

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA OCTOBER 16, 2020



MEDFORD
OREGON

Commission Members

Jeff Bender, Chair

Bill Chmelir, Vice Chair

Jim Catt

David Culbertson

Paul McClay

Bob Neathamer

Marcy Pierce

Milo Smith

Chris Zelmer

Regular Commission meetings are held on
the first and third Fridays of every month

Beginning at 12:00 Noon

City of Medford

Council Chambers,

Third Floor, City Hall

411 W. 8th Street

Medford, OR 97501

(541) 774-2380

SITE PLAN & ARCHITECTURAL COMMISSION AGENDA



October 16, 2020

12:00 P.M.

Medford City Hall, Council Chambers
411 West 8th Street, Medford, Oregon

10. Roll Call

20. Consent Calendar

20.1 **AC-20-205** Final Order for a six-phase, multi-family development consisting of 14 buildings with six units each—a total of 84 dwelling units. (The proposed development is running concurrent with LDS-20-207 and CUP-20-208.) The property consists of a single parcel totaling 4.76 acres, and is located at the corner of Skypark Drive and Crater Lake Avenue (1971 Skypark Drive). The property is zoned MFR-15 (Multiple Family Residential, 15 dwelling units per gross acre). (371W18AA TL 2300).

20.2 **AC-20-226** Final Order for an approximately 23,000 square foot single story trucking warehouse on a 1.54 acre parcel located at 1567 N Central Avenue in the General Industrial (I-G) zoning district (372W24701).

30. Approval or Correction of the Minutes.

30.1 Consideration for approval of minutes from the October 2, 2020, meeting.

40. Oral Requests and Communications from the Audience

COMMENTS WILL BE LIMITED TO 3 MINUTES PER INDIVIDUAL OR 5 MINUTES IF REPRESENTING A GROUP OR ORGANIZATION. **PLEASE SIGN IN.**

50. Public Hearings

COMMENTS ARE LIMITED TO A TOTAL OF 10 MINUTES FOR APPLICANTS AND/OR THEIR REPRESENTATIVES. YOU MAY REQUEST A 5-MINUTE REBUTTAL TIME. ALL OTHERS WILL BE LIMITED TO 3 MINUTES PER INDIVIDUAL OR 5 MINUTES IF REPRESENTING A GROUP OR ORGANIZATION. **PLEASE SIGN IN.**

Old Business.

50.1 **AC-20-227** Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres, located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300). Applicant: Southern Oregon Subaru; Agent: J.B. Steele, Inc.; Planner: Dustin Severs

Meeting locations are generally accessible to persons with disabilities. To request interpreters for hearing impaired or other accommodations for persons with disabilities, please contact the ADA Coordinator at (541) 774-2074 or ada@cityofmedford.org at least three business days prior to the meeting to ensure availability. For TTY, dial 711 or (800) 735-1232.

- 60. Written Communications None.
- 70. Unfinished Business None.
- 80. New Business None.
- 90. Report from the Planning Department
- 100. Messages and Papers from the Chair
- 110. Propositions and Remarks from the Commission
- 120. Adjournment

COMMISSION REPORT

for a type-III quasi-judicial decisions: **Site Plan & Architectural Review**

Project Creekside Village
Applicant: Windy Creek, LLC; Agent: Joe Slaughter Consulting

File no. AC-20-205

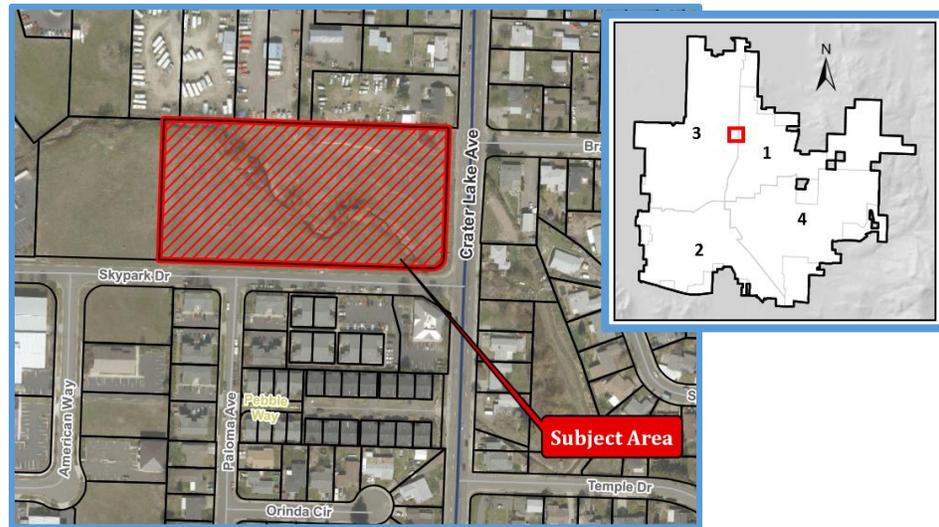
Date October 2, 2020

BACKGROUND

Proposal

Consideration of plans for a six-phase, multi-family development consisting of 14 three-story buildings with six units each—a total of 84 dwelling units. (The proposed development is running concurrent with LDS-20-207, CUP-20-208, and GF-20-288) The property consists of a single parcel totaling 4.76 acres, and is located at the corner of Skypark Drive and Crater Lake Avenue (1971 Skypark Drive). The property is zoned MFR-15 (Multiple Family Residential, 15 dwelling units per gross acre) (371W18AA TL 2300).

Vicinity Map



Subject Site Characteristics

Zoning: MFR-15
GLUP: UM (Urban Medium Density Residential)
Overlay(s): A-C (Airport Area of Concern)
Use(s): vacant

Surrounding Site Characteristics

North Zone: SFR-6 (Single Family Residential, 6 dwelling units per gross acre) & I-L (Light Industrial)
Use(s): Residential

South Zone: MFR-20 (Multiple Family Residential, 20 dwelling units per gross acre)
Use(s): Residential

East Zone: SFR-4 (Single Family Residential, 4 dwelling units per gross acre)
Use(s): Residential

West Zone: I-L
Use(s): Vacant

Related Projects

CP-99-120 GLUP amendment from UH to UM
AC-00-173 Multi-family project (expired)
PUD-07-078 Multi-use PUD (terminated in 2008)
AC-07-285 Creekside Village (expired)
AC-09-008 Creekside Village Apartments (expired)
LDS-20-207 LDS application running concurrent with subject request
GF-20-288 Riparian reduction request (approved)

Applicable Criteria

Medford Municipal Code §10. 200(E)(2) – Site Plan & Architectural Review Criteria

(2) The approving authority shall approve a site plan and architectural review application for a residential development if the proposed development complies with the applicable provisions of all city ordinances, or if the Site Plan and Architectural Commission has approved either of the following:

(a) Any Exceptions, as provided for in MLDC Section 10.186, which resolve(s) any instances of non-compliance with those provisions.

(b) Any Adjustments or Exceptions from the Special Development Standards for Multiple-Family Dwellings, as provided for in MLDC Section 10.715A through 10.717.

(c) Any Adjustments or Exceptions from the Development Standards for a Cottage Cluster Development, as provided for in MLDC Section 10.818A.

Approval Authority

This is a Type III land use decision. SPAC is the approving authority under MLDC 10.110(D).

Corporate Names

According to the Oregon Secretary of State Business Name Registry, the principal place of business, Windy Creek LLC., is located in Ashland, Oregon, and its registered agent is Laura Knapp.

ISSUES AND ANALYSIS

Project Summary

Current site conditions

The subject site consists of a single, vacant parcel totaling 4.76 acres. The parcel has two street frontages: Skypark Drive, a Standard Residential street, along its southerly boundary; and Crater Lake Avenue, a Major Arterial street, along its easterly boundary. The parcel includes significant environmental constraints: Lone Pine Creek, a protected waterway classified as a Riparian Corridor, traverses the parcel east/west; and a substantial portion of the parcel is located within a 1% floodplain.

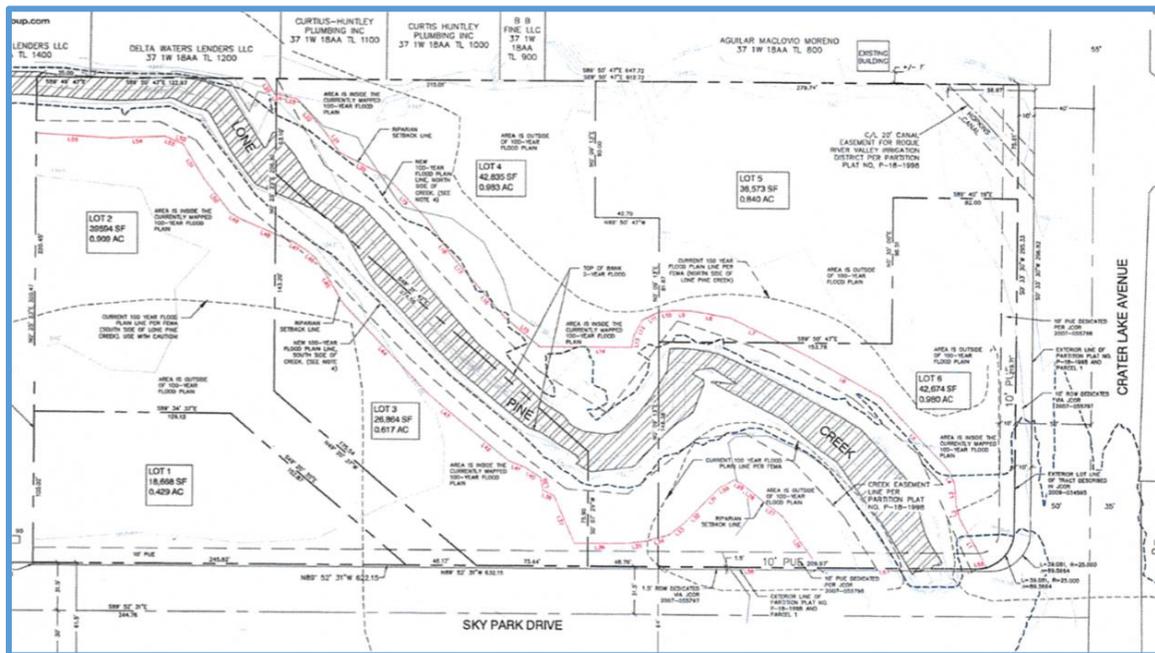


Previous Land Use Actions

The subject property has received previous approvals through SPAC for a multi-family development; however, building permits were never issued for the development, and the SPAC approvals ultimately expired.

The subject application was submitted concurrent with requests for a 6-lot subdivision; a Conditional Use Permit in order to construct a driveway crossing, a multi-use path, and drainage facilities within the riparian corridor of Lone Pine Creek; and a request for a 25-foot riparian reduction submitted to the Planning Director. The CUP request was heard before the Planning Commission on September 10, 2020, and the final order approving the proposal was adopted September 24, 2020. The riparian reduction request was approved by the Planning Director on September 30, 2020 (Exhibit Q).

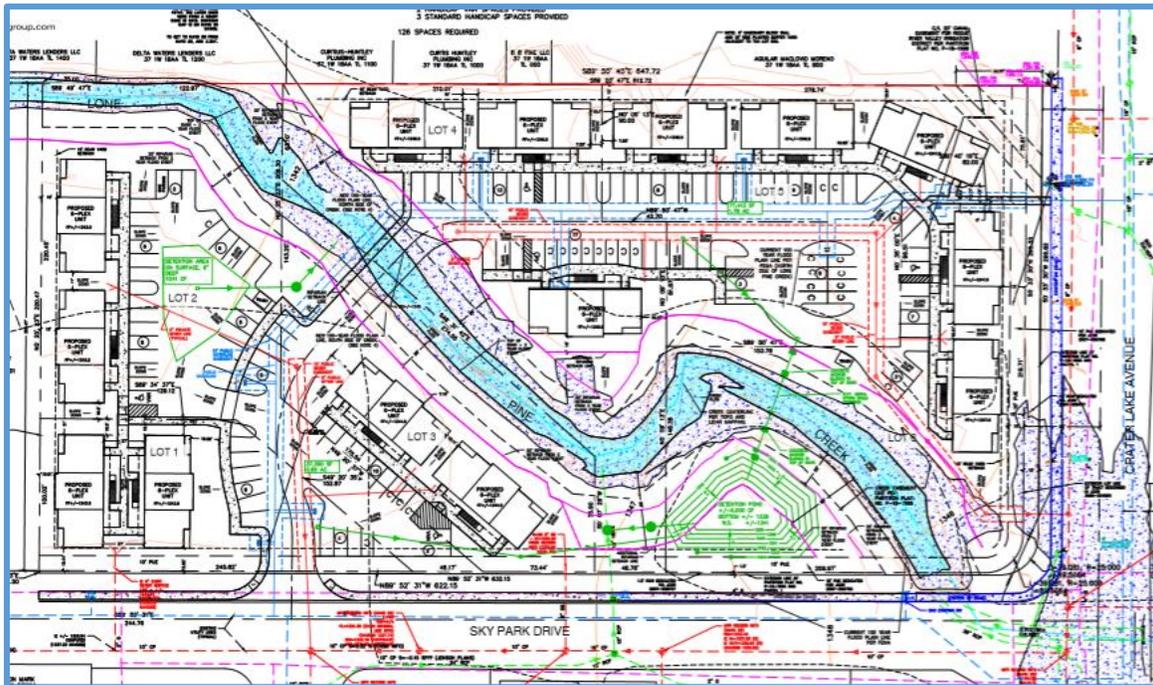
Approved Tentative Plat (Exhibit F)



With the subject request, the applicant is proposing a multi-family development, consisting of 14 buildings with six units each—a total of 84 dwelling units. Each building is three stories, and will include five single-story units, and one two-story unit. It is the applicant’s intent to develop the property in six phases—reflecting the individual lots shown on the tentative plat—and, pursuant to MLDC 10.200(G)(2), has requested the maximum timetable of five years to develop the property.

As part of staff’s review, the bulk standards (e.g., setbacks, lot coverage) for each building shown on the site plan were calculated based on the lots individually, while density, landscaping, and parking were calculated for the development as a whole.

Proposed Site Plan (Exhibit B)



Per MLDC 10.550(3), driveway access off of an arterial or collector street is prohibited when a development abuts a lower-order street. Accordingly, the submitted site plan shows access to the site via a single driveway off of Skypark Drive. It is this Code provision—restricting vehicular access off of Crater Lake Avenue—which necessitates the construction of the bridge across Lone Pine Creek in order to provide vehicular access to the northeasterly half of the development. The construction of the driveway over Lone Pine Creek—a protected riparian corridor—received approval from the Planning Commission as part of the applicant's CUP request.

Stormwater detention and water quality is proposed to be addressed via a detention pond located at the southeast corner of the site, within Lot 6; and a second detention area located to the west of the site, within Lot 2.

The subject request is contingent on the applicant obtaining approval from the Planning Director for a 25-foot reduction within the riparian corridor of Lone Pine Creek, pursuant to MLDC 10.927. The Planning Director approved the riparian reduction request on September 30, 2020 (Exhibit Q).

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Density

Density Table

MFR-15	Allowed	Shown
Min. /Max. Density <i>10 to 15 dwelling units per gross acre</i>	56 min. / 85 max.	84 units

Based on 5.64 gross acres, the Commission can find that the proposal of 84 units meet the density requirements as outlined in MLDC 10.714. The required density was calculated based on the project as a whole, and not for each lot individually.

Development Standards

Residential Development Table (MLDC 10.721)

MFR-15 Zone	Allowed/Required	Proposed/Shown
Height	35 feet	27.4 feet
Lot Coverage	50%	10% – 23% (15% overall)
Setback (front)	15 feet	15 ft.
Setback (rear yard)	10 ft.	10 ft.
Setback (side yard)	5.5 ft.	5.5 ft.
Setback (street side yard)	10 ft.	10 ft.

As shown in the Site Development Table above, it can be found that the structures identified on the submitted site plan meet the bulk standards for the MFR-15 zoning district as found in Article V of the Medford Land Development Code.

Parking

Parking Table (MLDC 10.743-751)

	Required	Proposed/Shown
Vehicle Spaces	126 min. (no max)	126
Handicap Spaces	5	5
Bicycle Spaces	84	84

The submitted site plan shows a total of 126 parking stalls, including five handicap spaces, consistent with the requirements outlined in MLDC 10.743-751. The required parking was calculated based on the project as a whole, and not each lot individually. As such, a shared parking agreement will be required with the final plat of the site.

The applicant's findings state, "Secured bicycle parking will be provided within each of the units"; however, the submitted floor plan does not identify areas designated for bicycle parking. As a condition of approval, the applicant will be required to submit a revised floor plan showing the units providing areas designated for secure bicycle parking, consistent with the requirements outlined in MLDC 10.747-750.

Landscaping

Frontage Landscaping (MLDC 10.797)

Crater Lake Ave.	Required	Shown
Trees	13	13
Shrubs	80	113

Skypark Drive	Required	Shown
Trees	24	25
Shrubs	153	195

Parking Area Planters (MLDC 10.746)

	Required	Shown
Trees	15	15
Shrubs	30	103
Total coverage	2,520 sf.	2,551 sf.

The applicant's submitted Planting Plan (Exhibit G-1) shows landscaping along the site's two street frontages and parking area planters within the site's parking area, meeting the minimum requirements of the Code as outlined in MLDC 10.797 and 10.746.

Pedestrian Access (MLDC 10.772-776)

The site plan shows pedestrian walkways connecting to the public sidewalks along both Crater Lake Avenue and Skypark Drive, connecting the proposed buildings to

each other, and connecting to the abutting properties, all consistent with the pedestrian connectivity standards found in MLDC 10.772-776.

Concealments

Trash Enclosure (MLDC 10.781)

The applicant's site plan shows two trash enclosures: one located on the south side of Lone Pine Creek, and one located on the north side. The applicant has also submitted an elevation plan of the trash enclosure, showing the enclosure constructed with materials consistent with the requirements outlined in MLDC 10.781.

HVAC Equipment (MLDC 10.782)

All HVAC equipment is required to be concealed using the architectural elements of the building (i.e., roof forms, parapets, wing walls, alcoves, etc.). The HVAC equipment proposed for the project is not shown on the site plan nor is it addressed in the applicant's findings.

As a condition of approval, the applicant will be required to submit plans showing that all proposed HVAC equipment is concealed consistent with the requirements outlined in MDLC 10.782.

Architecture



All fourteen proposed buildings have identical floor plans and dimensions. The elevation plans are likewise identical, with the exception of windows being excluded from the side elevations of those buildings not facing the street. (The side elevation of four buildings—two on the west side of the access driveway, one on the east side of the driveway, and one at the northeast corner of the site—will have side elevations facing a street, and will include at least 15% windows, as required.) Building materials

include Hardi Board and batten siding and Hardi lap siding. Wall surfaces also include building articulations such as off-sets, balconies, windows, and entries that break-up the building both vertically and horizontally.

MLDC 10.715(A) through 10.717, establishes a series of clear and objective standards for multi-family developments consisting of three or more attached dwelling units. The special development standards—as outlined in MLDC 10.717—address building orientation and entrances; building mass and façade; building articulation; building materials; roof forms; and vehicle circulation and parking.

Per MLDC 10.719(A)(a-b), cited below, an applicant may request an optional adjustment of the special development standards if it can be found that they meet the applicable criteria.

10.719 Optional Adjustment of Special Development Standards, Review Criteria

A. Notwithstanding Sections 10.715A through 10.717, if an applicant affirmatively elects to request review (in writing) under this section, the approving authority may approve a site plan and architectural review application for a multiple-family dwelling development if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

(a) The requested adjustment will allow the project to achieve an equivalent or higher quality design than would otherwise result through strict adherence to the standards, through architectural massing, features or details to distinguish elements of the building; vibrant facades with visual detail; and enhanced public and private spaces that contribute positively to the site, streetscape, and adjoining properties; and

(b) The requested adjustment will allow the project to achieve an equivalent or higher quality design than would otherwise result through strict adherence to the standards through an overall site design that promotes safety, security, and privacy, and reduces visual, noise, and lighting impacts of the development on adjacent properties.

B. Denial of the application. If the approving authority finds that an application for residential development reviewed under this section does not satisfy the conditions of 10.719A, the approving authority shall also review the application as set forth in Sections 10.715A through 10.717. If the application does not satisfy the requirements of Sections 10.715A through 10.717 either, the Site Plan and Architectural Commission shall make such findings on the record.

Pursuant to MLDC 10.719(A)(a-b), cited above, the applicant has requested that the optional adjustments of the special development standards be applied to the project. The buildings meet all of the development standards outlined in MLDC 10.717(A-F), with the exception of three, addressed below.

MLDC 10.717(A) – Building Orientation and Entrances

This standard requires that multi-family buildings be oriented to public streets and intersections and to have the main entrance(s) of the ground floor units facing the street frontage. Four of the six buildings adjacent to streets meet this standard; however, the building entrances for two of the buildings along Crater Lake Avenue are proposed to face the central parking area, and not the street.

MLDC 10.717(F) – Vehicle Circulation and Parking

This standard restricts parking spaces from being located within any required front yard area, as well as restricting automobile circulation or parking areas from being located between buildings and the street. The site plan is designed generally with the buildings around the perimeter of the site and the parking in the center. The two buildings located on Lot 3, however, do deviate from this standard, with the parking located between the two buildings and Skypark Drive. The applicant's findings state that this parking area "is located between the building and the street due to the shape of the buildable areas within the site."

MLDC 10.717(B)(3) – Building Mass and Façade

This standard requires that street facing façades contain windows covering a minimum of 15% of the façade length for each floor level. The submitted elevation plan show windows meeting this standard for the front elevations, but does not show windows featured on the third floor on the side elevations. As a condition of approval, the applicant will be required to submit a revised Elevation Plan showing the windows covering 15% of the façade length on the third floor level, consistent with MLDC 10.717(B)(3).

Staff is supportive of the applicant's request for the optional adjustment of the special development standards to be applied to the site, as allowed per MLDC 10.719, with the imposition of a condition of approval, requiring that the applicant submit revised plans showing windows covering 15% of the façade length on the third floor level for the buildings' side elevations which face the street, consistent with MLDC 10.717(B)(3).

In regards to MDLC 10.717(A) – *Building Orientation and Entrances*, the two subject buildings front along Crater Lake Avenue, a Major Arterial street which will generate significant noise. The applicant is proposing to install plantings, as well as a fence, along the site's frontage with Crater Lake Avenue, features which will serve as a visual and sound buffer. And though this adjustment would require the buildings' rear elevations to face the street, the rear elevations provide architectural elements similar to the front elevations (e.g., off-sets, windows, balconies, etc.). Given the circumstances, it is staff's view that locating the buildings' entrances toward Crater Lake Avenue, from a livability/design standpoint, would be both impractical and

undesirable. It is further staff's view that this adjustment will allow the project to achieve a higher quality design than would otherwise result in the strict adherence to the standards though an overall site design which reduces visual and noise impacts from the adjoining street.

In regards to *MLDC 10.717(F) – Vehicle Circulation and Parking*, it is staff's view that the proposed site layout—locating the parking between Skypark Drive and the two buildings located on Lot 3—is necessary given the environmental constraints of the site. Additionally, the report submitted by ODFW (Exhibit M-1) states that they would prefer the buildings be located to the rear, as the buildings will provide shade to the creek. Finally, it is staff's view that the requested adjustment will achieve a higher quality design than would otherwise result through strict adherence to the standards outlined in MLDC 10.717(A-F); and, therefore, meets the criteria of MLDC 10.719(A)(a-b) for optional adjustments of the special development standards.

Floodplain

A significant portion of the property is located within a 1% flood Hazard Area. Prior to the issuance of building permits, the applicant will be required to comply with all requirements outlined in the Floodplain report (Exhibit O).

Landscape buffering

Per MLDC 10.790, bufferyards are utilized in order to minimize potential conflicts caused by types and intensity of uses on adjacent properties. The subject site abuts the SFR-6 zoning district along approximately 333 feet of a single parcel (Tax lot 800), located along the site's northwesterly boundary.

Table 10.790-3. Bufferyard Types

<u>Type</u>	<u>Width</u>	<u>Wall</u>
A	10 feet	Six (6) foot concrete or masonry wall.
B	20 feet	Eight (8) foot concrete or masonry wall

Per MLDC 10.790-3, a Type A bufferyard—consisting of a six-foot concrete or masonry wall and ten feet of landscaping—is required along the site's boundary abutting the SFR-6 zoning district. The applicant's site plan notates the construction of a six-foot masonry wall and the installation of ten feet of landscaping along the site's boundary with tax lot 800, consistent with the requirements outlined in MLDC 10.790.

Riparian reduction request

The site is encumbered by the presence of Lone Pine Creek, which traverses the property east/west, effectively splitting the property into two tracts of land. Per MLDC 10.922, Lone Pine Creek is identified as a protected waterway within the City. As such, a 50-foot riparian corridor—measured horizontally from the top-of-bank on both sides of the creek—is applied to the section of Lone Pine Creek running through the site, restricting development within this established corridor.



Per MLDC 10.925, water-related or water-dependent uses—including drainage facilities, crossings, and multi-use paths—are allowed within a riparian corridor subject to the approval of a Conditional Use Permit (CUP). The applicant requested a CUP in order to construct a driveway crossing, drainage utilities, and a multi-use path within the riparian corridor of Lone Pine Creek. The Planning Commission approved the CUP request at the meeting held on September 24, 2020.

