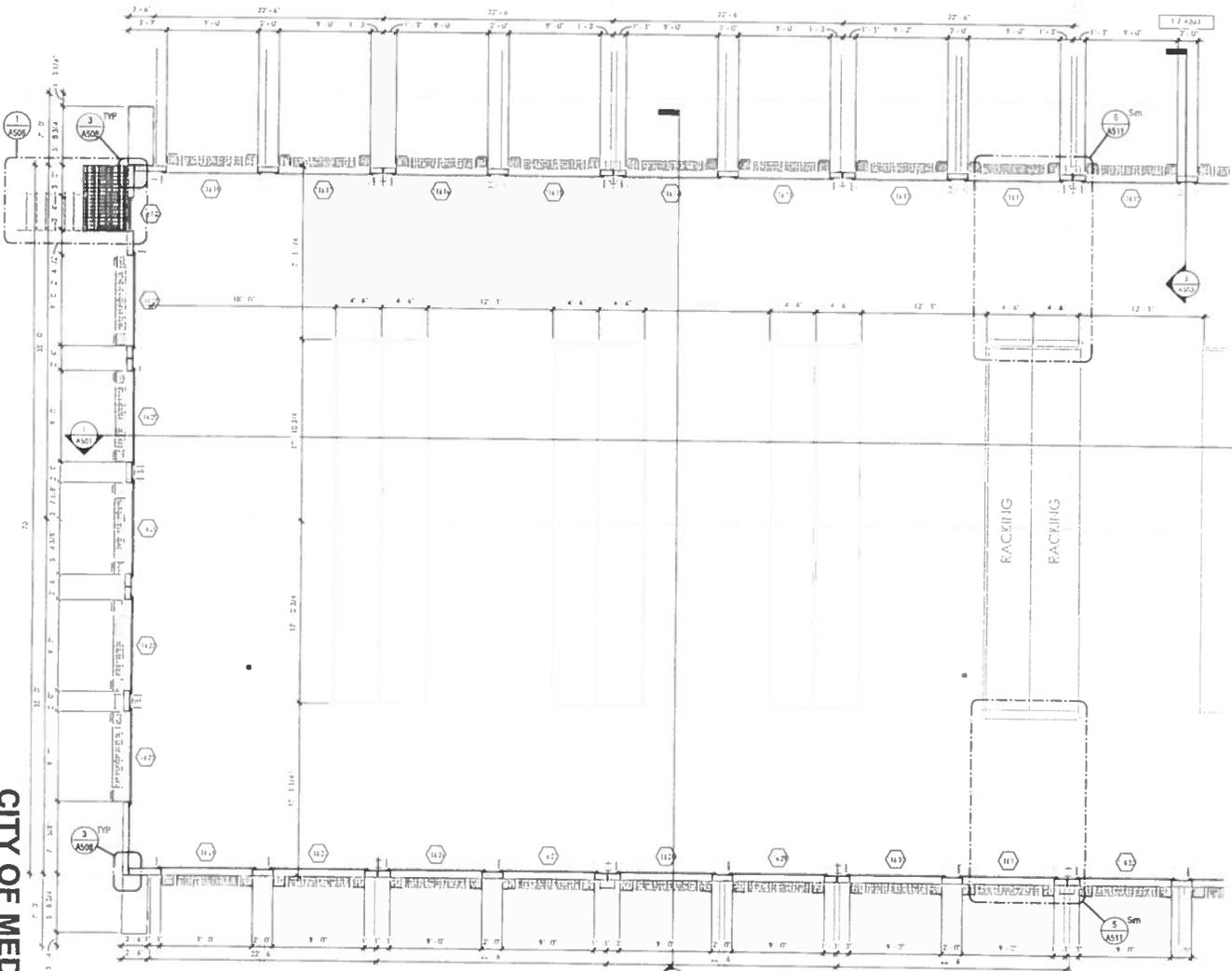
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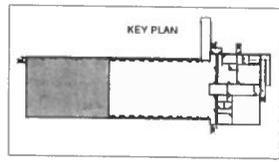
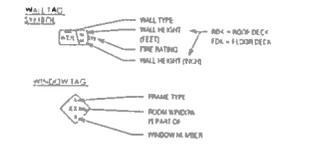
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EXHIBIT #



1 A201 FINISH FLOOR - ENLARGED - TERMINAL - DIM 3/16" = 1'-0"

Wall Schedule	
WALL	2 1/2" CONCRETE BLOCK
WALL	WOODEN OR FLOOR CL. T. WALL 3.5" METAL STUD W/ 5/8" GYP. LIN. 5/8"
WALL	INTERIOR 3 1/2" METAL STUD W/ 5/8" GYP. LIN. 5/8" SHEET
WALL	INTERIOR 2 1/2" METAL STUD W/ 5/8" GYP. LIN. 5/8" SHEET
WALL	CONCRETE 8" WALL WITH 2" CORE PC. W/ 1" METAL STUD W/ 5/8" GYP. LIN. 5/8"
WALL	CONCRETE 8" WALL WITH 2" CORE PC. W/ 1" METAL STUD W/ 5/8" GYP. LIN. 5/8"



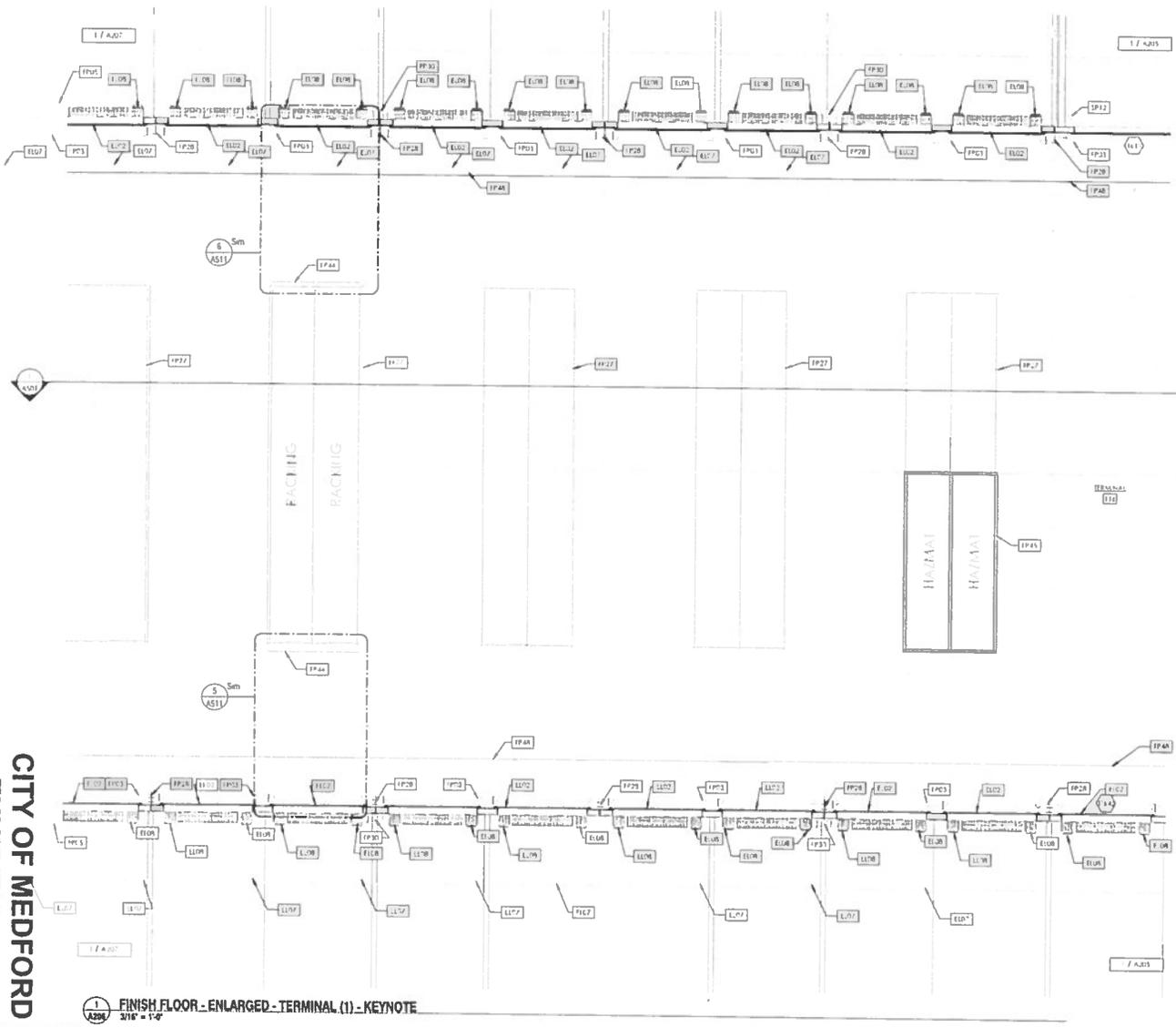
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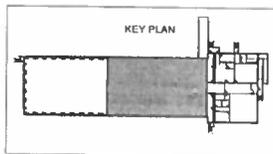
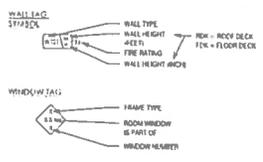
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1 FINISH FLOOR - ENLARGED - TERMINAL (1) - KEYNOTE
3/18 - 1/4"

Wall Schedule	
Code	Description
W001	7" x 12" CONCRETE BLOCK
W002	INLET OF 1/2" DIA. WALL 3/4" MIN. INSULATION WITH 1/2" GYP CHG. S&G
W003	INTERIOR 1/2" PARTIAL GYPSUM WALL WITH GYP LINENHANGERS
W004	INTERIOR 1/2" METAL STUD PARTIAL WALL WITH 5/8" GYP. LINENHANGERS
W004A	INTERNAL STUD WITH ACROUSTICAL INSULATION SYSTEM

Element Schedule	
Code	Description
EP102	INTERIOR DOOR
EP102	DOOR BY DOCK BUNNIPS
EP103	DOCK BUNNIPS
EP104	INTERIOR WINDOW 4' x 7' 6" ALLIG. SEE DETAIL 11/4/14
EP105	7/8" x 1/2" INSULATED GLAZED UNIT (IGU) WITH 1/2" AIR SPACE
EP106	4" POLYURETHANE FOAM INSULATION WITH 1/2" GYP. LINENHANGERS
EP107	DOCK LAMINATE SEE DETAIL 11/4/14
EP108	INTERIOR PARTIAL WALL 3/4" MIN. INSULATION WITH 1/2" GYP. LINENHANGERS
EP109	4" POLYURETHANE FOAM INSULATION WITH 1/2" GYP. LINENHANGERS
EP110	4" POLYURETHANE FOAM INSULATION WITH 1/2" GYP. LINENHANGERS
EP111	4" POLYURETHANE FOAM INSULATION WITH 1/2" GYP. LINENHANGERS
EP112	4" POLYURETHANE FOAM INSULATION WITH 1/2" GYP. LINENHANGERS



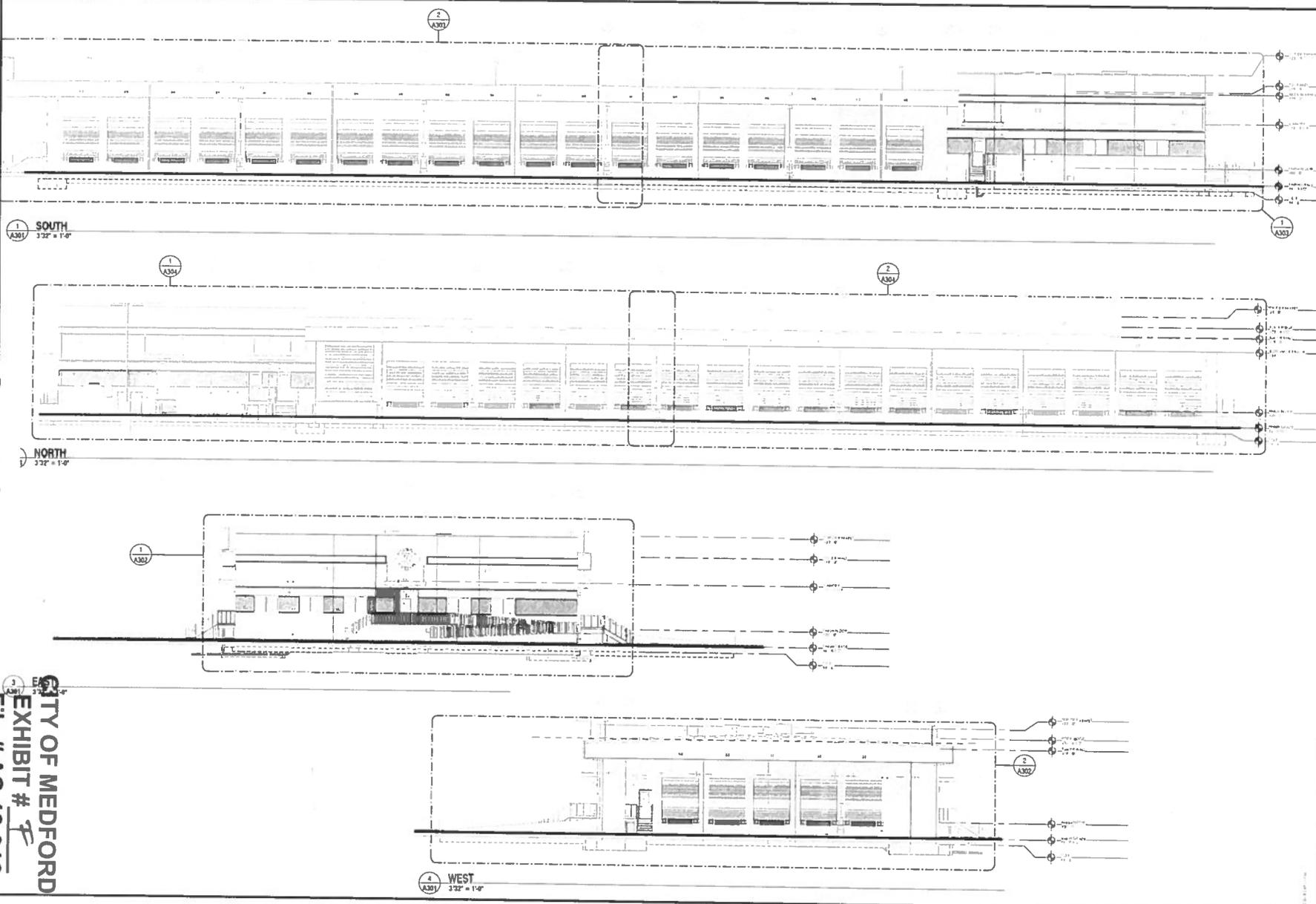
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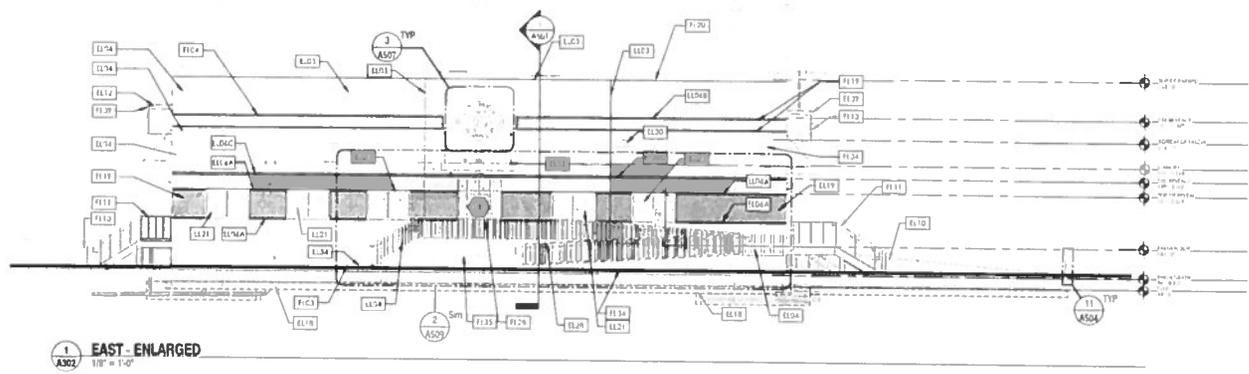
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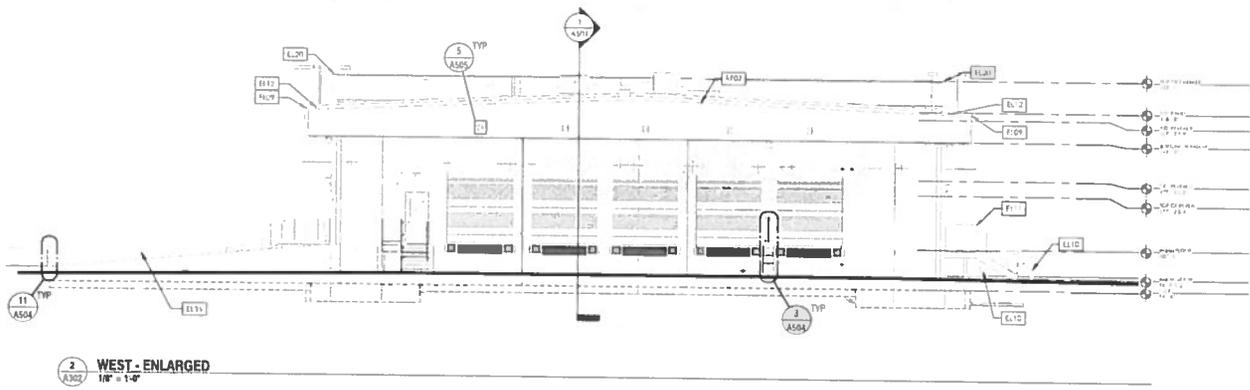
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1 EAST - ENLARGED
1/8" = 1'-0"