In addition to the CUP request, the applicant has requested—pursuant to MLDC 10.927—a 25-foot reduction in order to construct areas of the development within the outer 25-feet of the required 50-foot riparian corridor. A request to reduce or deviate from a riparian corridor boundary, however, is a Type-I ministerial decision rendered by the Planning Director, and is a separate from the subject SPAC request.

As required per MLDC 10.927, the applicant has submitted Planting and Irrigation Plans (Exhibit G-1) intended to show the restoration and enhancement of the riparian area. These plans were also forwarded to the Oregon Department of Fish and Wildlife (ODFW) for a habitat mitigation recommendation. Staff received a report from ODFW

(Exhibit M-1) on September 30, 2020. In the report, ODFW forwarded a favorable recommendation for the proposed reduction.

On September 30, 2020, the applicant received approval for the requested 25-foot riparian reduction from the Planning Director (Exhibit Q).

Facility Adequacy

Per the agency comments submitted to staff, (Exhibits J-L), it can be found that there are adequate facilities to serve the future development of the site.

Other Agency Comments

Parks Department (Exhibit P)

The applicant has also been working with the Parks Department concerning the future installation of the multi-use path identified in the City's Leisure Services Plan. On September 3, 2020, staff received a memo from the Parks Department, stating that the applicant will install a soft-surface trail within the riparian corridor. Details concerning the location of the path are still being worked out with the Parks Department.

As a condition of approval, the applicant will be required to comply with all requirements of the Parks Department, prior to the issuance of a building permit for vertical construction.

Committee Comments

No comments were received from a committee, such as BPAC.

Neighbor Comments

Staff received a letter on August 27, 2020, from a neighboring business, Winkelman-Bruce & Traux, LLP, an accounting firm located at 2732 American Way. In the letter, Ms. Winkelman-Bruce requests that parking be disallowed on Skypark Drive on one side of the street, and that parking be disallowed the last few feet leading to the corner of Skypark Drive and Crater Lake Avenue.

Citizen requests pertaining to traffic matters—including requests to eliminate public on-street parking spaces—are heard by the Traffic Coordinating Committee (TCC), which provides recommendations to the Public Works Department. The subject letter has been forwarded to the Public Works Department for processing, and the neighbor has been informed.

DECISION

At the public hearing held on October 2, 2020, the Commission voted unanimously to approve the request. During the hearing, staff added two new exhibits into the record: A revised Floor Plan, included as Exhibit C-1; and a revised Elevation Plan, included as Exhibit D-1. The two revised plans were submitted by the applicant in response to a condition of approval requiring that the plans show windows on the third floor of the buildings, as required per the Special Development standards for multi-family buildings. As the revised plans satisfied the condition, the Commission struck the condition.

The two new exhibits (Exhibits C-1 & D-1) and the revised Conditions of Approval (Exhibit A-2) are included in this Commission Report.

FINDINGS AND CONCLUSIONS

Medford Municipal Code §10. 200(E)(2) – Site Plan & Architectural Review Criteria

(2) The approving authority shall approve a site plan and architectural review application for a residential development if the proposed development complies with the applicable provisions of all city ordinances, or if the Site Plan and Architectural Commission has approved either of the following:

(a) Any Exceptions, as provided for in MLDC Section 10.186, which resolve(s) any instances of non-compliance with those provisions.

This criterion is not applicable.

(b) Any Adjustments or Exceptions from the Special Development Standards for Multiple-Family Dwellings, as provided for in MLDC Section 10.715A through 10.717.

Based on the applicant's site plan and submitted findings, the Commission can find that, with the imposition of conditions outlined in Exhibit A, the requested adjustment from the special development standards will achieve an equivalent or higher quality design than would otherwise result through strict adherence to the standards outlined in MLDC 10.717(A-F), and therefore, meets the criteria of MLDC 10.719 for optional adjustments of the special development standards.

(c) Any Adjustments or Exceptions from the Development Standards for a Cottage Cluster Development, as provided for in MLDC Section 10.818A.

This criterion is not applicable.

ACTION TAKEN

Adopted the findings as recommended by staff and directed staff to prepare a Final Order for approval of AC-20-205 per the Commission Report dated October 2, 2020, including:

- Exhibits A-2 through Q;
- The granting of the maximum timetable of five years for the project to be developed in phases, pursuant to MLDC 10.200(G)(2); and,
- The approval of optional adjustments of special development standards, pursuant to MLDC 10.719.

EXHIBITS

- A-2 Conditions of Approval, drafted October 2, 2020.**
B Site Plan, submitted September 21, 2020.
C-1 Floor Plans, submitted October 1, 2020.
D-1 Elevations, submitted October 1, 2020.
E Trash enclosure elevation, submitted September 8, 2020.
F Applicant's Approved Tentative Plat, submitted July 13, 2020.
G-1 Applicant's Planting & Irrigation Plans (8 of 8), submitted September 26, 2020.
H Applicant's Site Work and Protection Plan, submitted July 13, 2020.
I Applicant's Findings, submitted July 13, 2020.
J Public Works Report, received August 25, 2020.
K Fire Department Report, received August 19, 2020.
L Medford Water Commission report/map, received August 19, 2020.
M-1 Oregon Department of Fish and Wildlife report, received September 30, 2020.
N Neighbor Letter, received August 27, 2020.
O Floodplain report, received September 2, 2020.
P Parks Department report, received September 3, 2020.
Q Riparian Reduction report (GF-20-288), approved on September 30, 2020.
Vicinity Map

SITE PLAN AND ARCHITECTURAL COMMISSION:

JEFF BENDER, CHAIR

SPAC AGENDA

SEPTEMBER 18, 2020

OCTOBER 2, 2020
OCTOBER 16, 2020

EXHIBIT A-2

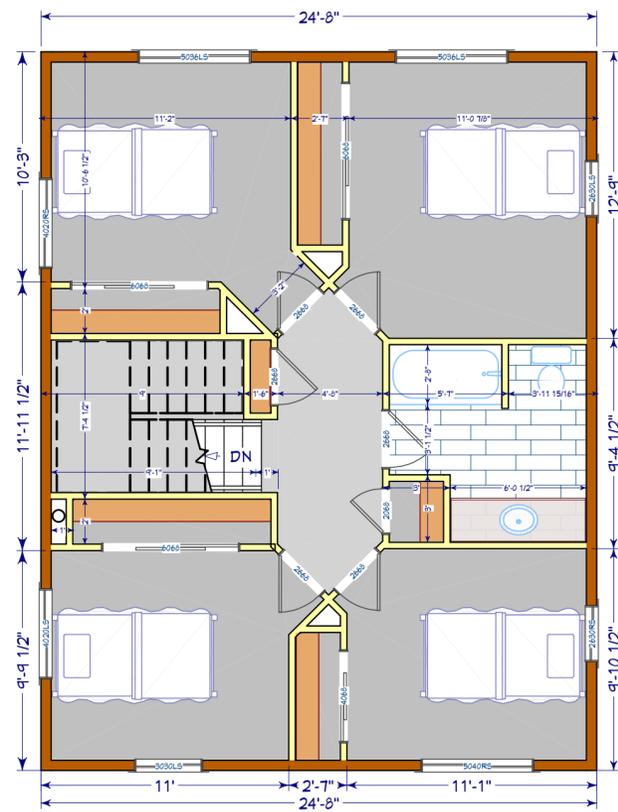
Creekside Village
AC-20-205
Conditions of Approval
October 2, 2020

CODE REQUIRED CONDITIONS

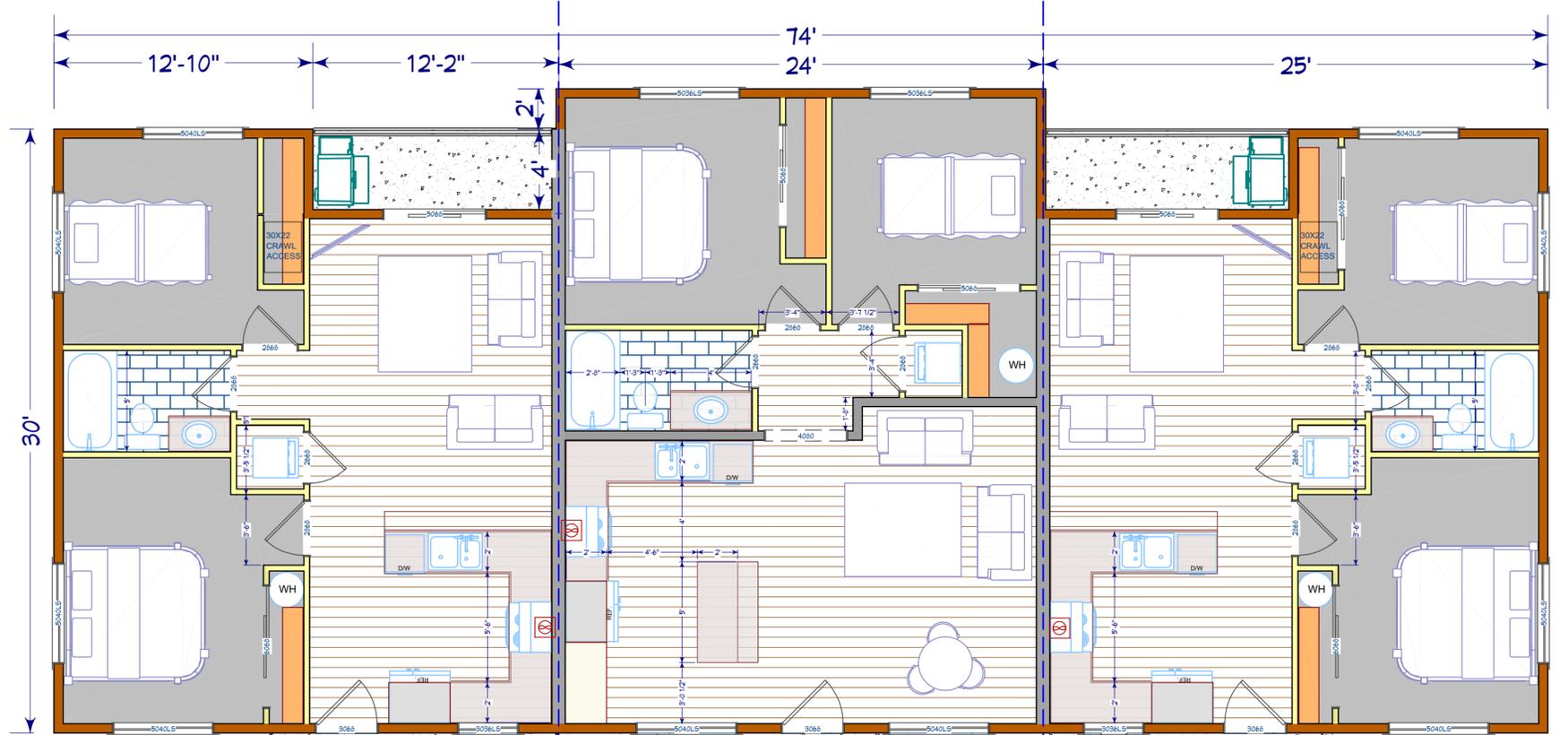
Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit J).
2. Comply with all conditions stipulated by the Medford Water Commission (Exhibit L).
3. Comply with all requirements of the Medford Fire Department (Exhibit K).
4. Comply with all requirements of the Parks Department (Exhibit P).
5. Comply with all requirements outlined in the floodplain report (Exhibit O).
6. Submit plans showing that all HVAC equipment will be concealed, consistent with the requirements outlined in MDLC 10.782.
7. Submit revised floor plans showing the units providing areas designated for secure bicycle parking, consistent with the requirements outlined in MLDC 10.747-750.

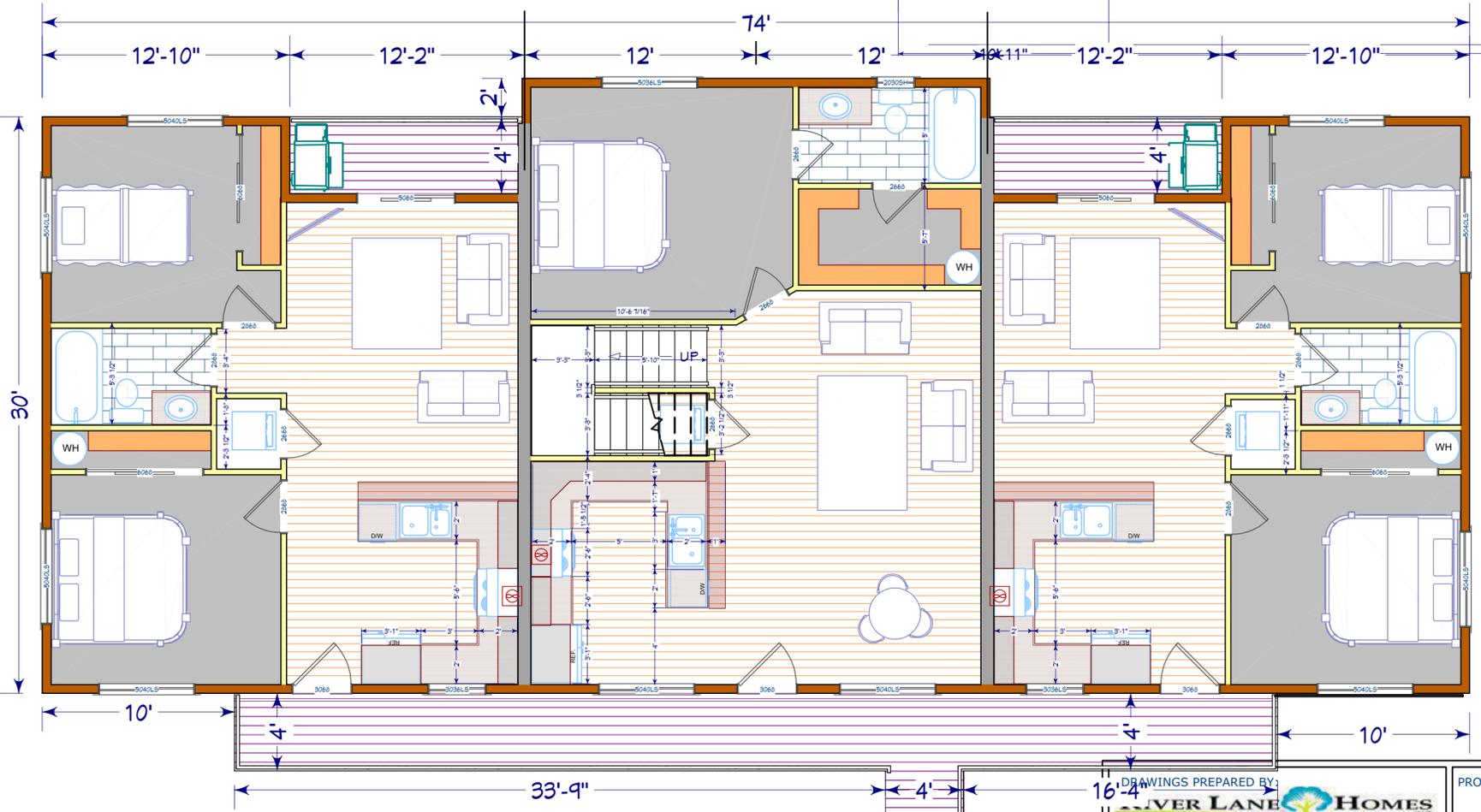




Floor Plan View Third Floor



Floor Plan View Main Floor



Floor Plan View Second Floor



EXHIBIT C-1

DRAWINGS PREPARED BY:
RIVER LANE HOMES INC.
 1070 Plaza Ave, Ashland, OR 97520
 Email: riverlanehomes@outlook.com
 Phone: 541-210-8490 CCB# 91267

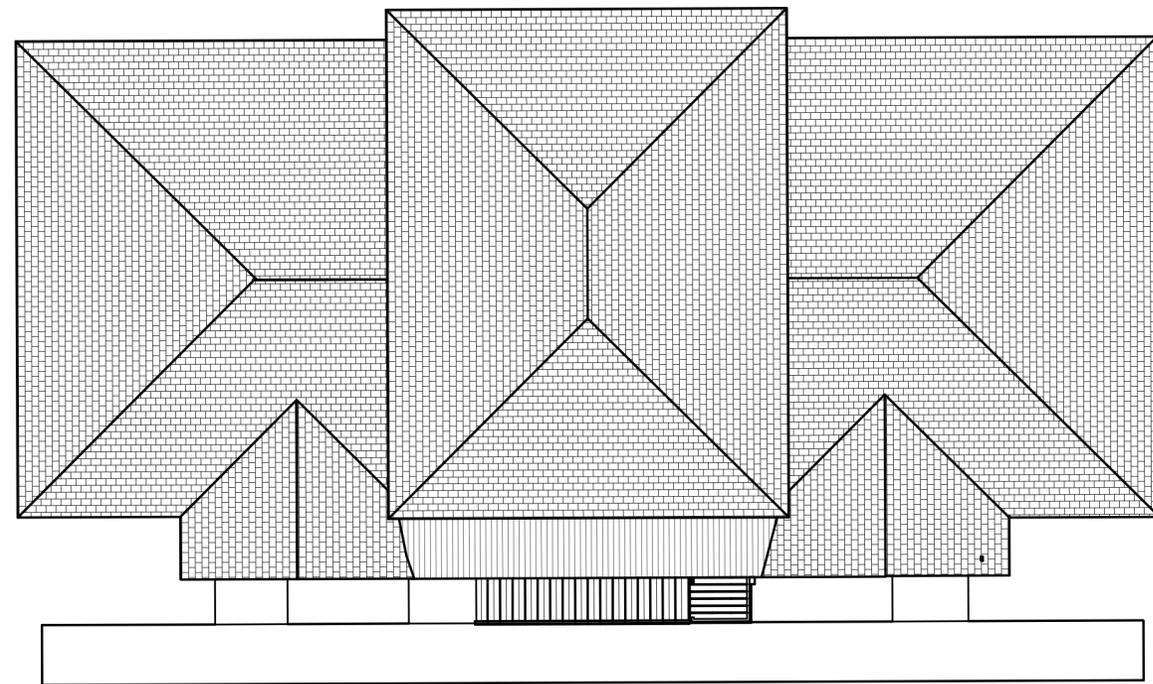
PROJECT INFO: 3T 1W 18AA TL 2300
CREEKSIDE VILLAGE
 5 acres, 84 rental units
 1971 Skypark Drive
 Medford, OR 97504



SHEET INFO: Floor Plans		
DATE: 10/1/2020	SCALE: Printed on C Paper	SHEET: A-1



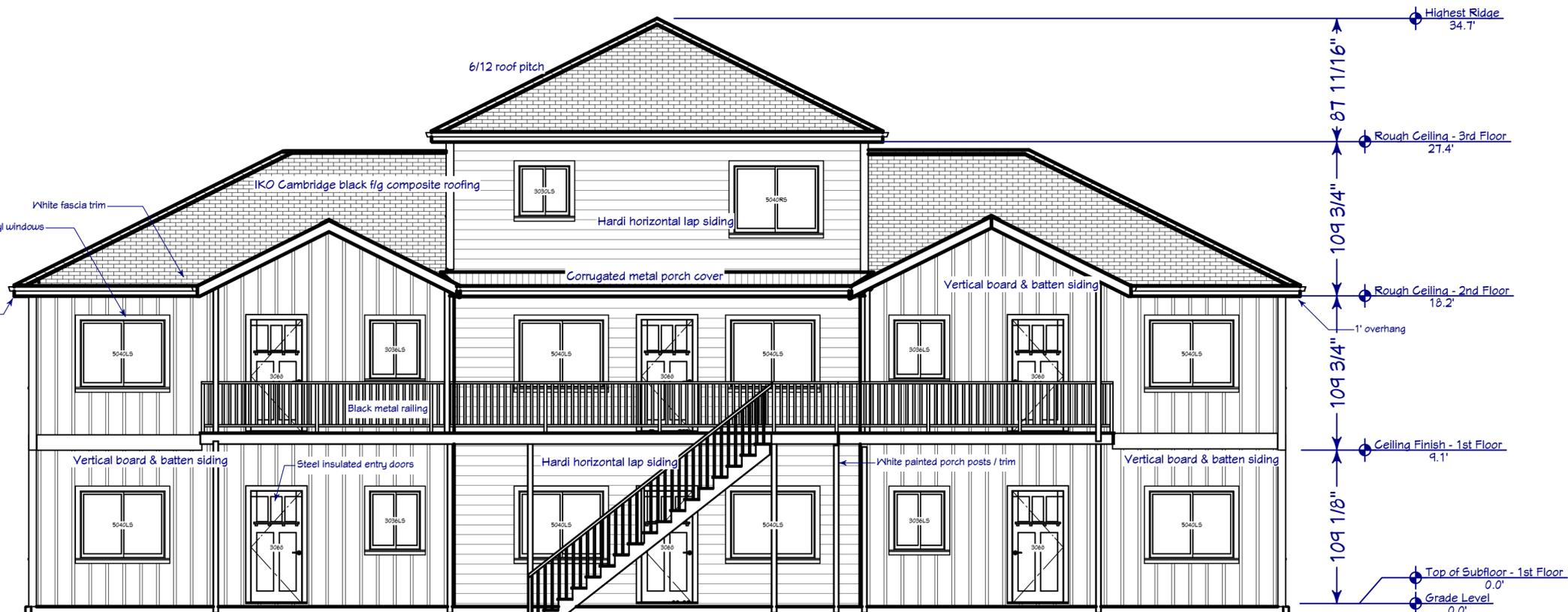
Rear Elevation



Roof Plan



Left Elevation



Front Elevation

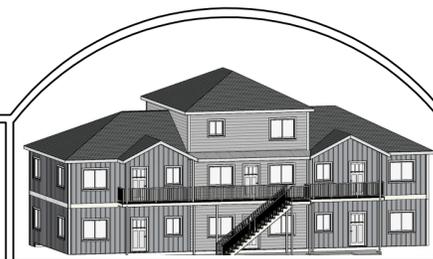


Right Elevation



DRAWINGS PREPARED BY:
RIVER LANE HOMES INC.
 1070 Plaza Ave, Ashland, OR 97520
 Email: riverlanehomes@outlook.com,
 Phone: 541-210-8490 CCB# 91267

PROJECT INFO: 3T 1W 18AA TL 2300
CREEKSIDE VILLAGE
 5 acres, 84 rental units
 1971 Skypark Drive
 Medford, OR 97504



SHEET INFO:		
Elevations		
DATE:	SCALE:	SHEET:
10/1/2020	Printed on C Paper	A-2

COMMISSION REPORT

for a Type-III quasi-judicial decision: **Site Plan and Architectural Review**

Project Ritchie Trucking
Applicant and Agent: JB Steel, Inc.

File no. AC-20-226

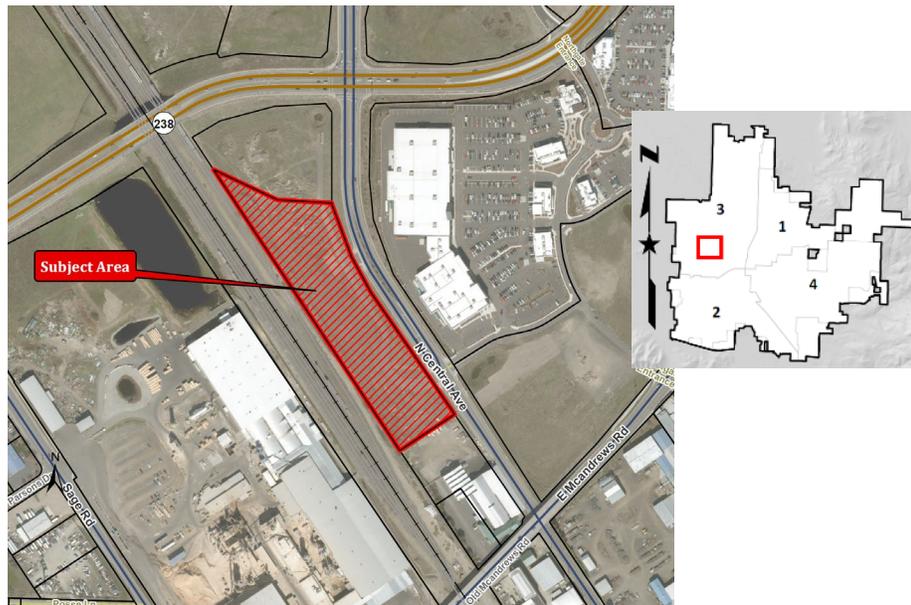
Date October 2, 2020

BACKGROUND

Proposal

Consideration of a proposal for an approximately 23,000 square foot single story trucking warehouse on a 1.54 acre parcel located at 1567 N Central Avenue in the General Industrial (I-G) zoning district (372W24701).

Vicinity Map



Subject Site Characteristics

GLUP	HI	Heavy Industrial
Zoning	I-G	General Industrial
Overlay	AC	Airport Area of Concern
Use	Vacant	

Surrounding Site Characteristics

<i>North</i>	Zone:	I-G & I-L (Light Industrial)
	Use:	Vacant
<i>South</i>	Zone:	I-H (Heavy Industrial)
	Use:	Timber Products
<i>East</i>	Zone:	C-R (Regional Commercial)
	Use:	Northgate Commercial development
<i>West</i>	Zone:	GI (Jackson County General Industrial Zoning)
	Use:	Timber Products

Related Projects

LDP-08-025	Two-lot Partition
ZC-08-026	Zone Change from County Zoning to General Industrial
PLA-20-097	Property Line Adjustment
LDP-20-183	Land Partition

Applicable Criteria

Medford Municipal Code §10. 200(E)(1) – Site Plan & Architectural Review Criteria

(E) Site Plan and Architectural Review Approval Criteria (Type II & III).

(1) The Site Plan and Architectural Commission, SPAC – Type III Review, shall approve a site plan and architectural review application for a commercial or industrial development, if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

(a) The proposed development is compatible with uses and development that exist on adjacent land, and

(b) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in Section 10.186.

Approval Authority

This is a Type III land use decision. The Site Plan & Architectural Commission is the approving authority under MLDC 10.108(1).

Corporate Names

Scott Stritenberg is listed as the Registered Agent for JB Steel, Inc. according to the Oregon Secretary of State Business Registry. Robert Batzer is listed as the President and Richard Batzer is listed as the Secretary.

ISSUES AND ANALYSIS

Background

A consolidated application for a two-lot land division and zone change from Jackson County zoning to the City's General Industrial (I-G) zone was approved in April of 2008.

Current Proposal

Land Partition

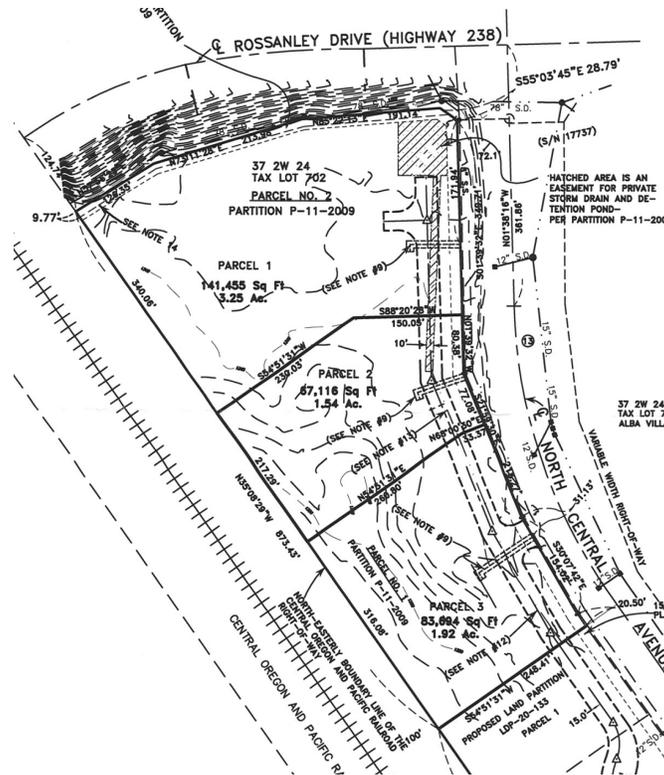


Figure 1 - LDP-20-183

The subject site is shown as Parcel 2 above. Land Partition application LDP-20-183 received tentative plat approval earlier this year.

At the time this staff report was prepared, final plat for the subject site had not yet been received. However, as the proposed development does meet all applicable site development standards within the existing parcel, the recordation of the final plat for the partition is not necessary and, therefore, was not included as a condition of approval.

Proposed Building



Figure 2 - Proposed Trucking Warehouse

The proposed development consists of a 23,048 square foot, single-story trucking facility. Apart from the trucking warehouse, the development will include an office area, break room, conference room, and bathrooms.

The site is served by a 24-foot wide private road extending to the subject site from North Central Avenue.

Development Standards

Site Development Table

I-G	Allowed/Required	Proposed
Height	85 feet	22 feet
Lot Coverage	90%	20.3% (proposed partition)
Setback (front)	10 feet	100+ feet

Setback (rear and side yards)	1 foot	20' (west – proposed partition) 20' (east – proposed partition) 10' (south)
Minimum Lot Area	10,000 square feet	67,167 (proposed)
Minimum Lot Width	70 feet	300+' (proposed)
Minimum Lot Depth	100 feet	150+' (proposed)
Minimum Lot Frontage	70 feet	150' (proposed)

As shown in the Site Development Table above, it can be found that the proposed building identified on the submitted site plan, meets the bulk standards for the I-G zoning district as found in Article V of the Medford Land Development Code for the existing parcel dimensions, as well as the proposed parcel.

Parking

Parking Table (MLDC 10.743-751)

	Required	Provided
Vehicle Spaces	13	24
Bicycle Spaces	5	5

The submitted site plan shows a total of 24 parking stalls, meeting the minimum required as per MLDC 10.743(1). The site plan also shows the requisite number of handicap spaces and bicycle spaces, as per MLDC 10.746(8)(a) and 10.748, respectively.

Parking Area Planters per MLDC 10.746(3) are not required as the amount of provided parking stalls does not exceed 24 spaces.

Landscaping

Frontage Landscaping (MLDC 10.797)

	Required	Shown
Trees	5	5
Shrubs	23	23+

The applicant's submitted landscape plan (Exhibit F) shows landscaping along the site's frontage (157 feet) with the existing private street, meeting the minimum required number of trees and shrubs.

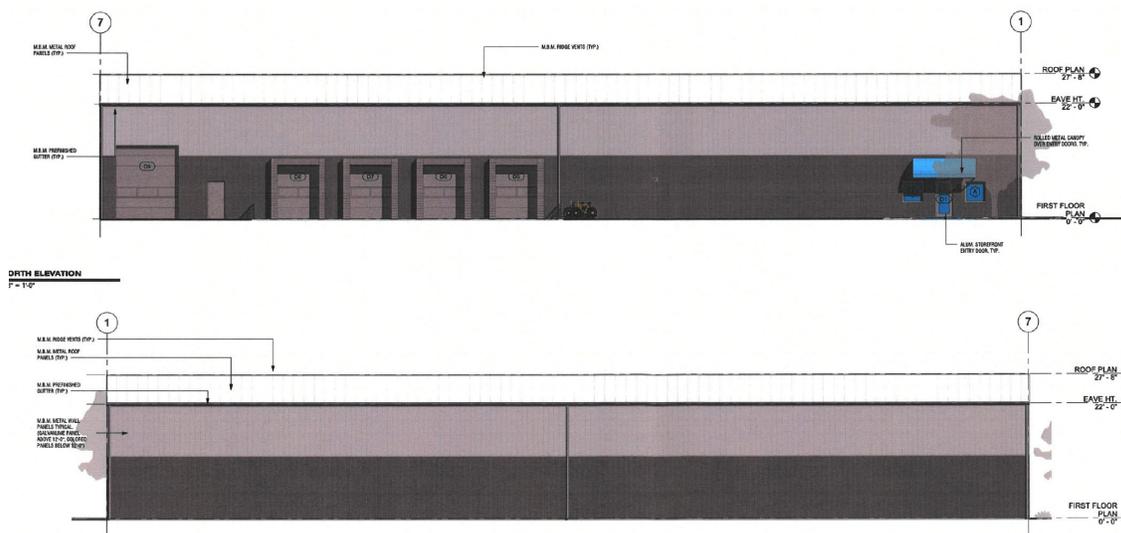
Concealments

Trash Enclosure (MLDC 10.781)

The applicant's site plan shows a trash enclosure located at the northwest corner of the building, and the submitted findings state that a CMU wall is proposed to screen trash dumpster.

Proposal meets Code requirements, as per MLDC 10.781.

Architecture



Per the applicant's findings, *the structure will consist of a pre-engineered metal building with an automatic fire-sprinkler system. The architectural composition of the proposed building mass has been articulated in a manner intended to mitigate the perceived mass of the structure and yet retain industrial character. A variety of architectural elements and exterior finish materials are intended to 'soften' the presence of the structure and to contribute appropriately to the surrounding architectural context. Primarily, the mass of the structure is visually softened using two colors of metal wall panels.*

Other Agency Comments

Rogue Valley Sewer Services (RVSS)

The subject property is within RVSS service area. ~~No comments were submitted by the time this staff report was published.~~ **Staff received comments from RVSS on October 1, 2020 (Exhibit N).**

Oregon Department of State Lands (DSL) (Exhibit M)

The DSL responded to a Wetlands Land Use Notification (WLUN) by staff with a WLUN Response form. The response states that the proposed project does not appear to impact jurisdictional wetlands, waterways, or other waters of the state.

Committee Comments

No comments were received from committees such as BPAC.

No other issues were identified by staff.

FINDINGS OF FACT

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the applicant's findings and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

FINDINGS AND CONCLUSIONS

Staff has reviewed the applicant's findings and conclusions (Exhibit G) and recommends the Commission adopt the findings as recommended by staff.

ACTION TAKEN

Adopted the findings as recommended by staff and directed staff to prepare the final order for approval of AC-20-226 per the staff report dated September 25, 2020, including Exhibits **A-1 through N**.

EXHIBITS

- A-1 Conditions of Approval, dated October 2, 2020**
- B Info & Site Plan, received July 28, 2020
- C Floor Plan, received July 28, 2020
- D Roof Plan, received July 28, 2020
- E Elevations, received July 28, 2020
- F Landscape Plan, received July 28, 2020
- G Applicant's Findings and Conclusions, received July 28, 2020
- H Public Works Report, dated September 9, 2020
- I Building Department Memo, dated September 8, 2020
- J Fire Department Memo, dated September 8, 2020
- K Medford Water Commission Memo, dated August 27, 2020
- L Landscape and Irrigation Information
- M Wetland Land Use Notice Response, dated September 18, 2020
- N RVSS Report, received October 1, 2020**
 - Vicinity map

**SITE PLAN AND ARCHITECTURAL COMMISSION AGENDA: OCTOBER 2, 2020
OCTOBER 16, 2020**

JEFF BENDER, CHAIR

EXHIBIT A-1

Ritchie Trucking
AC-20-226
Conditions of Approval
October 2, 2020

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit H).
2. Comply with all conditions stipulated by the Medford Water Commission (Exhibit K).
3. Comply with all requirements of the Medford Fire Department (Exhibit J).
4. **Comply with all conditions of the RVSS Report (Exhibit N).**





ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 97502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

September 29, 2020

City of Medford Planning Department
200 S. Ivy Street
Medford, Oregon 97501

Re: Ritchie Trucking (Map 372W24B, TL 701)

ATTN: Steffen,

The subject property is within the RVSS service area. There is an 8 inch sewer main adjacent to the property along Central Avenue and a 4 inch service is stubbed to the development. Sewer service for proposed development can be had by connecting to the existing service. The sewer connection permit will be issued by the City of Medford. However, Sewer System Development Charges will be owed to RVSS.

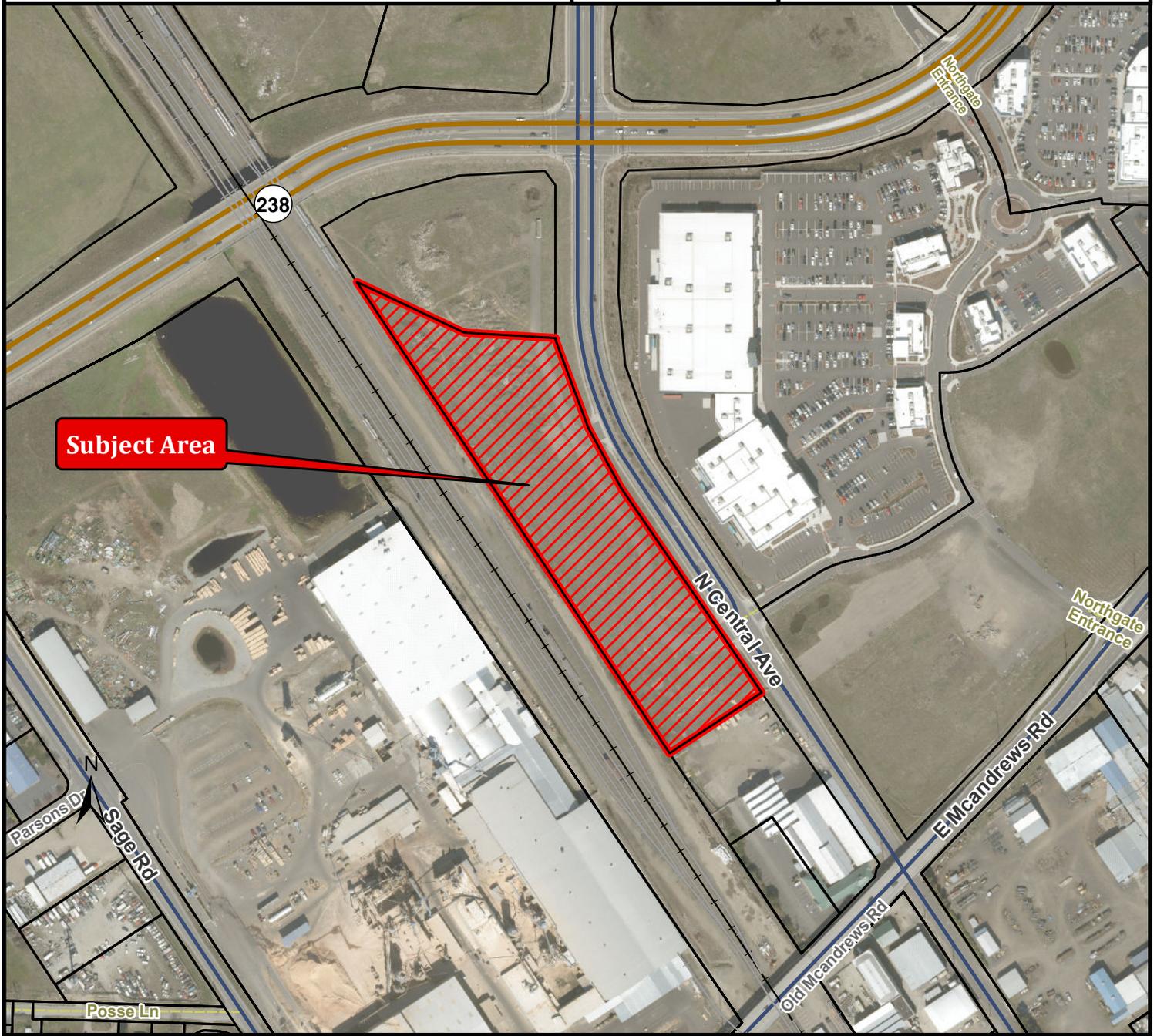
Rogue Valley Sewer Services requests that approval of this application be subject to the following conditions:

1. All development must be designed and constructed per RVSS standards.
2. The applicant must submit architectural drawings to RVSS for the calculation of System Development Charges.
3. The applicant must pay related development fees prior to the issuance of building permits.

Please feel free contact me with any questions.

Sincerely,

Nicholas R. Bakke, P.E.
District Engineer

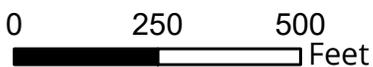


Project Name:

Ritchie Trucking

Map/Taxlot:

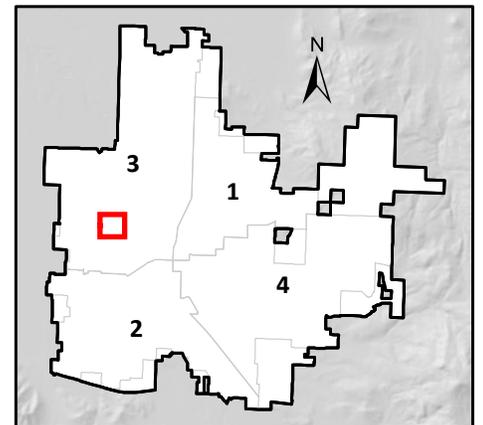
372W24 TL 701



Legend

-  Subject Area
-  Tax Lots

Date: 8/7/2020



SITE PLAN & ARCHITECTURAL COMMISSION MINUTES



October 2 2020

12:00 P.M.

Medford City Hall, Council Chambers
411 West 8th Street, Medford, Oregon

The regular meeting of the Site Plan and Architectural Commission was called to order at noon in the Council Chambers on the above date with the following members and staff in attendance:

Commissioners Present

Jeff Bender, Chair
Bill Chmelir, Vice Chair
Dave Culbertson
Paul McClay
Bob Neathamer
Chris Zelmer

Staff Present

Kelly Evans, Assistant Planning Director
Katie Zerkel, Assistant Senior City Attorney
Doug Burroughs, Public Works/Eng. Develop Svs. Mgr.
Steffen Roennfeldt, Planner III
Dustin Severs, Planner III
Debbie Strigle, Recording Secretary

Commissioners Absent

Jim Catt, excused
Marcy Pierce, excused
Milo Smith, excused

10. Roll Call

20. Consent Calendar. None

30. Approval or Correction of the Minutes

30.1 The minutes from the September 18, 2020, meeting were approved as submitted.

40. Oral Requests and Communications from the Audience. None.

50. Public Hearings

Katie Zerkel, Senior Assistant City Attorney, read the rules governing the public hearings.

Continuance Request.

50.1 **AC-20-227** Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres, located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300). Applicant: Southern Oregon Subaru; Agent: J.B. Steele, Inc.; Planner: Dustin Severs.

Chair Bender asked for any potential conflicts of interest, ex-parte communications, or site visits. There were none.

Motion: Continue this item to the October 16, 2020, meeting, per the applicant's request.

Moved by: Commissioner Culbertson Seconded by: Commissioner Neathamer

Roll Call Vote: Motion passed, 6-0

Old Business.

50.2 **AC-20-205** Consideration of plans for a six-phase, multi-family development consisting of 14 buildings with six units each—a total of 84 dwelling units. (The proposed development is running concurrent with LDS-20-207 and CUP-20-208.) The property consists of a single parcel totaling 4.76 acres, and is located at the corner of Skypark Drive and Crater Lake Avenue (1971 Skypark Drive). The property is zoned MFR-15 (Multiple Family Residential, 15 dwelling units per gross acre). (371W18AA TL 2300). Applicant: Windy Creek LLC; Agent: Slaughter Consulting; Planner: Dustin Severs.

Chair Bender asked for any potential conflicts of interest, ex-parte communications, or site visits.

Commissioner Neathamer disclosed he has an on-going working relationship with the applicant under a different name, and one of the project engineers, however he has not worked on this particular project. He believed he did not have a conflict of interest and would continue participating in this hearing.

Dustin Severs, Planner III, gave a PowerPoint presentation of the September 25, 2020, Staff Report. He pointed out two additional exhibits. Staff recommended approval.

The public hearing was opened and the following testimony was given:

a) Joe Slaughter, agent for the applicant, highlighted the history of the property and the complexity and cost associated with the development of it. He added they had submitted a letter of map amendment application with FEMA for the floodplain to correct the mapping based off of the correct information about what the floodplain looks like and should look like. This will show that most of this property is not in the floodplain.

Mr. Slaughter stated that along the west and north boundaries there will be a six foot tall fence. Along the northeast side there will be a concrete buffer wall. On the east and south sides they will be installing a four foot tall fence.

Chair Bender reserved time for rebuttal.

There being no more testimony, the public hearing was closed.

Motion: Adopt the findings as recommended by staff and direct staff to prepare a final order for approval of AC-20-205 per the revised staff report dated September 30, 2020, including Exhibits A-1 through Q, including C-1, D-1, and M-1, and also including the following:

- The granting of the maximum timetable of five years for the project to be developed in phases, pursuant to MLDC 10.200(G)(2)
- The approval of optional adjustments of special development standards, pursuant to MLDC 10.719
- Striking of condition #7

Moved by: Commissioner Culbertson Seconded by: Commissioner Neathamer

Commissioner Neathamer complimented everyone who worked on the design of this project. He said it was well done and something that was needed. Chair Bender concurred.

Commissioner Zelmer wanted to know if the letter submitted by a neighbor regarding the parking on Skypark Drive had been addressed. Mr. Severs answered that he had called that neighbor and explained

that there is a body who hears those issues and the letter had been forwarded on to the Public Works Department for their response.

Roll Call Vote: Motion passed, 6-0

New Business

50.3 **AC-20-226** Consideration of a proposal for an approximately 23,000 square foot single story trucking warehouse on a 1.54 acre parcel located at 1567 N Central Avenue in the General Industrial (I-G) zoning district (372W24701). Applicant & Agent, JB Steel Inc.; Planner, Steffen Roennfeldt.

Chair Bender asked for any potential conflicts of interest, ex-parte communications, or site visits. Commissioner McClay declared that his firm is working directly on this project and recused himself.

Commissioner Zelmer disclosed that he had seen this project through ODOT but does not feel he has any conflict of interest.

Steffen Roennfeldt, Planner III, gave a PowerPoint presentation of the September 25, 2020, Staff Report. Staff recommended approval.

The public hearing was opened and the following testimony was given:

a) Gary Caperna, agent for the applicant, thanked Mr. Severs for his presentation and stated he had nothing further to add.

Chair Bender reserved time for rebuttal.

There being no more testimony, the public hearing was closed.

Motion: Adopt the findings as recommended by staff and direct staff to prepare the final order for approval of AC-20-226 per the staff report dated September 25, 2020, including Exhibits A-1 through N.

Moved by: Commissioner Culbertson Seconded by: Commissioner Neathamer

Roll Call Vote: Motion passed, 5-0

60. **Written Communications** None.

70. **Unfinished Business** None.

80. **New Business** None.

90. **Report from the Planning Department**

90.1 Ms. Evans stated there would be business scheduled for both the October 16, 2020, and November 6, 2020, meetings.

90.2 Ms. Evans revealed that October is National Community Planning Month.

100. **Messages and Papers from the Chair** None.

110. **Propositions and Remarks from the Commission** None.

120. Adjournment

120.1 The meeting was adjourned at approximately 12:39 p.m. The proceedings of this meeting were digitally recorded and are filed in the City Recorder's office.

Submitted by:

Debbie Strigle
Recording Secretary

Jeff Bender
Site Plan and Architectural Commission Chair

Approved: October 16, 2020

STAFF REPORT

for a type-III quasi-judicial decisions: **Site Plan & Architectural Review**

Project Southern Oregon Subaru
Applicant: Southern Oregon Subaru, LLC; Agent: J.B. Steel, Inc.

File no. AC-20-227

To Site Plan & Architectural Commission *for October 16, 2020 hearing*

From Dustin Severs, Planner III

Reviewer Kelly Evans, Assistant Planning Director

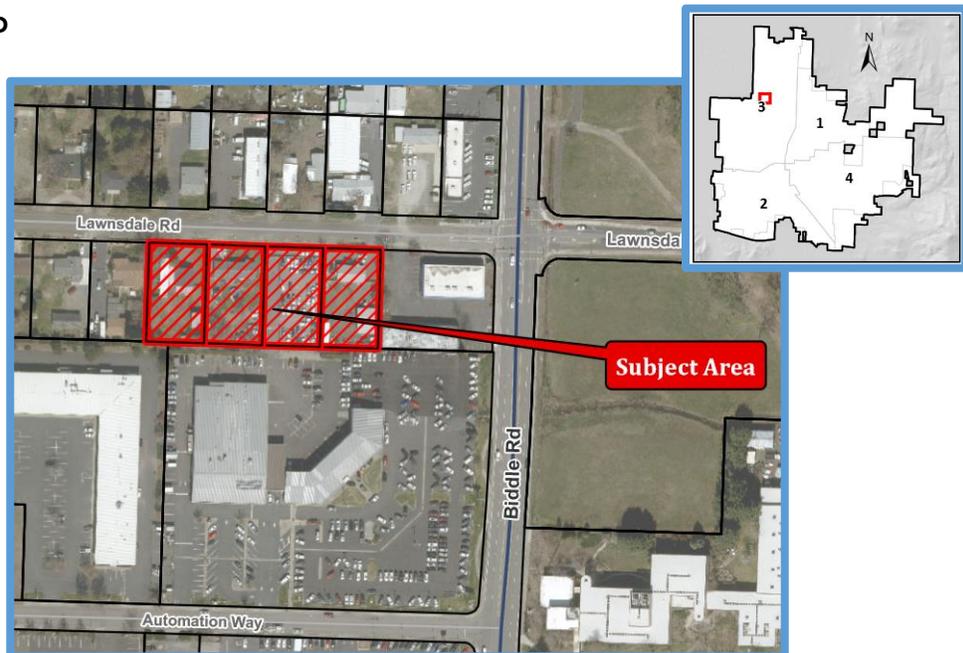
Date October 9, 2020

BACKGROUND

Proposal

Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres, located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300).

Vicinity Map



Subject Site Characteristics

Zoning: General Industrial (I-G)
GLUP: General Industrial (GI)
Overlay(s): Airport Area of Concern (AC)
Use(s): Single-family residences and parking lot

Surrounding Site Characteristics

North Zone: I-G
Use(s): Residential

South Zone: Light Industrial (I-L)
Use(s): Southern Oregon Subaru

East Zone: I-G
Use(s): Residential

West Zone: I-L
Use(s): Residential

Related Projects

None

Applicable Criteria

Medford Municipal Code §10. 200(E)(1) – Site Plan & Architectural Review Criteria

The Site Plan and Architectural Commission, SPAC – Type III Review, shall approve a site plan and architectural review application for a commercial or industrial development, if it can find that the proposed development conforms, or can be made to conform through the imposition of conditions, with the following criteria:

- (a) The proposed development is compatible with uses and developments that exist on adjacent land; and*
- (b) The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.186.*

Approval Authority

This is a Type III land use decision. SPAC is the approving authority under MLDC 10.110(D).