2 WEST - ENLARGED
1/8" = 1'-0"

Key Symbol	Keynote Text
ELC1	PANTRY CABINET SUPPORT BRACKETS
ELC2	CONCRETE TIE-ROD PANELS PER 1707 C. MINIMAL GROUT PER 1707 REVISED - REASONING - ADD TIE-ROD (SEE NOTE #1)
ELC3	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC4	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC5	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC6	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC7	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC8	CONCRETE PANEL REVEAL - SEE DETAIL #1000
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ELC10	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC11	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC12	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC13	CONCRETE PANEL REVEAL - SEE DETAIL #1000
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ELC38	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC39	CONCRETE PANEL REVEAL - SEE DETAIL #1000
ELC40	CONCRETE PANEL REVEAL - SEE DETAIL #1000

- NOTES**
1. ALL EXPOSED WALL FLASHING TO BE GALVANIZED AND PAINTED CANNAS LEFT TO MATCH PANELS
 2. EXTERIOR PAINT TO BE APPLIED TO ALL EXPOSED SURFACES. EXTERIOR PAINT TO BE APPLIED TO ALL EXPOSED SURFACES. EXTERIOR PAINT TO BE APPLIED TO ALL EXPOSED SURFACES. EXTERIOR PAINT TO BE APPLIED TO ALL EXPOSED SURFACES.
 3. PROVIDE PAINT DRAIN DOWNS FOR ALL EXPOSED SURFACES AND FLASHING.
 4. ALL CONCRETE WALLS TO BE BACK AND PAINTED BEFORE FINISHING.

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Key Value	Description
LL03	PANEL ABOVE SEE DETAIL S. A500
LL04	COGNIC. DEEP PANELS PAINTED TO MATCH EXISTING INTERIOR WALLS. SEE DETAIL S. A500
LL05	COGNIC. PANEL REVEAL SEE DETAIL S. A503
LL06	COGNIC. PANEL REVEAL SEE DETAIL S. A503
LL07	COGNIC. PANEL REVEAL SEE DETAIL S. A503
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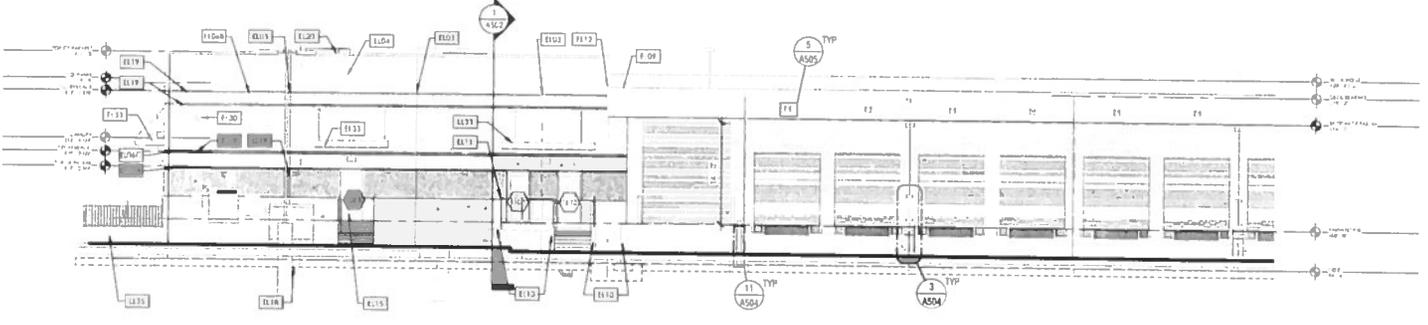
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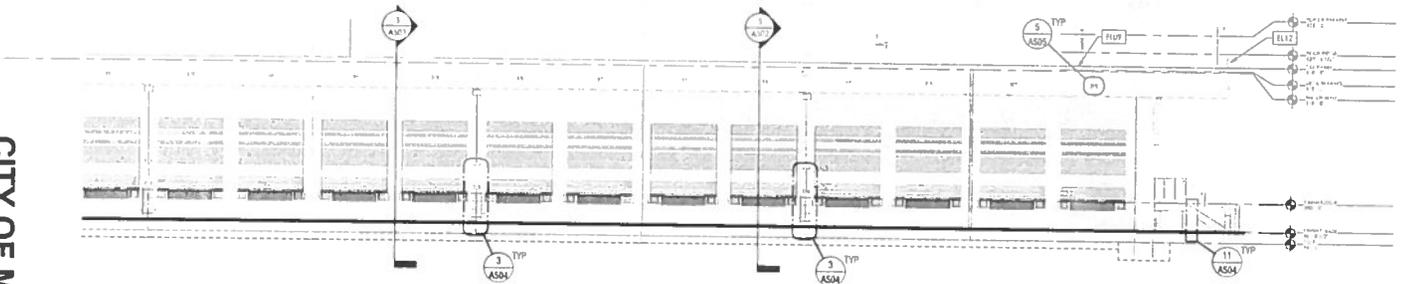
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1 NORTH - ENLARGED - OFFICE
1/8" = 1'-0"



2 NORTH - ENLARGED - TERMINAL
1/8" = 1'-0"

- NOTES**
- ALL EXPOSED WALL FINISHING TO BE GALVANNEED AND PAINTED TO MATCH EXISTING WALLS.
 - EXTERIOR PAINT SYSTEM TO BE AS SHOWN UNLESS NOTED OTHERWISE. MATCH EXISTING PAINT SYSTEM AS SHOWN UNLESS OTHERWISE NOTED.
 - PROVIDE PAINT DRAWING DOWN FOR ALL EXPOSED COGNIC. WALLS.
 - ALL EXPOSED WALLS TO BE SALT AND PATCHED BEFORE PAINTING.

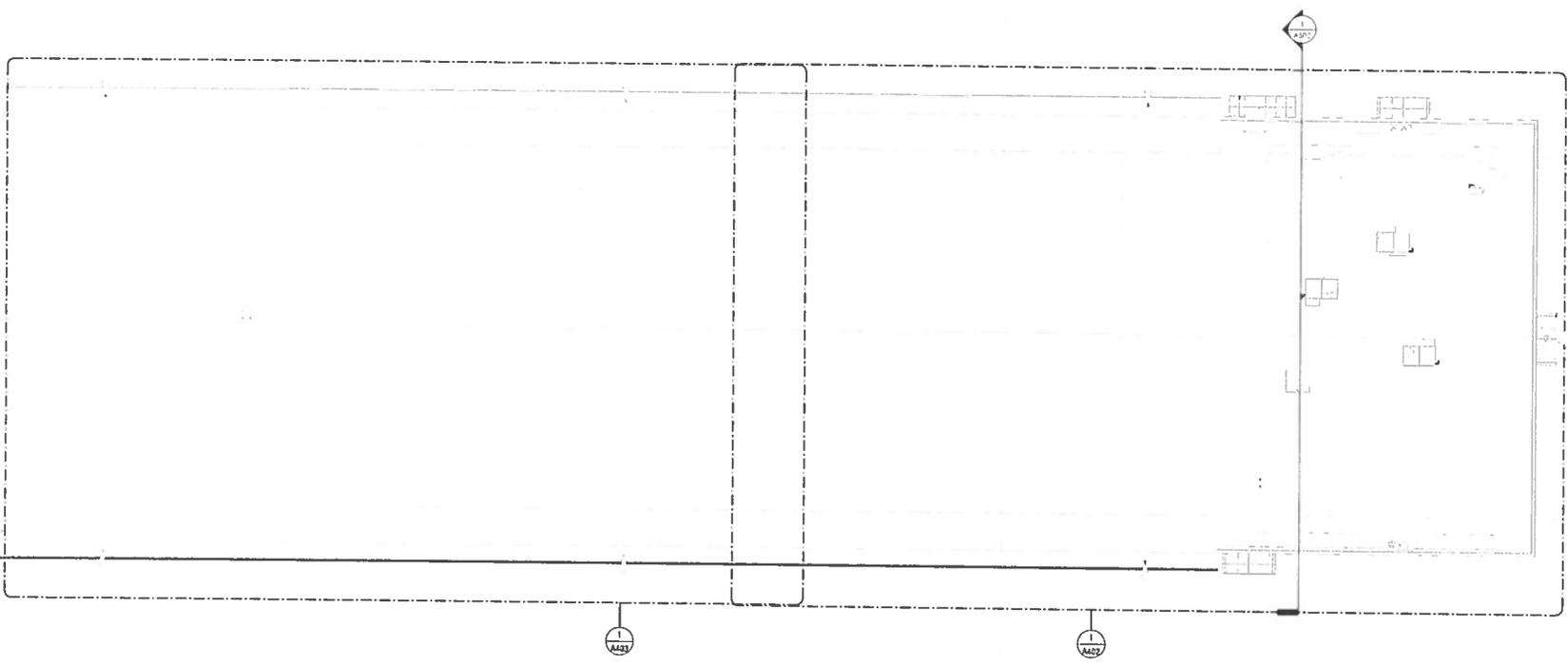
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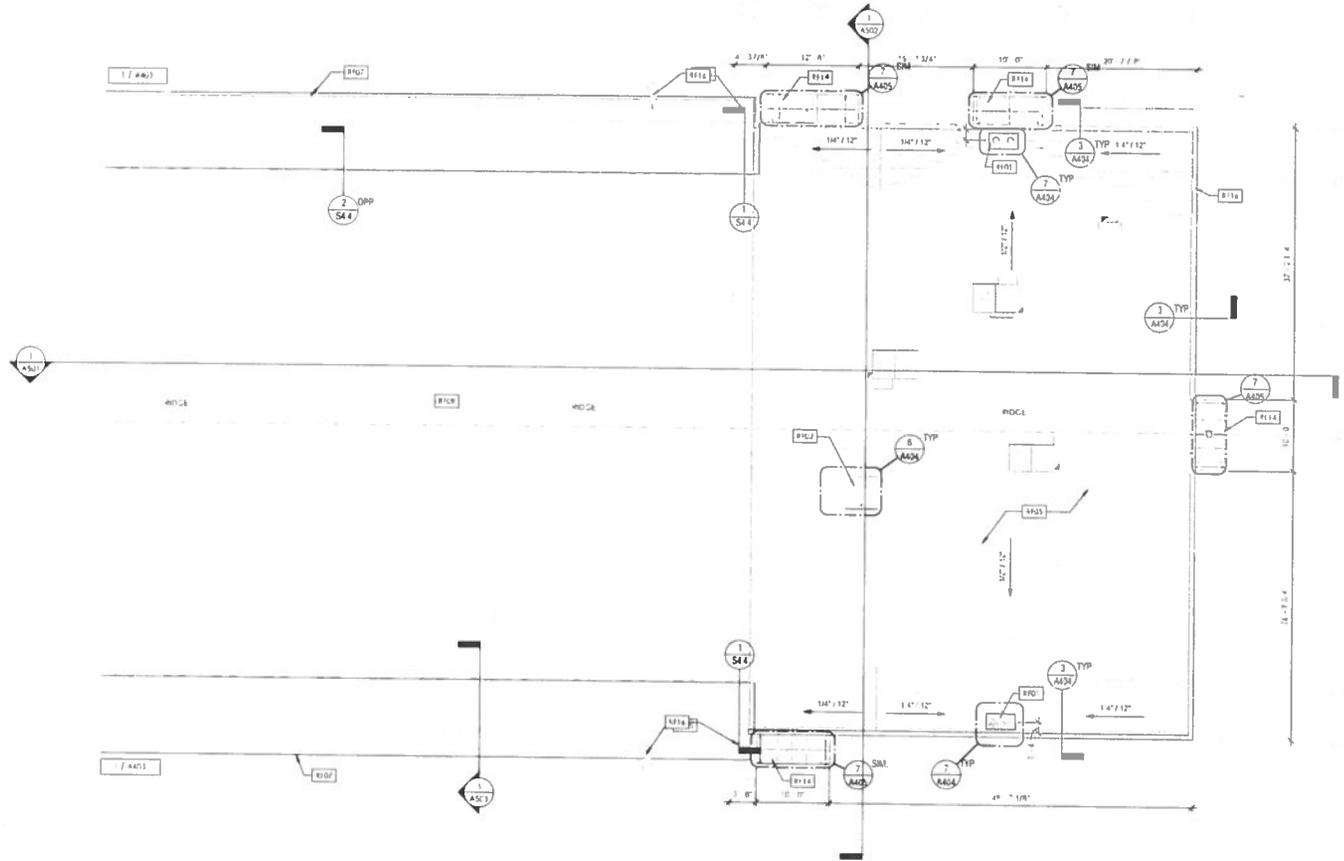
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1 ROOF PLAN - OVERALL
 2/2/18 = 1/4"





1 A402 ROOF PLAN - ENLARGED - OFFICE
1/8" = 1'-0"



Key Symbol	Description
R101	ROOF TRAP/W/ VENT/W/ W/ FRAM. SEE P. LMS 104.
R102	ROOF ACCESS HATCH. MAX. 18 SQ. FT.
R103	SINGLE PLY FLASHING. THE EXT. APPROX. EQUAL TO THE UPDRA ROOF. IN THE FIELD AND FLASHING. NUMBER OVER 25 INCH FROM PERIOD OF DIAL. (TYP. ALL PLS MARKED). DIMENSIONS FOR CONSTRUCTION AND SPEC. OF CARBONS. 20' YEAR WARRANTY MIN.
R107	MEAL SERVICE DECK. 1.00" MIN. DEPTH BRIDGE. PROVIDE SUBMITTALS FOR APPROVAL.
R115	TERRAZZO. APPROX. LIGHT TO BE PLACED 2' FROM FROM TERRAZZO. SEE SPECIFICATIONS.
R116	WALL FLASHING - CONCRETE. 24" MIN. FROM WALL. PROVIDE SUBMITTALS FOR APPROVAL.

ROOFING NOTES (TYPICAL):

1. PROVIDE FULL MANUFACTURER'S WARRANTY. VERIFY ALL MATERIALS MEET MANUFACTURER'S REQUIREMENTS. OBTAIN APPROVAL OF OWNER AND DESIGN PROFESSIONAL.
2. ALL COLORS, ASSEMBLIES AND WATER ALL AS SET. APPROVAL OF OWNER AND DESIGN PROFESSIONAL.
3. PROVIDE PRODUCT SUBMITTALS FOR PPE APPROVAL PRIOR TO ORDERING AND INSTALLATION.
4. INITIAL ASHON OF ALL ROOFING, FLASHINGS ETC. TO BE IMPROVED DURING INSTALLATION.
5. PROVIDE STEPPED FLASHING ON ALL WALLS AND ANGLES AS REQUIRED. CHECK MEASUREMENTS BEFORE ORDERING. USE UP ADJACENT WALLS AND DISCONTINUOUS. TYPICAL.
6. INSTALL ROOF PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS. MANUFACTURER TO IMPROVE INSTALLATION AS REQUIRED BY WARRANTY. PROVIDE 1/4" PER 1" SLOPE MIN. SLOPE.
7. FOR ALL PLUMBING PENETRATIONS REFER TO DEPT'S FOR ANY ADDITIONAL INFORMATION. SEE PLUMBING PLANS.
8. FOR MSC ROOM BEAMS SEE A403.
9. TYPICAL MINIMUM ROOF SLOPE = 1/4" MIN PER FOOT ALL PLACES.
10. SEE MECHANICAL PLANS FOR ROOF MOUNTED EQUIPMENT AND CURBS, ET.