ISSUES AND ANALYSIS

Project Summary

Current site



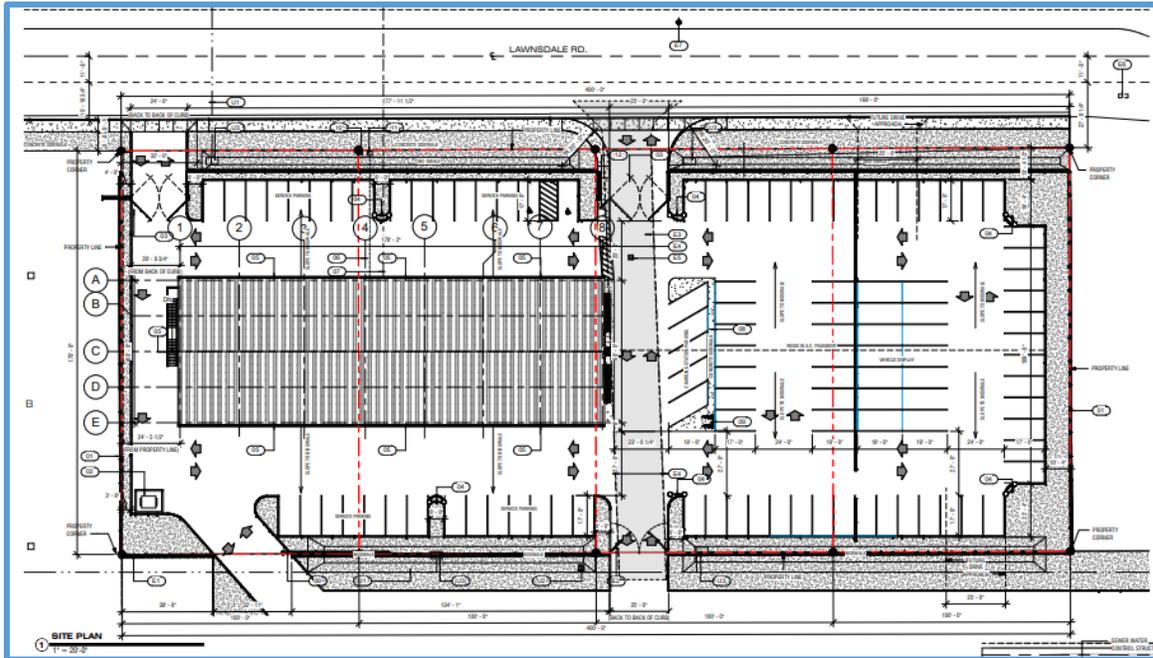
The subject site consists of four contiguous parcels (each parcel is 0.39 acres) totaling 1.56 acres. Lots 7000, 7200, and 7300 currently have single-family residences that will be demolished with the proposed development. Lot 7100 has been developed as a parking lot for Southern Oregon Subaru (abutting the subject parcels to the south) and will remain a parking area with the proposed development.

Cross access to the abutting Southern Oregon Subaru location is provided at the southeast corner of lot 7100, and will remain with the proposed development.

All four parcels front Lawnsdale Road, a Commercial Street under the jurisdiction of Jackson County.

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Proposal



With the subject request, the applicant is proposing to develop the property as an expansion of the existing Southern Oregon Subaru development located to the south. The development will consist of a 10,680 square foot structure used as an automobile service and repair facility for Southern Oregon Subaru. The facility will consist of a drive thru car wash (not open to the public), several detail bays, a lube pit bay, multiple service bays, restrooms, storage areas, and a parts mezzanine.

Access to the site is proposed at two driveways off of Lawnsdale Road. Additionally, the site plan shows two driveways at the site's southerly boundary, providing cross-access to the existing Southern Oregon Subaru site.

Stormwater detention and water quality will be addressed via bio swales proposed within the planter strips located along the site's northerly and southerly boundaries.

In order to meet building setback requirements, the applicant's findings state that they will be applying for a property line adjustment. The applicant intends to consolidate lots 7300 and 7200, and adjust the lot line between lots 7200 and 7100.

As a condition of approval, the applicant will be required to gain approval for a property line adjustment consistent with the site development standards outlined in MDLC 10.721, prior to the issuance of a building permit for vertical construction.

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Development Standards

Industrial Development Table (MLDC 10.721)

I-G Zone	Allowed/Required	Proposed/Shown
Height	85 feet	25 feet
Lot Coverage	90%	15%
Setback (front)	10 feet	56 feet
Setback (rear yard)	2.5 feet	56 feet
Setback (side yard)	2.5 feet	22 feet

As shown in the Site Development Table above, it can be found that the proposed structure identified on the submitted site plan meets the bulk standards for the I-G zoning district as found in Article V of the Medford Land Development Code.

Parking

Parking Table (MLDC 10.743-751)

	Required	Proposed/Shown
Vehicle Spaces	25 min. / 32 max.	31
Handicap Spaces	2	2
Bicycle Spaces	3	3

The submitted site plan shows a total of 31 parking stalls provided for employees and visitors, including two handicap spaces and three bicycle spaces, all consistent with the requirements outlined in MLDC 743-751. The site plan shows 41 additional spaces for the storage of the new vehicles for the automobile facility, as well as five electric vehicle charging stations.

Landscaping

Frontage Landscaping (MLDC 10.797)

Lawnsdale Road	Required	Shown
Trees	13	13
Shrubs	63	63

Parking Area Planters (MLDC 10.746)

	Required	Shown
Trees	6	16
Shrubs	12	32
Total coverage	1,000	1,026

The applicant's submitted landscape plan (Exhibit F) shows landscaping along the site's frontage with Lawnsdale Road, and parking area planters within the site's parking area, meeting the minimum requirements of the Code as outlined in MLDC 10.797 and 10.746, respectively. Since the Landscape Plan was submitted, however, the applicant has made minor revisions to their Site Plan, and the Landscape Plan has not been updated to reflect the changes.

As a condition of approval, the applicant will be required to submit an updated Landscape Plan prior to the issuance of a building permit for vertical construction.

Concealments

Trash Enclosure (MLDC 10.781)

The applicant's site plan shows a trash enclosure located at the southwest corner of the site, and the submitted findings state that the proposed trash enclosure will be constructed with split face concrete masonry units, consistent with the requirements outlined in MLDC 10.781.

HVAC Equipment (MLDC 10.782)

All HVAC equipment are required to be concealed using the architectural elements of the building (i.e., roof forms, parapets, wing walls, alcoves, etc.). The HVAC equipment proposed for the project is not shown on the site plan nor is it addressed in the applicant's findings.

As a condition of approval, the applicant will be required to submit plans showing all proposed HVAC equipment concealed consistent with the requirements outlined in MDLC 10.782.

Space intentionally left blank

Architecture



The applicant's submitted narrative (Exhibit G) describes the building's proposed architecture and how it fits with and complements adjacent buildings, as the following:

The proposed building is a pre-engineered steel structures with traditional pre-finished metal siding on the sides and rear of the building. The east elevation of the proposed building is split face CMU with windows and painted steel canopies. The sides and rear of the building will be constructed with pre-finished metal wall panels and a split face CMU wainscot to four feet above the finished floor. The proposed combination of split face CMU and pre-finished metal wall panels is consistent with the commercial buildings in the near vicinity of this project. Entries and feature windows have been articulated with human scale elements.

Block Length

The block surrounding the subject parcel exceeds the maximum block length requirements as outlined in MLDC 10.426(1). The Planning Commission may approve block length and/or perimeter lengths that exceed the maximum required—as outlined in MLDC 10.426(C)(2), shown below—contingent on the applicant's findings effectively demonstrating that certain constraints exist, making a street connection impractical or inappropriate.

The applicant is requesting relief to exceed the maximum block length standards of the Code.

MLDC 10.426(C)(2)

2. The approving authority may find that proposed blocks that exceed the maximum block and/or perimeter standards are acceptable when it is demonstrated by the findings that one or more of the constraints, conditions or uses listed below exists on, or adjacent to the site:
- a. Topographic constraints, including presence of slopes of 10% or more located within the boundary of a block area that would be required by subsection 10,426 C.1.,
 - b. Environmental constraints including the presence of a wetland or other body of water,
 - c. The area needed for a proposed Large Industrial Site, as identified and defined in the Medford Comprehensive Plan Economic Element, requires a block larger than provided by section 10.426 C.1.e. above. In such circumstances, the maximum block length for such a Large Industrial Site shall not exceed 1,150 feet, or a maximum perimeter block length of 4,600 feet,
 - d. Proximity to state highways, interstate freeways, railroads, airports, significant unbuildable areas or similar barriers that make street extensions in one or more directions impractical,
 - e. The subject site is in SFR-2 zoning district,
 - f. Future development on adjoining property or reserve acreage can feasibly satisfy the block or perimeter standards,
 - g. The proposed use is a public or private school, college or other large institution,
 - h. The proposed use is a public or private convention center, community center or arena,
 - i. The proposed use is a public community service facility, essential public utility, a public or private park, or other outdoor recreational facility.
 - j. When strict compliance with other provisions of the Medford Land Development Code produce conflict with provisions in this section.

In their submitted findings, the applicant cites MLDC 10.426(C)(2)(d) above, stating that the subject parcels are surround by fully developed private properties, making a street connection impractical.

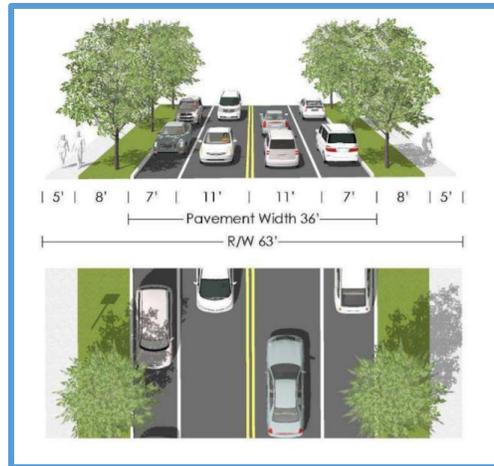
The applicant is also requesting relief from constructing an accessway in lieu of a roadway, as required per MLDC 10.464. The applicant’s findings cite MLDC 10.464(b), stating that the existing fully developed neighborhood likewise makes construction of an accessway impractical.

Staff concurs with the applicant’s findings, as the construction of a public street and/or a public accessway—pursuant to MLDC 10.426(1) and 10.464, respectively—are impractical and/or unnecessary, as the existing development and layout of the surrounding area act as barriers, making street extensions in one or more directions impractical.

Space intentionally left blank

Public Improvements

Commercial Street (MLDC 10.429)



Lawnsdale Road is classified as a Commercial Street. In accordance with MLDC 10.429, the applicant is required to install a five-foot sidewalk, and an eight-foot planter strip between the sidewalk and curb, along the site's frontage with Lawnsdale Road.

MLDC 10.501(A)

(5) Alignment. Sidewalks on arterial and major collector streets shall be set back a minimum of ten (10) feet from the back of the face of curb (See the street diagrams for arterial and collector streets above), unless curvelinear as provided below.

Sidewalks on all other streets shall be located a minimum of eight (8) feet from the back of curb unless curvelinear as provided below. If there are existing sidewalks on the same side of the street in the same block, then the sidewalk location shall be determined by the approving authority. The approving authority may approve curvelinear or meandering sidewalks for aesthetic purposes or for other reasons such as topography or to avoid existing trees. Where approved, such sidewalks shall tie to adjacent property lines in such a manner as to allow standard sidewalk construction on that property. (Effective Dec. 1, 2013.)

The applicant's site plan shows a curb-tight sidewalk with plantings located behind the sidewalk. In their submitted findings, the applicant argues that the proposed arrangement continues the pattern established in the surrounding neighborhood. The applicant has formally request relief, pursuant to MLDC 10.501(A), cited above, in order to install a curb-tight sidewalk along the site's frontage with Lawnsdale Road, as shown on their site plan.

Staff is unsupportive of the request. Planter strips move pedestrians a safe distance from the edge of the roadway, and Lawnsdale Road will be the primary route for pedestrians traveling to and from this development.

As a condition of approval, the applicant will be required to submit a revised site plan showing street improvements—including the installation of an eight-foot planter strip—along the site’s frontage with Lawnsdale Road, consistent with the requirements outlined in MLDC 10.429.

Facility Adequacy

Per the agency comments submitted to staff, (Exhibits H-J), it can be found that there are adequate facilities to serve the future development of the site.

Other Agency Comments

Jackson County Roads (Exhibit K)

Lawnsdale Road is a Commercial Street under the jurisdiction of Jackson County. As a condition of approval, the applicant will be required to comply with all applicable requirements of Jackson County Roads prior to the issuance of a building permit for vertical construction.

Committee Comments

No comments were received from a committee, such as BPAC.

Neighbor Comments

None

FINDINGS AND CONCLUSIONS

1. *The proposed development is compatible with uses and development that exist on adjacent land;*

The Commission can find that there is sufficient evidence contained in the applicant’s findings and the Staff Report to determine that the proposal is compatible with the uses and development on adjacent land. This criterion is satisfied.

2. *The proposed development complies with the applicable provisions of all city ordinances or the Site Plan and Architectural Commission has approved (an) exception(s) as provided in MLDC § 10.253.*

The Commission can find that the proposal can be made to comply with the applicable provisions of the Code with the imposition of conditions of approval contained in Exhibit A. This criterion is satisfied.

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RECOMMENDED ACTION

Adopt the findings as recommended by staff and direct staff to prepare a Final Order for approval of AC-20-227, per the staff report dated October 9, 2020, including:

- Exhibits A through K.
- The granting of relief from constructing a public street or a public accessway, pursuant to MLDC 10.426(C)(2) and 10.464(1), respectively.

EXHIBITS

- A Conditions of Approval, drafted October 9, 2020.
- B Site Plan, submitted October 7, 2020.
- C Floor Plans (2 of 2), submitted September 24, 2020.
- D Elevations, submitted September 24, 2020.
- E Roof Plan, submitted July 24, 2020.
- F Landscape Plan, submitted July 24, 2020.
- G Applicant’s Findings, submitted September 24, 2020.
- H Public Works Report, received October 8, 2020.
- I Fire Department Report, received October 6, 2020.
- J Medford Water Commission report/map, received September 2, 2020.
- K Jackson County Roads, received August 19, 2020.
Vicinity Map

SPAC AGENDA:

OCTOBER 16, 2020

EXHIBIT A

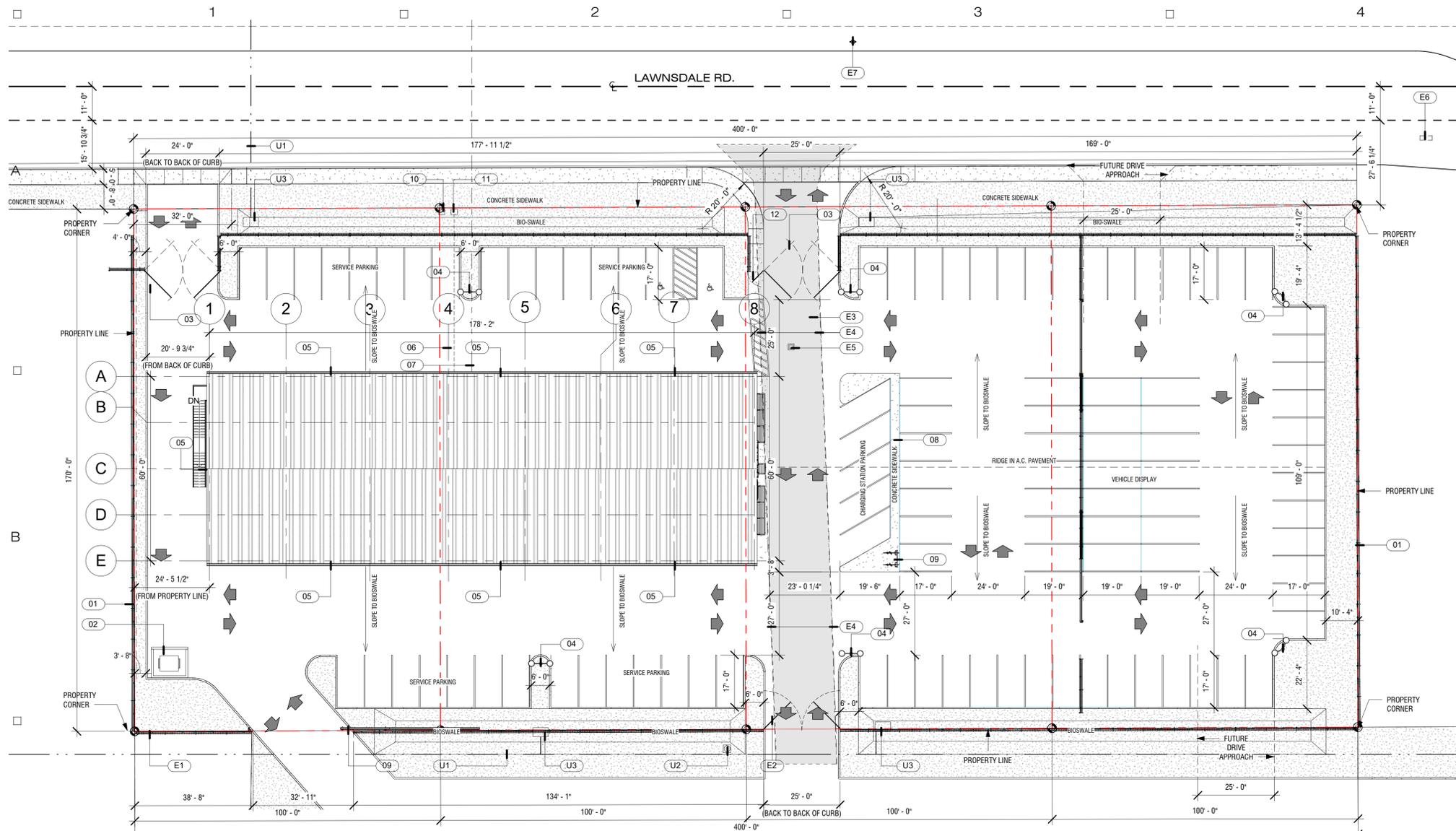
Southern Oregon Subaru
AC-20-227
Conditions of Approval
October 9, 2020

CODE REQUIRED CONDITIONS

Prior to the issuance of a building permit for vertical construction, the applicant shall:

1. Comply with all conditions stipulated by the Public Works Department (Exhibit H).
2. Comply with all requirements of the Medford Fire Department (Exhibit I).
3. Comply with all conditions stipulated by the Medford Water Commission (Exhibit J).
4. Comply with all applicable requirements of Jackson County Roads (Exhibit K).
5. Obtain approval for a property line adjustment consistent with the site development standards outlined in MDLC 10.721.
6. Submit plans showing all HVAC equipment concealed, consistent with the requirements outlined in MDLC 10.782.
7. Submit a revised site plan showing street improvements—including the installation of an eight-foot planter strip—along the site’s frontage with Lawnsdale Road, consistent with the requirements outlined in MLDC 10.429.
8. Submit an updated Landscape Plan, meeting the minimum requirements of the Code as outlined in MLDC 10.797 and 10.746.





1 SITE PLAN
1" = 20'-0"

GENERAL NOTES:

SITE PLAN KEYNOTES

NOTE NUMBER	DESCRIPTION
01	8' HIGH CHAIN LINK FENCE
02	TRASH ENCLOSURE
03	8' SWING GATE (BY OTHERS)
04	PROPOSED STREET LIGHTS
05	WALL PACK LIGHT
06	WATER LINE (TO BUILDING)
07	SANITARY SEWER (TO BUILDING)
08	CONCRETE SLAB (TO HOUSE CHANGING STATION EQUIPMENT)
09	8' SLIDING GATE (BY OTHERS)
10	WATER METER (FOR LANDSCAPE)
11	WATER METER (DOMESTIC)
12	MAN GATE (BY OTHERS)

EXISTING SITE CONDITIONS KEYNOTES

NOTE NUMBER	DESCRIPTION
E1	EXISTING CHAIN LINK FENCE (TO REMAIN)
E2	EXISTING SWING GATE TO REMAIN
E3	EXISTING ROAD
E4	EXISTING CHAIN LINK FENCE TO BE DEMO (RE-USE FENCE WHERE POSSIBLE)
E5	EXISTING CATCH BASIN
E6	EXISTING STORM DRAIN
E7	EXISTING FIRE HYDRANT

UTILITIES KEYNOTES

NOTE NUMBER	DESCRIPTION
U1	EXISTING STORM SEWER LINE
U2	EXISTING MANHOLE
U3	SEWER WATER CONTROL STRUCTURE



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CCB No. 132902
Web: WWW.JBSTEELINC.COM

CLIENT:
Southern Oregon Subaru

PROJECT:
SO SUBARU

PROJECT LOCATION:
3103 Bidelle Road

No.	Date	Description

ISSUE: 10/07/2020
PROJECT NO: #2920-20
DRAWN BY: NPR
CHECKED BY: GRC

APPROVED
SHEET TITLE:
SITE PLAN

PLOT DATE:
10/7/2020 2:16:26 PM

A0

PRELIMINARY



3 WEST 3D VIEW_09/25/2020
12" = 1'-0"



4 EAST 3D VIEW_09/25/2020
12" = 1'-0"



2 PROJECT SITE PLAN
4" = 80'-0"

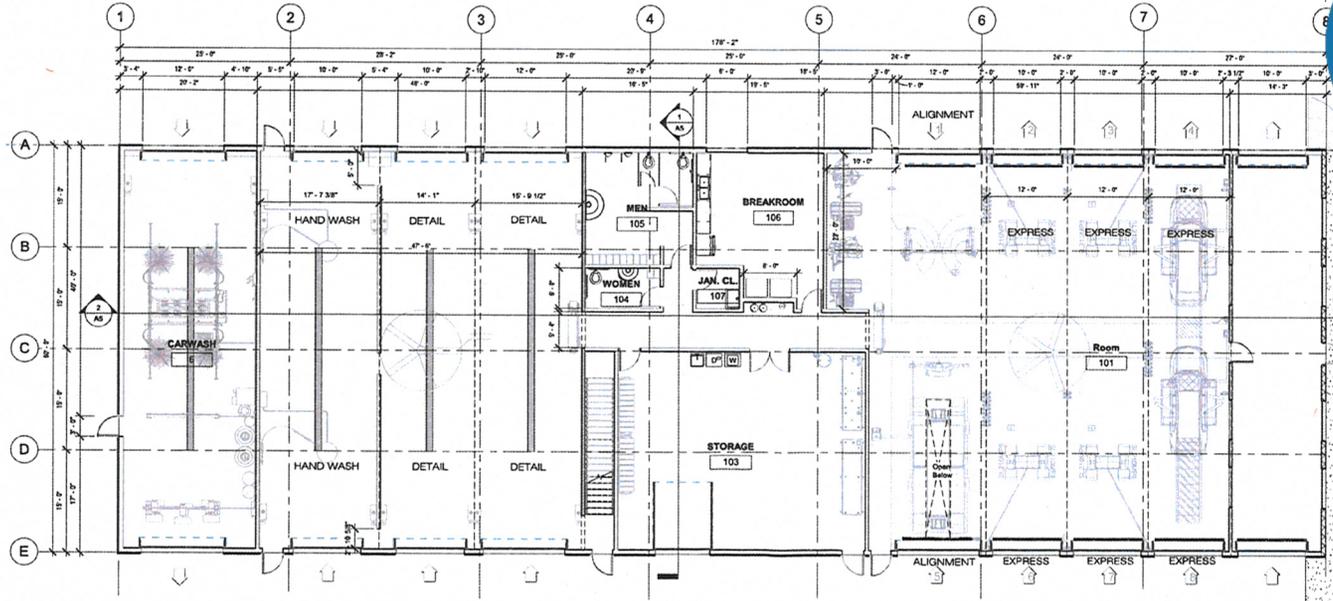


EXHIBIT

B

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NLM
 202008.038.14.PW
 WORKING PROPOSED CURRENT PROJECT SUBMITTAL Oregon Subaru/JB Steel Subaru Phase 1_071820-1



1 FIRST FLOOR PLAN
 1/8" = 1'-0"

EXHIBIT

C

M

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 STATE OF OREGON

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 COMMERCIAL & INDUSTRIAL

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 CEN No. 123992
 Web: WWW.JBSTEELINC.COM

PROJECT: SO SUBARU

PROJECT LOCATION:
 3103 Siskiyou Road

CLIENT:
 Southern Oregon Subaru

No.	Date	Description

ISSUE: 07/23/2020
 PROJECT NO. #2920-20
 DRAWN BY: NLM
 CHECKED BY: GRC

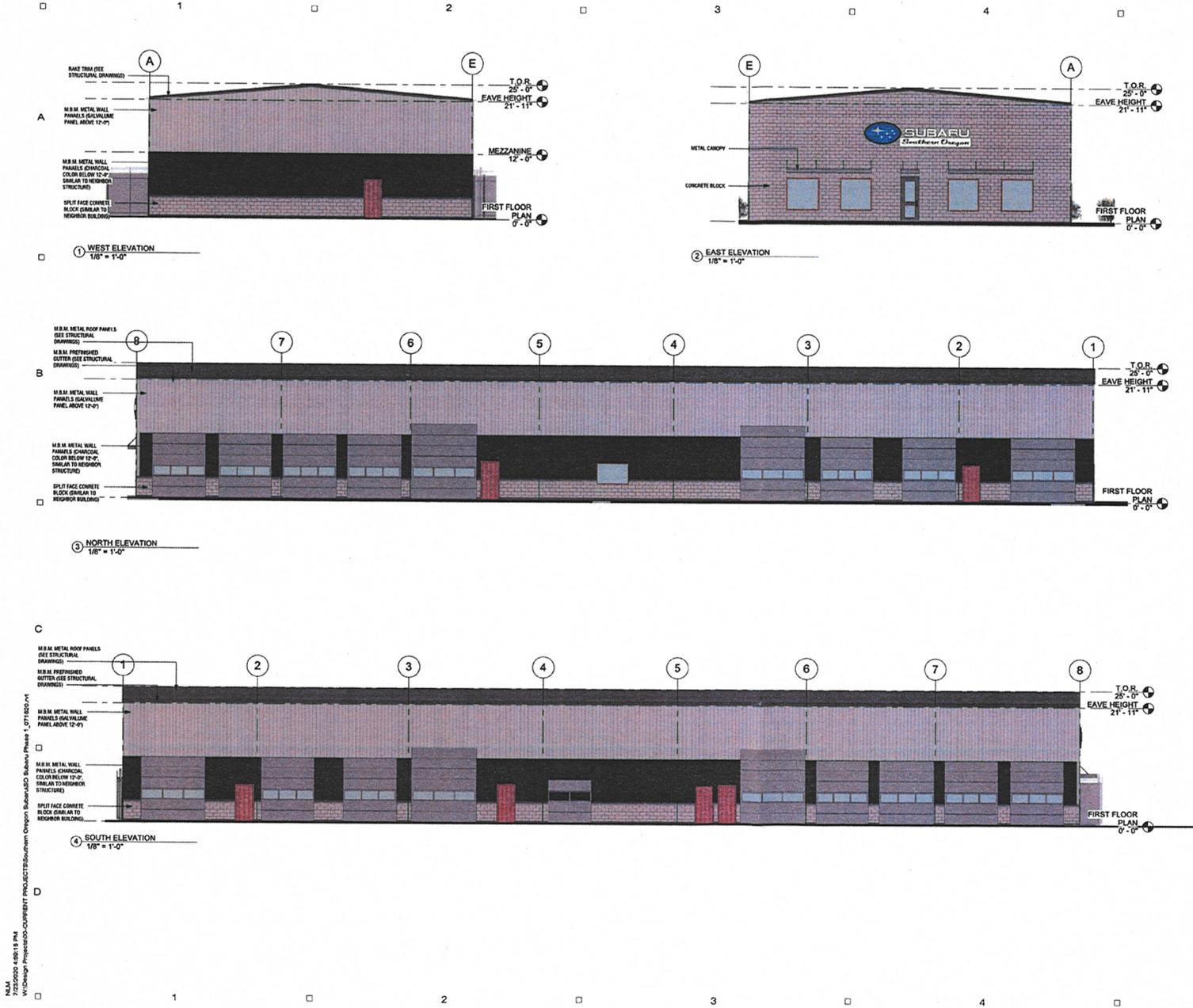
APPROVED

SHEET TITLE:
FLOOR PLAN

PLOT DATE:
 7/23/2020 4:38:14 PM

A1.1

Design Development
 PRELIMINARY



GARY R. CAPERNA ARCHITECT
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CLIENT:
 Southern Oregon Subaru

PROJECT: **SO SUBARU**
 PROJECT LOCATION:
 3103 South Road

No.	Date	Description

ISSUE: 07/23/2020
 PROJECT NO.: #2020-20
 DRAWN BY: NLM
 CHECKED BY: GRC

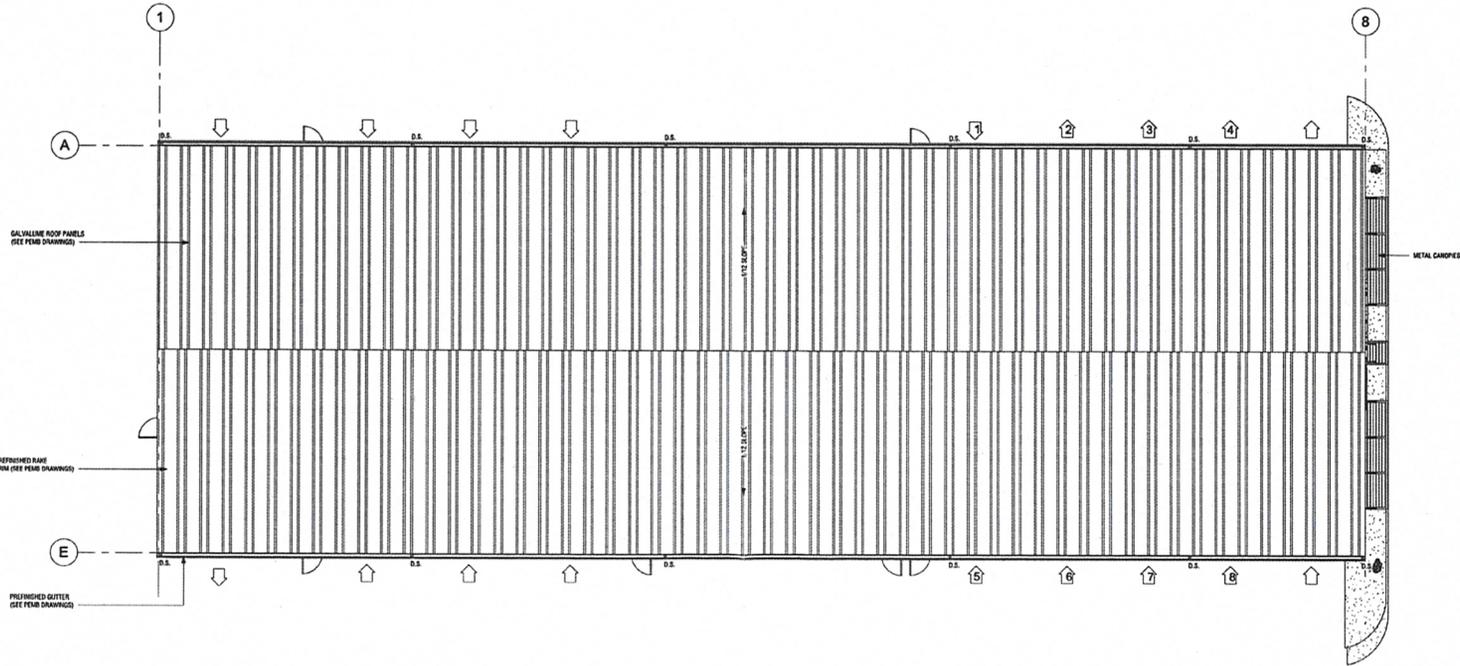
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EXTERIOR ELEVATIONS
 PLOT DATE:
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A3
 Design Development
 PRELIMINARY

NLM
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- ROOF PLAN GENERAL NOTES:**
1. SEE DETAILS TAK-40 FOR VENT PIPE FLASHING DETAILS.
 2. REFER MECHANICAL PLANS FOR HVAC UNIT LOCATIONS AND VENT PIPE PENETRATIONS.
 3. PROVIDE HEAT TAPE IN ALL OF THE ROOF DRAINS, DOWNSPOUTS AND THE OVERFLOW DRAINS.
 4. REFER TO STRUCTURAL FR FRAMING REQUIREMENTS AND ADDITIONAL INFORMATION.

- ROOF PLAN LEGEND:**
- D.S. 4" DOWN SPOUT U.N.O.
 - SLOPE → SLOPE ROOF DIRECTION OF ARROW
 - MECH MECHANICAL ITEMS - SEE MECHANICAL



1 ROOF PLAN
 1/8" = 1'-0"



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PROJECT: SO SUBARU
PROJECT LOCATION:
 3110 Blaine Road
 CLIENT:
 Southern Oregon Sales

Iss	Date	Description

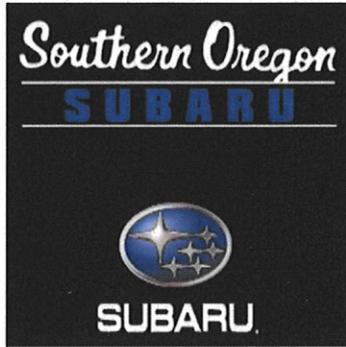
ISSUE: 07/23/2020
 PROJECT NO: #2020-20
 DRAWN BY: NLM
 CHECKED BY: GRC

APPROVED
 SHEET TITLE:
ROOF PLAN
 PLOT DATE:
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A4
 Design Development
 PRELIMINARY

City of Medford Site Plan and Architectural Review Narrative

For an automobile service and repair facility:



Project Address: 898, 908, 924, & 952 Lawnsdale Road (3103 Biddle Road)
Medford, Oregon 97504

Tax Map: 37-2W-12D, Tax Lots 7000, 7100, 7200 & 7300

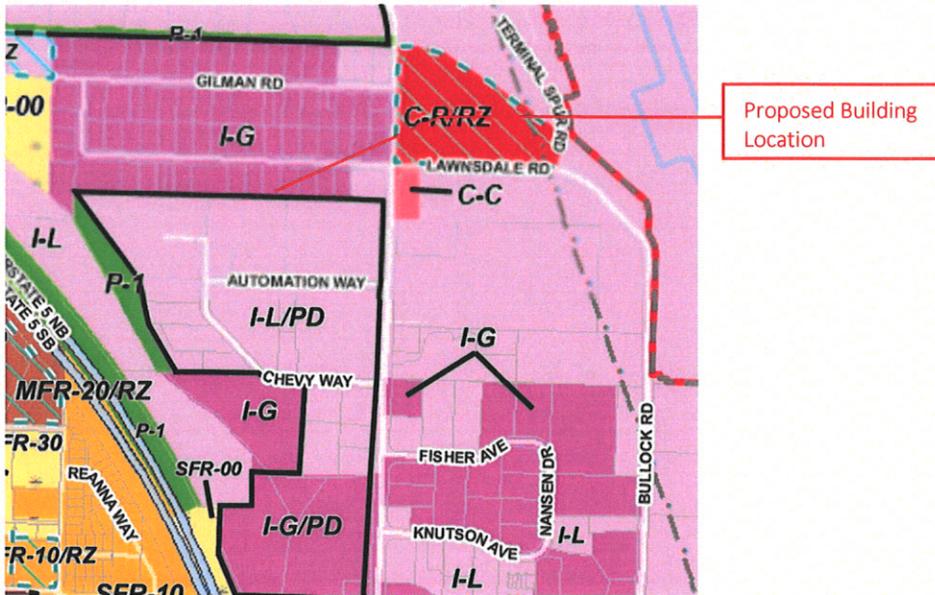


Southern Oregon Subaru Service Center Perspective.

SECTION I - Narrative

The proposed project is a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The project will be located in a General Industrial zoning designation (I-G) on tax lots 7000, 7100, 7200, and 7300. Each of the tax lots are .39 acres in area.





A portion of the City of Medford zoning map.

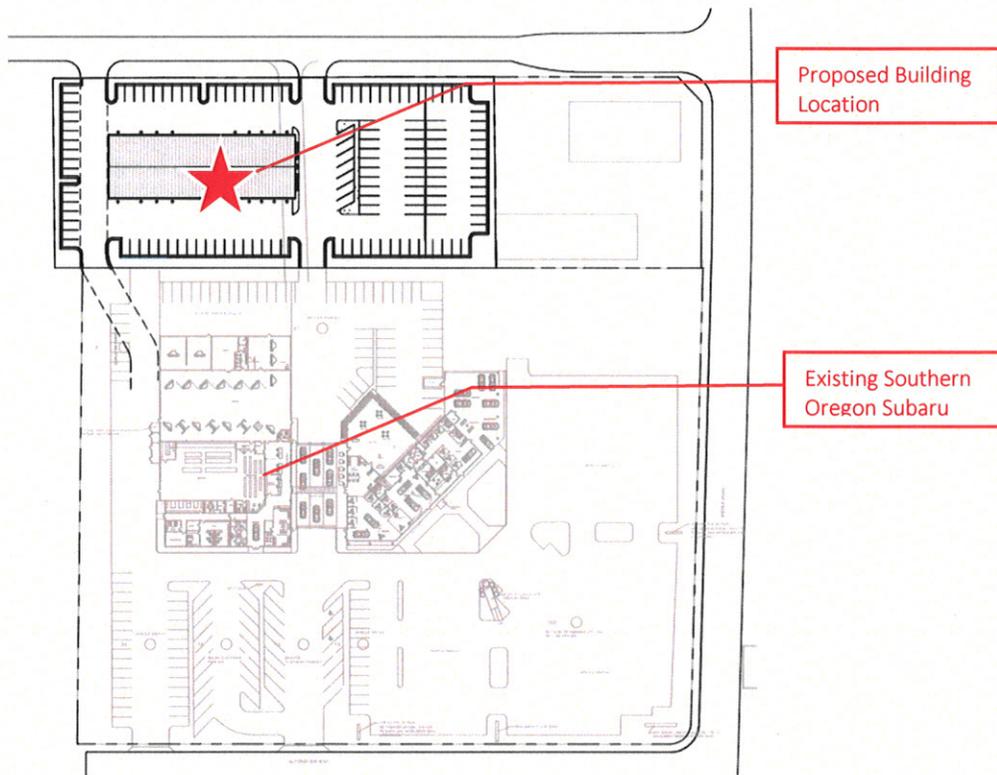
The lot line between tax lots 7300 and 7200 will be abandoned and the lot line between lots 7200 and 7100 will be adjusted to a location centered on the north south access aisle that connects the existing Southern Oregon Subaru to Lawnsdale Road.



Subject property tax lot numbers.

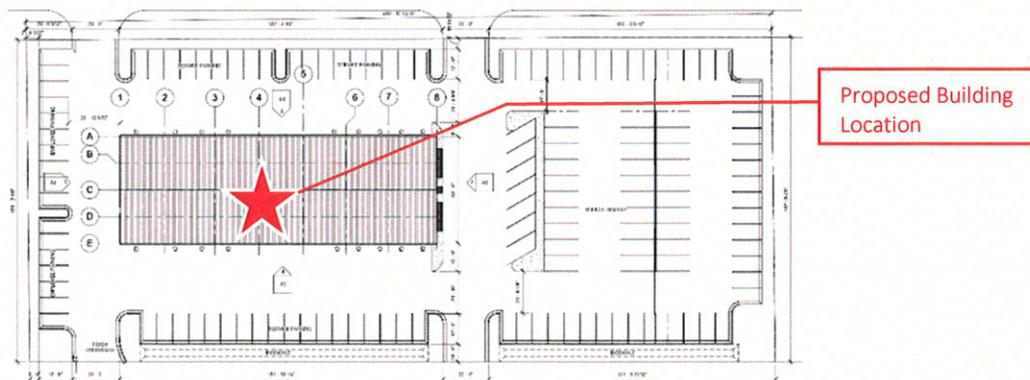
Lots 7000 and 7100 will be developed into paved storage of new vehicles to be sold. A portion of lot 7100 will be developed to accommodate charging stations for electric vehicles. The proposed building will consist of a pre-engineered steel structure with pre-finished metal wall panels and split face concrete masonry units. The internal activities will consist of a drive thru car wash (not open to the public), several detail bays, a lube pit bay, multiple service bays, rest rooms, storage areas, and a parts mezzanine. The site plan also depicts a concrete block trash enclosure and a fenced (chain link) yard.

City of Medford Site Plan and Architectural Review Narrative



Proposed Site Plan for Southern Oregon Subaru Service Center.

The foundation will consist of a concrete slab on grade. The finish floor will be approximately one foot above the height of the existing asphalt on Lawnsdale Road. The structure will consist of a pre-engineered metal building with light gauge metal or wood framing at the interior partitions. The facility will be equipped with an automatic fire sprinkler system per 903.2.9.1 OSSC. The site design implements various Best Management Practices strategies for the management of storm water runoff. Storm water from the roof top impervious surfaces will be directed to landscaped bios-wales located in the planter situated along the south property line of the subject lots. The bio-swale is intended to address the water quality of the storm water runoff generated by the new impervious surface resulting from this project. Treated storm water will then discharge through a control structure at a reduced rate into the public storm water collection system that exists in an easement that runs parallel with the southern property line of the subject lots. All vehicular circulation areas will be paved.



Enlarged Site Plan for Southern Oregon Subaru Service Center.

Zoning:

The project site address is 898 Lawnsdale Road, and is within the City limits. The site is classified on the City's General Land Use Plan Map for General Industrial (I-G). The subject property does not lie within any zoning overlay districts. The lots are fully serviced by roads and utility infrastructure including electricity, gas, telephone, water, sanitary sewer. Presently, the parcels have a collection of single family residences and related out building that will be demolished.

Existing Structures:

Several of the subject properties, tax lots 7000, 7200, 7300, currently have single family residences that will be demolished. Tax lot 7100 has been developed as a parking lot and will remain a parking area.

Utilities:

The site is fully serviced by roads and utility infrastructure including electricity, gas, telephone, water, sanitary sewer. The subject properties, tax lots 7000, 7100, 7200, and 7300, were once fully serviced and the utilities can be easily restored. Currently, there is an existing fire hydrant (H4174) on the north side of Lawnsdale Rd directly across from lot 7000. Additionally, there are also 3 existing fire hydrants on the larger Southern Oregon Subaru development.

Parking:

The proposed site plan depicts a total of 47 standard size vehicle parking spaces. Table 10.743-1 of the City of Medford Municipal code allows between 2.2 and 3 spaces per 1000 square feet of building for commercial structures less than 25,000 square feet in area. Based on the proposed building area (10680 sf), the minimum number of parking spaces allowed by the Development Code is 25 spaces and the maximum number is 32. The proposed site plan shows 2 ADA parking spaces resulting in a total of 49 standard parking stalls, as well as a new vehicle storage area to the east of the proposed building. The extra 21 spaces will be used as employee parking for the adjacent existing facility. Section 10.748 of the Medford Municipal Code requires the provision of bicycle parking spaces calculated at 10% of the total number of required vehicle parking spaces. In response to this requirement, the site plan shows a bicycle parking rack that is intended to accommodate 3 bicycles.

Landscaping:

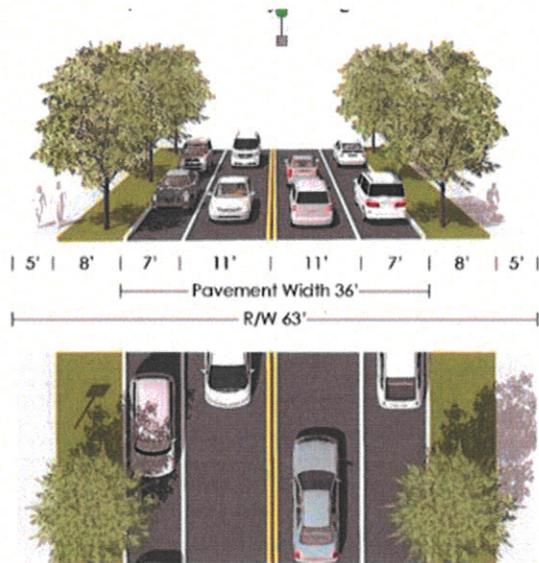
This application proposes front yard plantings that are consistent with the requirements of 10.780 and 10.797 of the City of Medford Municipal Code. Specifically, Section 10.797 requires 4 street trees and 25 shrubs per 100 feet of frontage. The planting count requirements are as follows:

TAX LOT	LENGTH	STREET	TREES REQ'D	SHRUBS REQ'D
Lot 7000-7300	420.00'	Lawnsdale	13	63

The application also proposes landscaped bio-swailes planted in accordance with BMP's that are intended to address the quality of storm water that is released from the site into the public storm water sewer system. Additional wider landscaped areas are proposed in some of the parking areas to meet the landscape density requirements of 10.746.

Frontage Improvements:

Lawnsdale is considered a Commercial Street with a 7 foot parking lane.



The total existing right of way is 60 feet. The Commercial Street design standard requires a R.O.W. of 63 feet. The applicant anticipates a dedication of approximately 2'-7" to 4'-3". In addition to the 5 foot sidewalk, 8 foot planter strip, and curb, this application proposes reconstruction of the paved section of Lawnsdale to 12 feet north of the center line of the R.O.W. This application also proposes a 6 foot chain link fence with 3 strand barbed wire at the top, located 10 feet from the north property line that abuts Lawnsdale.

Vehicular Ingress/Egress:

This application proposes a two drive approaches from Lawnsdale separated by approximately 182'. The proposed drive approaches will conform to the City of Medford Development Standards.

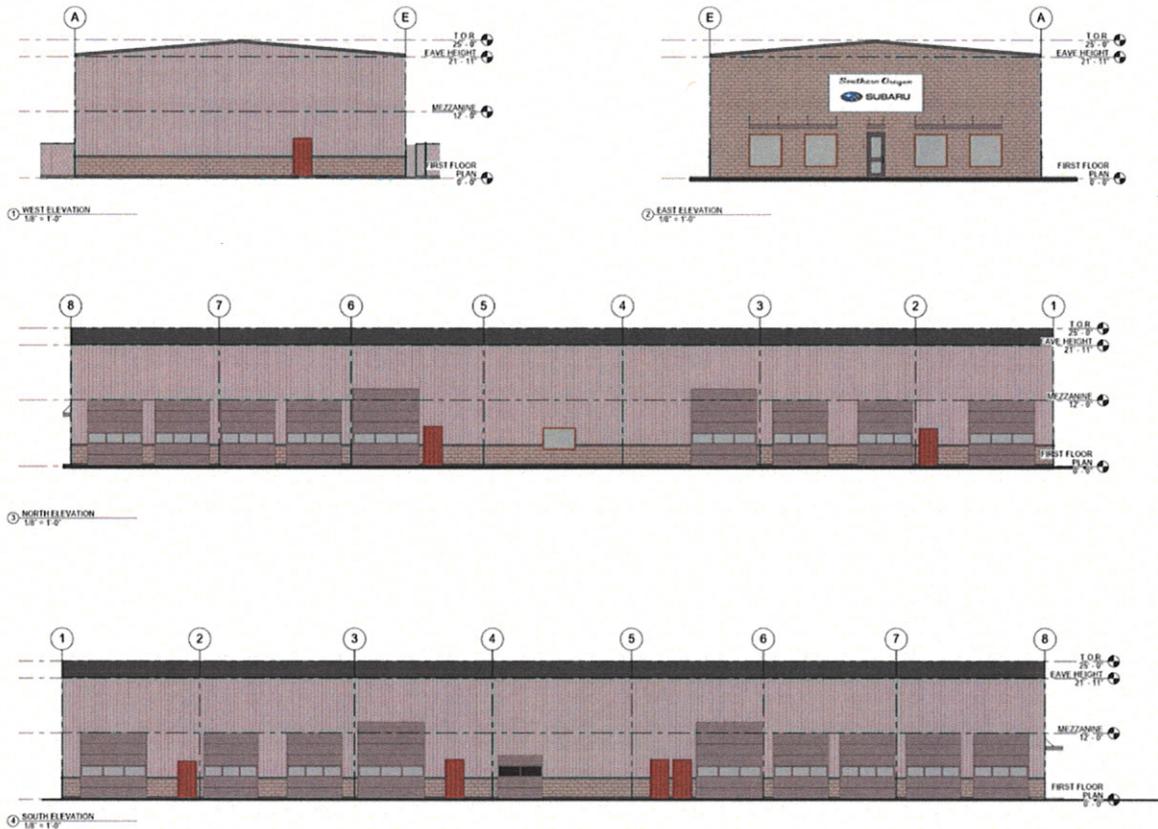
Storm Drainage:

This proposed site work intends to address the quality of the newly created storm water runoff in a manner that is acceptable to the City of Medford Public Works and Engineering departments. See Section II, item "H" below for a specific description of the Storm Water Management system.

Trash enclosures:

The proposed site plan depicts a trash and recycle structure in the south west portion of lot 7300. The trash enclosure has been located near the ingress point to allow for easy trash truck access. The

enclosure will be constructed from split face concrete masonry units with an opaque painted metal gate assembly.



Proposed exterior elevations for Southern Oregon Subaru Service Center.



Birds eye view of proposed Southern Oregon Subaru Service Center.