11. HATCH PATTERN FOR HATCH

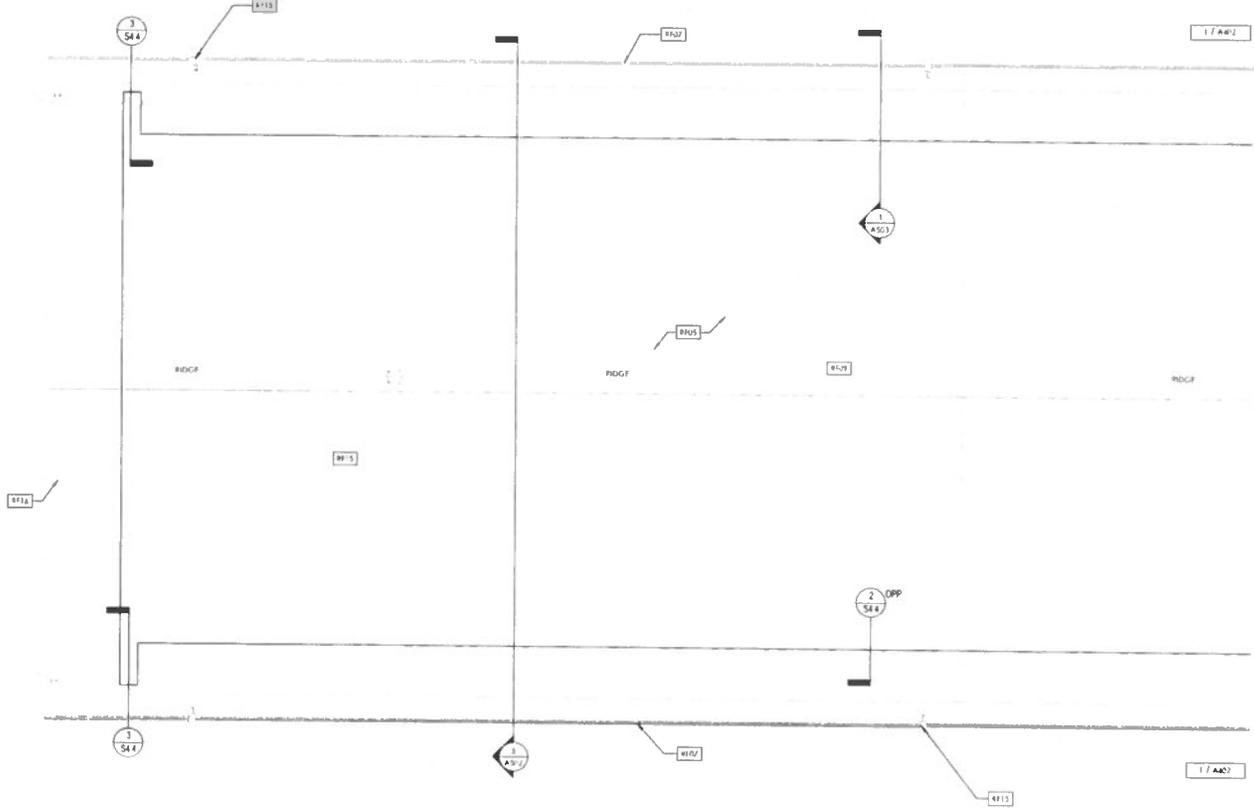
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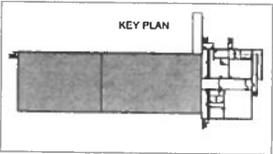
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2018.01.31



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A403 ROOF PLAN - ENLARGED - TERMINAL
1" = 1'-0"



Key Symbol	Description
RF12	SINGLE PLY (TPO) OR CEILING DISCONTINUITY FROM ROOF TO WALL. PROVIDE APPROVED 2" INSULATION POLYESTER FIBER GLASS FLEECE MANUFACTURER'S RECOMMENDATIONS AND COVER WITH 20 YEAR WARRANTY MEM.
RF13	METAL EXHIBIT - ONCE MANUFACTURER PROVIDE DETAILS FOR APPROVAL
RF15	TERRAZZO FINISH TO BE PROVIDED FROM RIDGE TO EXTERIOR WALL
RF16	METAL FLASHING, COLOR MATCHED BRONZE. PROVIDE SUBMIT FOR APPROVAL

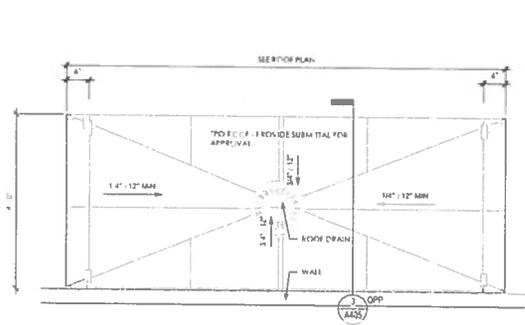
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website: aeurbia.com

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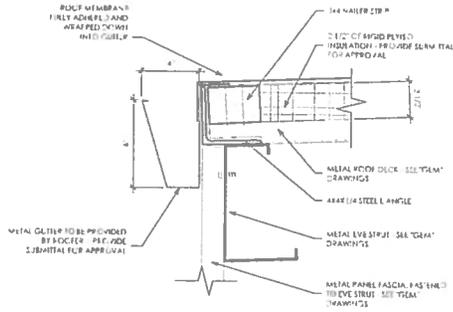
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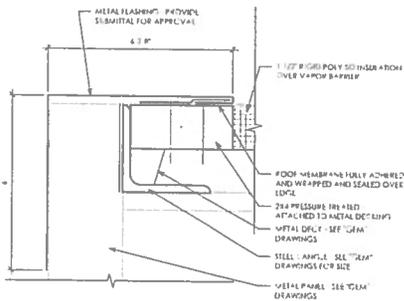
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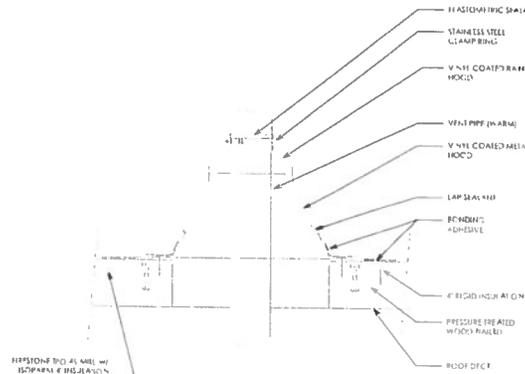
7 ENTRY CANOPY DETAIL
3/4" = 1'-0"



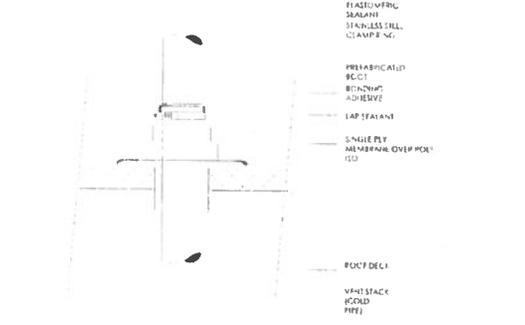
5 ROOF @ GUTTER
1/4" = 1'-0"



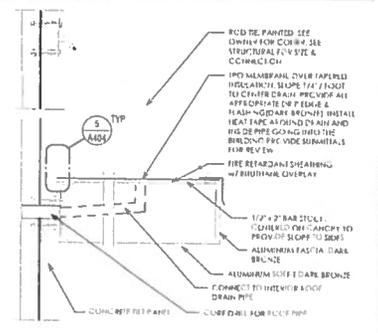
6 ROOF DETAIL @ RAKE
8" = 1'-0"



1 TYP. VENT PIPE (WARM)
3" = 1'-0"



2 TYP. VENT STACK (COLD)
3" = 1'-0"



3 CANOPY CONNECTION DETAIL
1" = 1'-0"

ae urbia
architects and engineers
287 1/2 South Decker Lake Drive, Suite 275
Medford, OR 97504
Phone: 541-746-0456 Fax: 541-746-0459
Webpage: aeurbia.com

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2/2/2018 2:54:03 PM

OLD DOMINION Medford
5449 BIERSON WAY, MEDFORD, OREGON

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PROJECT ENGINEER LB
 102 WEST 100 NORTH
 SALT LAKE CITY, UT 84119
 PHONE (313) 654-1628
 www.legendengr.com



PROJECT CHANGER LB
 DESIGNER LB



DATE: Jan 30, 2018

LEGEND

LOT LINES (PROPERTY)

EXISTING CURB AND GUTTER

PROPOSED CURB AND GUTTER

PROPOSED SPILL CURB AND GUTTER

PROPOSED STORM DRAIN LINE

EXISTING STORM DRAIN LINE

EXISTING POWER

EXISTING FENCE

EXISTING WATER

EXISTING GAS

EXISTING SANITARY SEWER

EXISTING STORM DRAIN

EXISTING GRADE CONTOUR LINES

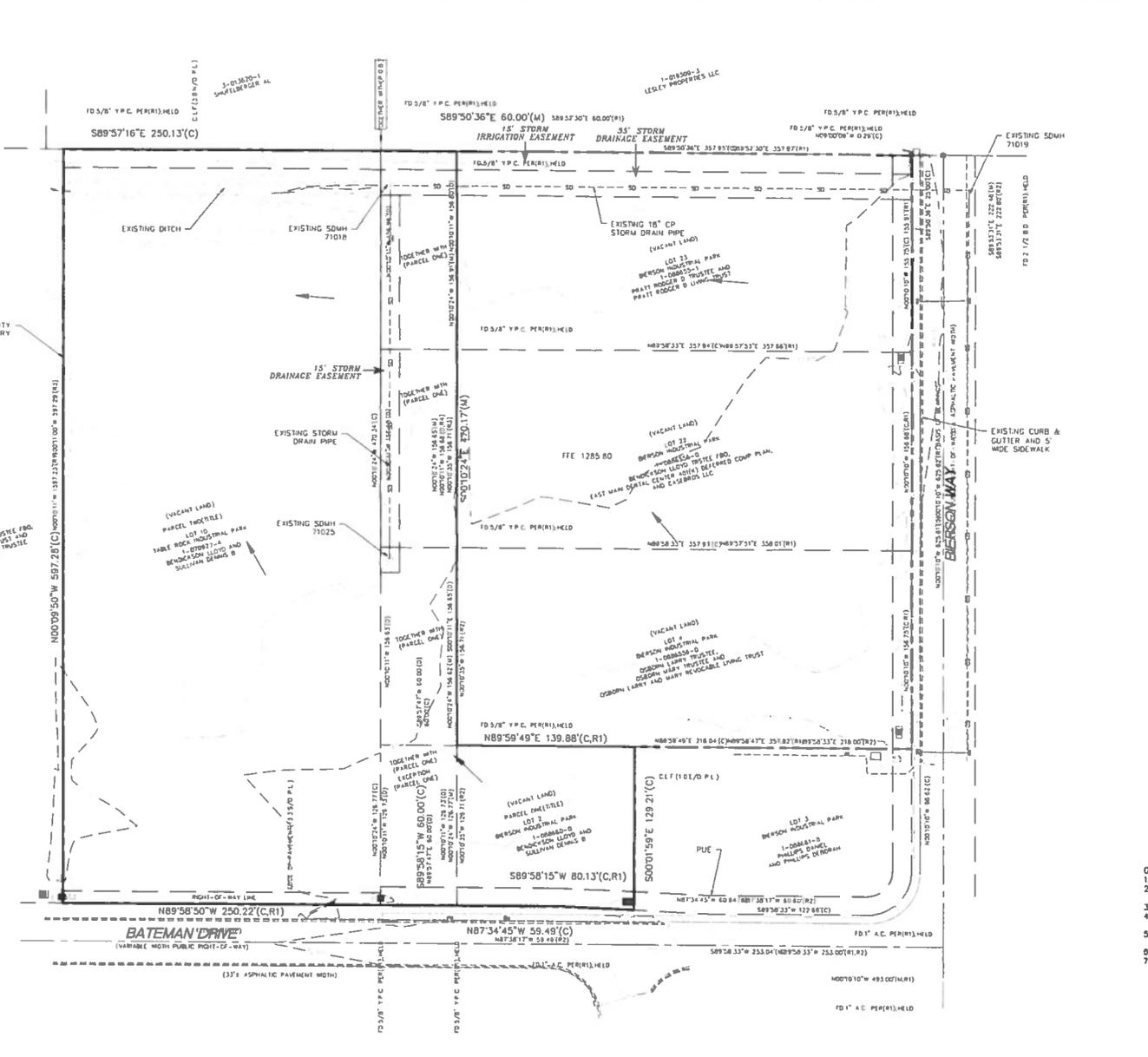
EXISTING GRADE SLOPE

- GENERAL NOTES:
1. CONTRACTOR TO NOTIFY BLUE STAKES PRIOR TO CONSTRUCTION
 2. CONTRACTOR TO VERIFY LOCATION AND ELEVATION OF ALL EXISTING UTILITY LINES AND STRUCTURES PRIOR TO CONSTRUCTION
 3. ALL PROPOSED WATER LINES TO HAVE A MINIMUM OF 5' OF COVER
 4. ALL SEWER, WATER AND STORM DRAIN PIPES SHALL BE BACKFILLED WITH SELECT GRANULAR FILL AS PER CITY STANDARDS
 5. ANY OFF SITE DAMAGE TO EXISTING ASPHALT, CURB & GUTTER, LANDSCAPING AND ALL UTILITIES TO BE REPLACED IN KIND.
 6. SEE UTILITY PLAN FOR CONSTRUCTION OF SEWER AND WATER LINES
 7. ALL WORK TO BE ACCORDING TO CITY STANDARDS

PLANNING DEPT

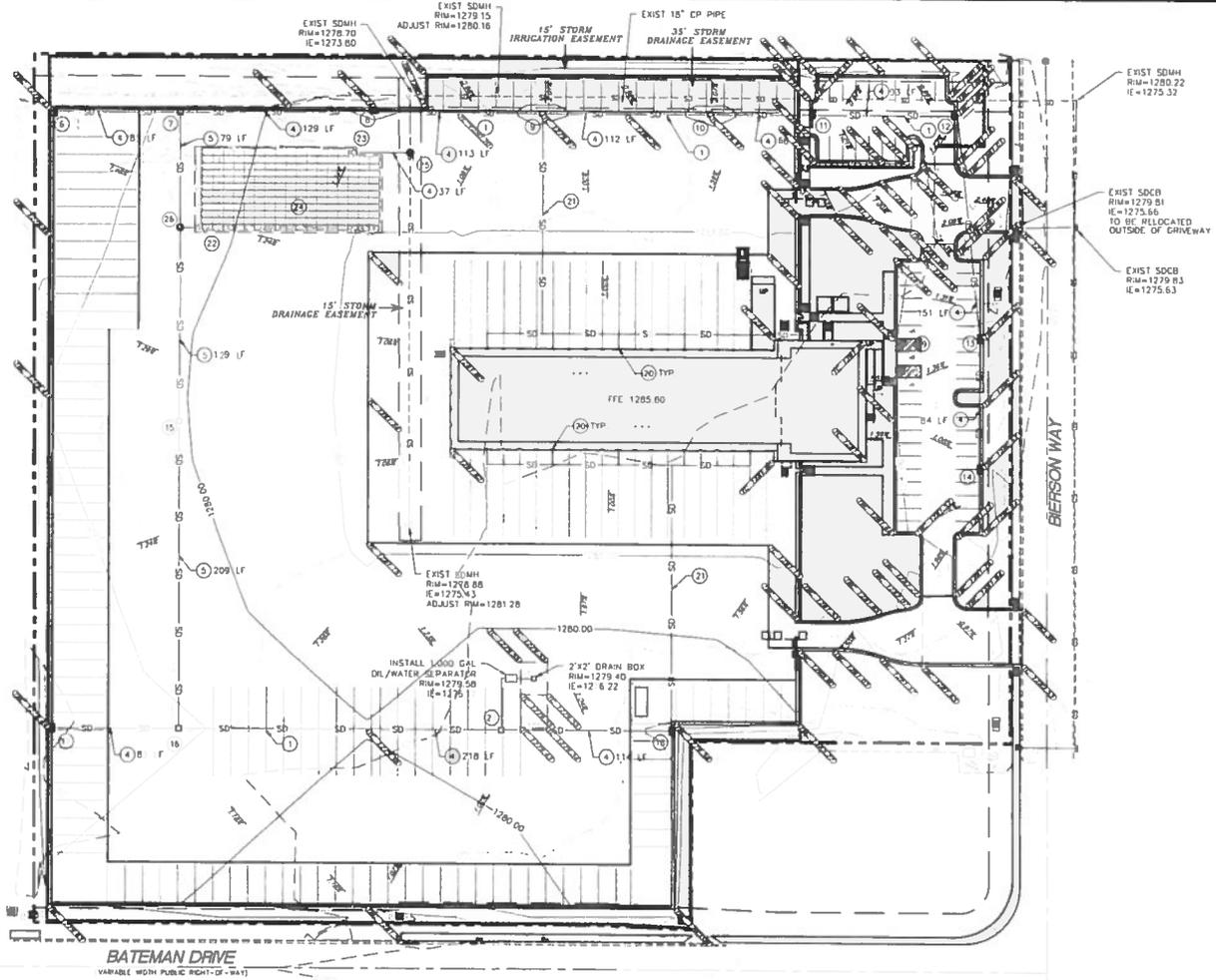
FEB 09 2010

1" = 40'
Scale in Feet
(111877 not to scale)