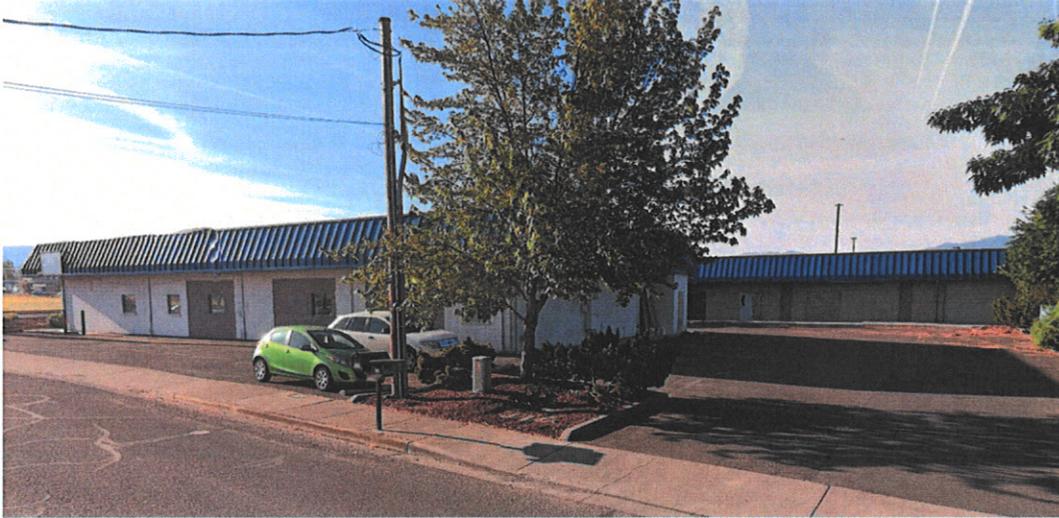
Aerial view looking at the vicinity of Biddle Rd and Lawnsdale at the project site.



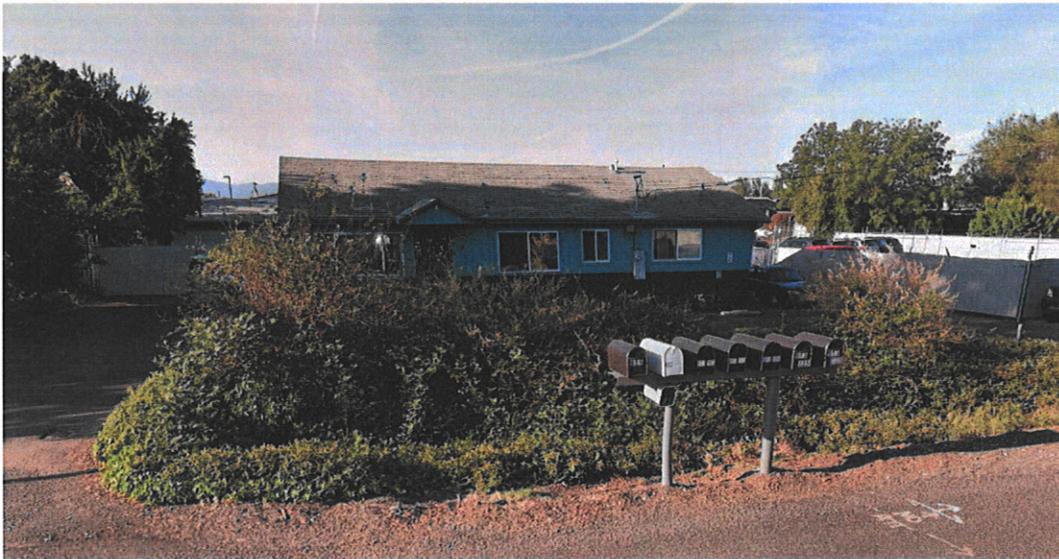
Street view looking west on Automation Way.



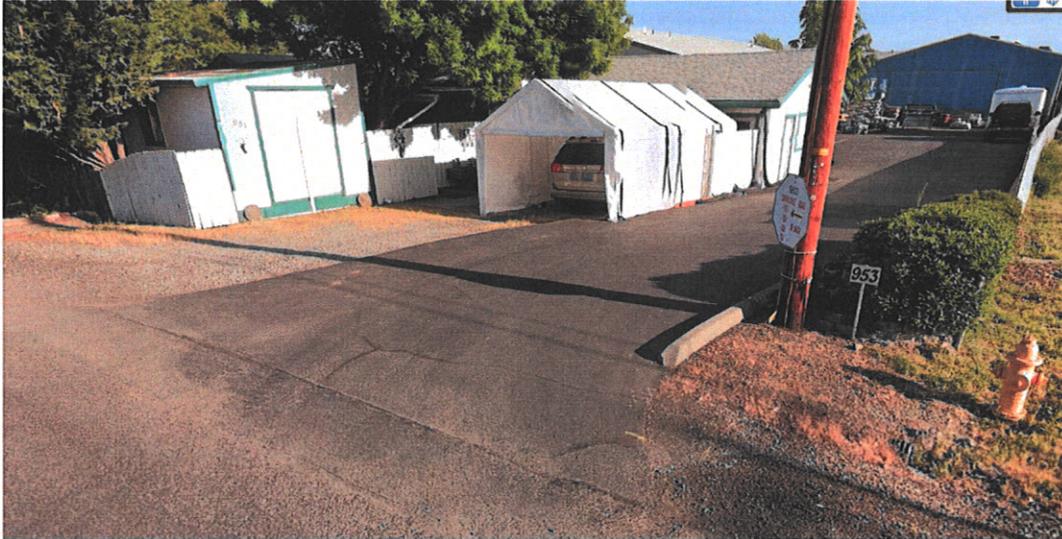
Street view looking at Southern Oregon Subaru from Biddle Rd.



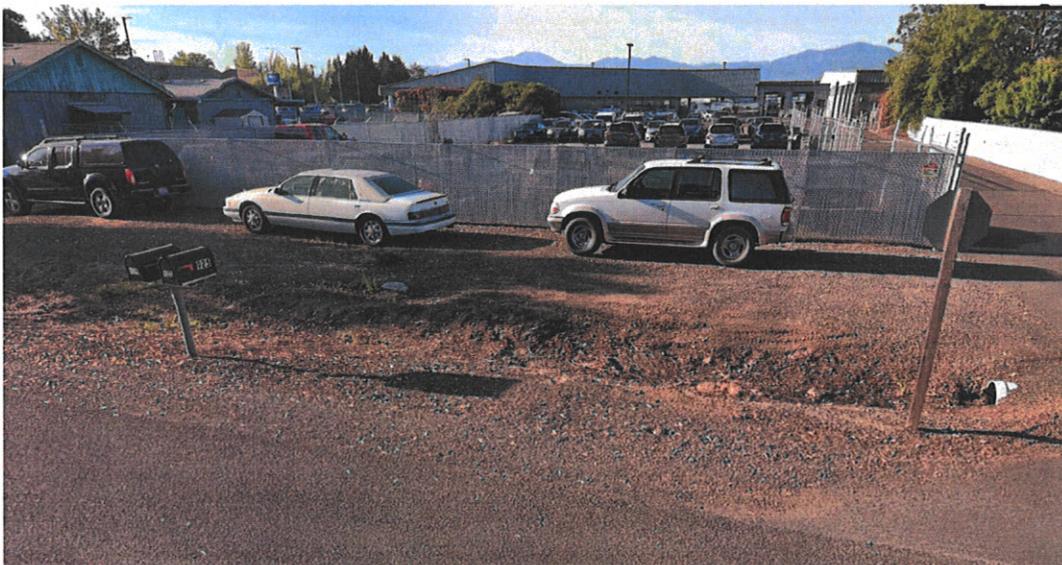
Street view of Commercial center at 3265 and 3263 Biddle Road.



Street view of house at 952 Lawnsdale on lot 7000.



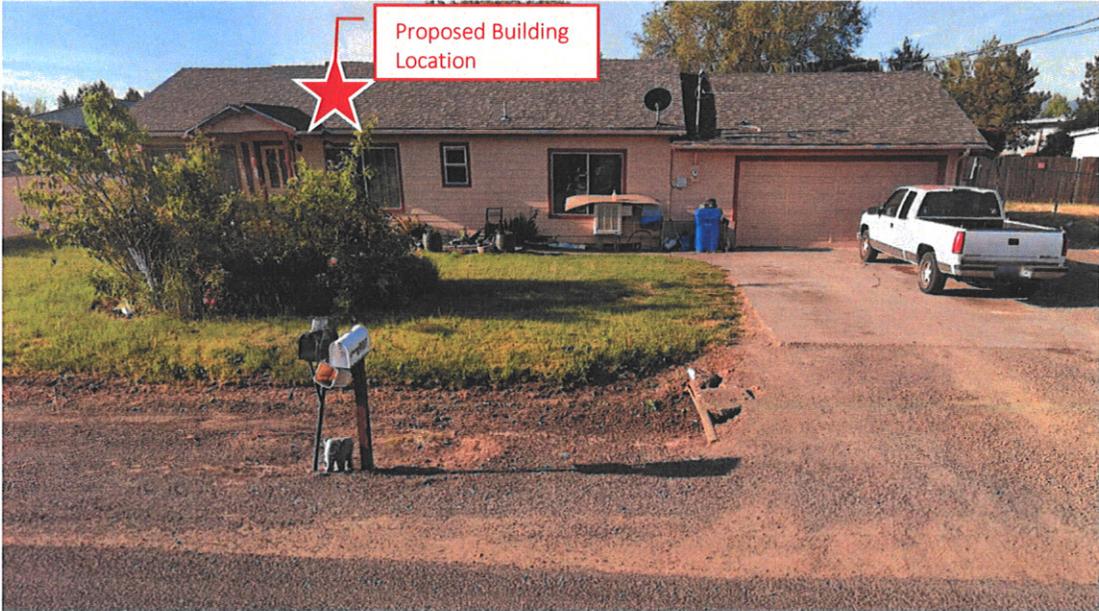
Street view of house at 952 Lawnsdale (across from lot 7000).



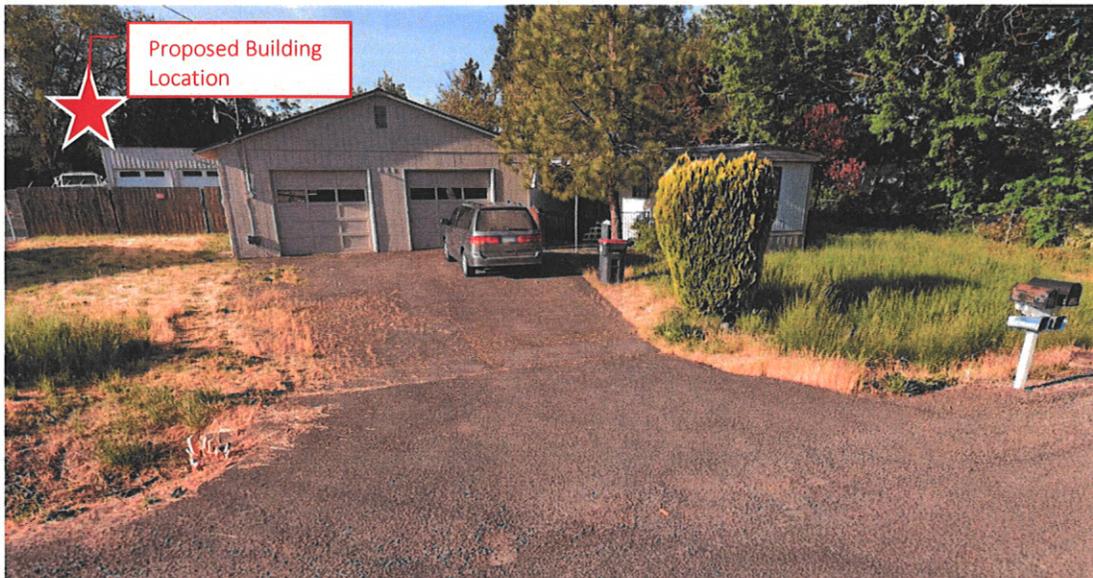
Street view of existing Subaru parking (924 Lawnsdale on lot 7100).



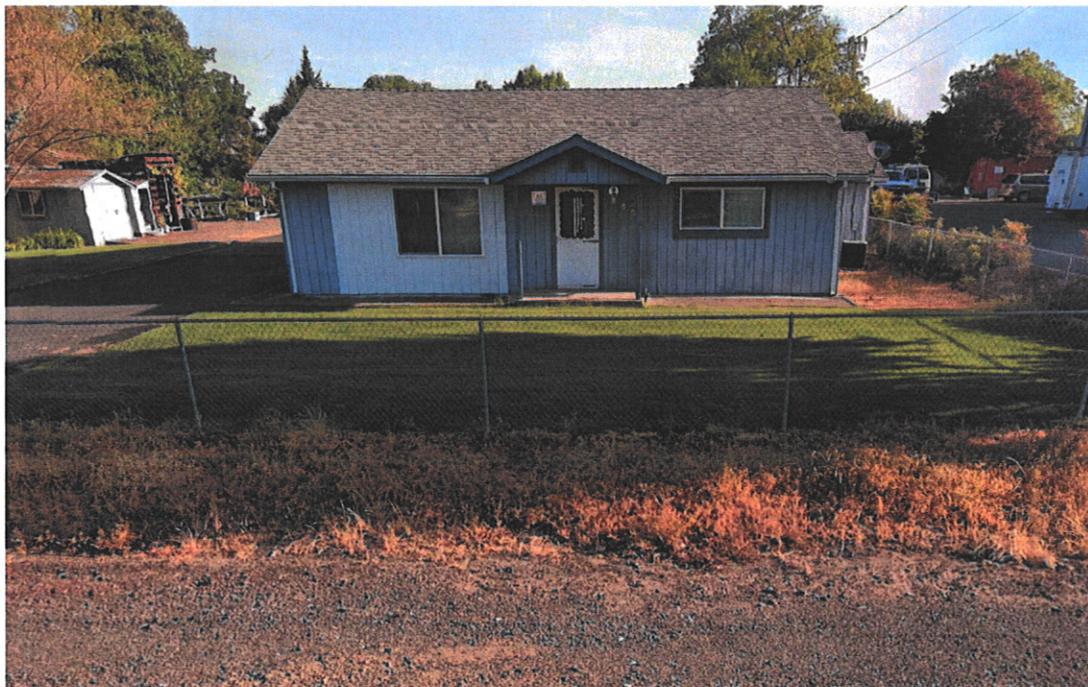
Street view looking at Alpha Heat and Air (907 Lawnsdale across from lot 7200).



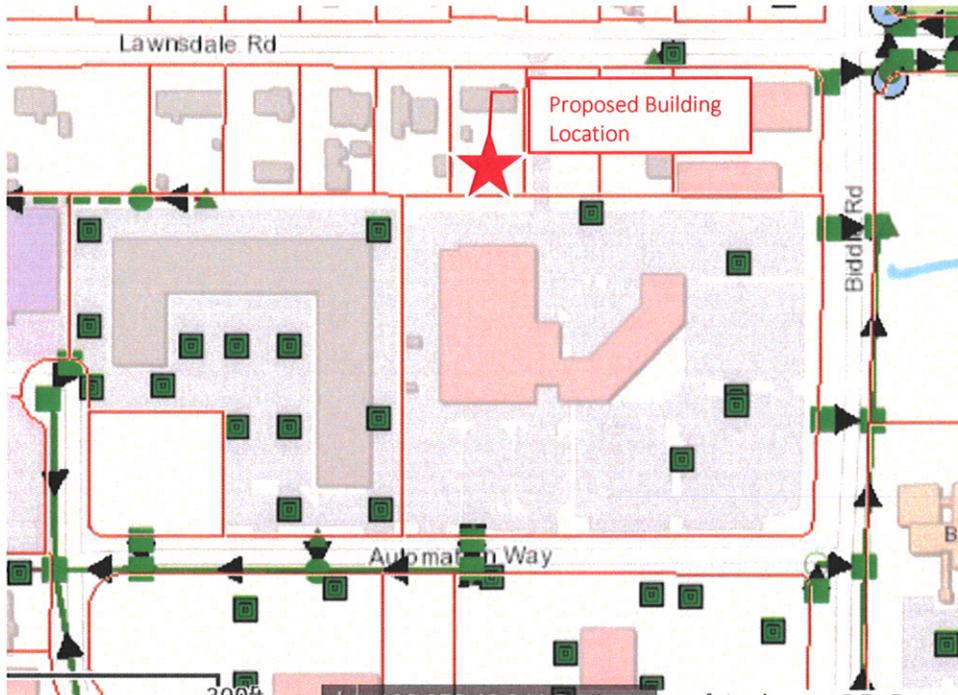
Street view of existing house at 908 Lawnsdale (lot 7200).



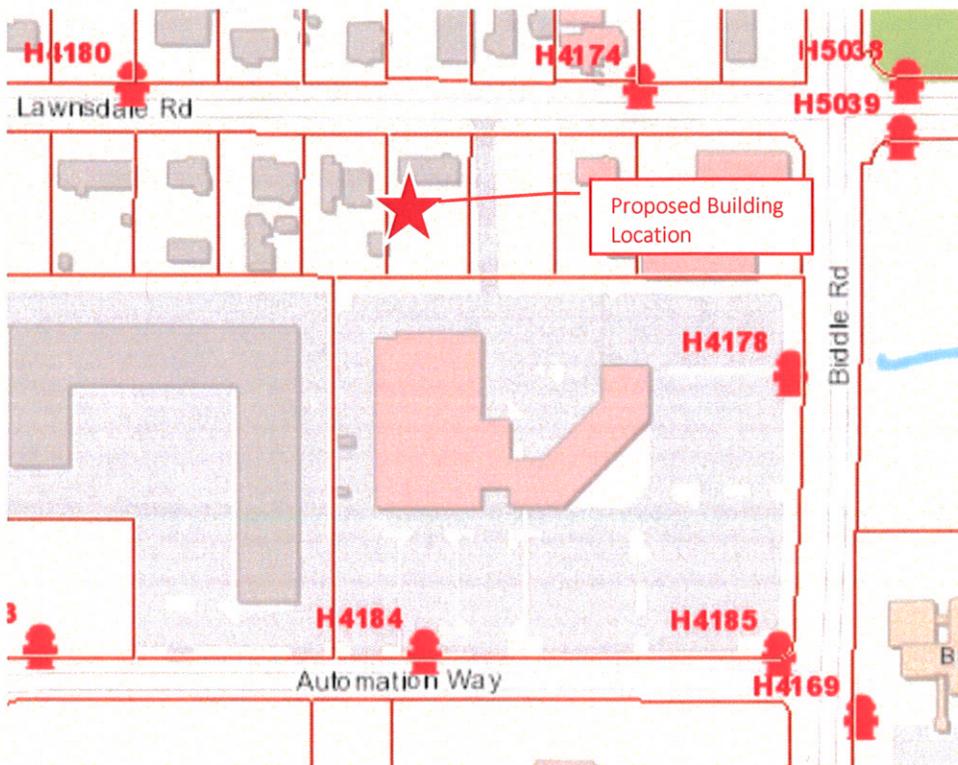
Street view of existing house and mobile home at 898 Lawnsdale (lot 7300).



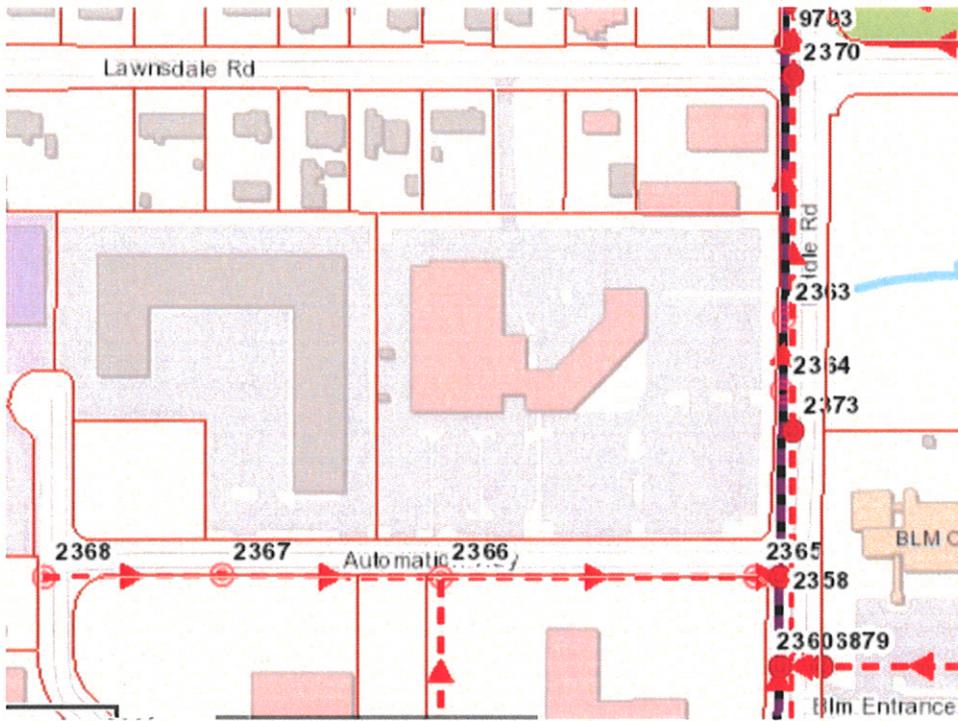
Street view of house at 895 Lawnsdale (across from lot 7300).



Storm drain location in Biddle Rd and Lawnsdale.

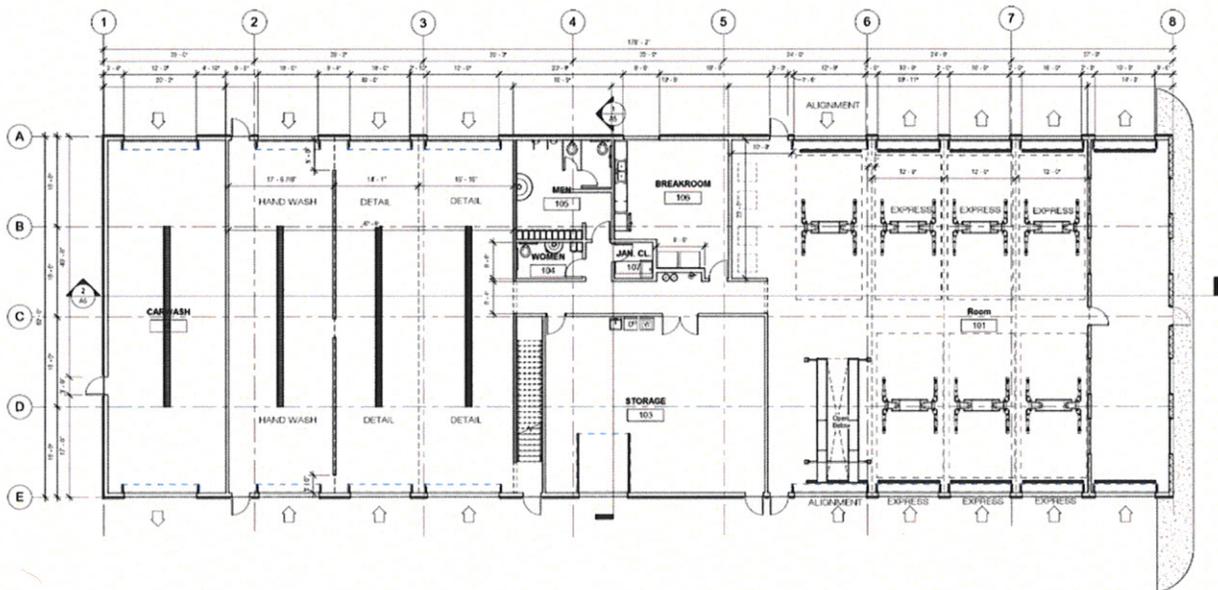


Locations of existing onsite fire hydrants.



Locations of existing sanitary sewer.

The proposed building measures approximately 178' x 60'. The tallest portion of the structure is approximately 25'-8" tall (measured to the tallest ridge).



Floor plan of proposed facility.

The architectural composition of the proposed building mass has been articulated in a manner intended to mitigate the perceived mass of the structure and yet retain an industrial character. A variety of architectural elements and exterior finish materials are intended to “soften” the presence of the structure and to contribute appropriately to the surrounding architectural context. Primarily, the mass of the structure is visually softened by the use of pre-finished metal wall panels and split face CMU.

The proposed building will be located approximately 18’ from the proposed East property line, 52’ from the West (rear) property line, 65’ from the South property line, and 56’ from the North property line.

SECTION II - Compatibility: Criterion No. 1

Note: The questions from the Site Plan and Architectural Review application are written below in full in italics. The Applicant response follows each question.

A. List existing uses and development adjacent to your project site. Along with this list, describe the architecture (materials, colors, etc.) age, and condition of the adjacent buildings (you may use photographs to supplement this information).

Adjacent development can be classified as generally residential to the north of Lawnsdale with the exception of Alpha Heat and Air which can be classified as light industrial. Alpha Heat and Air is a pre-engineered metal building with pre-finished metal wall panels. The existing residential buildings tend to be single-storied and in varied states of disrepair and appear to have been constructed in the mid 19th century (see images above). Typical exterior materials include painted wood side and asphalt composition roofing. The commercial center at the corner of Biddle and Lawnsdale is painted CMU with a standing seam metal mansard roof. The commercial center to the west of Southern Oregon Subaru at 943 Automation Way is constructed of painted CMU with a standing seam metal roof.

B. Describe the building architecture and exterior treatments in your proposal, and how they fit with and complement adjacent buildings and development.