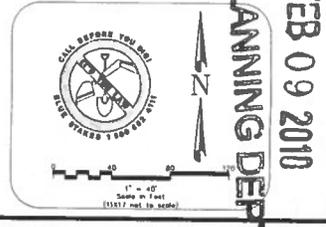




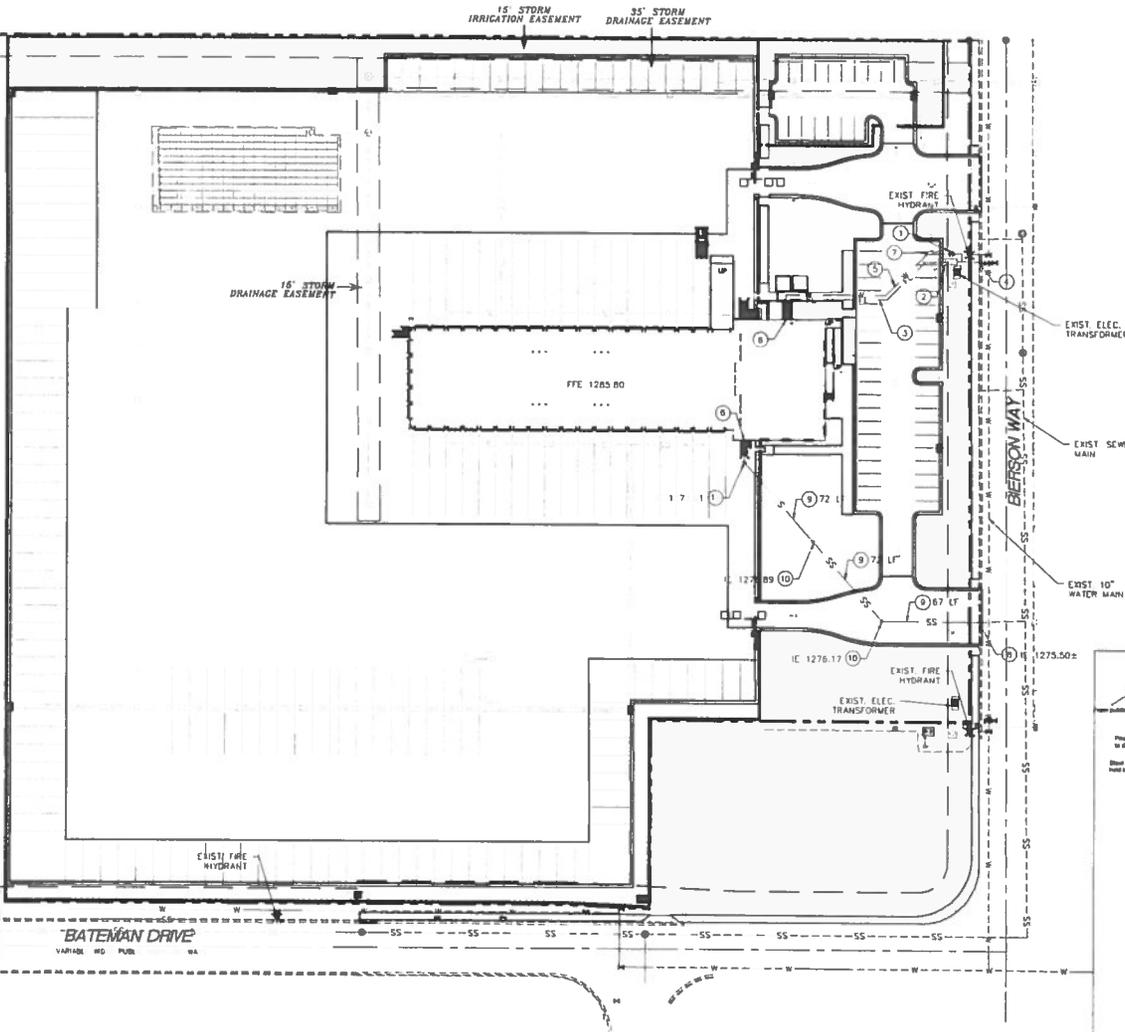
LEGEND	SYMBOL
LOT LINES (PROPERTY)	---
EXISTING CURB AND GUTTER	=====
PROPOSED CURB AND GUTTER	=====
PROPOSED SPALL CURB AND GUTTER	=====
PROPOSED STORM DRAIN LINE	---SD---
EXISTING STORM DRAIN LINE	---SD---
EXISTING POWER	---P---
EXISTING FENCE	---F---
EXISTING WATER	---W---
EXISTING GAS	---G---
EXISTING SANITARY SEWER	---SS---
EXISTING STORM DRAIN	---SD---
GRADE BREAK	---GRADE BREAK---
FINISH GRADE CONTOUR LINES	---4960---
EXISTING GRADE CONTOUR LINES	---EXISTING---
FINISH GRADE SLOPE	---SLOPE---
GRADE BREAK	GB
INVERT ELEVATION	IE
TOP OF GRATE	TOG
TOP OF ASPHALT	TA
TOP BACK OF CURB	TBC
PROPOSED	PROP
EXISTING	EX
FINISHED GRADE	FG
FINISHED FLOOR ELEVATION	FFE
BACK OF SIDEWALK	BOW



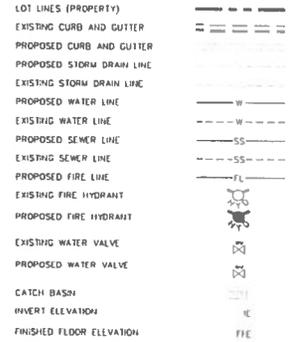
- GENERAL NOTES:
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 - ALL PROPOSED WATER LINES TO HAVE A MINIMUM OF 5' OF COVER
 - ALL SEWER, WATER AND STORM DRAIN PIPES SHALL BE BACKFILLED WITH SELECT GRANULAR FILL AS PER CITY STANDARDS.
 - ANY OFF SITE DAMAGE TO EXISTING ASPHALT, CURB & GUTTER, LANDSCAPING AND ALL UTILITIES TO BE REPLACED BY HAND.
 - SEE UTILITY PLAN FOR CONSTRUCTION OF SEWER AND WATER LINES.
 - ALL WORK TO BE ACCORDING TO CITY STANDARDS.



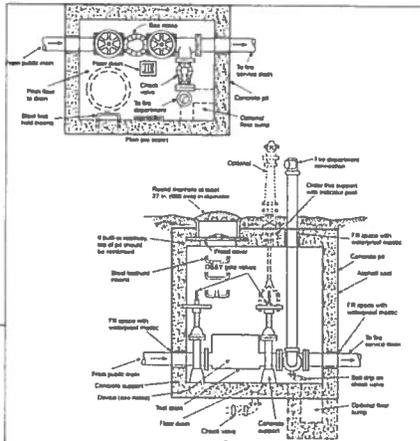
- | | | | |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>1) CONSTRUCT 1'-W ROLL GUTTER PER DTL 3 SHEET C-4</p> <p>2) RI-VERSE PAN CURB PER DTL 2, SHEET C-4</p> <p>3) STANDARD CURB AND GUTTER PER PER DTL 1 SHEET C-4</p> <p>4) 18" CORRUGATED HOPE PIPE</p> <p>5) 24" CORRUGATED HOPE PIPE</p> <p>6) CURB INLET CATCH BASIN PER DTL 3 SHEET C-4
RIM=1278.82
IE=1274.64</p> <p>7) 4'x4' JUNCTION BOX WITH LID
RIM=1278.13
IE=1274.39</p> <p>8) CURB INLET CATCH BASIN PER DTL 7 SHEET C-4
RIM=1279.92
IE=1274.4</p> | <p>9) 3'x3' WLET BOX PER DTL 6, SHEET C-4
RIM=1279.82
IE=1275.11</p> <p>10) 3'x3' INLET BOX PER DTL 6, SHEET C-4
RIM=1279.82
IE=1275.45</p> <p>11) CURB INLET CATCH BASIN PER DTL 7 SHEET C-4
RIM=1275.85
IE=1275.85</p> <p>12) CURB INLET CATCH BASIN PER DTL 7, SHEET C-4
RIM=1275.93
IE=1275.93</p> <p>13) CURB INLET CATCH BASIN PER DTL 7, SHEET C-4
RIM=1281.00
IE=1276.30</p> <p>14) CURB INLET CATCH BASIN PER DTL 7, SHEET C-4
RIM=1280.95
IE=1276.62</p> | <p>15) 4'x4' JUNCTION BOX WITH LID
RIM=1279.83
IE=1274.53</p> <p>16) 4'x4' JUNCTION BOX WITH LID
RIM=1278.75
IE=1275.16</p> <p>17) CURB INLET CATCH BASIN PER DTL 7 SHEET C-4
RIM=1278.01
IE=1275.41</p> <p>18) CURB INLET CATCH BASIN PER DTL 7, SHEET C-4
RIM=1278.01
IE=1278.41</p> <p>19) ADA RAMP PER DTL 5, SHEET C-4
ALL ADA STALLS SHALL HAVE SLOPES OF LESS THAN 2% IN ALL DIRECTIONS</p> <p>20) 8" ROOF DRAIN DOWNSPOUT CONNECTION. SEE MECHANICAL PLANS FOR CONTINUATION</p> <p>21) 12" ROOF DRAIN LINE</p> | <p>22) 4" WLET MANHOLE. SEE SHEET C-4 FOR DETAIL
RIM=1279.89
IE=1274.13
SUMP=1271.13</p> <p>23) 4" DUCTILE MANHOLE WITH 3.0" DRANCE. SEE DETAIL SHEET C-4
RIM=1280.35
IE=1274.13</p> <p>24) STORMTECH UNDERGROUND DETENTION SYSTEM
VOL=16.041 CF
SEE SHEET C-5 FOR DETAILS</p> <p>25) INSTALL 4" STORM DRAIN MANHOLE PER RVSS STANDARD
RIM=1280.38
IE=1274.02</p> <p>26) INSTALL 4" STORM DRAIN MANHOLE PER RVSS STANDARDS
RIM=1279.55
IE=1274.15</p> <p>27) 4'x4' JUNCTION BOX WITH LID
RIM=1279.39
IE=1275.98</p> |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|



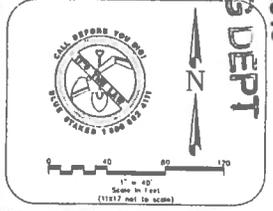
- GENERAL NOTES:
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 2. CONTRACTOR TO VERIFY LOCATION AND ELEVATION OF ALL EXISTING UTILITY LINES AND STRUCTURES PRIOR TO CONSTRUCTION
 3. ALL PROPOSED WATER LINES TO HAVE A MINIMUM OF 5' OF COVER
 4. ALL SEWER, WATER AND STORM DRAIN PIPES SHALL BE BACKFILLED WITH SELECT GRANULAR FILL AS PER CITY STANDARDS.
 5. ANY OFF-SITE DAMAGE TO EXISTING ASPHALT, CURB & GUTTER, LANDSCAPING AND ALL UTILITIES TO BE REPLACED IN KIND.
 6. SEE GRADING AND DRAINAGE PLAN FOR CONSTRUCTION OF SEWER AND WATER LINES.
 7. ALL WORK TO BE ACCORDING TO CITY STANDARDS.



- WATER/SEWER DESIGN NOTES:
1. INSTALL 8"x2" TEE AND 2" WATER METER PER MEDFORD WATER COMMISSION STANDARDS
 2. INSTALL 6" DOUBLE CHECK DETECTOR ASSEMBLY AND FGD PIT PER DETAIL (THIS SHEET)
 3. 6" PVC C-900 FIRELINE
 4. CONNECT TO EXISTING WATER 8" WATER SERVICE LATERAL. INSTALL 6"X8" REDUCER PER MEDFORD WATER COMMISSION STANDARDS.
 5. INSTALL 2" POLY WATER SERVICE
 6. END ALL UTILITIES 5' FROM BUILDING. SEE PLUMBING PLANS FOR CONTINUATION
 7. MAINTAIN 18" VERTICAL SEPARATION BETWEEN SEWER, WATER & STORM DRAIN LINES
 8. CONNECT TO EXISTING SANITARY SEWER LATERAL. CONTRACTOR TO VERIFY LOCATION AND ELEVATION PRIOR TO CONSTRUCTION
 9. INSTALL 8" PVC SDR-35 SEWER PIPE AT 1% MIN SLOPE
 10. INSTALL 8" SEWER CLEANOUT (TRAFFIC RATED)



1. Verify local fire department regulations accept effluent devices at the connection between public water main and private fire service main.
 2. The device shown in the pit should be one or a combination of the following:
 - (a) Utility check valve
 - (b) Reduced pressure zone (RPZ) device
 - (c) Double check valve
 - (d) Reduced backflow
 3. Double check valve assembly.
- Some local fire department regulations prohibit these devices from being installed in a pit.
- In all cases, the device in the pit should be approved or listed as necessary. The requirements of the local or municipal water department should be reviewed prior to design or installation of the device.
- Pressure drop should be accounted prior to the installation of any backflow prevention device.



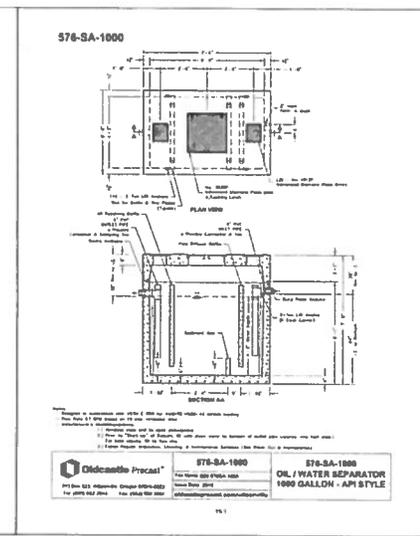
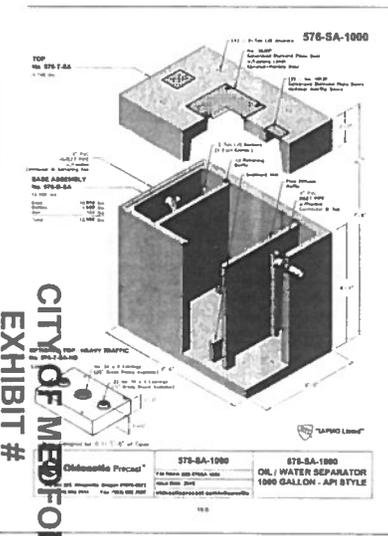
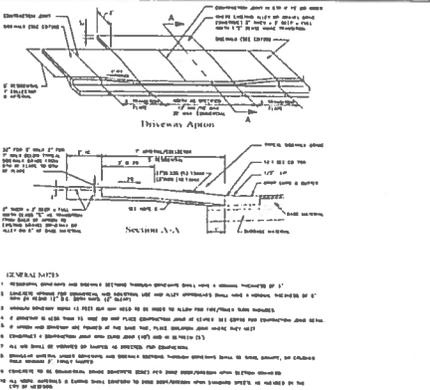
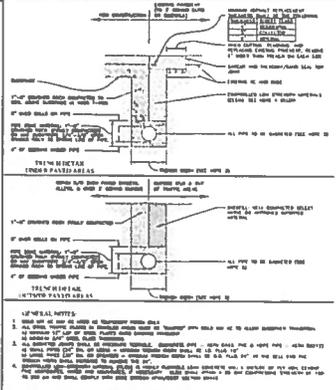
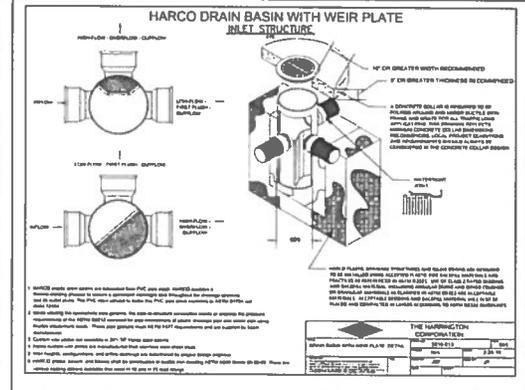
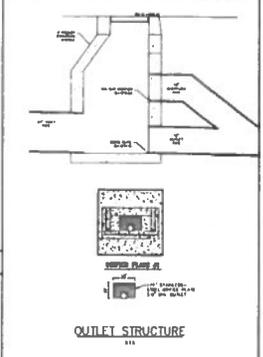
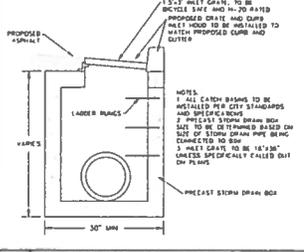
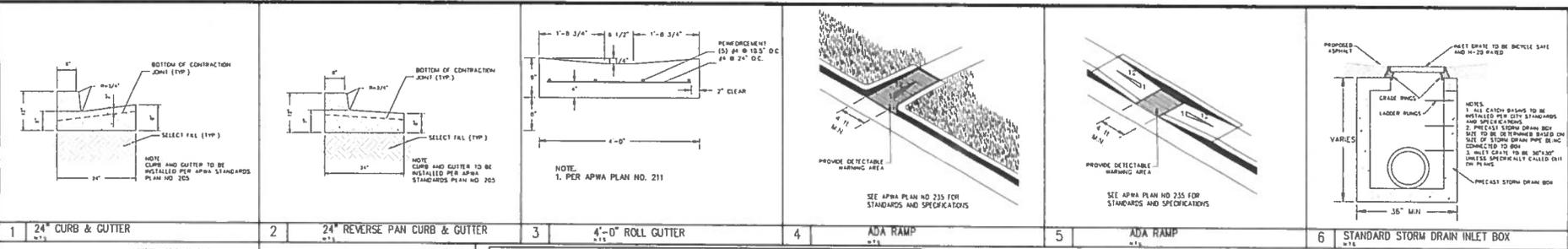
PROJECT NUMBER	LB
REVISION	LB
DATE	