The proposed building is a pre-engineered steel structures with traditional pre-finished metal siding on the sides and rear of the building. The east elevation of the proposed building is split face CMU with windows and painted steel canopies. The sides and rear of the building will be constructed with pre-finished metal wall panels and a split face CMU wainscot to four feet above the finished floor. The proposed combination of split face CMU and pre-finished metal wall panels is consistent with the commercial buildings in the near vicinity of this project. Entries and feature windows have been articulated with human scale elements.



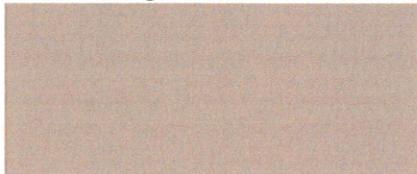
Perspective view of West end of the proposed building.



Perspective view of East end of the proposed building.

PALLETTE OF EXTERIOR MATERIALS AND PAINT COLORS

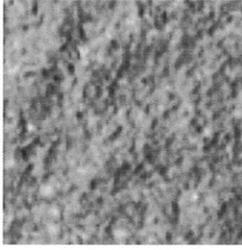
Base Building and Roof color: MBCI Ash Gray



Accent Panel color: MBCI Charcoal Gray



Split Face CMU:



- C. *Describe the proposed architecture and exterior treatments that break up large facades and give relief to the building mass.*

As stated above, the proposed building structure houses utilitarian activities and consequently requires a fairly utilitarian building. As stated above, the building mass is mitigated through the use of a four foot CMU wainscot, decorative canopies, and overhead sectional doors.

- D. *Describe how the placement and orientation of the proposed building(s) relate(s) to the street facilities, and how this orientation promotes a more pedestrian-friendly site design.*

The proposed building is part of the larger operation of Southern Oregon Subaru and is generally not available to the public, or pedestrians for that matter. The primary pedestrian activity will be employees moving around the various buildings. The site plan depicts ample drive aisles intended to make plenty of room for pedestrians and motor vehicles. Access from the proposed structure to the public way is straightforward and direct, but, by necessity, occurs in tandem with the limited onsite vehicular maneuvering areas.

- a. *If the site lies within 600-feet of an existing or planned transit stop, as designated by the Transportation System Plan (TSP), describe compliance with the standard 10.808, New Commercial and Institutional Development.*

The proposed development on this site does is not located within 600 feet of an existing or proposed transit stop and consequently is not subject to the requirements of 10.808.

- E. *Describe the pedestrian facilities and amenities to your site (usable outdoor space, benches, etc.) and how they will function for pedestrians.*

The site plan indicates a sidewalk set back from the roadway by an approximately 8' wide planter strip which serves to provide separation of the pedestrian from the vehicular roadway traffic. The configuration of the public sidewalk and the planter strip is consistent with the nearby development. As well a 5' wide pedestrian walkway ties the building entrance to the public way. Other than the connection to the sidewalk along Lawnsdale Road, there are no opportunities to provide pedestrian access to the adjacent properties.

- F. *Describe vehicle and pedestrian access to the site, and how it relates internally on the site, and to adjacent sites.*

This proposal intends to utilize the existing private access aisle leading from the Subaru Dealership to Lawnsdale. The existing approach will be reconstructed to City of Medford standards.

- G. *Describe if and how the proposed plan is sensitive to retaining any existing trees or significant native vegetation on the site. Should existing trees be preserved, a Tree Protection Plan shall be included in this application*

There are no significant trees or site features existing on the subject parcel. Any new trees planted will be along the street landscape frontage will conform to the City of Medford frontage landscape requirements. A Tree Protection Plan will not be required.

- H. *Describe storm water detention facilities on the site (underground storage, surface ponds, etc.). If these facilities will be landscaped areas, describe how the proposed landscaping will be integrated with other landscaping on the site.*

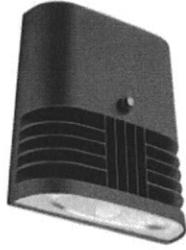
The existing underground public storm sewer runs along the south property line of the subject parcels. This project is proposing to direct the storm water runoff from the proposed impervious surfaces to bio swales where the storm water will be treated before it is released at a controlled rate into the public storm water sewer system. The control structures at the low end of the bio swales will limit the storm water outflow which will be restricted to meet City of Medford maximum flow requirements as it is directed through the control structure and introduced into the public system. Collection of storm water in the asphalt areas will be achieved by lynch style catch basins which will be piped to the bio swales. At the time of permitting/construction the property owner anticipates entering into a "Storm Water Management Agreement" with the City of Medford. Every effort has been made to develop a sustainable storm water management system that will utilize BMP's (Best Management Practices) to ensure a superior quality storm water management system.

- I. *Describe how your proposed landscaping design will enhance the building and other functions on the site.*

The proposed landscape plan endeavors first to satisfy the front yard and planter strip requirements as defined in the City of Medford LDO, and second, to augment the storm water management system. The landscaping includes the required front yard planter with minor internal planter areas, and a larger bio swale at the south portion of the property. The proposed impervious hardscape and new roof area represents a large portion of the entire lot and, consequently, require a significant amount of landscaped bio swale.

- J. *Describe how your exterior lighting illuminates the site and explain how the design of fixtures does not diminish a view of the night sky, or produce glare on adjacent properties, consistent with the standards of 10. 764.*

The proposed exterior lighting is limited to wall mounted LED fixtures specifically designed to eliminate glare beyond the property lines. All exterior lighting will be either located and/or directed away from adjacent properties to avoid glare and light pollution.



Lithonia OLWX1 40 watt LED wall mounted light

- K. *Describe any proposed signage, and how it will identify the location of the occupant and serve as an attractive complement to the site.*

A wall mounted back lit sign displaying the company logo will be installed on the east elevation facing the new car storage area. The sign will be permitted under a separate permit.

- L. *Explain any proposed fencing, including its purpose, and how you have incorporated it as a functional, attractive component of your development. (See Sections 10.731-10.733).*

The proposed development will be entirely fenced. The proposed chain link fence will be 6 feet tall with 3 strands of barbed wire and comply with all requirements of Section 10.732 of the City of Medford Municipal Code. The fence will be equipped with several sliding gates.

- M. *Explain how any potential noise generated by future occupants will be mitigated on the proposed site, consistent with the standards of 10.752-10.761*

Noise generated by activity on the site will comply with the requirements listed in 10.752 through 10.761. This project does not propose uses or activities on the site that are not consistent with the existing activities in the neighborhood or with the activities implied by the zoning designation of the subject parcel.

- N. *Explain anything else about your project that adds to the compatibility of the project with adjacent development and uses.*

The proposed use is compatible with adjacent development and consistent with the intent of the City of Medford Comprehensive plan. Additionally, the proposed development attempts to achieve the industrial use and character intended by the underlying zoning designation. The character and quality of the neighborhood will be enhanced by this project and is in keeping with the intended industrial character of the neighborhood.

- O. *List and explain any exceptions or modifications requested and provide reasons for such.*

No exceptions or modifications to the Land Development Code are sought for the proposed project.

SECTION II -- Code Compliance: Criterion No. 2

Refer to information filled in on SPAC application.

CONCLUSION:

This application is presented with confidence that it will be found to be consistent with the City of Medford Municipal Code and Land Development Ordinance because the criteria for approval are met or can feasibly be met with conditions.

Prepared by:

 ARCHITECT

Signature and Title

Gary R. Caperna, AIA
Name (Please Print)

PUBLIC WORKS DEPARTMENT STAFF REPORT

Southern Oregon Subaru Service/Repair Facility 898-952 Lawnsdale Road (TLs 7000, 7100, 7200, 7300)

- Project:** Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres.
- Location:** Located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300).
- Applicant:** Applicant: Southern Oregon Subaru; Agent: J.B. Steele, Inc.; Planner: Dustin Severs.

NOTE: The items listed here shall be completed and accepted prior to the respective issuances of permits and certificates:

Prior to issue of the first building permit, the following items shall be completed and accepted:

- Submittal and approval of plans for site grading and drainage, and detention, if applicable.
- Completion of all public improvements, if required. The Applicant may provide security for 120% of the public improvements prior to issuance of building permits. Construction plans for the public improvements shall be approved by the Public Works Engineering Division prior to acceptance of security.
- Items A – D, unless noted otherwise.

Prior to issue of Certificate-of-Occupancy for completed structures, the following items shall be completed and accepted:

- Paving of all on-site parking and vehicle maneuvering areas
- Verification by the design Engineer that the stormwater quality and detention system was constructed per the approved plan, if applicable.
- Completion of all public improvements, if applicable.



A. STREETS

1. Dedications

Lawnsdale Road is classified as a Commercial street within the Medford Land Development (MLDC) 10.429. The Developer shall dedicate for public right-of-way, sufficient width of land along the frontage of this development to comply with the half width of right-of-way, which is 31.5-feet. **The Developer's surveyor shall verify the amount of additional right-of-way required.**

In accordance with MLDC 10.471, **the property owner shall dedicate a 10-foot wide public utility easement (PUE)** adjacent to the right-of-way line along this Developments frontage to Lawnsdale Road.

The right-of-way and easement dedications shall be submitted directly to the Engineering Division of the Public Works Department. The submittal shall include: the right-of-way and easement dedication, including an exhibit map; a copy of a current Lot Book Report, Preliminary Title Report, or Title Policy; a mathematical closure report (if applicable), and the Planning Department File Number; for review and City Engineer acceptance signature prior to recordation by the applicant. Releases of interest shall be obtained by holders of trust deeds or mortgages on the right-of-way and PUE area.

2. Public Improvements

a. Public Streets

Lawnsdale Road shall be improved to Commercial street standards in accordance with MLDC 10.429. The Developer shall improve the south half plus 12-feet north of the centerline or to the far edge of the existing pavement, whichever is greater, along the frontage of this development.

As an option, the Developer may elect to provide evidence of the existing structural section to Public Works for consideration in order to determine if the extent of construction may be reduced. Depending on the results, the Developer still may be responsible for the improvements noted above or at minimum improve the remainder of the south half of Lawnsdale Road from a point 1-foot inside the existing edge of pavement.

b. Street Lights and Signing

The Developer shall provide and install in compliance with Section 10.495 of the Medford Municipal Code (MMC). Based on the preliminary plan submitted, the following number of street lights and signage will be required:

Street Lighting & Signage – Developer Provided & Installed:

- A. 2 – Type R-150 LED

Traffic Signs and Devices – City Installed, paid by the Developer:

A. NONE

**NOTE: Remove PPL Street Light on PPL pole #2/853. Power source should come out of existing COM pole #3460. (Junction box would need to be cut into existing lighting conduit).*

Numbers are subject to change if changes are made to the plans. All street lights shall be installed per City standards and be shown on the public improvement plans. Public Works will provide preliminary street light locations upon request. All street lights shall be operating and turned on at the time of the final “walk through” inspection by the Public Works Department.

The Developer shall be responsible for the preservation and re-installation of all signs removed during demolition and site preparation work. The Developer’s contractor shall coordinate with the City of Medford Public Works, Maintenance and Operations Division to remove any existing signs and place new signs provided the Developer.

c. Pavement Moratoriums

There is no pavement cutting moratorium currently in effect along this developments respective frontages.

The Developer shall be responsible for notifying by certified letter all utility companies, as well as all current property owners of parcels which are adjacent to any Public Street being constructed or paved as part of this project. The letter shall inform the utility companies and property owners of the City's street moratorium policy with respect to pavement cutting for future utility services. The utility companies and property owners shall be given the opportunity to install utility services within the right-of-way prior to paving and the subsequent moratorium. Notifications shall be mailed by the Developer at least 6 months before a street is resurfaced or rebuilt per Medford Municipal Code (MMC), Section 3.070. Copies of the certifications shall be submitted to the City Engineer with the submittal of the preliminary construction drawings.

d. Access and Circulation

The existing block length does not meet the requirements of MLDC 10.426. The applicant shall provide findings addressing the exception criteria in 10.426.C.2.

Gates shall be setback from the public right-of-way in accordance 10.732(5)

Driveway access to the site shall be in accordance with MLDC 10.550.

3. Section 10.668 Analysis

To support a condition of development that an Applicant dedicate land for public use or

provide a public improvement, the Medford Code requires a nexus and rough proportionality analysis which is essentially a codification of the constitutional provisions in Nollan and Dolan cases.

10.668 Limitation of Exactions

Notwithstanding any other provisions of this Chapter 10, an Applicant for a development permit shall not be required, as a condition of granting the application, to dedicate land for public use or provide public improvements unless:

(1) the record shows that there is an essential nexus between the exaction and a legitimate government purpose and that there is a rough proportionality between the burden of the exaction on the Developer and the burden of the development on public facilities and services so that the exaction will not result in a taking of private property for public use, or

(2) a mechanism exists and funds are available to fairly compensate the Applicant for the excess burden of the exaction to the extent that it would be a taking.

1. Nexus to a legitimate government purpose

The purposes for these dedications and improvements are found throughout the Medford Code, the Medford Transportation System Plan, and the Statewide Planning Rule, and supported by sound public policy. Those purposes and policies include, but are not limited to: development of a balanced transportation system addressing all modes of travel, including motor vehicles, transit, bicycles, emergency services and pedestrians. Further, these rights-of-way are used to provide essential services such as sanitary sewer, domestic water and stormdrain to serve the developed parcels. It can be found that the listed right-of-way dedications and improvements have a nexus to these purposes and policies.

2. Rough proportionality between the dedications and improvements, and the impacts of development.

No mathematical formula is required to support the rough proportionality analysis. Furthermore, benefits to the development resulting from the dedication and improvements when determining “rough proportionality” have been considered, including but not limited to: increased property values, intensification of use, as well as connections to municipal services and the transportation network.

As set forth below, the dedications and improvements recommended herein can be found to be roughly proportional to the impacts reasonably anticipated to be imposed by this development.

Lawnsdale Road:

The additional right-of-way on Lawnsdale Road will provide the needed width for a planter

strip and sidewalk. Lawnsdale Road is currently a 25 mile per hour facility. The planter strip moves pedestrians a safe distance from the edge of the roadway. Lawnsdale Road will be the primary route for pedestrians traveling to and from this development. The development shall construct approximately 400 linear feet of sidewalk along the frontage of the property. All developments in Medford are required to construct their frontage sidewalk and therefore this is roughly proportional.

Local street right-of-way dedication and construction requirements identified by the Public Works Department and required by the City are the minimum required to protect the public interest and are necessary for additional or densification of development in the City without detracting from the common good enjoyed by existing properties. Developments are required to provide all internal local streets and half-street improvements to abutting streets, including associated right-of-way dedications, to ensure that new development and density intensification provides the current level of urban services and adequate street circulation is maintained.

Dedication of the Public Utility Easement (PUE) will benefit development by providing public utility services, which are out of the roadway and more readily available to each lot or building being served. The additional traffic of all modes of travel generated by this proposed development supports the dedication and improvements for all modes of travel and utilities. As indicated above, the area required to be dedicated for this development is necessary and roughly proportional to that required in similar developments to provide a transportation system that meets the needs for urban level services.

The additional street lighting on Lawnsdale Road will provide the needed illumination to meet current MLDC requirements.

B. SANITARY SEWERS

This site lies within the Rogue Valley Sewer Service (RVSS) area. The Developer shall contact RVSS for conditions of connection to the sanitary sewer collection system.

C. STORM DRAINAGE

1. Drainage Plan

A comprehensive drainage plan showing the entire project site with sufficient spot elevations to determine direction of runoff to the proposed drainage system, and also showing elevations on the proposed drainage system, shall be submitted with the first building permit application for approval.

The Developer shall provide copies of either a Joint Use Maintenance Agreement or a private stormdrain easement for any stormwater draining onto or from adjacent private property.

A Site/Utility Plan shall be submitted with the building permit application to show the location of existing or proposed stormdrain lateral/s for the site.

All private stormdrain lines shall be located outside of the public right-of-way and/or any public utility easements (PUE).

2. Grading

A comprehensive grading plan showing the relationship between adjacent property and the proposed development will be submitted with the improvement plans for approval. Grading on this development shall not block drainage from an adjacent property or concentrate drainage onto an adjacent property without an easement. The Developer shall be responsible that the final grading of the development shall be in compliance with the approved grading plan.

3. Detention and Water Quality

Stormwater quality and detention facilities shall be required in accordance with MLDC Section 10.481 and 10.729.

4. Verification

Upon completion of the project, and prior to certificate of occupancy of the building, the Developer's design Engineer shall verify that the construction of the stormwater quality and detention system was constructed per plan. Verification shall be in writing and submitted to the Engineering Division of Public Works. Reference Rogue Valley Stormwater Quality Design Manual, Appendix I, Technical Requirements.

5. Erosion Prevention and Sediment Control

All development that disturbs 5,000 square feet or greater shall require an Erosion Prevention and Sediment Control Plan. Developments that disturb one acre and greater shall require a 1200C permit from the Department of Environmental Quality (DEQ). Erosion Prevention and Sediment Control Plans shall be submitted to the Building Department with the project plans for development. All disturbed areas shall be covered with vegetation or properly stabilized prior to certificate of occupancy.

D. GENERAL CONDITIONS

1. Design Requirements and Construction Drawings

All public improvements shall be constructed in accordance with the "Engineering Design Standards for Public Improvements", adopted by the Medford City Council. Copies of this document are available in the Public Works Engineering office.

2. Construction Plans

Construction drawings for any public improvements for this project shall be prepared by a professional Engineer currently licensed in the State of Oregon, and submitted to the Engineering Division of Medford Public Works Department for approval. Construction drawings for public improvements shall be submitted only for the improvements to be constructed with each phase. Approval shall be obtained prior to beginning construction. Only a complete set of construction drawings (3 copies) shall be accepted for review, including plans and profiles for all streets, minimum access drives, sanitary sewers, stormdrains, and street lights as required by the governing Commission's Final Order, together with all pertinent details and calculations. A checklist for public improvement plan submittal can be found on the City of Medford, Public Works web site (<http://www.ci.medford.or.us/Page.asp?NavID=3103>). The Developer shall pay a deposit for plan review and construction inspection prior to final plan approval. Public Works will keep track of all costs associated with the project and, upon our acceptance of the completed project, will reconcile the accounting and either reimburse the Developer any excess deposit or bill the Developer for any additional amount not covered by the deposit. The Developer shall pay Public Works within 60 days of the billing date or will be automatically turned over for collections.

In order to properly maintain an updated infrastructure data base, the Surveyor of Record shall submit an as-built survey prior to the Final Inspection and, the Engineer of Record shall submit mylar "as-constructed" drawings to the Engineering Division within sixty (60) calendar days of the Final Inspection (walk through). Also, the Engineer shall coordinate with the utility companies, and show all final utility locations on the "as built" drawings.

3. Construction and Inspection

The Developer or Developer's contractor shall obtain appropriate right-of-way permits from the Department of Public Works prior to commencing any work within the public right-of-way that is not included within the scope of work described within approved public improvement plans.

Contractors proposing to do work on public streets (including street lights), sewers, or stormdrain shall 'prequalify' with the Engineering Division prior to starting work. Contractors shall work off a set of public improvement drawings that have been approved by the City of Medford Engineering Division. Any work within the County right-of-way shall require a separately issued permit from the County.

For City of Medford facilities, the Public Works Maintenance Division requires that public sanitary sewer and stormdrain mains be inspected by video camera prior to acceptance of these systems by the City.

Where applicable, the Developer shall bear all expenses resulting from the adjustment of manholes to finish grades as a result of changes in the finish street grade.

4. Site Improvements

All on-site parking and vehicle maneuvering areas related to this development shall be paved in accordance with MLDC, Section 10.746, prior to issuance of certificate of occupancy for any structures on the site. Curbs shall be constructed around the perimeter of all parking and maneuvering areas that are adjacent to landscaping or unpaved areas related to this site. Curbs may be deleted or curb cuts provided wherever pavement drains to a water quality facility.

5. System Development Charges (SDC)

Buildings in this development are subject to system development charges (SDCs). All SDC fees shall be paid at the time individual building permits are issued.

Prepared by: Jodi K Cope

Reviewed by: Doug Burroughs

SUMMARY CONDITIONS OF APPROVAL
Southern Oregon Subaru Service/Repair Facility
898-952 Lawnsdale Road (TLs 7000, 7100, 7200, 7300)

AC-20-227

A. Streets

1. Street Dedications to the Public:

- **Lawnsdale Road** – No additional right-of-way required.
- Dedicate 10-foot Public Utility Easement (PUE) along the frontage.

2. Improvements:

Public Streets

- Improve **Lawnsdale Road** to Commercial street standards, ½ plus 12-feet.
- Public Improvement Plans for this work shall be submitted directly to the Public Works Engineering Department.

Lighting and Signing

- Developer supplies and installs all street lights at own expense.

Access and Circulation

- The existing block length does not meet the requirements of MLDC 10.426. The applicant shall provide findings addressing the exception criteria in 10.426.C.2.
- Gates shall be setback from the public right-of-way in accordance 10.732(5)
- Driveway access to the site shall be in accordance with MLDC 10.550.

Other

- No pavement moratorium currently in effect along this frontage to Lawnsdale Road.
- Provide pavement moratorium letters.
- Provide soils report.

B. Sanitary Sewer:

- The site is situated within the RVSS area.

C. Storm Drainage:

- Provide a comprehensive grading and drainage plan.
- Provide water quality and detention facilities, calculations and O&M Manual.
- Provide engineers verification of stormwater facility construction.
- Provide copy of an approved Erosion Control Permit (1200C) from DEQ for this project.

- = City Code Requirement
- = Discretionary recommendations/comments

The above summary is for convenience only and does not supersede or negate the full report in any way. If there is any discrepancy between the above list and the full report, the full report shall govern. Refer to the full report for details on each item as well as miscellaneous requirements for the project, including requirements for public improvement plans (Construction Plans), design requirements, phasing, draft and final plat processes, permits, system development charges, pavement moratoriums and construction inspection.



Medford Fire-Rescue Land Development Report

Review/Project Information

Reviewed By: Shay, Mark

Review Date: 8/28/2020

Meeting Date: 9/2/2020

LD File #: AC20227

Planner: Dustin Severs

Applicant: Southern Oregon Subaru

Site Name: Southern Oregon Subaru

Project Location: 952, 924, 908 and 898 Lawnsdale Road

ProjectDescription: Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres, located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300).

Specific Development Requirements for Access & Water Supply

Reference	Comments	Conditions
		Description
OFC 912.2.1	The FDC shall be remote from the building, and within 75 feet of a hydrant, an additional hydrant will be required to meet this condition. If the building is not equipped with an NFPA 13 sprinkler system, per the calculated fire flow and based on the current hydrant spacing, an additional hydrant will be required per OFC C102.1 and Table C102.1.	Fire department connections shall be located on the street side of buildings or facing approved fire apparatus access roads, fully visible and recognizable from the street, fire apparatus access road or nearest point of fire department vehicle access or as otherwise approved by the fire code official.
OFC 912.4.1	Please provide locking FDC caps.	The fire code official is authorized to require locking caps on fire department connections for water-based fire protection systems where the responding fire department carries appropriate key wrenches for removal.



EXHIBIT

I

<p>OFC 503.4; D103.6; D103.6.1; D103.6.2</p>	<p>Apparatus access roads shall be posted both sides as a fire lane along curbs within the lot.</p> <p>Per OFC D104.1, only one fire department apparatus access is required, therefore posting of the fire lane is only required on one of the access roads.</p>	<p>Fire apparatus access roads 20-26' wide shall be posted on both sides as a fire lane. Fire apparatus access roads more than 26' to 32' wide shall be posted on one side as a fire lane (OFC D103.6.1).</p> <p>Where parking is prohibited for fire department vehicle access purposes, NO PARKING-FIRE LANE signs shall be spaced at minimum 50' intervals along the fire lane (minimum 75' intervals in 1 & 2 family residential areas) and at fire department designated turn-around's. The signs shall have red letters on a white background stating "NO PARKING-FIRE LANE" (See handout).</p> <p>For privately owned properties, posting/marketing of fire lanes may be accomplished by any of the following alternatives to the above requirement (consult with the Fire Department for the best option):</p> <p>Alternative #1: Curbs shall be painted red along the entire distance of the fire department access. Minimum 4" white letters stating "NO PARKING-FIRE LANE" shall be stenciled on the curb at 25-foot intervals.</p> <p>Alternative #2: Asphalt shall be striped yellow or red along the entire distance of the fire department access. The stripes shall be at least 6" wide, be a minimum 24" apart, be placed at a minimum 30-60 degree angle to the perimeter stripes, and run parallel to each other. Letters stating "NO PARKING-FIRE LANE" shall be stenciled on the asphalt at 25-foot intervals.</p> <p>Fire apparatus access roads shall not be obstructed in any manner, including the parking of vehicles. The minimum widths (20' wide) and clearances (13' 6" vertical) shall be maintained at all times (OFC 503.4; ORS 98.810-12).</p> <p>This restriction shall be recorded on the property deed as a requirement for future construction.</p> <p>A brochure is available on our website at: http://www.ci.medford.or.us/Files/Fire%20Lane%20Brochure.pdf</p>
<p>OFC 503.2.4</p>	<p>The turning radius on fire department access roads and fire lanes shall following Medford Fire Department requirements:</p> <p>Minimum inside turning radius: 25 feet</p> <p>Minimum outside turning radius: 35 feet</p>	<p>The required turning radius of a fire apparatus access road shall be determined by the fire code official.</p>
<p>OFC 507.3</p>	<p>Per Appendix B, fire flow for a Type IIB building is 2,250 gallons per minute for 2 hours. If the building is equipped with an NFPA 13 sprinkler system, the fire flow can be reduced by 75% per Table B105.2</p>	<p>Fire-flow requirements for buildings or portions of buildings and facilities shall be determined by an approved method.</p>

Construction General Information/Requirements

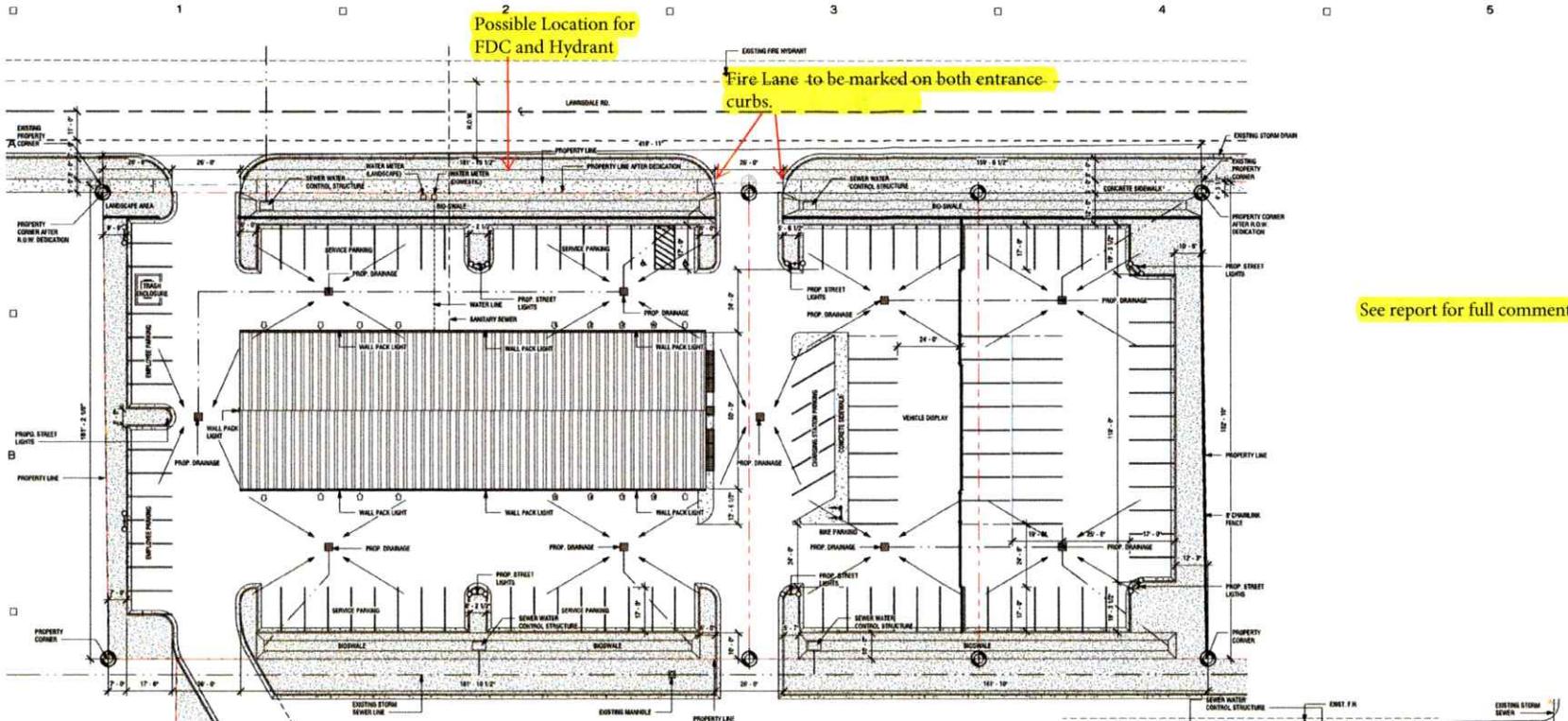
Development shall comply with access and water supply requirements in accordance with the Oregon Fire Code in affect at the time of development submittal. Fire apparatus access roads are required to be installed prior to the time of construction. The approved water supply for fire protection (fire hydrants) is required to be installed prior to construction when combustible material arrives at the site.

Specific fire protection systems may be required in accordance with the Oregon Fire Code. This plan review shall not prevent the correction of errors or violations that are found to exist during construction. This plan review is based on information provided only.

Design and installation shall meet the Oregon requirements of the International Fire, Building, Mechanical Codes and applicable NFPA Standards.

Medford Fire-Rescue, 200 S Ivy St. Rm 180, Medford OR 97501 541-774-2300

www.medfordfirerescue.org



See report for full comments

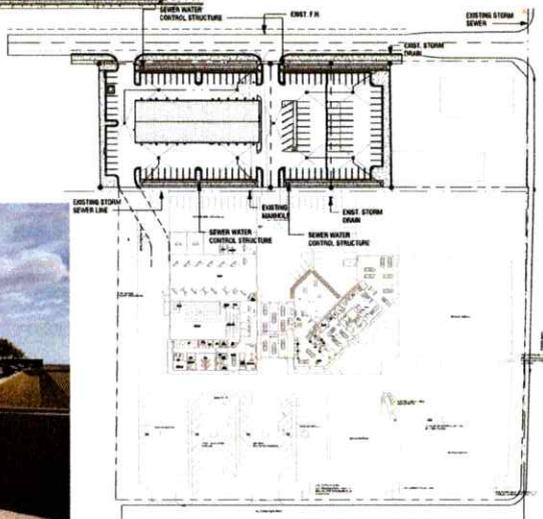
1 SITE PLAN
1" = 20'-0"



3 3D View 4, 07/19/20
12" = 1'-0"



4 3D View 1, 07/10/20
12" = 1'-0"



2 PROJECT SITE PLAN
1/4" = 80'-0"

GARY R. CAFFERNA ARCHITECT
ARCHITECTURE PLANNING
190 North First Lane, Medford Oregon
P.O. Box 440, 97501
Office: 541.773.8325 Fax: 541.773.8373
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Oregon 541.773.8325
Medford, Oregon 97501
Medford, Oregon 97501

STAMP
REGISTERED ARCHITECT
STATE OF OREGON
4444

J.B. STEEL, INC.
COMMERCIAL & INDUSTRIAL
P.O. Box 440, 190 North First Lane
Medford Oregon 97501
Office: 541.773.8325 Fax: 541.773.8323
E.O. 12812

CLIENT:
Southern Oregon Subaru

PROJECT: **SO SUBARU**
PROJECT LOCATION:
3103 State Road

No.	Date	Description

ISSUE: 07/24/2020
PROJECT NO: #2920-20
DRAWN BY: NPFL
CHECKED BY: GRC

APPROVED
SHEET TITLE:
SITE PLAN

PLOT DATE:
7/24/2020 11:41:58 AM

A0
Design Development
PRELIMINARY



Staff Memo

TO: Planning Department, City of Medford

FROM: Brian Runyen, P.E.(TX), Water Commission Staff Engineer

SUBJECT: AC-20-227

PARCEL ID: 372W12D TL 7000, 7100, 7200, and 7300

PROJECT: Consideration of plans for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling approximately 1.56 acres, located west of Biddle Road and fronting Lawnsdale Road (Addresses: 952, 924, 908 and 898 Lawnsdale Road). The site is zoned I-G (General Industrial). (372W12D TL 7000, 7100, 7200, and 7300).

MEMO DATE: August 27, 2020 **LAND DEVELOPMENT COMMITTEE DATE:** September 2, 2020

I have reviewed the above project application as requested. Comments and Conditions for approval are as follows:

COMMENTS

1. The applicant's Civil Engineer shall coordinate with MWC engineering Staff for approval of water facility plans. Expect additional comments once water construction plans are submitted.
2. MWC has adequate capacity to serve the property with water.
3. Access to MWC water lines is available. There is an existing 6" water line along the north side of Lawnsdale Road.
4. MWC-metered water service is available to each existing lot from the 6" water line in Lawnsdale Road. (SEE CONDITIONS 2-6 BELOW)
 - a. TL 7300 has an existing 1" service with a 3/4" water meter.
 - b. TL 7200 has an existing 1" service with a 3/4" water meter.
 - c. TL 7100 has an existing 1" service (no water meter present).
 - d. TL 7000 has an existing 1" service with a 3/4" water meter.
5. The project is within MWC's "Reduced" Pressure Zone.
6. Static water pressure is expected to be around 88 psi. (SEE CONDITIONS 2-6 BELOW regarding requirements for Pressure Reducing Valves.)

SEE CONDITIONS ON NEXT PAGE



EXHIBIT
J

Page 1 of 2

200 S. Ivy Street, Room 177
Medford, Oregon 97501
Phone (541) 774-2430

www.medfordwater.org
water@medfordwater.org
Fax (541) 774-2555

CONDITIONS

1. Water facility planning / design / construction process will be done in accordance with the current Medford Water Commission (MWC) "Regulations Governing Water Service" and "Standards For Water Facilities / Fire Protection Systems / Backflow Prevention Assemblies"
2. Lot line adjustments described in the Applicant's submitted narrative are required to be completed prior to approval of this project. (*"The lot line between tax lots 7300 and 7200 will be abandoned and the lot line between lots 7200 and 7100 will be adjusted..."*)
3. The applicant's Civil Engineer shall coordinate with MWC engineering Staff for approval of water facility plans. Expect additional comments once water construction plans are submitted.
4. Coordinate with MWC for metered water service.
 - a. Applicant shows new water meters for domestic and irrigation on the proposed combined TL 7300 & TL 7200. Requested meter sizes are not shown.
 - b. Existing metered services to tax lots 7200 and 7300 may be utilized for domestic and/or landscape water service (if appropriately sized for project needs).
 - c. If either / both water meter(s) on TLs 7300 & 7200 are not utilized, they will be required to be abandoned.
5. Installation of Pressure Reducing Valve (PRV) is required per Uniform Plumbing Code. Pressure Reducing Valves shall be installed on the "private" side of all water meters. PRV's shall be located as close as possible to the water meter serving the parcel being served. See attached document from the City of Medford Building Department on "Policy on Installation of Pressure Reducing Valves."
 - a. REQUIRED ON ALL WATER SERVICES
6. Installation of an Oregon Health Authority approved backflow device is required for all commercial, industrial, municipal, and multi-family developments. New backflow prevention devices shall be tested by an Oregon certified backflow assembly tester. See MWC website for list of certified testers at the following web link:
<http://www.medfordwater.org/Page.asp?NavID=35>
 - a. REQUIRED ON ALL WATER SERVICES
7. Applicant's Civil Engineer shall coordinate with Medford Fire Department for proposed Fire Hydrant locations for the proposed development.
8. If on-site fire hydrants are required in the proposed parking lot by the Medford Fire Department, then installation of a looped 8-inch water line (minimum) will be required to be installed within the proposed travel lanes within the parking lot. Water lines shall not be installed under proposed parking spaces or landscape islands.
9. A "Summer" fire hydrant "flow test" is required for any proposed design of sprinkler systems for the new building. (A Summer "flow test" shall be obtained between the months of July thru September.) Coordinate with MWC Engineering for fire hydrant flow testing for design of sprinkler system.

END CONDITIONS



BUILDING SAFETY DEPARTMENT
ROOM 277

CITY OF MEDFORD
LAUSMANN ANNEX
200 SOUTH IVY STREET
MEDFORD, OREGON 97501

TELEPHONE (541) 774-2350
FAX (541) 774-2575
E-MAIL:
bldmed@ci.medford.or.us

Policy on Installation of Pressure Reducing Valves

August 5, 2014

Section 608 of the 2011 Edition of the Oregon Plumbing Specialty Code requires a pressure regulator (commonly called a Pressure Reducing Valve or PRV) where the static pressure in the water supply piping exceeds 80 psi. Although this section gives limited guidance as to installation, it does require the device to be

“...accessibly located above ground or in a vault equipped with adequate means to provide drainage and shall be protected from freezing, and shall have the strainer readily accessible for cleaning without removing the regulator or strainer body or disconnecting the supply piping.”

“Accessible” and “readily accessible” are defined in chapter 2.

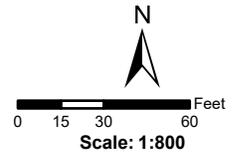
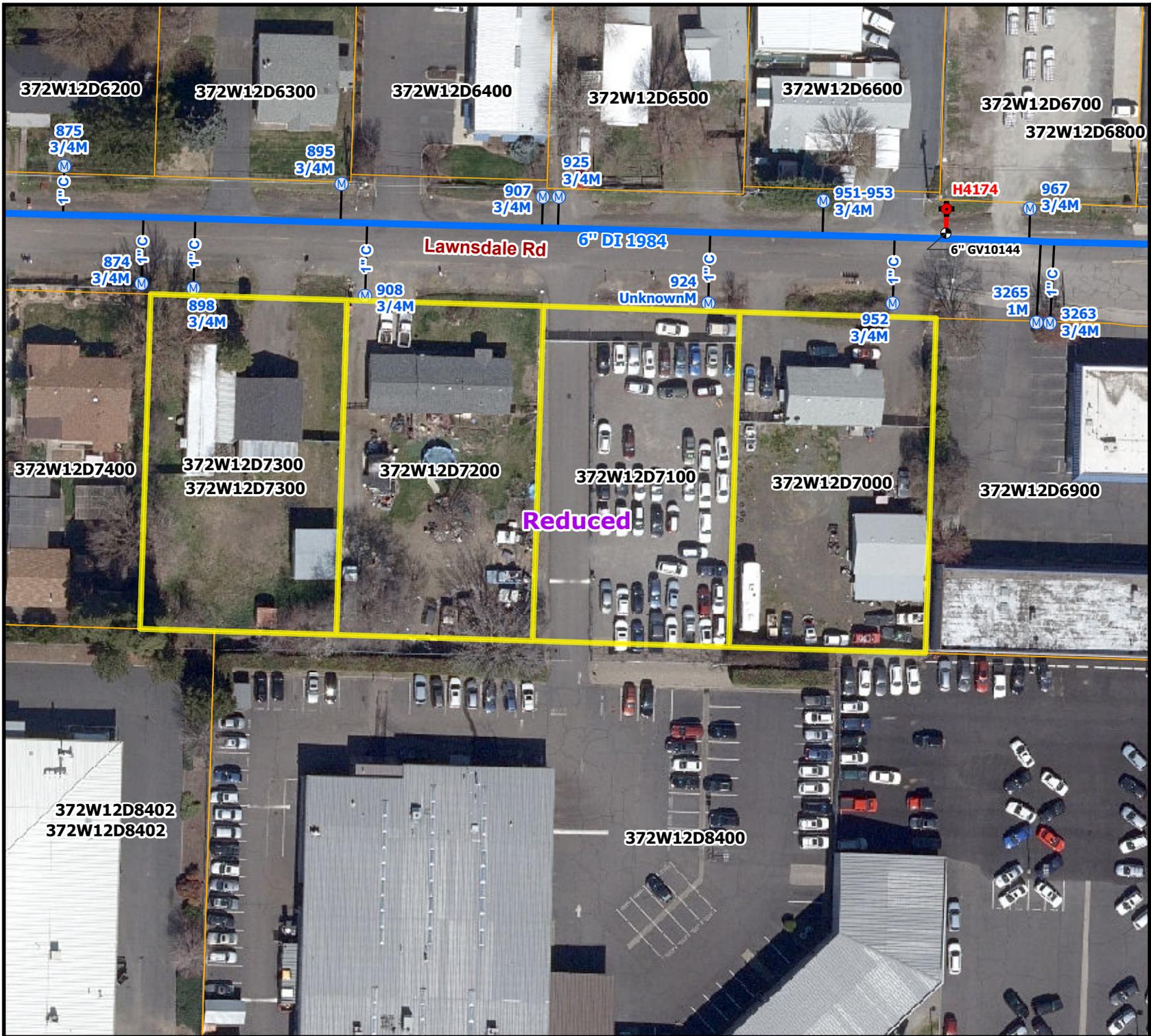
To assure uniform and appropriate installation of these devices within Medford, the following standards have been agreed to by the City of Medford Building Safety Department and the Medford Water Commission:

1. The need for these devices will be based on pressure information provided by the Medford Water Commission, and can be verified on-site with a pressure gage. While factory settings of these devices may be adjusted, MWC recommends that the regulated pressure be set no higher than 65 psi.
2. PRVs shall NOT be installed when static pressure is less than 50 psi, except for limited specific equipment-based needs.
3. The PRV shall be installed outside the street right of way as close as practical to the water meter.
4. No expansion tank is necessary.
5. No fixture, device or system is permitted between the meter and the PRV.
6. The PRV must NOT be direct buried nor installed in a crawl space.
7. PRVs shall be installed within a readily accessible valve box / vault following the same standard as used for double check backflow assemblies, as follows:

“On new installations, at least 12-inches clearance will be required as per section 603.3.4. When replacing an existing assembly, the 12-inch clearance requirement can be waived as long as there is at least 3-inches clearance between the bottom of the assembly and the ground, and the device is tested and serviced from the top.”

Sam Barnum

Building Safety Director



**Water Facility Map for
City of Medford
Planning Application:
AC-20-227**

Legend

- Air Valve
- Sample Station
- Fire Service
- Hydrant
- Reducer
- Blow Off
- Plugs-Caps

Water Meters:

- Active Meter
- On Well
- Unknown
- Vacant

Water Valves:

- Butterfly Valve
- Gate Valve
- Tapping Valve

Water Mains:

- Active Main
- Abandoned Main
- Reservoir Drain Pipe
- Pressure Zone Line

Boundaries:

- Urban Growth Boundary
- City Limits
- Tax Lots

MWC Facilities:

- Control Station
- Pump Station
- Reservoir



Date: 8/24/2020
Path: R:\Departments\GIS\MXDs\Bran's Projects\AC-20-227 - ArcGIS Pro Project.aprx



JACKSON COUNTY

Roads

Roads
Engineering

Chuck DeJanvier
Construction Engineer

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
dejanvca@jacksoncounty.org

www.jacksoncounty.org

August 19, 2020

Attention: Dustin Severs
Planning Department
City of Medford
200 South Ivy Street, Lausmann Annex, Room 240
Medford, OR 97501

RE: Consideration for construction of a 10,680 square-foot auto service and repair facility off Lawnsdale Road – a county-maintained road.
Planning File: AC-20-227

Dear Dustin:

Thank you for the opportunity to comment on this request for the construction of a 10,680 square foot automobile service and repair facility for Southern Oregon Subaru. The site consists of four contiguous parcels totaling 1.56 acres located west of Biddle Road and fronting Lawnsdale (Addresses: 898, 908, 924, & 952 Lawnsdale Road). The site is zoned General Industrial (I-G). (372W12D Tax Lots 7000, 7100, 7200, & 7300). Jackson County Roads has the following comments:

1. Lawnsdale Road may not have an adequate structural section to accommodate Commercial or Industrial uses. The City of Medford might want to require improvements as development occurs.
2. Jackson County's General Administration Policy #1-45 sets forth the County's position as it relates to the management of County roads located within existing or proposed city limits or Urban Growth Boundaries (UGB). The County has no current plans for improvements to Lawnsdale Road. Jackson County Roads recommends that the city request road jurisdiction.
3. Per Oregon Revised Statute 209.150, any survey monuments of record removed, disturbed or destroyed within the permit area must be referenced prior to construction and replaced after construction by a registered professional land surveyor. All costs associated with this surveying work are the responsibility of the permit holder.
4. Jackson County Roads would like to review and comment on the hydraulic report including the calculations and drainage plan. Capacity improvements or on-site detention, if necessary, shall be installed at the expense of the applicant. Upon

August 19, 2020

Page 2 of 2

completion of the project, the developer's engineer shall certify that construction of the drainage system was constructed per plan and a copy of the certification shall be sent to Jackson County Roads.

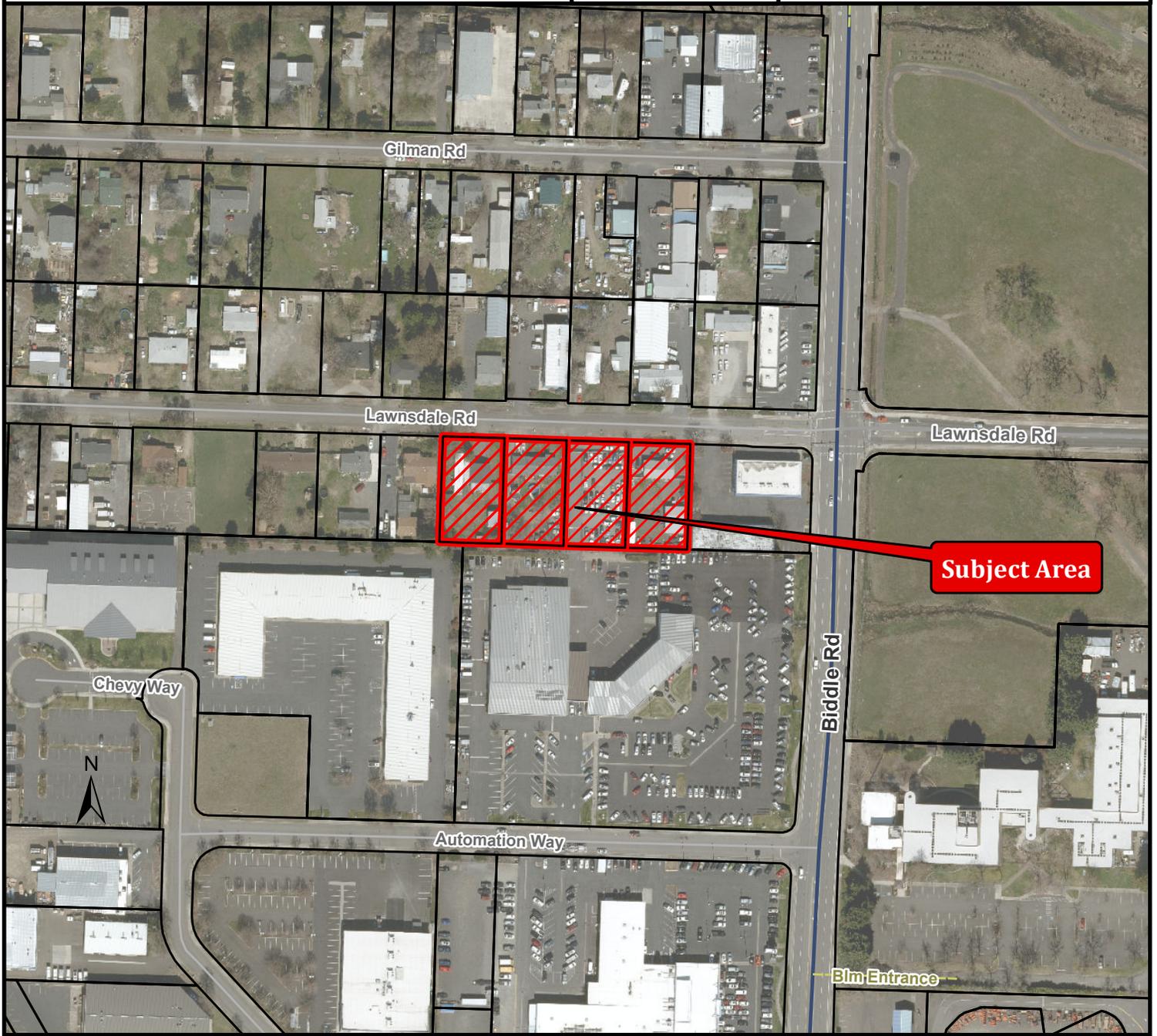
5. Please note that there are drainage problems in this area and the City of Medford maintains the storm water system.
6. Storm water should meet City of Medford requirements that also include water quality.
7. The applicant shall submit construction drawings to Jackson County Roads and obtain county permits if required.
8. We would like to be notified of future development proposals, as county permits may be required.
9. Any new or improved road approaches on Lawnsdale Road shall be permitted and inspected by the City of Medford.
10. If frontage improvements along Lawnsdale Road are required, they shall be permitted and inspected by the City of Medford.
11. Utility Permits are required from Roads for any utility work within the county road right-of-way. On longitudinal trenches within a travel lane 100' or greater in length, unless otherwise approved by the Engineer, the existing pavement shall be removed and replaced to full paving-machine width (normally 10'-12') for a travel lane restoration. Drag boxes or other pull-type asphalt spreaders will not be permitted for longitudinal trench pavement replacement.
12. Please note right-of-way dedication should be to the City of Medford.
13. Lawnsdale Road is a County Urban Local road and is county-maintained. Average Daily Traffic Counts are unavailable for this road.

If you have any questions or need further information feel free to call me at 774-6255.

Sincerely,



Chuck DeJanvier, PE
Construction Engineer

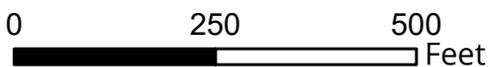


Project Name:

Southern Oregon Subaru

Map/Taxlot:

372W12D TL 7000, 7001, 7200, 7300



Legend

-  Subject Area
-  Tax Lots

Date: 8/7/2020