LEGEND ENGINEERING
 100 WEST 100th STREET
 MEDFORD, OR 97504
 PHONE: 531-554-4528
 www.legend-engineering.com



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 FEB 09 2010
 MEDFORD, OR
 PLAN RECEIVED
 11/11/17 NOT IN SCALE





NO.	REVISION	BY	DATE

LEGEND ENGINEERING
525 WEST 100th STREET
MEDFORD, OR 97504
PHONE: 531-554-1428
www.legendengr.com

OLD DOMINION FREIGHT LINE
DETAILS
MEDFORD, OR

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FEB 09 2010
C-4

NO.	REVISION	BY	DATE

LEGEND ENGINEERING
 3400 N. 17th Ave
 Suite 100
 Medford, OR 97504
 Phone: 531-654-1622
 www.legendeng.com



OLD DOMINION FREIGHT LINE
 STORMWATER POLLUTION PREVENTION PLAN
 MEDFORD, OR

RECEIVED
 FEB 09 2010
 PLANNING DEPT
 11
 6
 DATE: Jan 26, 2010



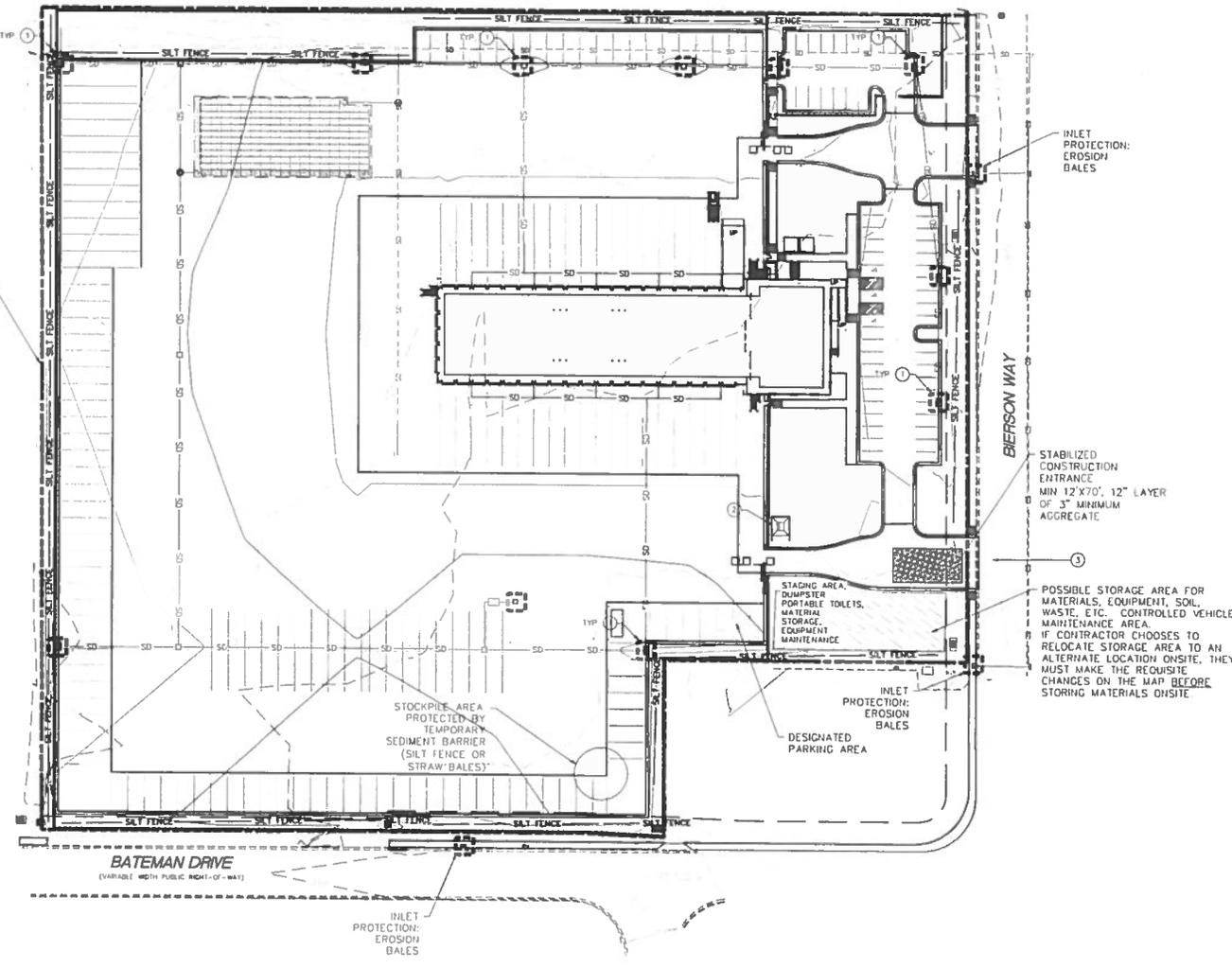
PROPERTY BOUNDARY: _____
 SILT FENCE: _____
 LIMITS OF DISTURBANCE: - - - - -
 WHEEL WASH AREA: [Symbol]
 INLET PROTECTION: [Symbol]

- DWP CALLOUTS**
- INSTALL STORM DRAIN INLET PROTECTION IMMEDIATELY AFTER STORM DRAIN SHEETS HAVE BEEN PLACED.
 - INSTALL CONCRETE WASTE MANAGEMENT AREA PER DETAIL SHEET C-7 MUST BE MIN. 2' ABOVE GROUNDWATER ELEVATION OR LINED.
 - STREET SLEEPING TO REMOVE SEDIMENT TRANSPORTED ONTO STREETS FROM CONSTRUCTION ACTIVITY.
- DNPS TO APPLY TO ENTIRE SITE**
- CONTRACTOR TO APPLY GRADING TECHNIQUES (SURFACE ROUGHENING AND ROADWAYS) AT TOPS OF CUTS & TRANSITIONS) TO BE USED IN CONJUNCTION WITH SOIL FACE PROTECTER OR SOIL BINDER.
- MATERIAL MANAGEMENT AND SPILL PREVENTION CONTROL: PAINTS, SOLVENTS, ADHESIVES AND FLAMING COMPOUNDS SHOULD BE STORED PROOF AWAY FROM RAINFALL AND OTHER FLOODS. PREVENT FUEL AND PETROLEUM SPILLS BY USING OFFSITE FACILITIES WHEN POSSIBLE. ONLY MAINTAIN/REPAIR EQUIPMENT ON DESIGNATED WASH.
- APPLY WATER DURING CONSTRUCTION OPERATION TO PREVENT EXCESSIVE WIND EROSION AND DUST GENERATION FROM EXPOSED SOIL SURFACES.

- GENERAL NOTES**
- DURING CONSTRUCTION**
- ALL EROSION CONTROL BEST MANAGEMENT PRACTICES SHALL BE INSPECTED AND MAINTAINED REGULARLY (MINIMUM ONCE A WEEK) AND AFTER EVERY STORM EVENT.
 - LAND DISTURBANCE SHALL BE KEPT TO A MINIMUM TO CONTROL RUNOFF FROM THE SITE.
 - LIMIT LAND CLEARING AND RESTORE ALL GRASSING AS SOON AS POSSIBLE.
 - STAGED SEEDING TO RE-VEGETATE CUT AND FILL SLOPES AS THE WORK IS IN PROGRESS.
 - AT ALL TIMES DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING AND CONTROLLING EROSION DUE TO WIND AND OTHER EROSION.
 - MAINTENANCE OF STREET STREETS TO BE KEPT CLEAN AND FREE FROM DEBRIS.
 - CONTRACTOR SHALL PROVIDE DUST CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION.
 - A COPY OF THE STORM WATER POLLUTION PREVENTION PLAN SHALL BE KEPT ON THE SITE DURING ALL CONSTRUCTION ACTIVITY.
- POST CONSTRUCTION**
- EROSION CONTROL STRUCTURES MAY BE REMOVED ONCE 70% REVEGETATION HAS BEEN ESTABLISHED.
 - EROSION CONTROL STRUCTURES W/SEEDED AREAS MUST REMAIN IN PLACE UNTIL THE ENTIRE AREA HAS BEEN ESTABLISHED.
 - EROSION CONTROL ON PROPOSED PAVEMENT AREAS SHALL REMAIN IN PLACE UNTIL PAVEMENT IS COMPLETE.
 - THE FOLLOWING PRECAUTIONS SHALL BE PERFORMED:
 - PERIODIC INSPECTION OF SEDIMENT BASIN AND CLEANING WHEN THE BASIN IS MORE THAN 1/2 FULL. INSPECTION SHALL BE DONE AFTER EVERY SIGNIFICANT RAINFALL AND EVERY 8 HOURS AS A MINIMUM. DISPOSAL OF ANY GARBAGE OR DEBRIS MUST BE DONE IN ACCORDANCE WITH CURRENT ENVIRONMENTAL REGULATIONS.
 - LITTER, DEBRIS AND CIGARETTES MUST BE PICKED UP AND KEPT IN A LOCATION TO PREVENT POLLUTION OF STORM WATER DISCHARGE.
 - PARKING AREAS SHALL BE KEPT FREE FROM AUTOMOBILE TIREDS AS TO NOT WASH INTO THE STORM DRAIN SYSTEM.



1" = 48'
 Scale in Feet
 (11817 not to scale)



PROPERTY BOUNDARY WILL SERVE AS LIMITS OF DISTURBANCE



1000 S. 10th St.
Medford, OR 97504
503-754-1111

Old Dominion Freight Lines
Medford Terminal
Medford, OR



RECEIVED
FEB 9 2010
Planning Department
Rev#1 Rev#2

Site Lighting

Project number
Date 1/16/2010
Drawn by AA
Checked by T

E1.10

Scale As indicated

GENERAL SHEET NOTES

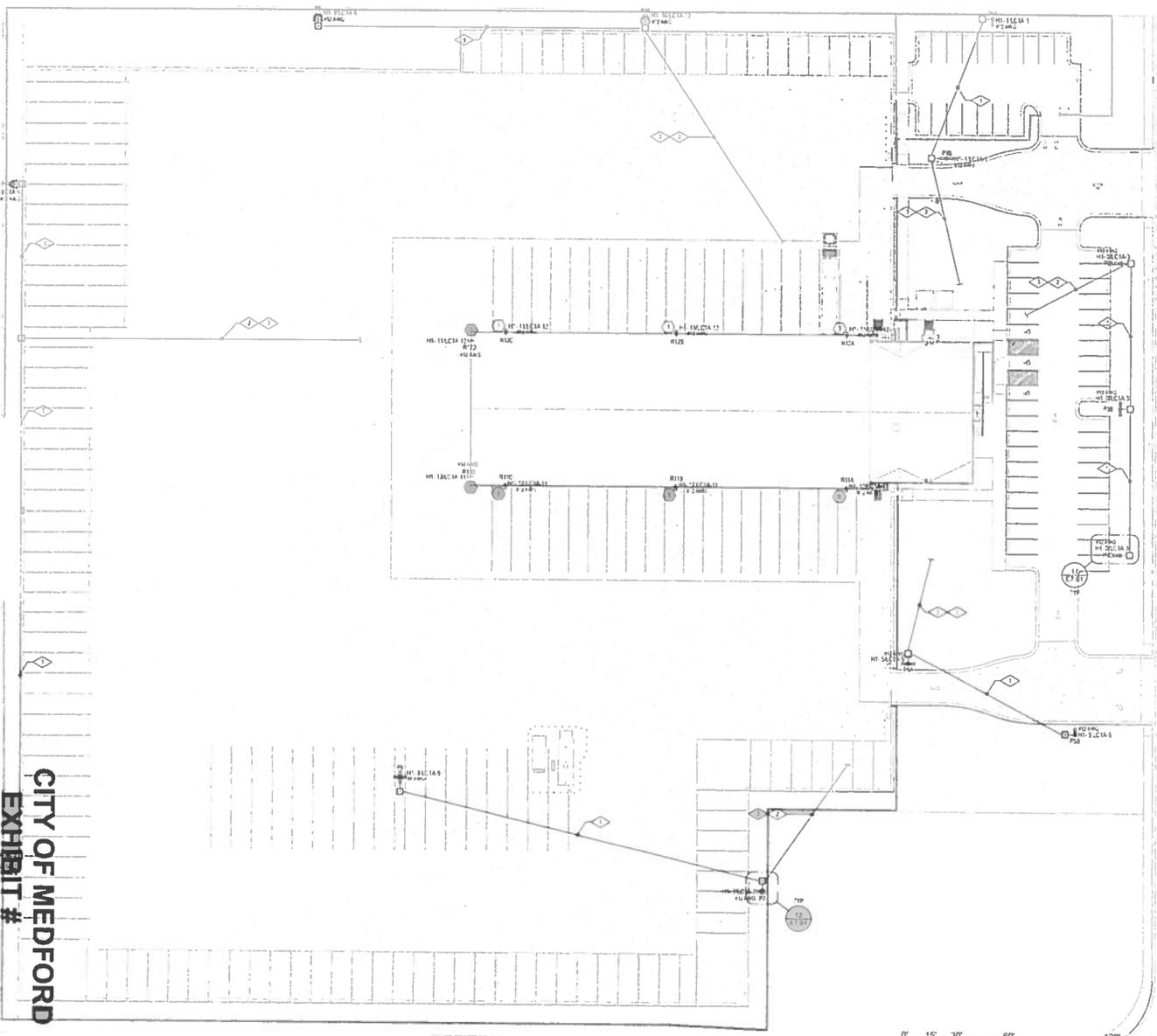
- A. Check location for lamp terminals & readability E.C.
- B. Conserve light bulb hours, avoid glare, or reduce path length. Lamp & mounting by E.C.

KEYED SHEET NOTES

1. Mounting Refer to a set of drawings for mounting details and locations.

CONDUIT SCHEDULE

1. EMT 1/2"
2. EMT 1/2" (1 allowed for even better practice and to lighting control panel E.C.A. Electric's Rule)
3. EMT 1/2" (1 allowed when 1/2" is better practice due to security lock for the lock key ring)



Page 129

File # AC-18-016

CITY OF MEDFORD
EXHIBIT #

1 Site Lighting



MOUNTING LOCATION SUMMARY

MOUNTING LOCATION LABEL	MPS HGT (FEET)	QTY. PER LOCATION	TYPE (MPS HEIGHT FT) APPL							
			VPS	VPS4	VPS4	VPS4	VPS4	VPS4	VPS4	
R11A	25	2	2							
R11B	41	1					1			
R11C	25	1							1	
R11D	25	1				2				
R11E	25	1					1			
R11F	41	1							1	
R11G	25	1								1
R11H	25	1								1
R11I	25	1								1
R11J	25	1								1
R11K	25	1								1
R11L	25	1								1
R11M	25	1								1
R11N	25	1								1
R11O	25	1								1
R11P	25	1								1
R11Q	25	1								1
R11R	25	1								1
R11S	25	1								1
R11T	25	1								1
R11U	25	1								1
R11V	25	1								1
R11W	25	1								1
R11X	25	1								1
R11Y	25	1								1
R11Z	25	1								1
R11AA	25	1								1
R11AB	25	1								1
R11AC	25	1								1
R11AD	25	1								1
R11AE	25	1								1
R11AF	25	1								1
R11AG	25	1								1
R11AH	25	1								1
R11AI	25	1								1
R11AJ	25	1								1
R11AK	25	1								1
R11AL	25	1								1
R11AM	25	1								1
R11AN	25	1								1
R11AO	25	1								1
R11AP	25	1								1
R11AQ	25	1								1
R11AR	25	1								1
R11AS	25	1								1
R11AT	25	1								1
R11AU	25	1								1
R11AV	25	1								1
R11AW	25	1								1
R11AX	25	1								1
R11AY	25	1								1
R11AZ	25	1								1
R11BA	25	1								1
R11BB	25	1								1
R11BC	25	1								1
R11BD	25	1								1
R11BE	25	1								1
R11BF	25	1								1
R11BG	25	1								1
R11BH	25	1								1
R11BI	25	1								1
R11BJ	25	1								1
R11BK	25	1								1
R11BL	25	1								1
R11BM	25	1								1
R11BN	25	1								1
R11BO	25	1								1
R11BP	25	1								1
R11BQ	25	1								1
R11BR	25	1								1
R11BS	25	1								1
R11BT	25	1								1
R11BU	25	1								1
R11BV	25	1								1
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R11BX	25	1								1
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R11DC	25	1								1
R11DD	25	1								1
R11DE	25	1								1
R11DF	25	1								1
R11DG	25	1								

February 7, 2018

Old Dominion Freight Line – Medford, Or
Applicant's questionnaire Narrative

We propose to build a freight transfer terminal. The terminal itself will be 20,423 sf and will have 42 dock doors. The yard will have 266,040 sf of paving for maneuvering and parking of trailers. It is estimated that 51 tractors will visit this facility per day. The terminal will operate 7 days a week.

Compatibility Criterion:

The existing uses around our site are industrial in nature. Many manufacturing or other uses. The buildings primarily are metal panel buildings.



Our building will be painted concrete tilt-up with a metal panel overhang and will have a clean look to them.



The Façade of the building is broken up by horizontal reveals and vertical panel joints. The logo is cast into the panel with a canopy underneath it. This accentuates the entry. Behind the front façade of the building are the docks which are recessed behind the façade, blocking the view of the actual dock. The overhang provides an architectural treatment differentiating the docks from the office.

The building entrance and public parking are located as close to the public road as possible and we have several connections through the site for pedestrian traffic. We take very seriously the truck traffic and pedestrian safety. We make sure that we only cross these lanes when absolutely necessary.

Our facility has underground storm retention facilities. All storm water is processed and then released per city guidelines.

The landscaping for our project is consistent with the area and exceeds that required by city guidelines.

Please let me know if you have further questions regarding this project.

Shawn Eaton
shawn@aeurbia.com



RECEIVED

MAR 20 2018

PLANNING DEPT.



909 West South Jordan Parkway
south jordan , utah 84095
phone: 801.746.0456 - fax: 801.575.6456
web page: aeurbia.com

MEMORANDUM

TO: City of Medford Planning Commission
c/o Steffen Roennfeldt

FROM: Gerald Reynolds, AE Urbia

SUBJECT: Revised Findings from Land Development Review Meeting

DATE: March 20, 2018

CC: Lindzi Bell, P.E., Legend Engineering
Chris Furstenau, Furst Construction
Shawn Eaton, AE Urbia

As a result of the land development review meeting last week with Steffen Roennfeldt, City of Medford Planner, and Lindzi Bell, Legend Engineering, the following revised findings are offered in regard to the Old Dominion Freight Line land development application:

- 1. Number of parking spaces.** On the proposed site plan, the number of vehicle parking spaces was calculated using Table 10.743-1, Medford Land Development Code. See the chart below for the specific number of employees. The desire is for each employee on the largest shift to have a parking space available to use, as well as to provide spaces for a few possible visiting customers, and to allow for ADA parking as well.

Information taken from Medford Land Development Code, Table 10.743-1

Land Use Category	square footage	ratio min.	ratio max.	required min.	required max.
Industrial / Warehouse	20423	0.2	0.3	54	56

<u>Number of employees</u>	
line haul drivers	17
fork lift operators	8
office management/personnel	24
total	49

*1.0 space per employee on largest shift plus ratio

CITY OF MEDFORD
EXHIBIT # L
File # AC-18-016

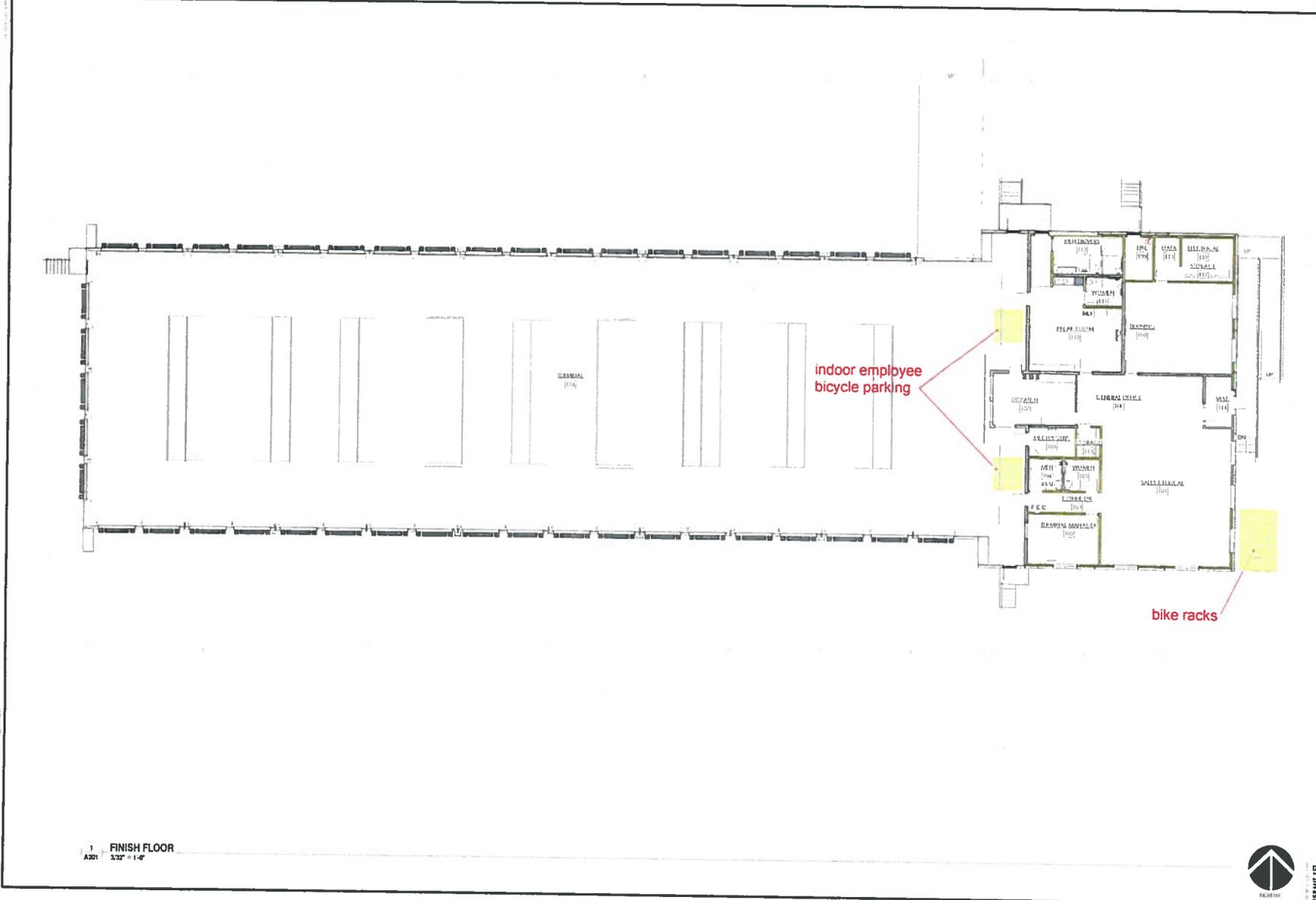
2. **Bicycle parking.** Due to the fact of where this site is located and because it is in the light industrial zone, it's anticipated there won't be very many employees riding their bicycles to work. However, in an effort to comply with the code, we are proposing to provide 3 bike racks out front of the building on a concrete pad, which will park 2 bikes at each rack, for a total of 6 bicycle parking spaces or 10% of the vehicle parking. Also, we are proposing to provide the remaining 10% of the required bicycle parking inside the building in a suitable, secure, and accessible location as allowed in section 10.749 of the Medford Land Development Code. The proper signage will be provided. See the attached exhibit showing the proposed bike parking locations.
3. **Wider Driveways.** We request to allow the driveways to be 36' wide. Bierson Way is expected to have over 500 ADT (average daily traffic), according to a conversation with Peter Mackprang, Associate Traffic Engineer for the City of Medford and Lindzi Bell, P.E. at Legend Engineering. Having wider driveways is also helpful for the semi-trucks to be able to more easily maneuver into and out of the yard.
4. **Final Order.** Should these requests be approved, we request to adopt a "final order" at the public hearing to be held April 6, 2018.

Sincerely,



Gerald Reynolds, Architectural Designer
gerald@aeurbia.com

aeurbia
architects and engineers



1 FINISH FLOOR
 A201 3/27/18 11-4"



ae urbia
 architects and engineers
 2875 south declair lake drive, suite 212
 salt lake city, utah 84115
 phone: 801.487.1100
 website: aeurbia.com

DESIGNATED
 BIKE RACK
 LOCATION
 CITY OF OREGON

3/20/2018 12:56:18 PM

**OLD DOMINION FREIGHT
 LINE**
 5449 BIERSON WAY, MEDFORD, OREGON

Project No: 2017.037
 Date: 03/20/2018
 Designer: J. L. CONNOR

2017.037
**FLOOR PLAN -
 OVERALL**
 DATE: 2018.02.27
 SHEET #
A201

PERMIT SET

THIS DOCUMENT IS THE PROPERTY OF AEURBIA ARCHITECTS AND ENGINEERS. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF AEURBIA ARCHITECTS AND ENGINEERS.

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

RECEIVED
 FEB 09 2018
 PLANNING DEPT

Section II – Code Compliance: Criterion No. 2

PROJECT SITE		
	PROPOSED	REQUIRED
• Zoning District	Light Industrial	Light Industrial
• Overlay District(s)		Airport Approach, Limited Industrial
• Proposed Use	Freight transfer	
• Project Site Acreage	8.52	
• Site Acreage (+ right-of-way)	9.03	
• Proposed Density (10.708)	n/a	
• # Dwelling Units	none	
• # Employees	15	

STRUCTURES		
	EXISTING	PROPOSED
• # Structures	none	1
• Structure Square Footage (10.710-10.721)	none	20,423

SITE DESIGN STANDARDS		
	PROPOSED	REQUIRED
• Front Yard Setback (10.710-721)	108	10
• Side Yard Setback (10.710-721)	198	0
• Side Yard Setback (10.710-721)	187	0
• Rear Yard Setback (10.710-721)	283	0
• Lot Coverage (10.710-721)	5.5%	50%

PARKING		
	PROPOSED	REQUIRED
• Regular Vehicular Spaces (10.743)	53	
• Disable Person Vehicular Spaces (10.746[8])	3	3
• Carpool/Vanpool Spaces (10.809)	3	1
• Total Spaces (10.743)	56	21(1/1,000 SF)
• Bicycle Spaces (10.748)	6	5.3(10% of parking stalls)
• Loading Berths (10.742)	42	1 type A

SITE PLAN AND ARCHITECTURAL REVIEW APPLICATION

LANDSCAPING

	PROPOSED	REQUIRED
• Total Landscape Area (square feet)	52125	_____
• Total Landscape Area in High Water Use Landscaping (square feet)	0	_____
• Total Landscape Area in High Water Use Landscaping (percentage)	0	_____
• Total % Landscape Coverage	14.5%	_____
• Required Organic Content (cu.yd.)	_____	_____
• Frontage Landscaping (10.797)		
• Street:	Bierson Way	Bierson Way
• Feet:	468'	468'
• # Trees:	14	14.04(3/100')
• # Shrubs:	70	70.2(15/100')
• Street:	Bateman Dr.	Bateman Dr.
• Feet:	449'	449'
• # Trees:	14	13.47(3/100')
• # Shrubs:	68	67.35(15/100')
• Bufferyard Landscaping (10.790)		
• Type:	N/A	_____
• Distance (ft):	_____	_____
• # Canopy Trees:	_____	_____
• # Shrubs:	10	_____
• Fence/Wall:	_____	_____
• Parking Area Planter Bays (10.746)		
• Type:	Planter Projections	Projections or islands
• # Bays:	7	n/a
• Area:	1,278	756 (325 sf/24 stalls)
• # Trees:	5	4.6 (2/24 stalls)
• # Shrubs:	10	9.33 (4/24 stalls)

STRUCTURE

PROPOSED

• Materials	Tilt-up Concrete Panels, Vertical Metal Paneling, Structural Steel shapes
• Colors	"Burmese Beige", "Canvas Cloth", "Light Stone" (metal paneling & trim), Dark Bronze cap flashing

Please remember that the information you provide in response to the questionnaire must be included with your SPAR application submittal. Remember to sign and date your written response.



Medford – A fantastic place to live, work and play

CITY OF MEDFORD

LD Date: 3/14/2018
File Number: AC-18-016

PUBLIC WORKS DEPARTMENT STAFF REPORT 5449 Bierson Way (TLs 111, 116, 117, 120, 121) Old Dominion Freight Line

- Project:** Consideration of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres
- Location:** Located northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district. (362W36D111).
- Applicant:** Applicant: Gerald Reynolds; Agent: Gerald Reynolds; Planner: Steffen Roennfeldt.

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention.
- Completion of all public improvements, if required. The applicant may provide security for 120% of the improvements prior to issuance of vertical building permits. Construction plans for the improvements will need to be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas.
- Certification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan.
- Completion of all public improvements, if applicable.

A. STREETS

1. Dedications

Bierson Way and **Bateman Drive** are classified as Commercial streets within the Medford Land Development Code (MLDC) 10.429. Right-of-way has already been dedicated with the final plat for Bierson Industrial Park (Survey #15015) (TLs 116, 117, 120 and 121) and Table Rock Industrial Park (Survey #9673) (TL 111). **No additional right-of-way is required.**

There is currently a 15-foot PUE along the frontage of Bierson Way and Bateman Drive (TLs 116, 117, 120 and 121) that was originally dedicated with the final plat for Bierson Industrial Park (Survey #15015). There is currently a 10-foot PUE along the frontage of Bateman Drive (TL 111) that was originally dedicated with the final plat for Table Rock Industrial Park (Survey #9673)

2. Public Improvements

a. Public Streets

Standard street section improvements have been completed on **Bierson Way** and **Bateman Drive**, including pavement, curb and gutter and partial sidewalk as part of The Bierson Industrial Park Subdivision (P1090D), and Bateman Drive – Road & SS Construction improvements (P1285D). **No additional improvements are required except for sidewalk and street lights.**

NOTE: All projects subject to Site Plan and Architectural Review shall be required per MLDC 10.501 (6), as a condition of approval, to repair all frontage sidewalks as determined by the Engineering Division. When attached as a condition of approval of a Site Plan and Architectural Review application the sidewalk maintenance procedures set forth in Chapter 3, Section 3.025, Notice on Hearing of City Repair of Sidewalks, through 3.035, Notice of Sidewalk Repair, are hereby superseded.

b. Street Lights and Signing

The developer shall provide and install in compliance with Section 10.495 of the Medford Municipal Code (MMC). Based on the preliminary plan submitted, the following number of street lights and signage will be required:

Street Lighting – Developer Provided & Installed:

- A. 5 – Type R-150
- B. This development could use two existing fixtures that have been installed but no power brought to them. They are adjacent to TL 121 and TL 119. Both existing fixtures would need:
 - a. 150w cobra heads installed
 - b. New wire in poles
 - c. Hand hole covers replaced
 - d. Power brought to them

NOTE: The most cost effective way to accomplish lighting Bierson Way is to have the Developer install the three residential street lights on Bierson Way on the west side of the street along the Developer's frontage. Please contact the Traffic Division for further information and/or coordination.

Traffic Signs and Devices – City Installed, paid by the Developer:

A. None

Numbers are subject to change if changes are made to the plans. All street lights shall be installed per City standards and be shown on public improvement plans. Public Works will provide preliminary street light locations upon request. All street lights shall be operating and turned on at the time of the final “walk through” inspection by the Public Works Department.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer’s contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this frontage to Bierson Way or Bateman Drive.

d. Access to Public Street System

Driveway access shall be per MLDC 10.550. The driveways do not appear to comply with maximum width and radius standards as shown.

3. Section 10.668 Analysis

To support a condition of development that an applicant dedicate land for public use or provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and supported by sound public policy. Those purposes and policies include, but are not limited to: development of

a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and storm drains to serve the developed parcels. It can be found that the listed improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining “rough proportionality” have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Bierson Way & Bateman Drive:

The additional street lighting will provide the needed illumination to meet current MLDC requirements.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the project’s impacted site with sufficient information to determine the direction of runoff to the existing or proposed drainage system, and also showing elevations of the proposed drainage system (if applicable), shall be submitted with the first building permit application for approval.

All private storm drain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Storm Drainage Conditions

Within the limits of this project, the Developer needs to show capacity for, or make improvements to the existing public storm drainage ditch along the north property line to provide capacity for the 10 year storm with a foot of freeboard. Developer needs to provide a 10-foot easement at the location of the public drainage facility or show that an easement already exists. Developer needs to provide access easement to the public drainage.

Drainage Ditch (offsite) – Due to the stormwater discharge being to an unmaintained drainage system, additional precautions are necessary to insure the down gradient system is not burdened by this development. The development can provide stormwater detention per city code if they are able to provide calculations showing the northerly ditch can convey the 10-year storm with 6” of freeboard. Alternatively, the development can provide detention to accommodate the 25-year storm with a release rate of the predevelopment 10-year storm.

3. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

4. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

If the proposed development is to be constructed in phases, then each phase will be required to have its own stormwater detention and water quality treatment. If the Developer desires to do so, a Stormdrain Masterplan may be submitted in lieu of requiring each phase to have separate stormwater detention and water quality treatment. The Stormdrain Masterplan shall be submitted and reviewed with each phase’s construction plans and shall be constructed with any phase to be served by the facility.

5. Certification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer’s design engineer shall certify that the construction of the stormwater quality and detention system was constructed per plan. Certification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

6. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. GENERAL CONDITIONS

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

If required, construction drawings for any public improvements for this project shall be prepared by a professional engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Construction drawings for public improvements shall be submitted only for the improvements to be constructed with each phase. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, storm drains, and street lights as required by the governing commission's Final Order, together with all pertinent details and calculations. A checklist for public improvement plan submittal can be found on the City of Medford, Public Works web site (<http://www.ci.medford.or.us/Page.asp?NavID=3103>). The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

In order to properly maintain an updated infrastructure data base, the Surveyor of Record shall submit an as-built survey prior to the Final Inspection and, the Engineer of Record shall submit mylar "as-constructed" drawings to the Engineering Division within sixty (60) calendar days of the Final Inspection (walk through). Also, the engineer shall coordinate with the utility companies, and show all final utility locations on the "as built" drawings.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans. Pre-qualification is required of all contractors prior to application for any permit to work in the public right-of-way.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges (SDC)

Buildings in this development are subject to street, sanitary sewer treatment and stormdrain SDCs. All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL

5449 Bierson Way (TLs 111, 116, 117, 120, 121)

Old Dominion Freight Line

AC-18-016

A. Streets

1. Street Dedications to the Public:

- **Bierson Way** and **Bateman Drive** – No additional right-of-way required.
- 15-foot and 10-foot Public Utility Easements have been dedicated.

2. Improvements:

Public Streets

- **Bierson Way** and **Bateman Drive** – No improvements are required, except sidewalk and street lights.

Lighting and Signing

- Developer supplies and installs all street lights at own expense.
- City installs traffic signs and devices at Developer's expense.

Access to Public Street System

- Driveway access shall comply with MLDC 10.550.

Other

- There is no pavement moratorium currently in effect on **Bierson Way** or **Bateman Drive**.

B. Sanitary Sewer:

- The site is situated within the RVSS area.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Comply with Storm Drainage Conditions.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers certification of stormwater facility construction.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



Medford Fire-Rescue Land Development Report

Review Info

Reviewed By: Kleinberg, Greg

Review Date: 3/6/2018
Meeting Date: 3/14/2018

LD #: AC18016

Planner: Steffan Roennfeldt

Applicant: Applicant: Gerald Reynolds

Project Location: northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district

Project Description: Consideration of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located northwest of the intersection of Bierson Way and Bateman Drive within the I-L (Light Industrial) zoning district. (362W36D111) Applicant: Gerald Reynolds; Agent: Gerald Reynolds; Planner: Steffan Roennfeldt.

Specific Requirements for Access & Water Supply

Violation Code	Reference	Comments	<u>Conditions</u> Description
OFC 503.2.1	OFC 503.2.1	General comments	Fire apparatus access roads shall have an unobstructed width of not less than 20 feet and unobstructed vertical clearance of not less than 13 feet 6 inches. The required width of a fire apparatus access road shall not be obstructed in any manner, including parking of vehicles. Minimum required widths and clearances established under section 503.2.1, shall be maintained at all times. The fire apparatus access road shall be constructed as asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 60,000 pounds. (See also OFC 503.4; D102.1) The turning radius on fire department access roads shall meet Medford Fire Department requirements (OFC 503.2.4).
OFC 503.4	OFC 503.4	Parking shall be posted as prohibited along fire lanes.	Fire apparatus access roads 20-26' wide shall be posted on both sides as a fire lane. Fire apparatus access roads more than 26' to 32' wide shall be posted on one side as a fire lane (OFC D103.6.1). Where parking is prohibited for fire department vehicle access purposes, NO PARKING-FIRE LANE signs shall be spaced at minimum 50' intervals along the fire lane (minimum 75' intervals in 1 & 2 family residential areas) and at fire department designated turn-around's. The signs shall have red letters on a white background stating "NO PARKING-FIRE LANE" (See handout). For privately owned properties, posting/markings of fire lanes may be accomplished by any of the following alternatives to the above requirement (consult with the Fire Department for the best option): Alternative #1: Curbs shall be painted red along the entire distance of the fire department access. Minimum 4" white letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals. Alternative #2: Asphalt shall be striped yellow or red along the entire distance of the fire department access. The stripes shall be at least 6" wide, be a minimum 24" apart, be placed at a minimum 30-60 degree angle to the perimeter stripes, and run parallel to each other. Letters stating "NO PARKING-FIRE LANE" shall be stenciled on the asphalt at 25-foot intervals. Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12). This restriction shall be recorded on the property deed as a requirement for future construction. A brochure is available on our website at: http://www.ci.medford.or.us/Files/Fire%20Lane%20Brochure.pdf
OFC 903	OFC 903	903.2.9 Group S-1. An automatic sprinkler system shall be provided	Fire sprinkler system requirement information. Where a fire sprinkler it shall meet the requirements of the Oregon Fire cable National Fire Protection Association (NFPA)

throughout all buildings containing a Group S-1 occupancy where one of the following conditions exists: 1. A Group S-1 fire area exceeds 12,000 square feet (1115 m2). 2. A Group S-1 fire area is located more than three stories above grade plane. 3. The combined area of all Group S-1 fire areas on all floors, including any mezzanines, exceeds 24,000 square feet (2230 m2). 4. A Group S-1 fire area used for the storage of commercial trucks or buses where the fire area exceeds 5,000 square feet (464 m2). 5. A Group S-1 occupancy used for the storage of upholstered furniture or mattresses exceeds 2,500 square feet (232 m2).

Standard. Consult the Medford Water Commission for proper water meter sizing for fire sprinkler systems.

OFC 3201	OFC 3201	High-piled storage	A maximum of 500 sq. ft. of high-piled combustible storage area is allowed without additional requirements. High-piled storage constitutes storage heights over 12' for normal combustibles or storage heights over 6' for certain high-hazard commodities. If the threshold is exceeded, the additional requirements of Oregon Fire Code Chapter 32 must be met (Additional fire protection and building design features for high piled storage).
OFC 503	OFC 503	Gate requirements	Access control devices must be approved by Medford Fire Department. All gates shall have approved locking devices. Manual gates shall have a lock connected to a long length of chain. Automatic gates shall be equipped with an approved emergency services activated opening device (radio frequency microphone click from fire engines opens gate). OFC 503.1; 503.4; 503.5; 503.6

General Information/Requirements

Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only. Specific fire protection systems may be required in accordance with the Oregon Fire Code.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300



Continued from Previous Page

COMMENTS

1. Off-site water line installation is not required.
2. On-site water facility construction is required.
3. Static water pressure is approximately 94 psi. (See Condition 4 above)
4. MWC-metered water service does not exist to these properties at this time.
5. Access to MWC water lines is available. There are 10-inch diameter water lines in Bateman Drive, and Bierson Way. There are existing 8-inch diameter water lines stubbed to the back of the PUE along the west side of Bateman Road. See provided Water Facility Map.



**Water Facility Map
for
AC-18-016
(Old Dominion Freight Line)**

March 14, 2018

Legend

- ⬇ Air Valve
- Sample Station
- ⊕ Fire Service
- ⊕ Hydrant
- ▲ Reducer
- ⊠ Blow Off
- + Plugs-Caps

- Water Meters:**
- ⊕ Active Meter
 - On Well
 - Unknown
 - Vacant

- Water Valves:**
- ⊕ Butterfly Valve
 - ⊕ Gate Valve
 - ⊕ Tapping Valve

- Water Mains:**
- Active Main
 - - - Abandoned Main
 - Reservoir Drain Pipe
 - Pressure Zone Line

- Boundaries:**
- ▭ Urban Growth Boundary
 - ▭ City Limits
 - ▭ Tax Lots

- MWC Facilities:**
- C Control Station
 - P Pump Station
 - R Reservoir



This map is based on a digital aerial photo mosaic. It is not a legal document. It is not intended to be used for legal purposes. It is not intended to be used for legal purposes. It is not intended to be used for legal purposes.



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

March 2, 2018

City of Medford Planning Department
200 S. Ivy Street
Medford, Oregon 97501

Re: AC-18-016, Old Dominion Freight Line (362W36D – 111, 116, 117, 120, 121)

ATTN: Steffen,

The subject property is within the RVSS service area. There are 8 inch sewer mains along Bateman Road and Bierson Way with 4 inch services stubbed to tax lots 111, 116, 117 & 120. Sewer service for the proposed development can be had by connecting to any of these services.

The sewer connection permit will be issued by the City of Medford. However, there will be system development charges owed to Rogue Valley Sewer Services.

Rogue Valley Sewer Services requests that approval of this application be subject to the following conditions:

1. The applicant must provide RVSS with a site utility and plumbing fixture plans for review and the determination of system development charges.
2. The applicant must pay sewer system development charges to Rogue Valley Sewer Service prior to issuance of a building permit.

Please feel free contact me with any questions.

Sincerely,

Nicholas R. Bakke

Nicholas R. Bakke, P.E.
District Engineer

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CITY OF MEDFORD
EXHIBIT # 2
File # AC-18-016



JACKSON COUNTY

Roads

Roads
Engineering

Kevin Christiansen
Construction Manager

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
christke@jacksoncounty.org
www.jacksoncounty.org

March 5, 2018

Attention: Steffen Roennfield
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Freight terminal off Bateman Drive – a county maintained road and Bierson Way – a city maintained road.
Planning File: AC-18-016

Dear Steffen:

Thank you for the opportunity to comment on the consideration of plans for the construction of a 20,423 square foot freight terminal on one parcel totaling 8.6 acres located northwest of the intersection of Bateman Drive and Beirson Way the Light Industrial (I-L) zoning district, (36-2W-36D Tax Lots 111, 116, 117, 120 & 121). Jackson County Roads has the following comments:

1. If frontage improvements are required off Bateman Drive, they shall be permitted and inspected by the City of Medford.
2. Any new or improved road approaches off Bateman Drive shall be permitted and inspected by the City of Medford.
3. The applicant shall submit construction plans to Jackson County Roads, so we may determine if county permits will be required.
4. The County portion of Bateman Drive is a County Local Road and is maintained by the County. The Average Daily Traffic count was 1,389 on July 19, 2016, 50 feet east of Table Rock Road. As a comparison of capacity for Bateman Drive, the capacity of a two lane suburban road with ten foot lanes and no shoulders is 8,832 ADT.
5. Jackson County's General Administration Policy #1-45 sets forth the County's position as it relates to the management of County roads located within existing or proposed city limits or Urban Growth Boundaries (UGB). The County has no current plans for

CITY OF MEDFORD

ENGIN
R

File # AC-18-016

improvements to Bateman Drive Road. Jackson County Roads recommends that the city request jurisdiction of this road.

6. Storm water should meet City of Medford requirements that also include water quality.
7. Jackson County Roads would like to review and comment on the hydraulic report including the calculations and drainage plan. Capacity improvements or on site detention, if necessary, shall be installed at the expense of the applicant. Upon completion of the project, the developer's engineer shall certify that construction of the drainage system was constructed per plan and a copy of the certification shall be sent to Jackson County Roads.
8. We require that the applicant prepare a traffic study that addresses impacts at the intersection of Bateman Drive and Table Rock Road. The study should recommend mitigation if necessary and be reviewed and approved by Jackson County. If mitigations are recommended, they shall be required.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Kevin Christiansen
Construction Manager



Oregon

Kate Brown, Governor



3040 25th Street, SE
Salem, OR 97302-1125
Phone: (503) 378-4880
Toll Free: (800) 874-0102
FAX: (503) 373-1688

March 13, 2018

Steffen Roennfeldt
Planner
City of Medford
200 South Ivy St, Lausmann Annex Rm 240
Medford, OR 97501

Re: File No. AC-18-016 Old Dominion Freight Line

Dear Mr. Roennfeldt:

The Oregon Department of Aviation (ODA) appreciates the opportunity to review and comment on the proposed Old Dominion Freight Line terminal located at the corner of Bierson Way and Bateman Drive (362W36D TL 111, 116, 117, 120, 121).

The Oregon Department of Aviation would like to submit the following comments:

- Prior to issuance of a building permit the applicant must file and receive a determination from the Oregon Department of Aviation as required by OAR 738-070-0060 on FAA Form 7460-1 Notice of Proposed Construction or Alteration to determine if the structure will pose a hazard to aviation safety. A subsequent submittal may be required by the FAA due to its location to the Troutdale Airport.
- The height of the new structure should not penetrate FAA Part 77 Imaginary Surfaces, as determined by ODA and the FAA.
- Shields on any external lights should be designed as to not interfere with aircraft or airport operations.
- Marking Lights, per FAA design, may be needed to identify to structures.
- Coordination with the Rogue Valley Int'l Airport and the Medford Air Traffic Control tower may be needed to issue a NOTAM during the construction.

ODA appreciates the opportunity to comment on this application. The Department requests to be identified as a party of record for any future land use application.

If you have any questions or need further information or clarification on the comments, please feel free to contact me at 503-378-2529 or Jeff.Caines@aviation.state.or.us.

Sincerely,

Jeff Caines, AICP
Aviation Planner
Oregon Department of Aviation

CITY OF MEDFORD
EXHIBIT # 3
File # Ac-18-016

Steffen K. Roennfeldt

From: Marcy Black <BlackMA@jacksoncounty.org>
Sent: Friday, March 9, 2018 3:33 PM
To: Steffen K. Roennfeldt
Subject: File No. AC-18-016 Project Name: Old Dominion Freight Line

Steffen:

The Airport requests an Avigation, Noise & Hazard Easement be required as a condition for the project.

Also, due to the proximity to the Airport, the applicant needs to contact the FAA regarding filing a 7460-1 Notice of Proposed Construction or Alteration. The FAA contact is: Paul Holmquist, phone (425) 227-2625.

Thanks,

Marcy Black
Deputy Director-Administration

CITY OF MEDFORD
EXHIBIT # 1
File # AC-18-016

RECEIVED
FEB 09 2010
PLANNING DEPT

LEGAL DESCRIPTION FOR 5 COMBINED LOTS
OLD DOMINION FREIGHT LINE
TRUCKING FREIGHT TRANSFER TERMINAL

EXHIBIT "A"

Lot Ten (10) of **TABLE ROCK INDUSTRIAL PARK** in the City of Medford, according to the Official Plat thereof described in Volume 14, Page 100, Plat Records, Jackson County, Oregon.

TOGETHER WITH: Lots Two (2), Four (4), Twenty-two (22) and Twenty-three (23) of **BIERSON INDUSTRIAL PARK**, in the City of Medford, according to the Official Plat thereof described in Volume 21, Page 25, Plat Records, Jackson County, Oregon.



Planning Department

Working with the community to shape a vibrant and exceptional city

MEMORANDUM

Subject Legal Description
 File no. AC-18-016
 To Jon Proud, Engineering
 From Steffen Roennfeldt, Planning Department
 Date February 26, 2018

Please verify the attached legal description covering the below subject at your earliest convenience. See attached map.

1. AC-18-016
 Owner; Old Dominion Freight line, Jerry Canada
 Applicant: Gerald Reynolds

Steffen, the description describes the subject area as shown on vicinity map.

But there are 5 distinct units of land not officially "Combined" lots if it matters(?) Also multiple owners per front counter other than Old Dominion... if that matters.

cp

THANKS, Jon

Attachments:

3/2/18

Vicinity Map, Legal description

RECEIVED
FEB 09 2010
PLANNING DEPT

**LEGAL DESCRIPTION FOR 5 COMBINED LOTS
OLD DOMINION FREIGHT LINE
TRUCKING FREIGHT TRANSFER TERMINAL**

EXHIBIT "A"

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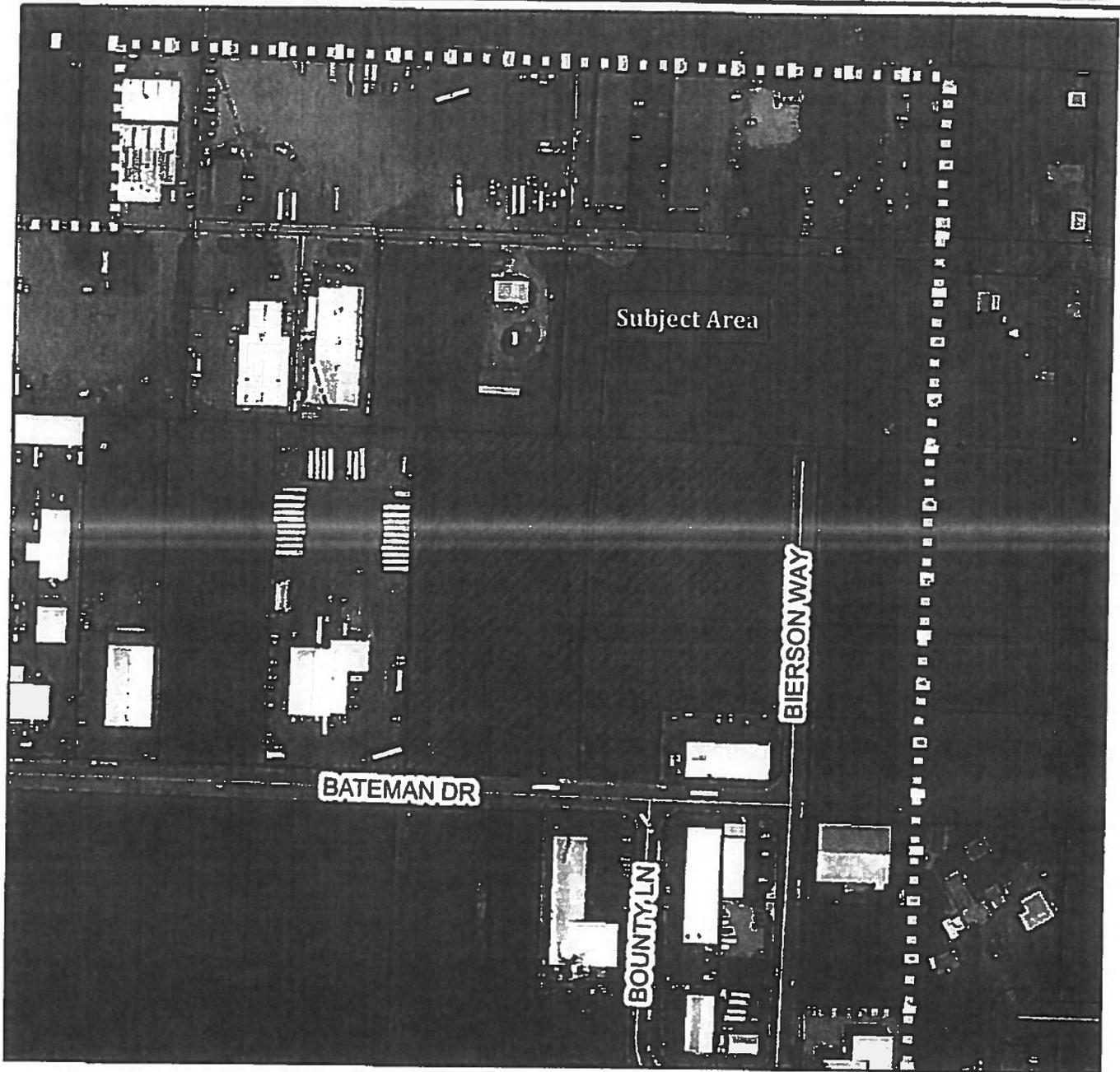
**CITY OF MEDFORD
EXHIBIT # _____
File # AC-18-016**



City of Medford
Planning Department

Vicinity
 Map

File Number:
AC-18-016



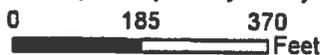
Project Name:

Old Dominion Freight Line

Map/Taxlot:

362W36D

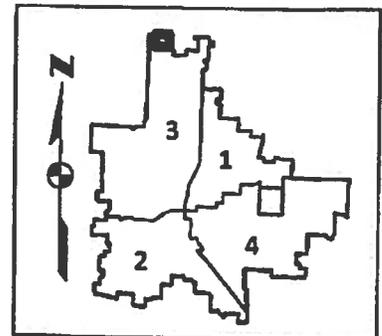
TL 111, 116, 117, 120, 121



02/28/2018

Legend

-  Subject Area
-  Tax Lots
-  City Limits
-  UGB





CITY OF MEDFORD PLANNING DEPARTMENT

DATE

APPLICANT

Re: File No.: PLA#17-172

The above property line adjustment application has been preliminarily approved consistent with MLDC Section 10.297. Within one year of the preliminary approval date of January 30, 2018, the following shall be submitted to the Planning Department for final review and approval:

1. A Map of survey showing the adjusted property lines prepared by an Oregon licensed surveyor in accordance with the procedures of ORS 92.060(7) and 209.250. This requirement applies to all properties regardless of size.
2. A report from a title company prepared within fifteen (15) days listing the current vested owners, easements of record, encumbrances, and other matters of record.
3. A copy of proposed easements to be recorded. Proposed easements may be included as a reservation on the property line adjustment deeds.
4. Deeds which include a statement that identifies the associated conveyance of property as a property line adjustment and labeled as a Property Line Adjustment. If a property line is being eliminated, the deeds shall be labeled Property Line Adjustment – Lot Consolidation.
5. Property descriptions attached to the deeds shall either describe the resultant properties or otherwise specify that the conveyed land shall be consolidated with the property of the grantee. A property line adjustment deed shall contain the names of the parties, the description of the adjusted line, references to original recorded documents, and signatures of all parties with proper acknowledgement.

Under MLDC 10.297(2)(b), the City must complete the final review within 25 days of the above submittals. We recommend that the final documents noted above be submitted no less than 30 days prior to the expiration date so that the approval does not expire during the final review.

If you have any questions regarding this application, please feel free to call me.

Debbie Waldron

Debbie Waldron
Planner I

“Working with the Community to Shape a Vibrant and Exceptional City”

Lausmann Annex • 200 South Ivy Street • Medford OR 97501

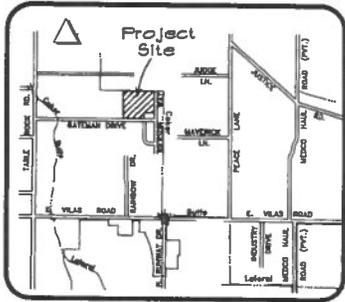
Phone (541)774-2380 • fax (541)618-1708

www.ci.medford.or.us

CITY OF MEDFORD
EXHIBIT # *✓*
File # *AC-18-016*

VICINITY MAP

N.T.S.



SCHOOL DISTRICT: 6 (CENTRAL POINT)
 IRRIGATION DISTRICT: R.R.V.I.D.
 SANITATION DISTRICT: R.V.S.S.
 AREA: 892 ACS
 ZONING: I-L LIMITED INDUSTRIAL
 EXISTING USE: VACANT
 PROPOSED USE: LIGHT INDUSTRIAL

Page 165

REGISTERED
 PROFESSIONAL
 LAND SURVEYOR

David M. Minnecci
 OREGON
 JULY 30, 1988
 DAVID M. MINNECCI
 2349

EXPIRES 12/31/18

26
 365 411 36D
 TL 111, 116, 117 120 & 121

RECEIVED

DEC 29 2017

PLANNING DEPT.



PROPERTY LINE ADJUSTMENT/
 CONSOLIDATION

RECEIVED

DEC 29 2017

PLANNING DEPT.

Located in:
 the S.W. 1/4 of Section 36 (36D)
 in Township 36S., R.11W., W.1M
 City of Medford,
 Jackson County, Oregon
 Tax Lot 111, 116, 117, 120 & 121
 December 29, 2017

APPLICANT

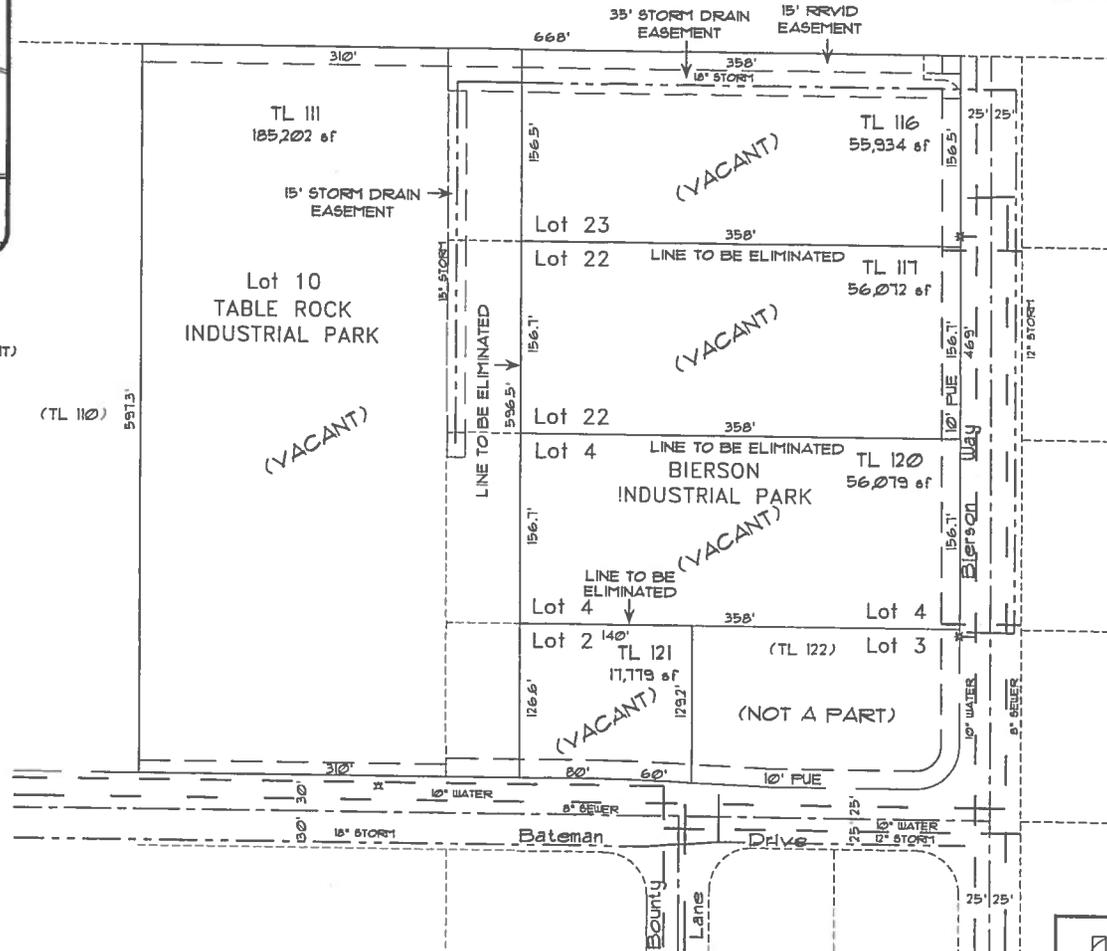
Old Dominion Freight Line
 c/o Lindzi Bell
 52 West 100 North
 Heber City, Utah 84032
 (435) 654-4828

OWNERS

- (TL 111 & 121)
 Llyod Bendickson/Dennis Sullivan
 1985 Rossanley Drive
 Medford, Oregon 97501
- (TL 116)
 Roger D. Pratt Living Trust
 3715 Brownsboro Hwy.
 Eagle Point, Oregon 97524
- (TL 117)
 Llyod Bendickson/Casebros, LLC
 386 Bateman Drive
 Central Point, Oregon 97502
- (TL 120)
 Larry & Mary Osborn Revocable
 Living Trust
 4761 Footh Creek Road
 Gold Hill, Oregon 97525

SURVEYING/AGENT

Hoffbuhr & Associates
 880 Golf View Dr., Suite 201
 Medford, Oregon 97504
 (541) 779-4641



CITY OF MEDFORD

EXHIBIT #

File # PLA-17-172

BY: DAVID MINNECCI	LS 2349
DATE:	DECEMBER 29, 2017
PROJECT:	
PROJECT NO.	17-131
DRAWING FILE NO.	17131SITE.DWG
SCALE:	1" = 100'
REVISION NO.	
REVISION DATE:	
BASIS OF BEARING:	
DRAWN BY:	DMM
REVIEWED BY:	
SHEET 1 OF 1	

RECEIVED
FEB 09 2010
PLANNING DEPT

DRAINAGE REPORT

Old Dominion Freight Line
387 Bateman Drive
Medford, OR 97502

Submitted To: City of Medford

Prepared For: Furst Construction
708 West North Temple
Salt Lake City, UT 84116
801 972-3838

Prepared By: Legend Engineering
52 West 100 North
Heber City, UT 84032
435 654-4828
866 310-9972 fax

January 26, 2018



IX. CONTROL STRUCTURE DESIGN

The detention system release rate will be controlled by an outlet structure (4-foot diameter precast manhole) with a 5.0" orifice. The outlet structure will have a bypass at the 10-year highwater elevation in order to pass the 100-year storm event.

ORIFICE PLATE			
$Q = CA\sqrt{2gh}$			
Q=	1.04		
H=	2.50		
		A=	0.136 ft ²
g=	32.2	=	19.63 in ²
C=	0.6		
		Diameter =	5.0 inches

X. WATER QUALITY

In order to provide water quality enhancement, the underground detention system is designed with a sedimentation (isolator) row. The sediment row is specifically designed to capture suspended sediment and floatables present in stormwater runoff as part of the complete system. Flows from the WQCV will enter the detention system first and will be routed through the sediment row where rock/sand/silt, trash and other debris are captured. The isolator row will release clean water into the downstream outlet pipe.

See additional information in Appendix B.

XI. SUMMARY

Old Dominion Freight Lines has been designed to treat and mitigate stormwater runoff. Stormwater facilities have been provided to control stormwater runoff per City of Medford requirements. As shown in the calculations herein, the total runoff volume provided exceeds the total volume required for the 10-year storm event. All stormwater will be treated and detained on-site and released at the existing downstream location. See grading and drainage plan for finished grades and storm water facility locations.

RECEIVED

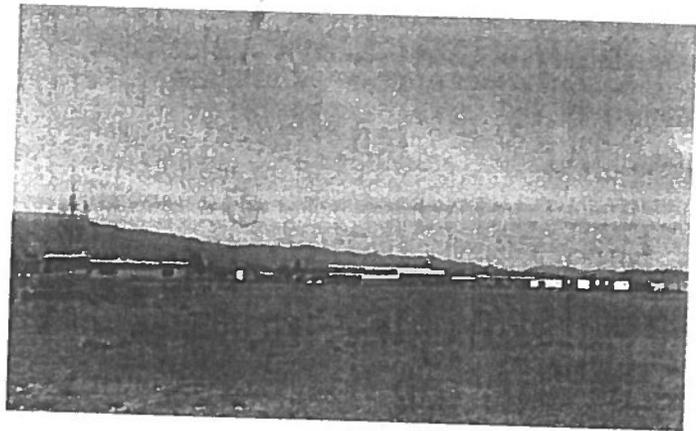
FEB 09 2010

PLANNING DEPT

Geotechnical Engineering Report

Old Dominion Freight Line Service Center - Medford
387 Bateman Drive
Medford, Oregon

October 27, 2017
Terracon Project No. 82175075



Prepared for:

Old Dominion Freight Line, Inc.
Thomasville, North Carolina

Prepared by:

Terracon Consultants, Inc.
Portland, Oregon

CITY OF MEDFORD

EXHIBIT # Y

File # AC-18-016

terracon.com

Terracon

Environmental



Facilities

Page 168

Geotechnical



Materials



City of Medford

Planning Department

Vicinity
Map

File Number:

AC-18-016



Project Name:

Old Dominion Freight Line

Map/Taxlot:

362W36D

TL 111, 116, 117, 120, 121



02/28/2018

Legend

-  Subject Area
-  Tax Lots
-  City Limits
-  UGB

